

# MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

## STATE PROJECT NO. L138-0003

### STRATFORD, CONNECTICUT

#### GENERAL NOTES:

- CONSTRUCTION SPECIFICATIONS: State of Connecticut Department of Transportation, Standard Specifications for Roads, Bridges, Facilities, and Incidental Construction, Form 819, dated 2024, Supplemental Specifications, and Special Provisions
- 400 FOOT GRID BASED ON CONNECTICUT COORDINATE SYSTEM N.A.D. 1983
- VERTICAL DATUM BASED ON NAVD 1988

#### DISCLAIMER

IT IS THE RESPONSIBILITY OF EACH BIDDER AND ALL OTHER INTERESTED PARTIES TO OBTAIN ALL BIDDING RELATED INFORMATION AND DOCUMENTS FROM THE TOWN OR THE TOWN'S REPRESENTATIVE.

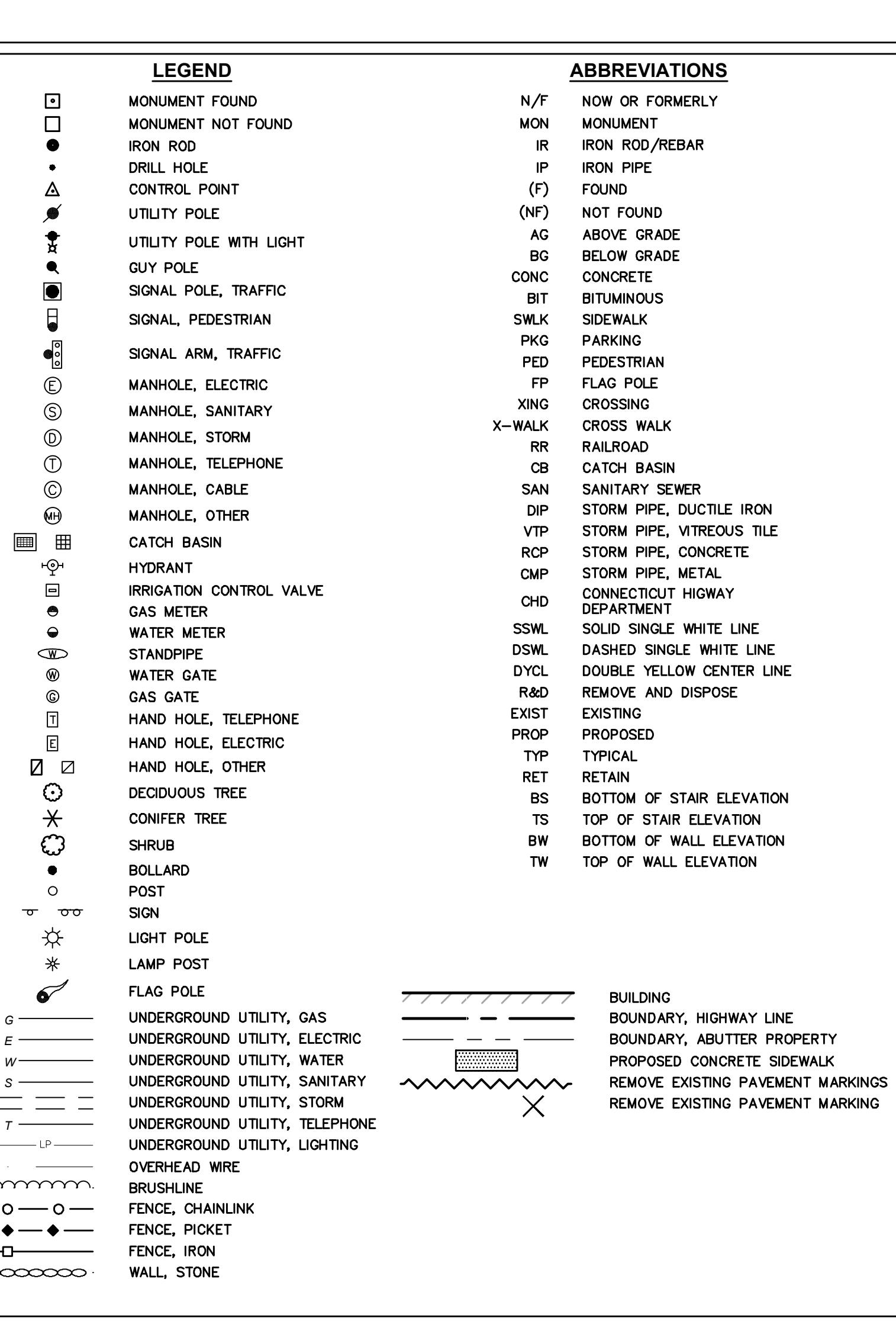
PERSONS AND/OR ENTITIES WHICH REPRODUCE AND/OR MAKE SUCH INFORMATION AVAILABLE BY ANY MEANS ARE NOT AUTHORIZED BY THE TOWN TO DO SO AND MAY BE LIABLE FOR CLAIMS RESULTING FROM THE DISSEMINATION OF UNOFFICIAL, INCOMPLETE AND/OR INACCURATE INFORMATION.

#### DESIGN CRITERIA

DESIGN STANDARD: CT DOT HIGHWAY DESIGN MANUAL, 2003 EDITION, INCLUDING REVISIONS TO FEBRUARY 2013

MAIN STREET (STATE ROUTE 113)  
FUNCTIONAL CLASSIFICATION: URBAN PRINCIPAL ARTERIAL - OTHER  
POSTED SPEED: 30 MPH

SURVEY PERFORMED BY  
BSC GROUP, INC.  
DATE: FEBRUARY 2023



LIST OF DRAWINGS			
SHEET NO.	TITLE	SHEET NO.	STANDARD DRAWINGS
T-01	TITLE SHEET	HW-1N1 1-2	HIGHWAY STANDARD SHEET INDEX
KEY-01	KEY PLAN	HW-286_01	DRAINAGE TRENCH EXCAVATION
XCP-01 - XCP-03	EXISTING CONDITIONS PLAN	HW-586_01	CATCH BASIN AND DROP INLETS TYPES "C" AND "C-L" STRUCTURES
TYP-01	TYPICAL SECTIONS	HW-586_04	PRECAST CATCH BASIN AND ROUND STRUCTURE
GEN-01 - GEN-04	GENERAL PLAN	HW-586_07A	CATCH BASIN TYPE "C" AND "C-L" TOPS
PMSP-01 - PMSP-04	PAVEMENT MARKING & SIGNAGE PLAN	HW-586_07C	CATCH BASIN TYPE "C" AND "C-L" DOUBLE GRATE TYPE II TOPS
GRAD-01	GRADING PLAN	HW-586_08	CATCH BASIN FRAMES AND GRATES
DET-01 - DET-02	MISCELLANEOUS DETAILS	HW-813_02	STONE CURBING
GS-01 - GS-16	STATE GUIDE SHEET DETAILS	HW-815_01	BITUMINOUS CONCRETE CURBING
1 - 4	SIGNAL PLANS	HW-921_01	CONCRETE SIDEWALKS
		HW-922_01	BITUMINOUS CONCRETE SIDEWALK AND BITUMINOUS CONCRETE DRIVEWAY
		HW-949_01A	LANDSCAPE PLANTING
		HW-949_01B	TREE STAKING
		TR-STD INDEX	TRAFFIC STANDARD SHEET INDEX
		TR-1000_01	GENERAL CLAUSES (TEST PROCEDURES)
		TR-1001_01	TRENCHING & BACKFILLING, ELECTRICAL CONDUIT
		TR-1002_01	TRAFFIC CONTROL FOUNDATIONS
		TR-1010_01	CONCRETE HANDBOLES
		TR-1102_01	PEDESTALS, PEDESTRIAN SIGNALS
		TR-1107_01	PEDESTRIAN PUSH BUTTON
		TR-1208_01	SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS
		TR-1208_02	METAL SIGN POSTS AND SIGN MOUNTING DETAILS
		TR-1210_04	PAVEMENT MARKING LINES AND SYMBOLS
		TR-1210_08	PAVEMENT MARKINGS FOR NON FREEWAYS
		TR-1210_09	PAVEMENT MARKINGS FOR BICYCLE LANES, PARKING STALLS, AND RAILROAD CROSSINGS
		TR-1220_01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
		TR-1220_02	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES

#### SEMI-FINAL DESIGN SUBMISSION FOR STREETSCAPE IMPROVEMENTS IN THE TOWN OF STRATFORD, CT

FROM STA. 10+00 TO STA. 54+68 ON MAIN STREET

TO BE MAINTAINED BY THE TOWN OF STRATFORD

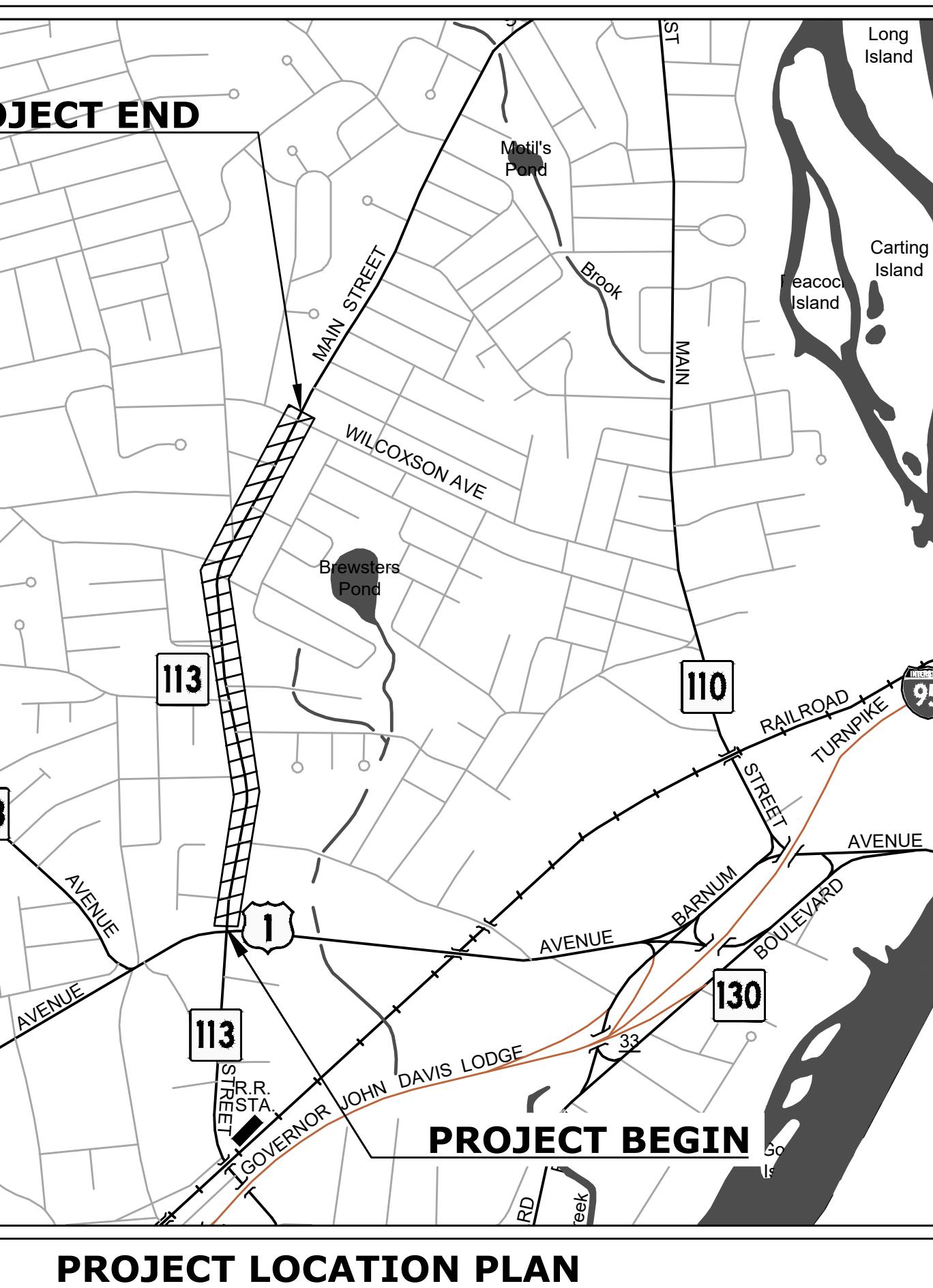
PLANS FOR:  
MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2  
STRATFORD, CONNECTICUT 06615

THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA, APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

SUBMITTED BY: ROBERT S. NEWTON, PE

APPROVAL BY:

APPROVAL BY:



PLANS PREPARED FOR:

METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

PLANS PREPARED BY:

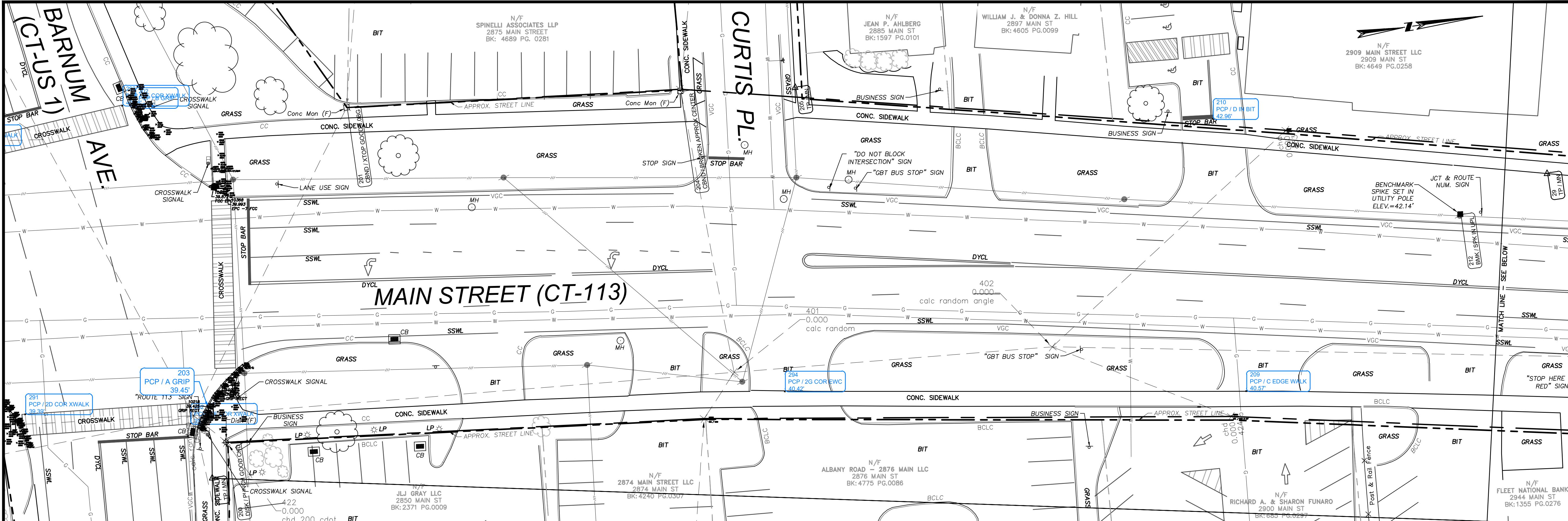
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Glastonbury, Connecticut  
06033  
860 652 8227

PROGRESS PRINT - 60% DESIGN - OCTOBER 2024

REV.	DATE	REVISION DESCRIPTION

T-01





MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

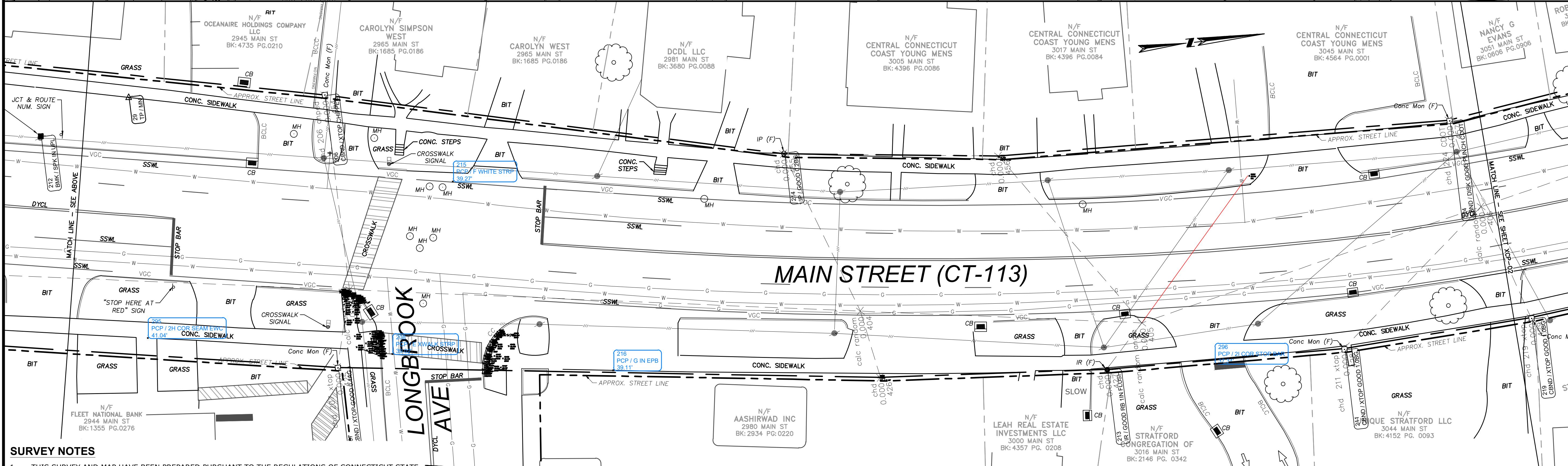
IN

STRATFORD  
CONNECTICUT

EXISTING CONDITIONS  
PLAN

OCTOBER 2024

REVISIONS:



PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

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SCALE: 1" = 20'  
0 10 20 40 FEET

FILE: 8387300\ CIVIL \ DRAWINGS  
DWG. NO: XCP-01  
JOB. NO: 83873.00

**SURVEY NOTES**

1. THIS SURVEY MAP HAVE BEEN PREPARED PURSUANT TO THE REGULATIONS OF CONNECTICUT STATE AGENCIES SECTIONS 20-300B-1 THROUGH 20-300B-20 AND THE "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT" AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. ON SEPTEMBER 26, 1996.
2. THIS IS A DATA ACCUMULATION PLAN.
3. THE INTENT OF THIS SURVEY AND PLAN: DOCUMENT EXISTING CONDITIONS
4. THE FIELD SURVEY WAS PERFORMED ON THE GROUND BY BSC GROUP IN AUGUST 2018 AND JANUARY 2019. ADDITIONAL INFORMATION WAS OBTAINED BY DRONE FLIGHT CONDUCTED JANUARY 6, 2019.
5. THE VERTICAL DATUM SHOWN IS BASED UPON NATIONAL AMERICAN VERTICAL DATUM OF 1988 (NAVD88), THE REFERENCE MERIDIAN (NORTH ARROW) SHOWN IS BASED UPON NORTH AMERICAN DATUM OF 1983 (NAD83), BOTH HORIZONTAL AND VERTICAL DATUMS ARE DETERMINED FROM RTK/G.P.S.
6. EXISTING UTILITIES, WHERE SHOWN HEREON, ARE APPROXIMATE. NO GUARANTEE IS IMPLIED OR INTENDED AS TO THE ACCURACY, LOCATION OR THAT ALL UTILITIES AND/OR SUBSURFACE STRUCTURES ARE SHOWN. CONSULT WITH THE APPROPRIATE UTILITY COMPANY OR AGENCY PRIOR TO DESIGNING IMPROVEMENTS, COMMENCING DEMOLITION OR CONSTRUCTION. "CALL BEFORE YOU DIG" 1-800-922-4455.

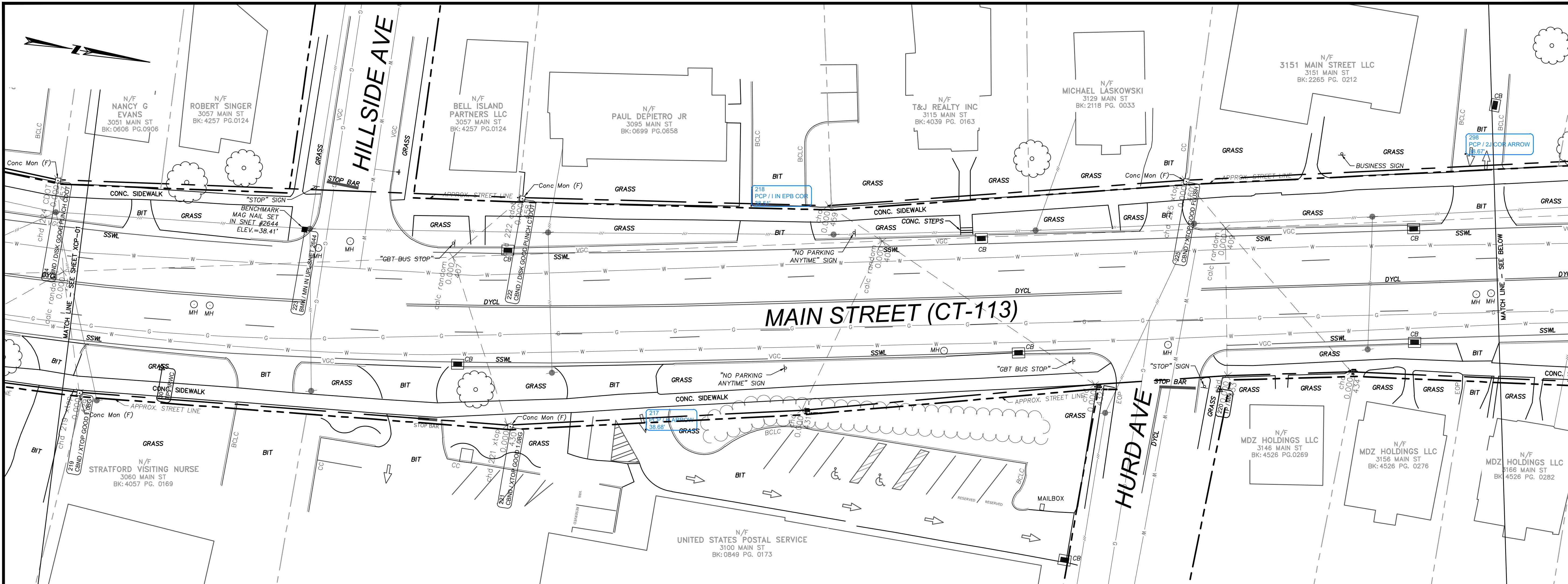
**STATE RIGHT-OF-WAY NOTES:**

1. THE CONTRACTOR SHALL CONTACT MR. VINCENT HANCHURUCK, DEPARTMENT'S DISTRICT SURVEY UNIT, AT 203-389-3112, PRIOR TO ANY CONSTRUCTION WITHIN THE STATE RIGHT-OF-WAY.
2. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL ENGINEERING COSTS SHOULD THE CTDOT BOUNDARY/SURVEY MARKERS BE DISTURBED OR DAMAGED.
3. IN THE EVENT THE DEPARTMENT DETERMINES THE SUBJECT CTDOT BOUNDARY/SURVEY MARKERS SHOULD BE REPLACED DUE TO THE PROPOSED DEVELOPMENT, THE DEPARTMENT WILL FURNISH NEW MONUMENTS, WHICH THE CONTRACTOR WILL BE REQUIRED TO INSTALL UNDER THE DIRECTION OF A LAND SURVEYOR LICENSED IN THE STATE OF CONNECTICUT.
4. THE CTDOT BOUNDARY/SURVEY MARKERS SHALL BE VERIFIED AND ACCEPTED BY THE DISTRICT 3 SURVEY UNIT PRIOR TO RELEASING THE ENCROACHMENT PERMIT BOND.

**MAP REFERENCE:**

1. "CONNECTICUT HIGHWAY DEPARTMENT RIGHT OF WAY MAP TOWN OF STRATFORD - MAIN-STREET FROM BARNUM AVE. CUT OFF SOUTHERLY TO EAST BROADWAY. ROUTE NO.8" SCALE: 1"=40' DATE: JAN. 31, 1934, REVISED TO JUNE 22, 1983. SHEETS 1 & 2.
2. "CONNECTICUT HIGHWAY DEPARTMENT RIGHT OF WAY MAP TOWN OF STRATFORD - MAIN-STREET FROM EAST BROADWAY SOUTHERLY TO STRATFORD AVE. ROUTE NO.8" SCALE: 1"=40' DATE: JAN. 31, 1934, REVISED TO FEBRUARY 11, 1970. SHEETS 1 & 2.
3. "CONNECTICUT DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS RIGHT OF WAY MAP TOWN OF STRATFORD - CONNECTICUT TURNPIKE - FROM THE BRIDGEPORT-STRATFORD TOWN LINE EASTERLY TO THE STRATFORD-MILFORD TOWN LINE" SCALE: 1"=80' DATE: APRIL 10, 1972, REVISED TO NOVEMBER 21, 1975. SHEET 5 OF 8.

PROGRESS PRINT - 60% DESIGN



MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

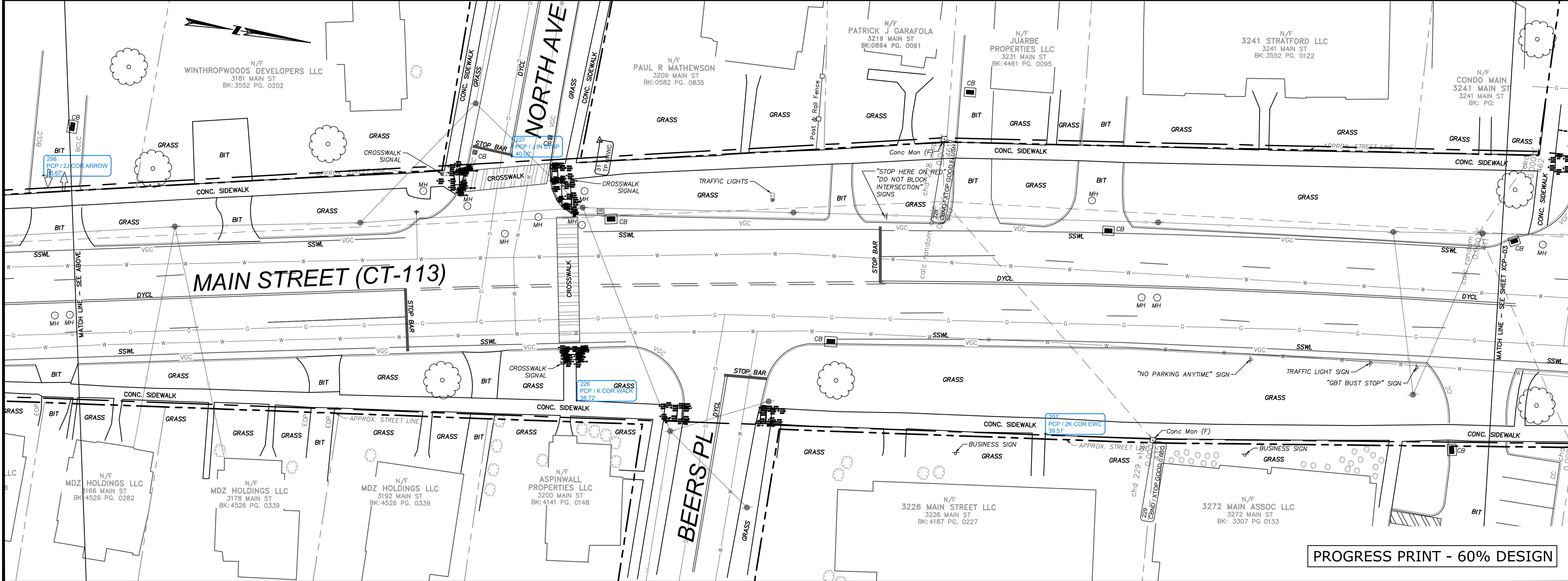
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MAIN STREET (CT-113)

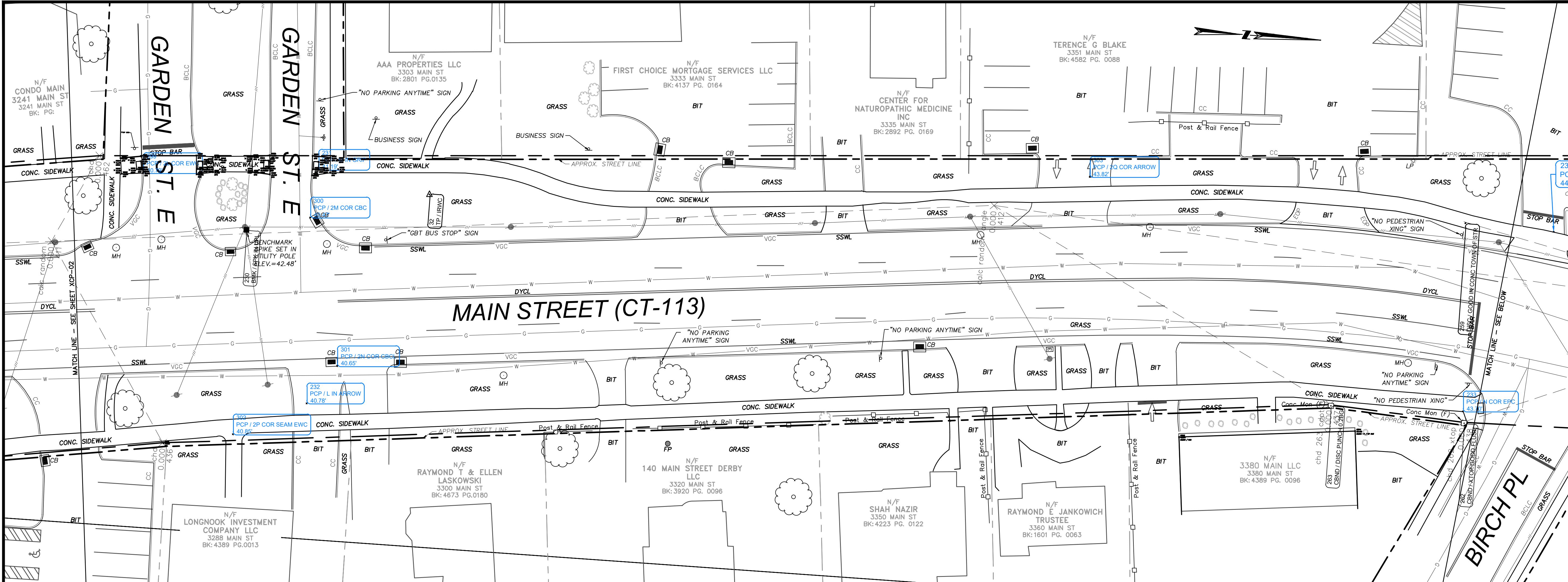
IN  
STRATFORD  
CONNECTICUT

EXISTING CONDITIONS  
PLAN

OCTOBER 2024

REVISIONS:





MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

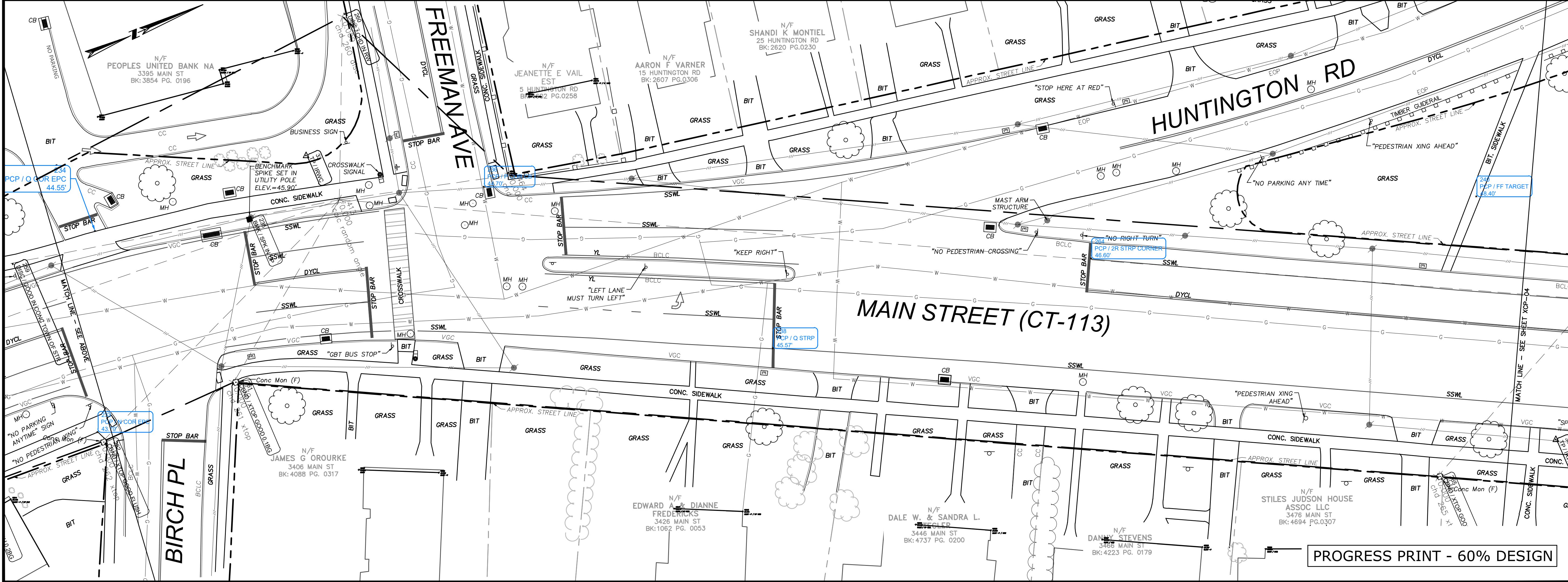
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MAIN STREET (CT-113)

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CONNECTICUT

EXISTING CONDITIONS  
PLAN

OCTOBER 2024

REVISIONS:



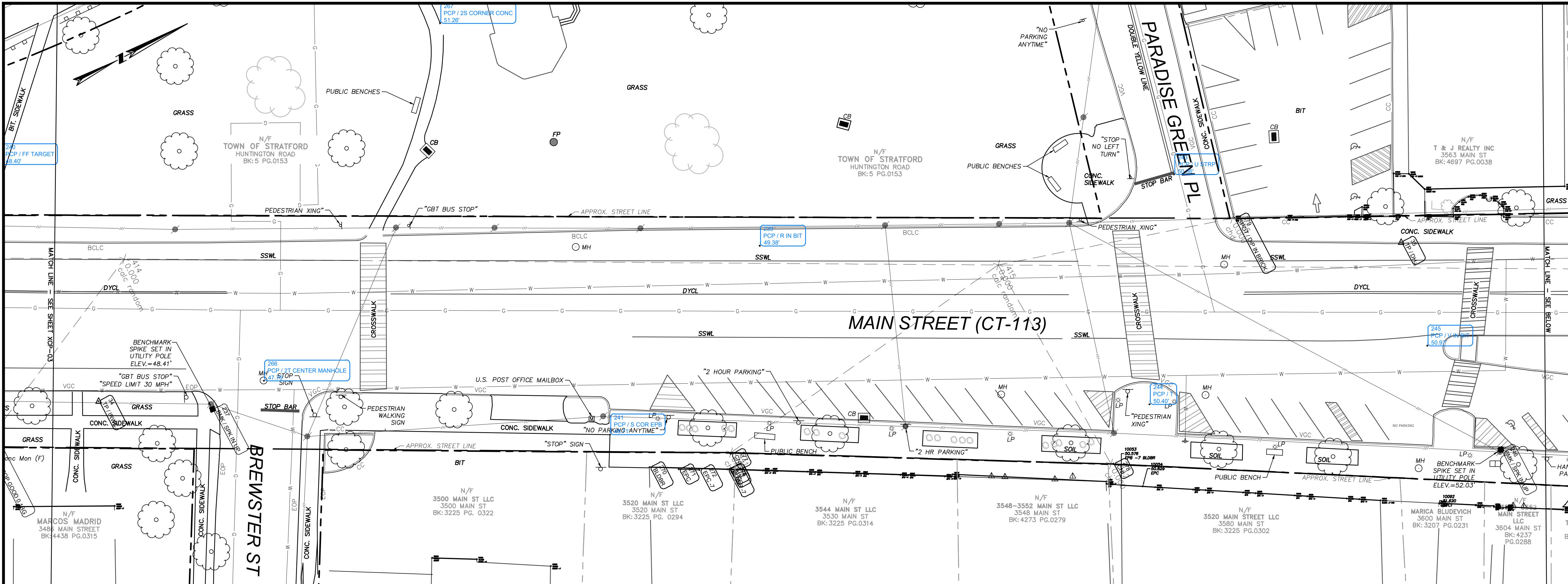
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SCALE: 1" = 20'  
0 10 20 40 FEET

FILE: 8387300\ CIVIL\ DRAWINGS  
DWG. NO:  
JOB. NO: 83873.00 XCP-03

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MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003

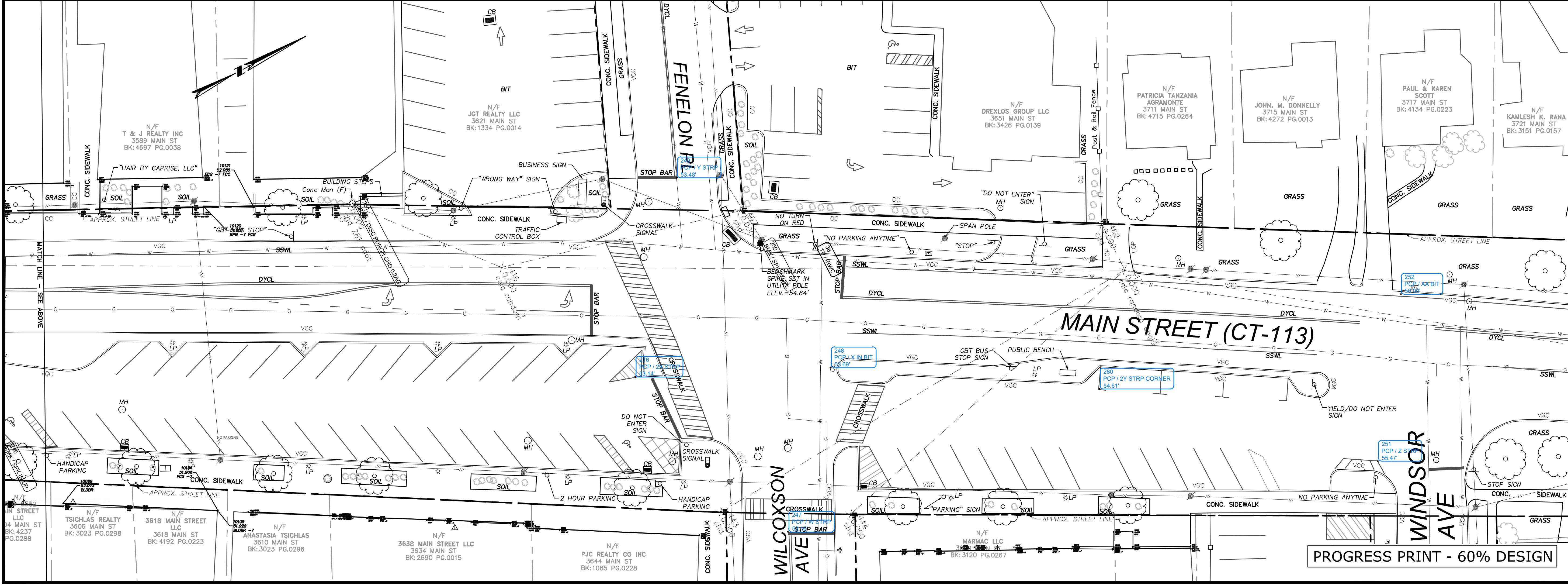
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

EXISTING CONDITIONS  
PLAN

OCTOBER 2024

REVISIONS:



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0 10 20 40 FEET

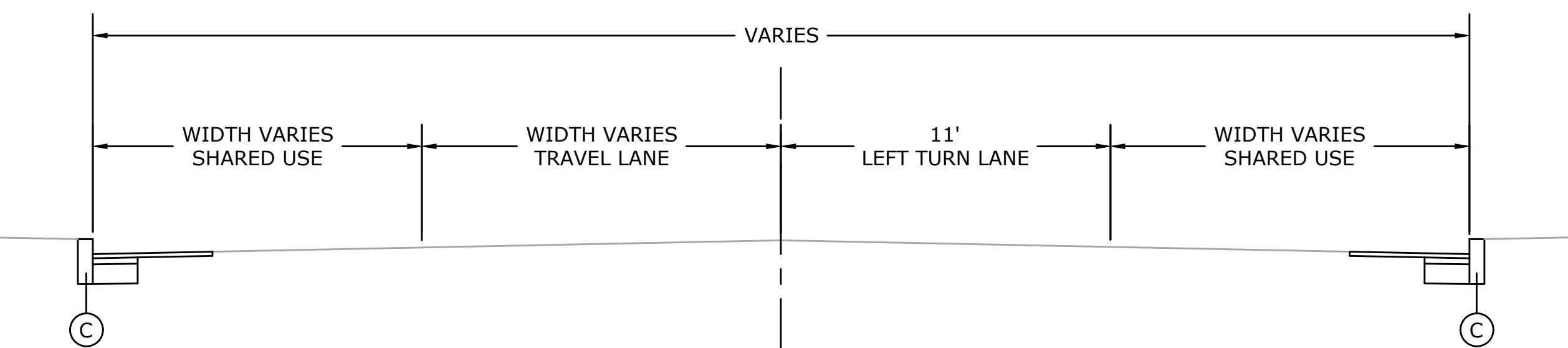
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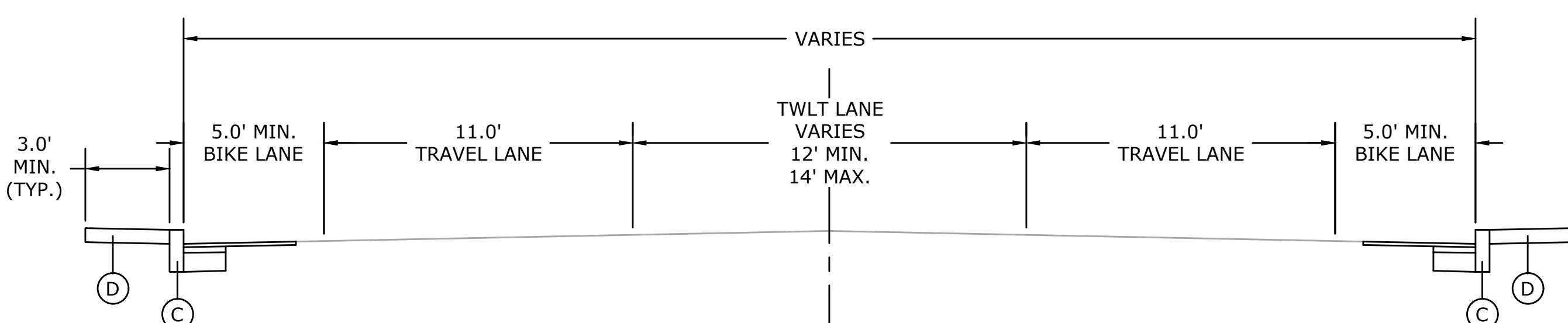
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**LEGEND**

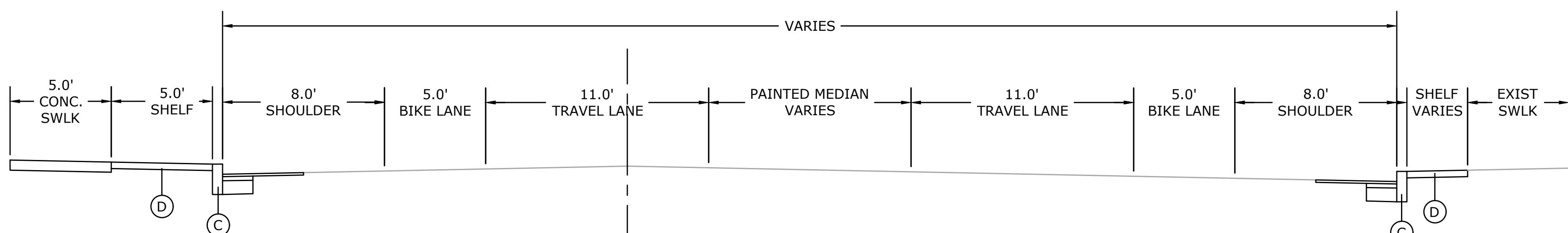
- (A) - 2.0" S0.5 LEVEL 2 HMA
- (B) - GRANITE STONE CURB (SEE PLANS)
- (C) - REMOVE/RESET EXISTING CURB (SEE PLANS)
- (D) - 4" TOPSOIL AND SEED
- (E) - 7" S1.0 LEVEL 2 HMA
- (F) - 10" PROCESSED AGGREGATE BASE
- (G) - CONCRETE UNIT PAVERS



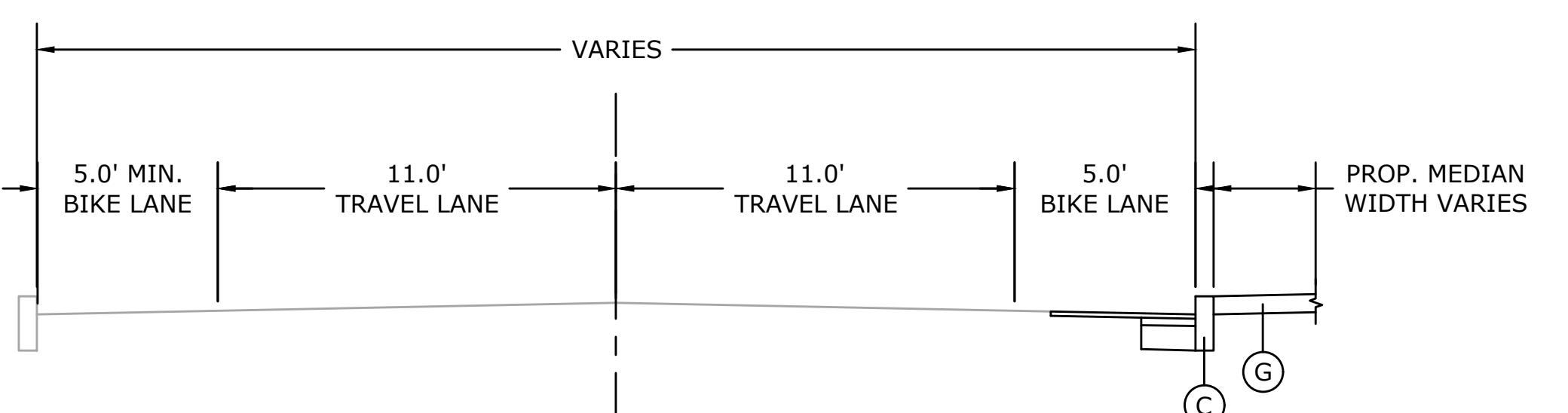
**TYPICAL SECTION**  
STA. 10+00 TO STA. 16+50  
SCALE: 1" = 4'



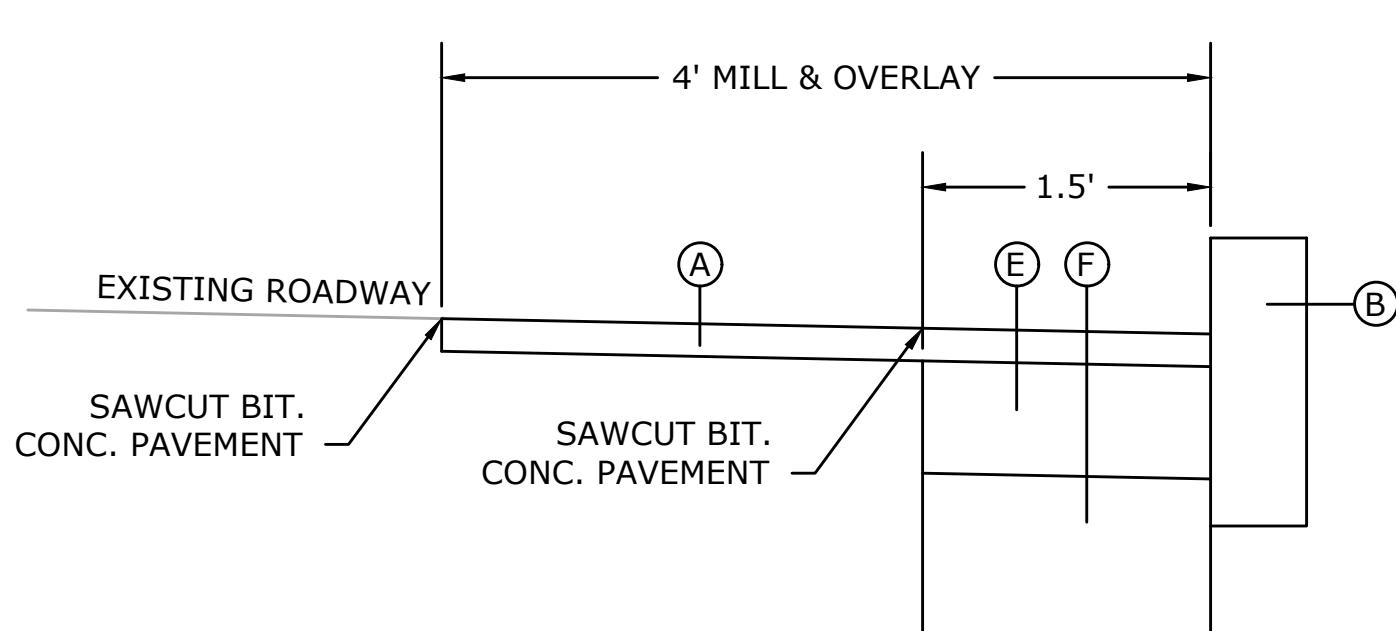
**TYPICAL SECTION**  
STA. 17+25 TO STA. 37+50  
SCALE: 1" = 4'



**TYPICAL SECTION**  
STA. 41+50 TO STA. 47+10  
SCALE: 1" = 4'



**TYPICAL SECTION**  
STA. 47+75 TO STA. 51+00  
SCALE: 1" = 4'



**CURB REPLACEMENT ENLARGEMENT (SEE PLANS)**

SCALE: 1" = 1'

**PROGRESS PRINT - 60% DESIGN**

**MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2**

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

TYPICAL SECTIONS

OCTOBER 2024

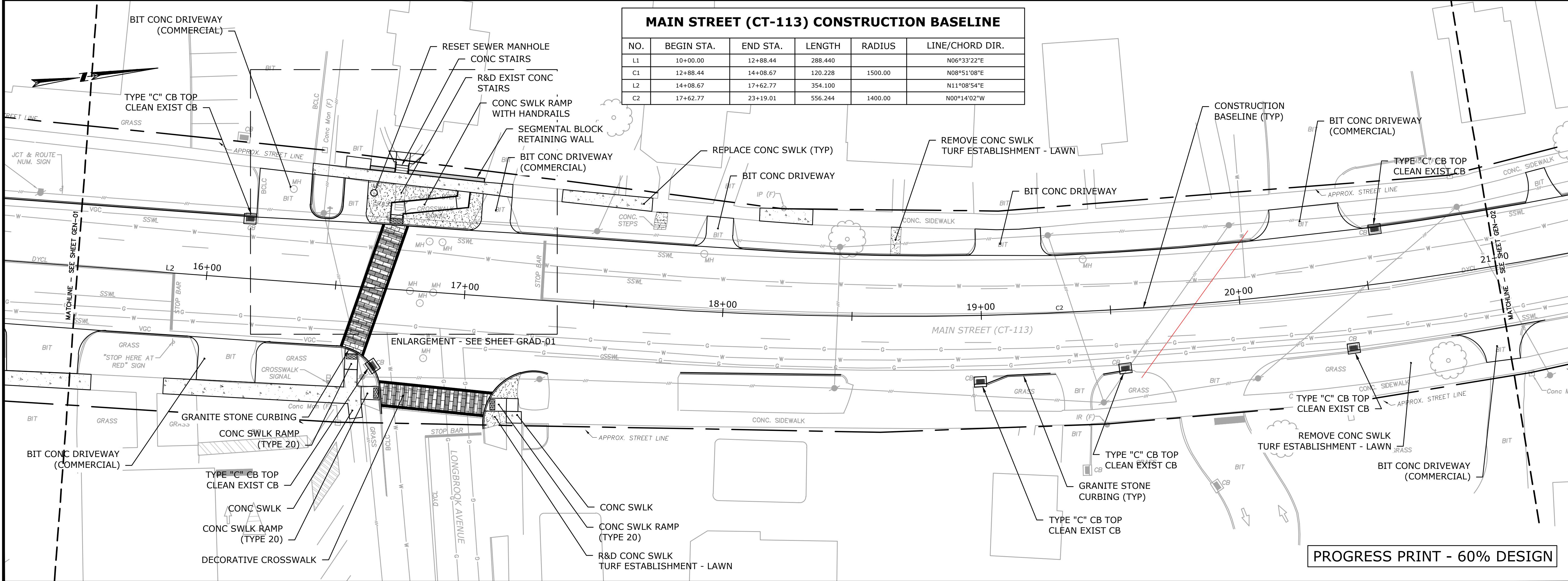
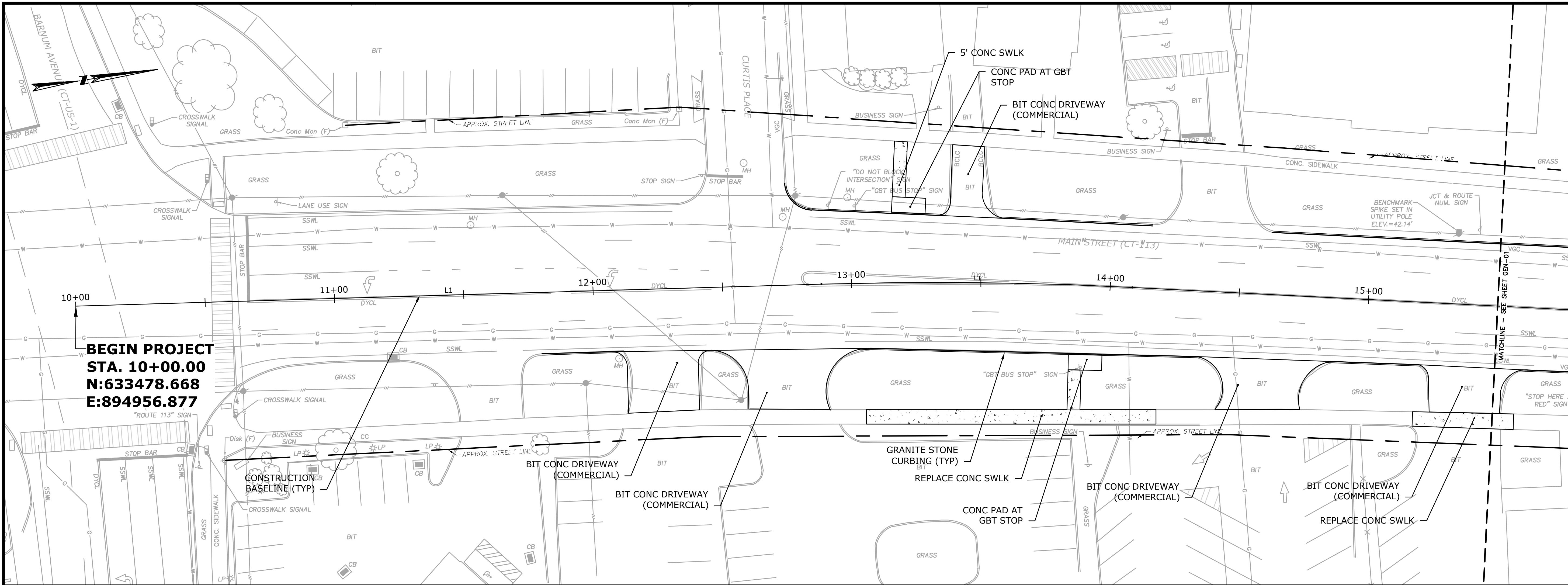
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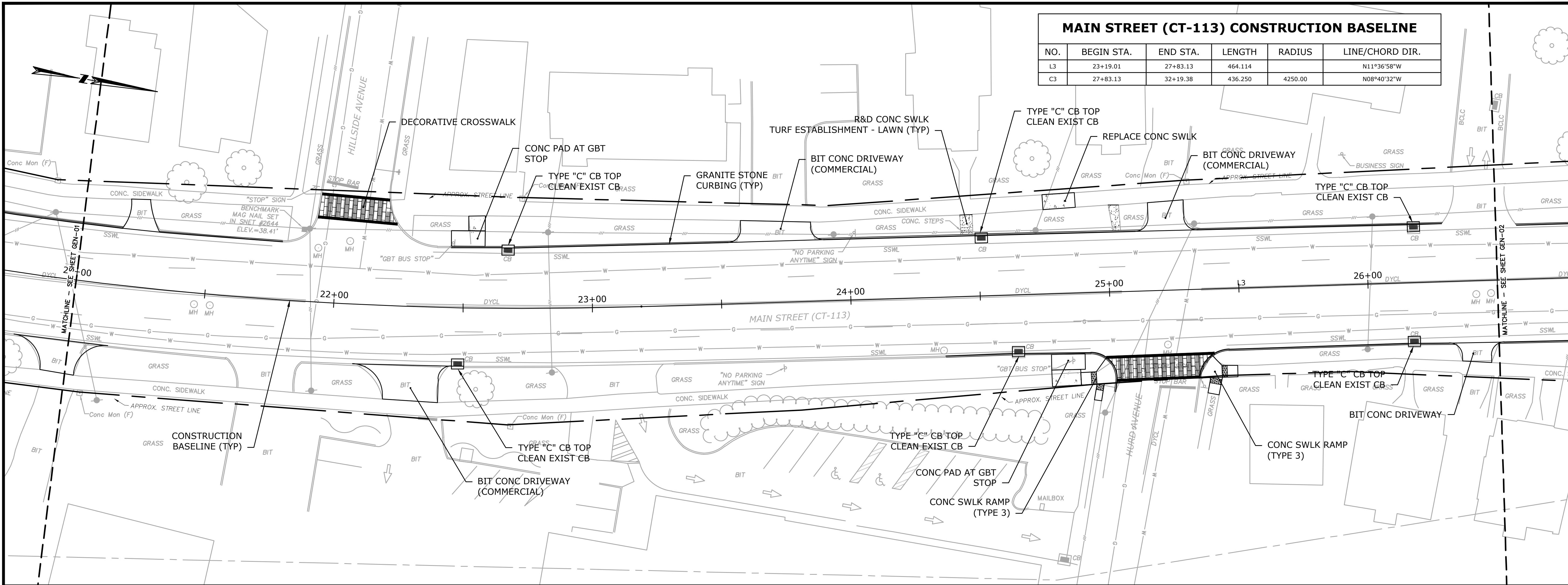
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0 2 4 8 FEET

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DWG. NO:   
JOB. NO: 83873.00 **TYP-01**





**MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2**

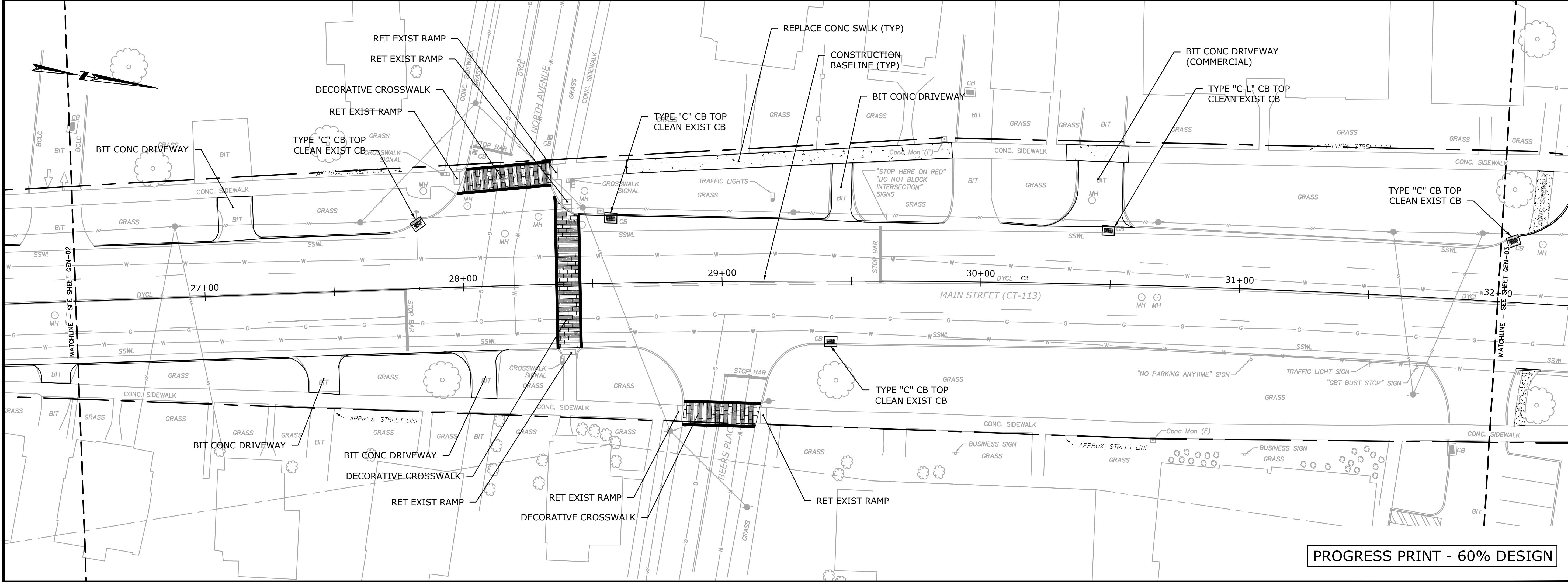
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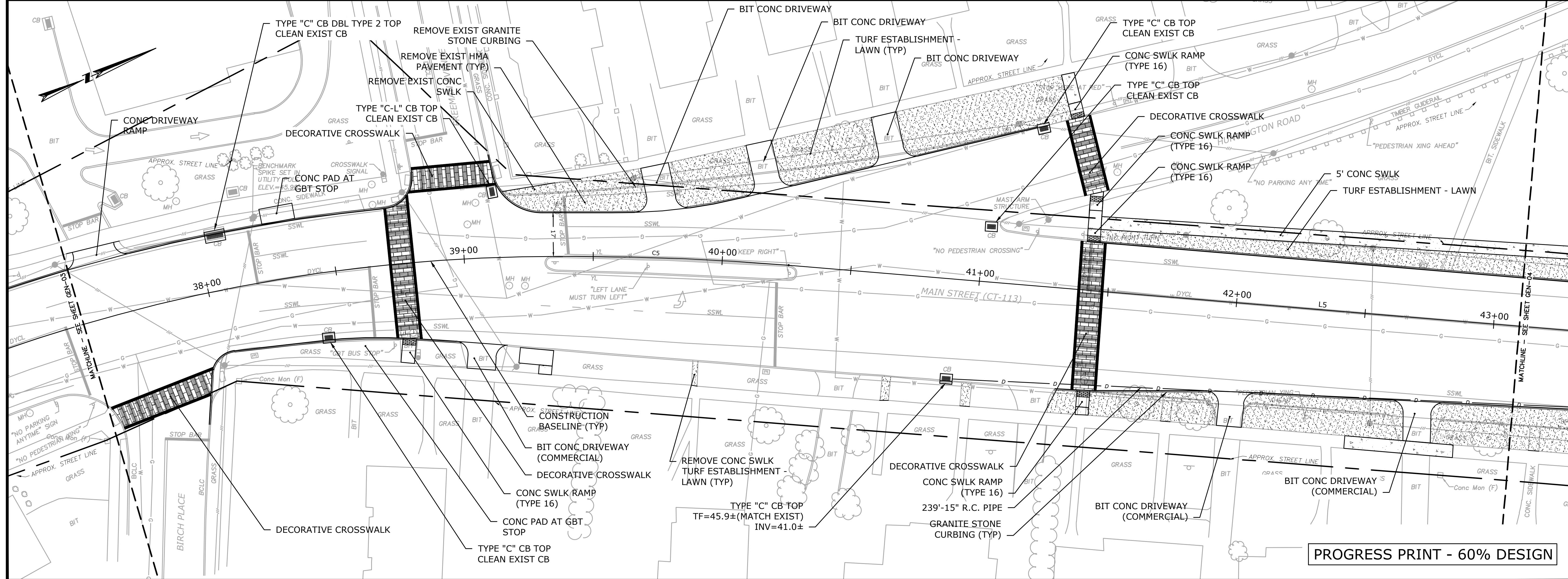
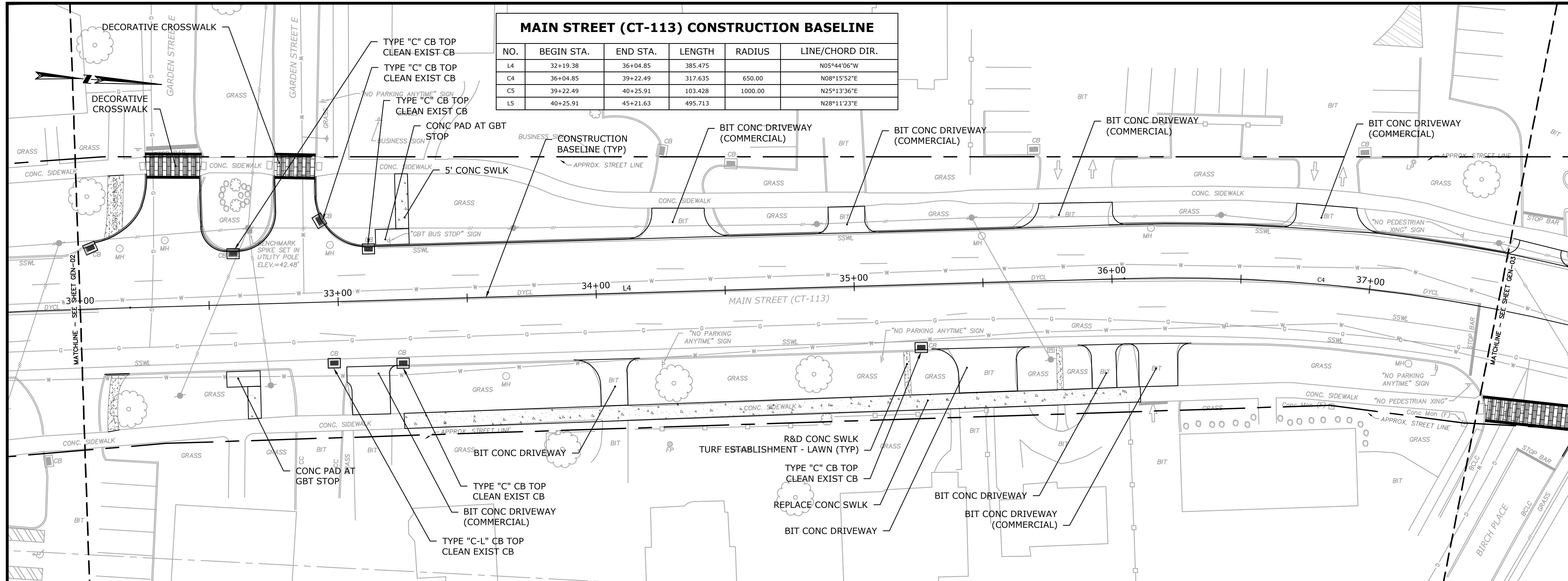
IN  
STRATFORD  
CONNECTICUT

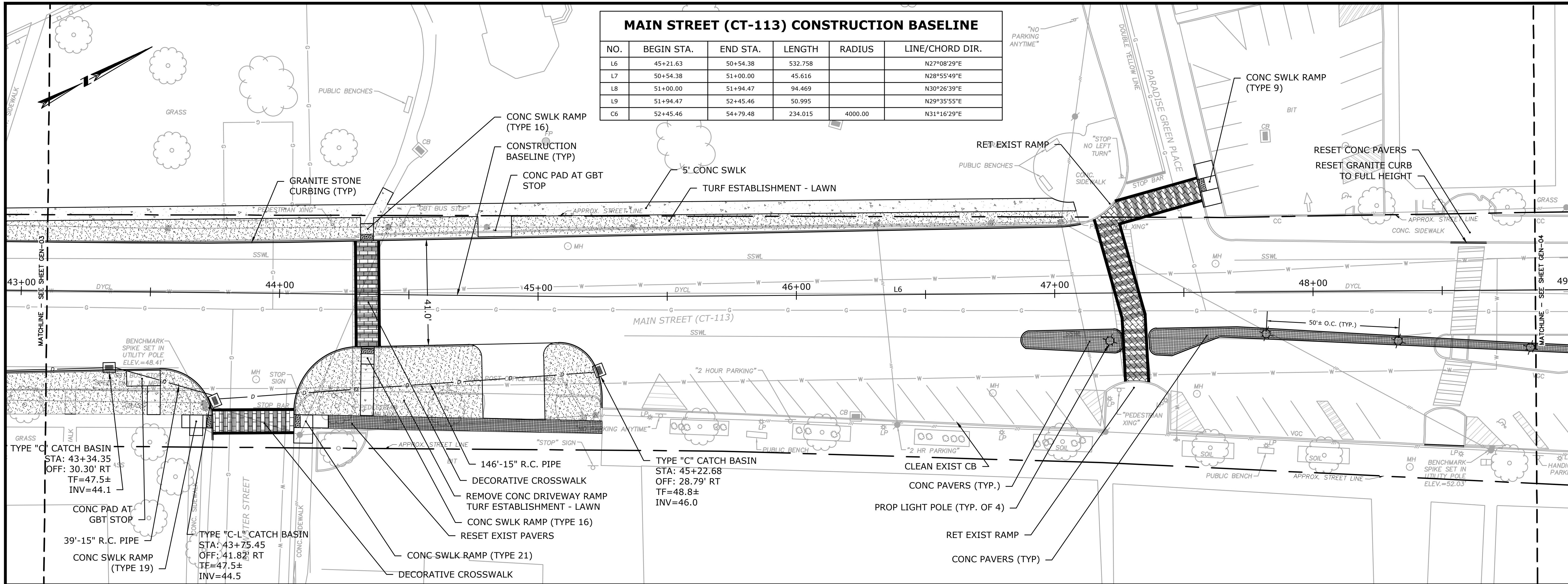
GENERAL PLAN

OCTOBER 2024

REVISIONS:







MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD

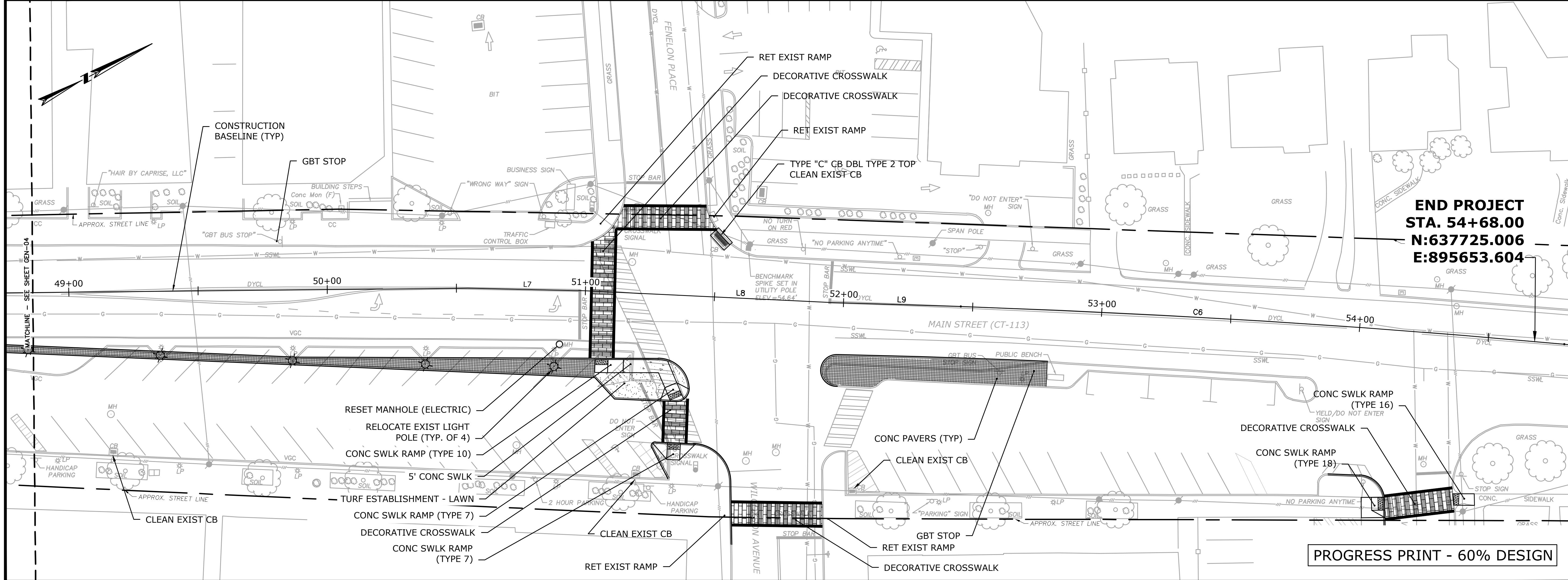
## MAIN STREET (CT-113)

IN  
**STRATFORD**  
**CONNECTICUT**

## GENERAL PLAN

OCTOBER 2024

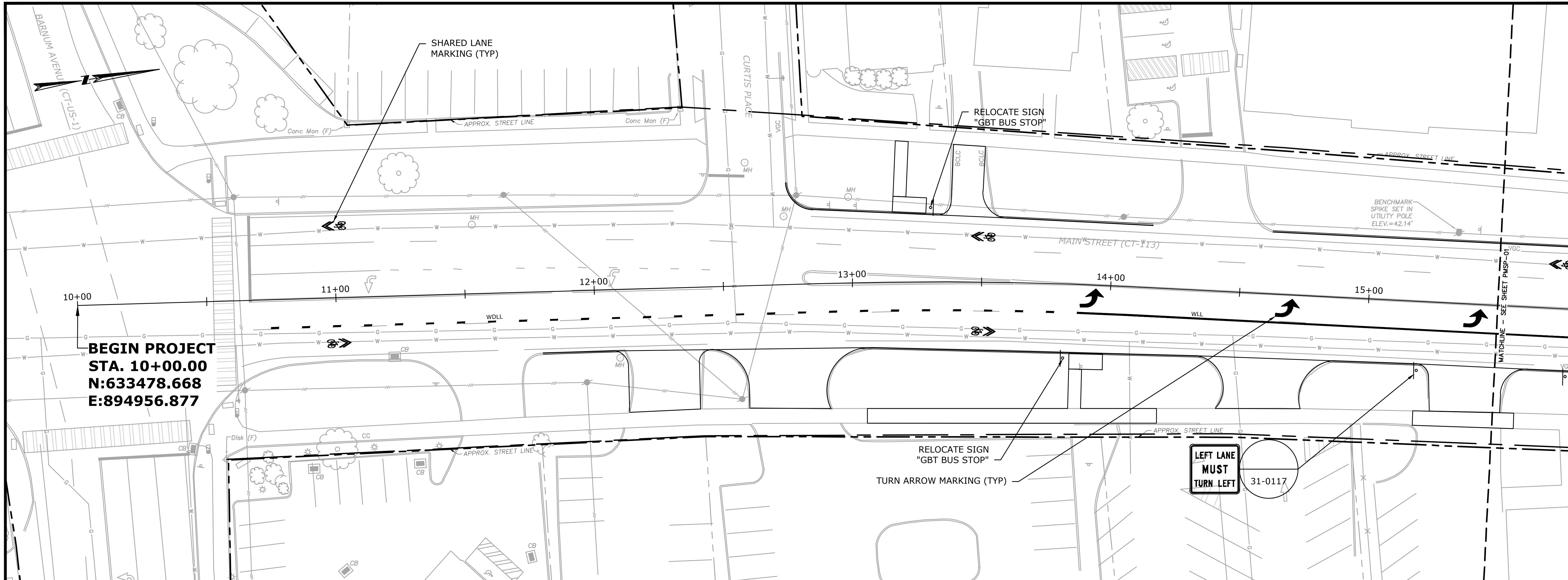
## VISIONS:



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SCALE: 1" = 20'

E: 8387300\CIVIL\\_DRAWINGS\  
G. NO: **GEN-04**  
S. NO: 83873.00



# MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

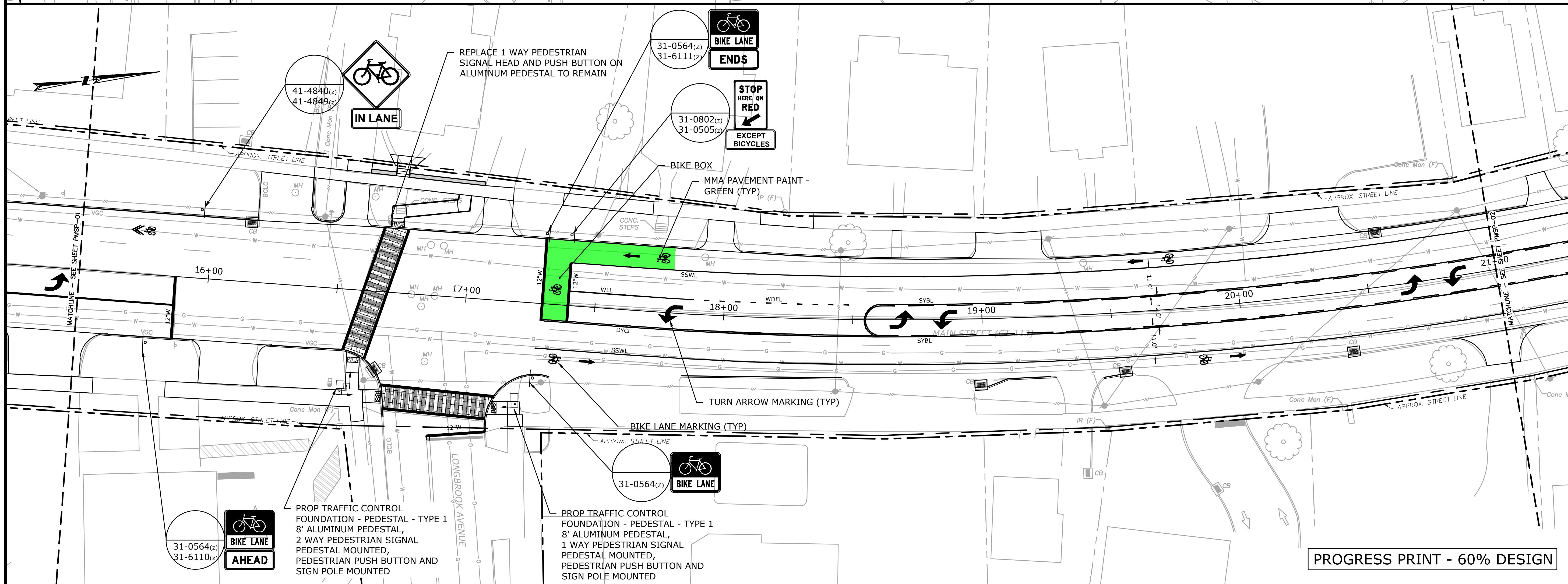
STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
**STRATFORD**  
**CONNECTICUT**

# PAVEMENT MARKINGS & SIGNAGE PLAN

OCTOBER 2024

## REVISIONS:



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1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
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06033

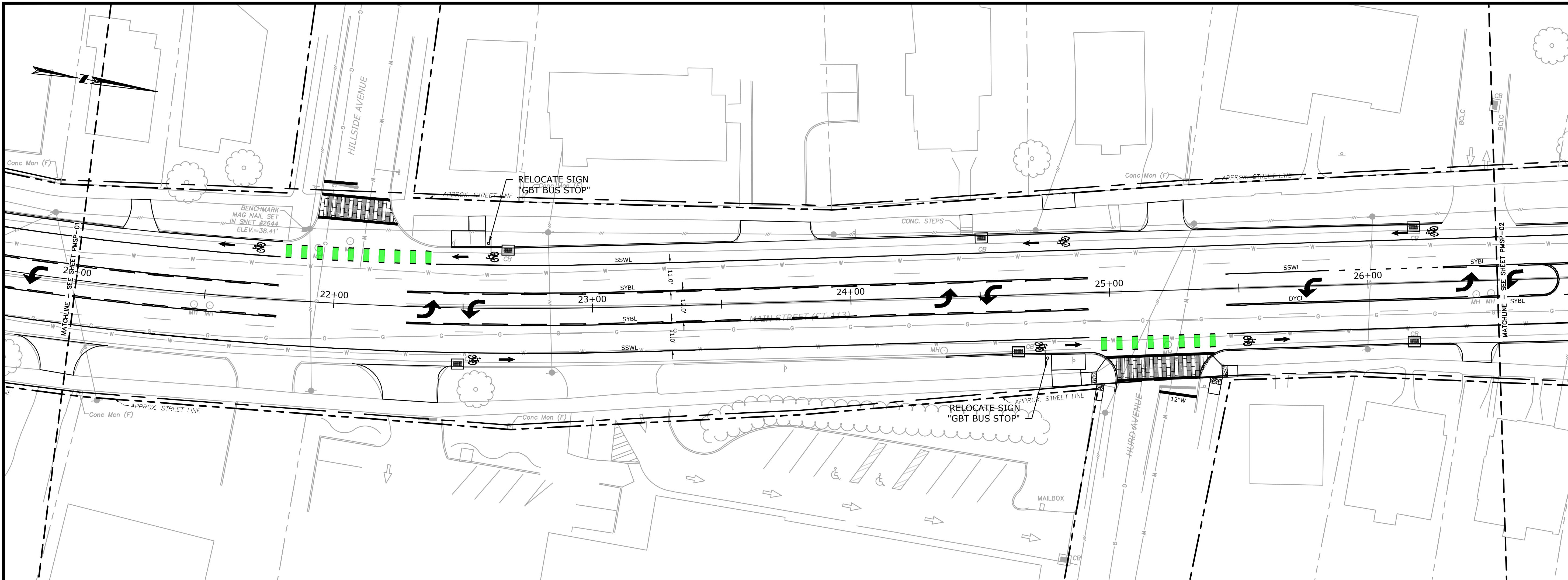
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SCALE: 1" = 20'

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FILE: 8387300\CIVIL\\_DRAWINGS\  
DWG. NO: **DM/CD 01**

JOB. NO: 83873.00



# MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

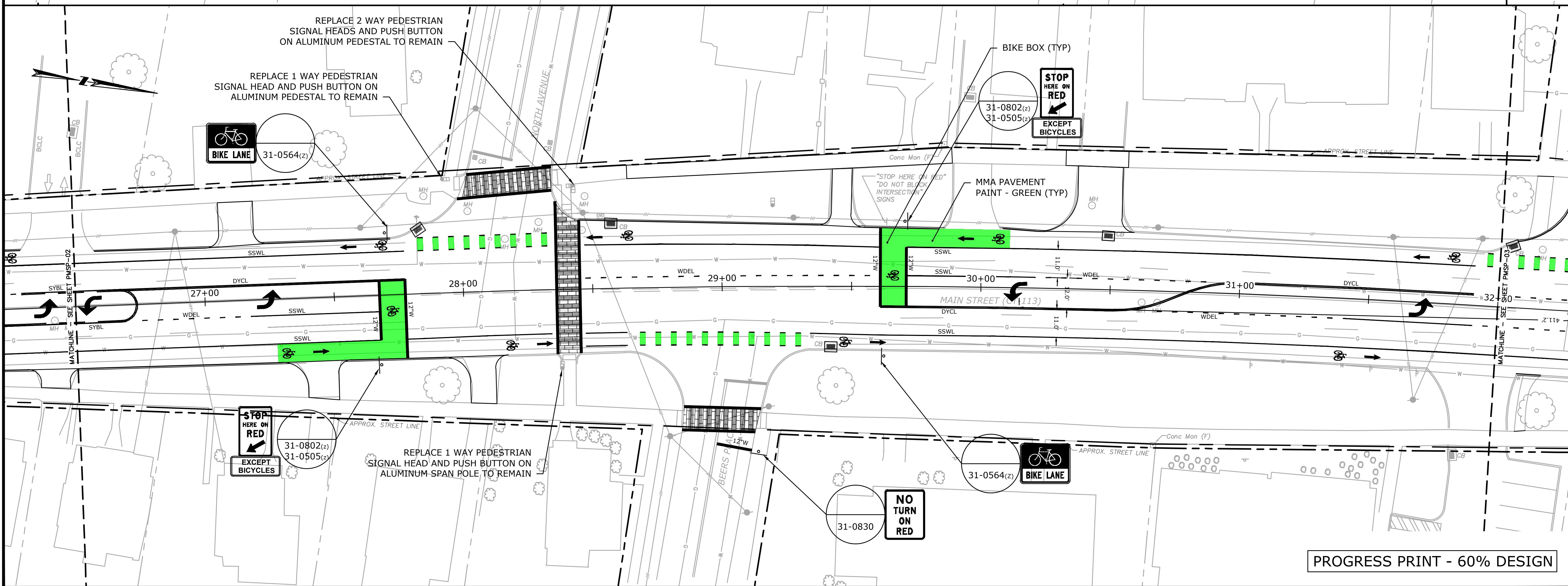
STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
**STRATFORD**  
**CONNECTICUT**

# PAVEMENT MARKINGS & SIGNAGE PLAN

OCTOBER 2024

## REVISIONS:



PREPARED FOR:  
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1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

# BSC GROUP

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665 Winding Brook Drive

665 Winding Brook Drive  
Glastonbury, Connecticut  
06033

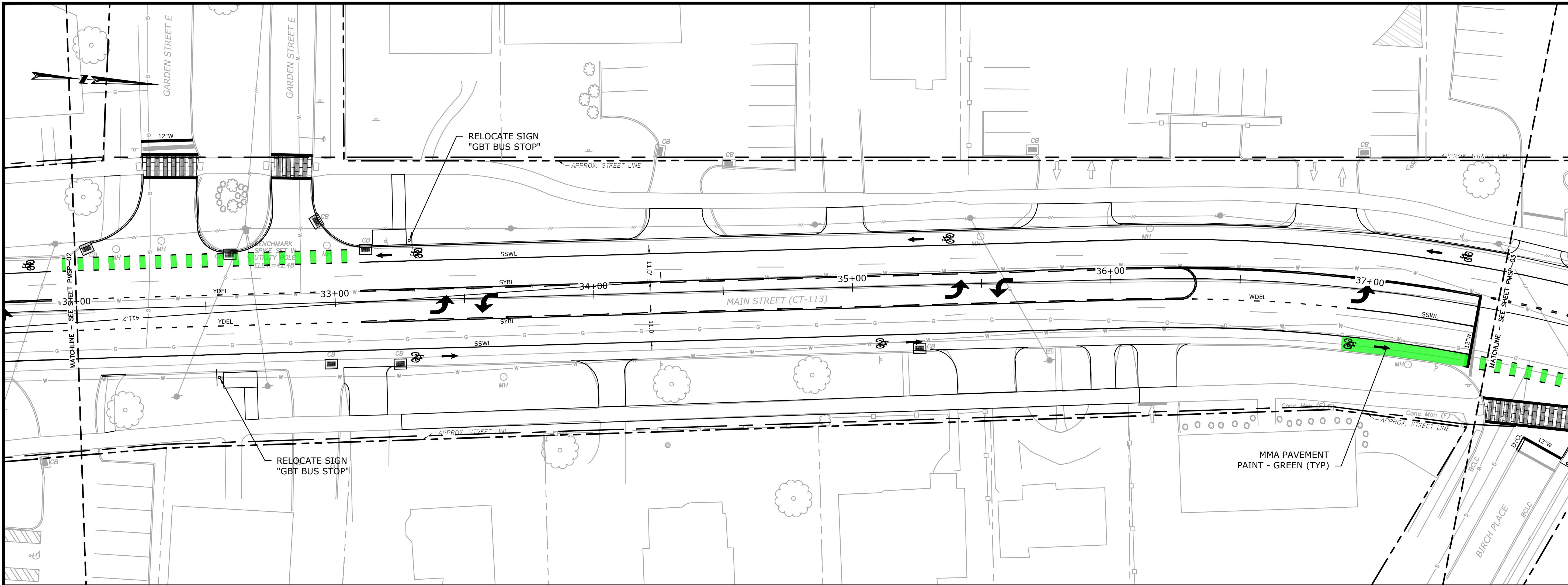
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A scale bar at the top of the map, consisting of a horizontal line with tick marks and numerical labels. The line is divided into four segments by tick marks. The first segment is labeled '0'. The second segment is labeled '10'. The third segment is labeled '20'. The fourth segment is labeled '40'. To the right of the '40' label is the word 'FEET' in capital letters.

FILE: 8387300\CVML\\_DRAWINGS\  
DWG. NO: **PMSP-02**  
JOB. NO: 83873.00

## PROGRESS PRINT - 60% DESIGN

## PMSP-02



MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

PAVEMENT MARKINGS &  
SIGNAGE PLAN

OCTOBER 2024

REVISIONS:

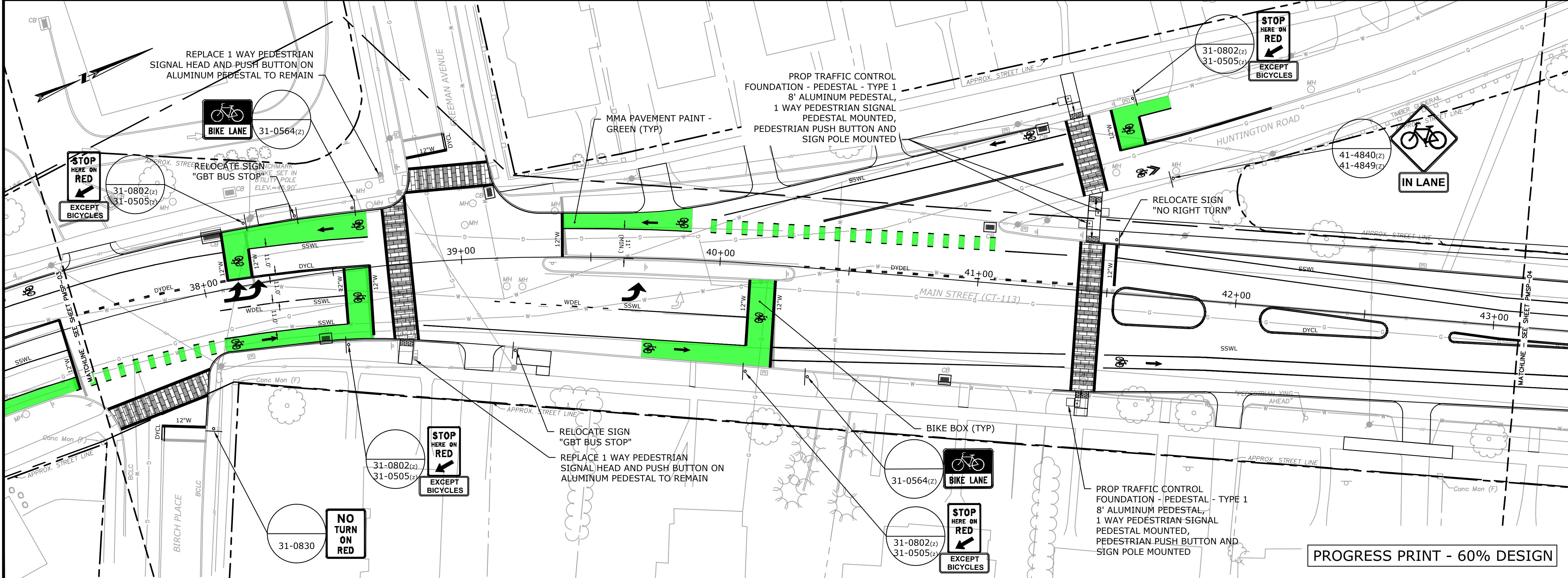
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1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

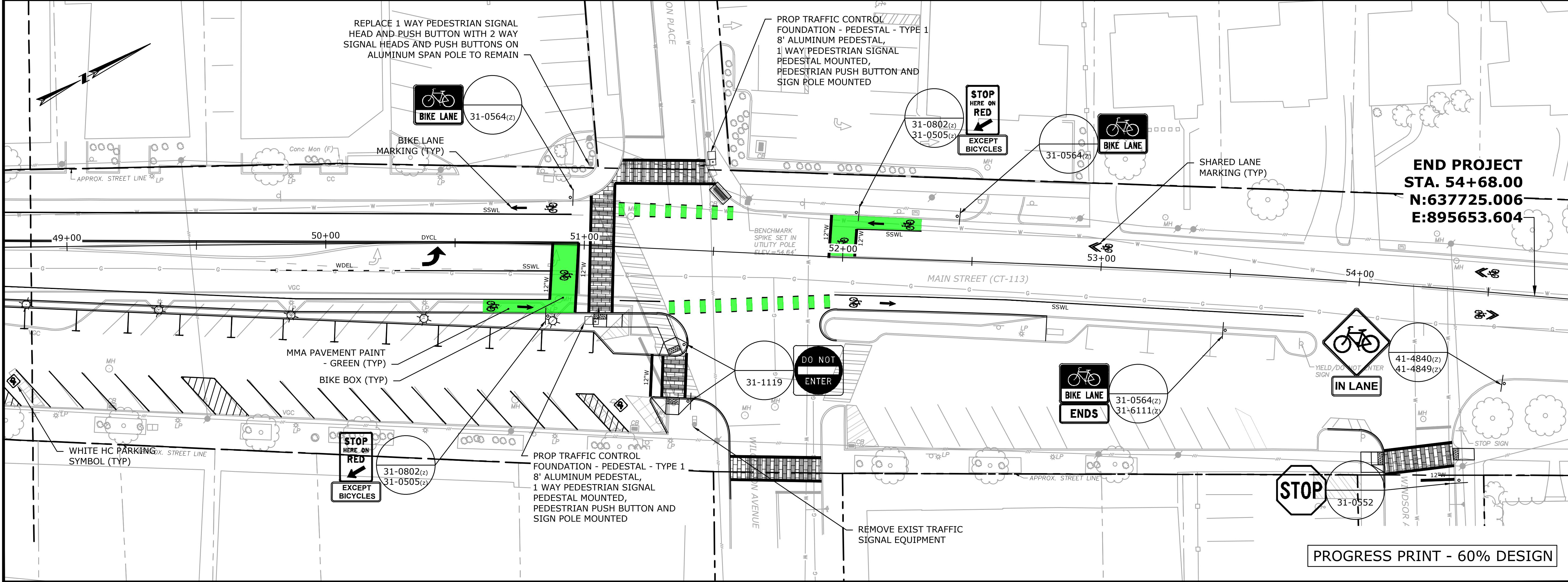
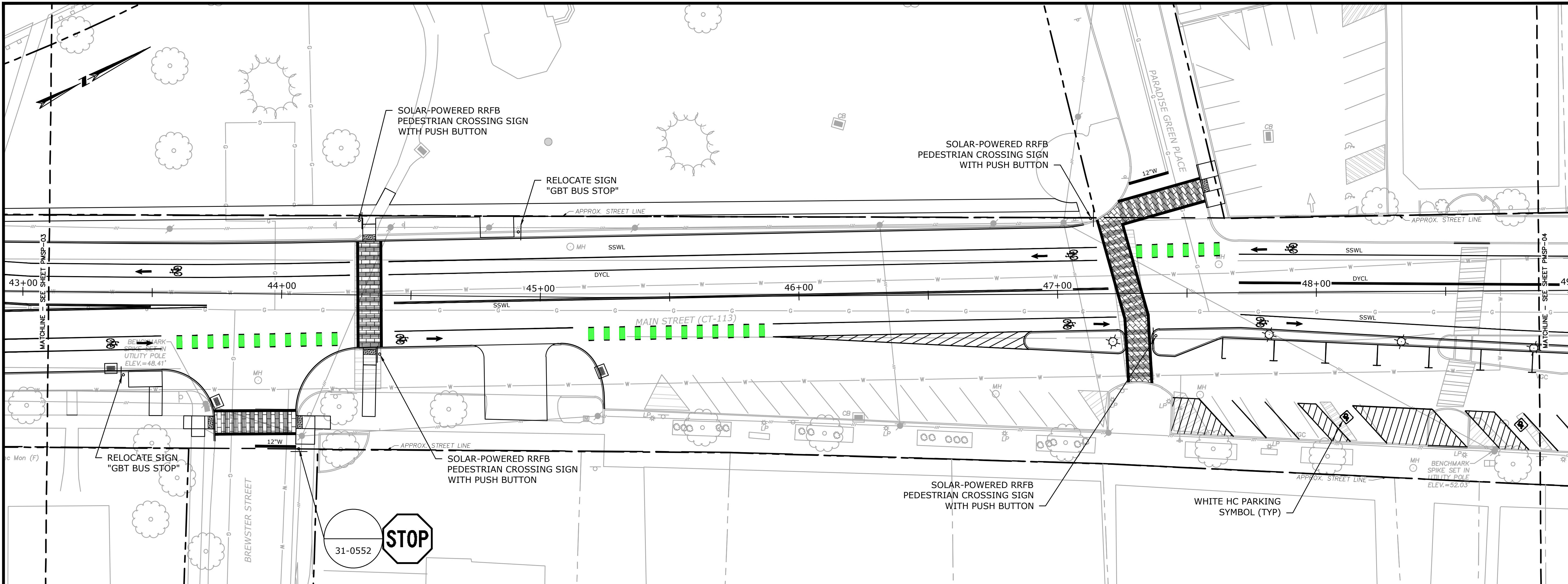
**BSC GROUP**   
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860 652 8227

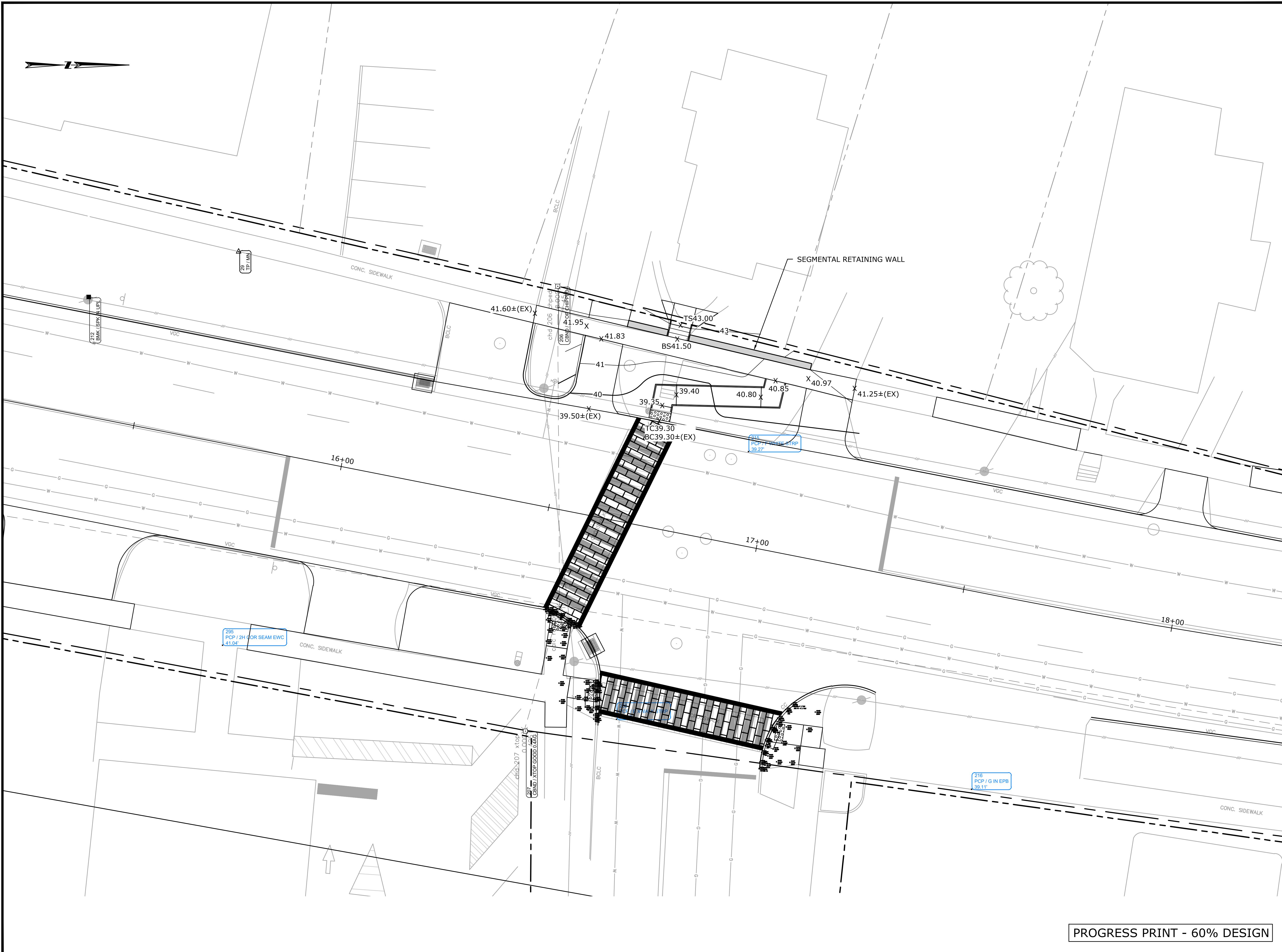
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SCALE: 1" = 20'  
0 10 20 40 FEET  
FILE: 8387300\ CIVIL\ DRAWINGS  
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JOB. NO: 83873.00

PROGRESS PRINT - 60% DESIGN

PMSP-03







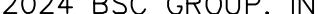
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STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
**STRATFORD  
CONNECTICUT**

## GRADING PLAN

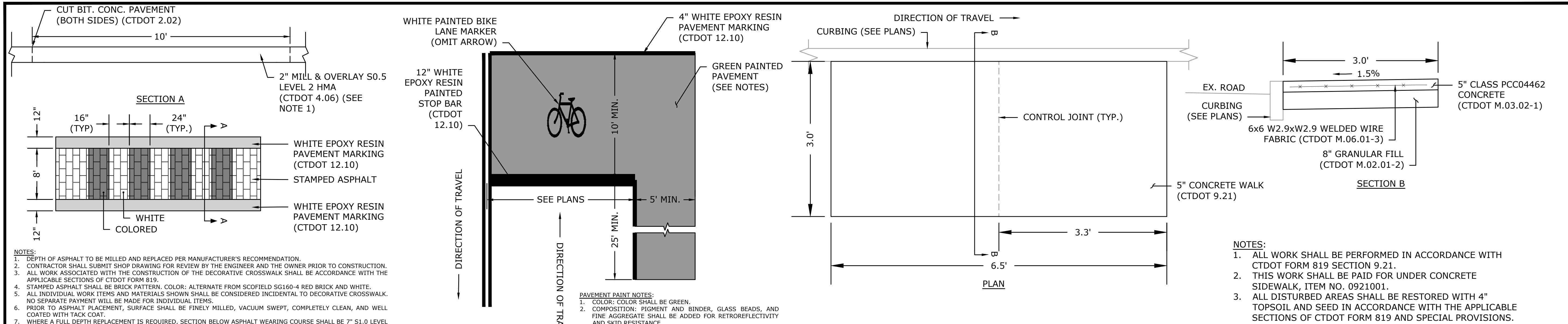
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SCALE: 1" = 10'  
  
0 5 10 20 FEET

FILE: 8387300\CVIL\\_\DRAWINGS\  
DWG. NO: GRAD-01  
JOB. NO: 83873.00

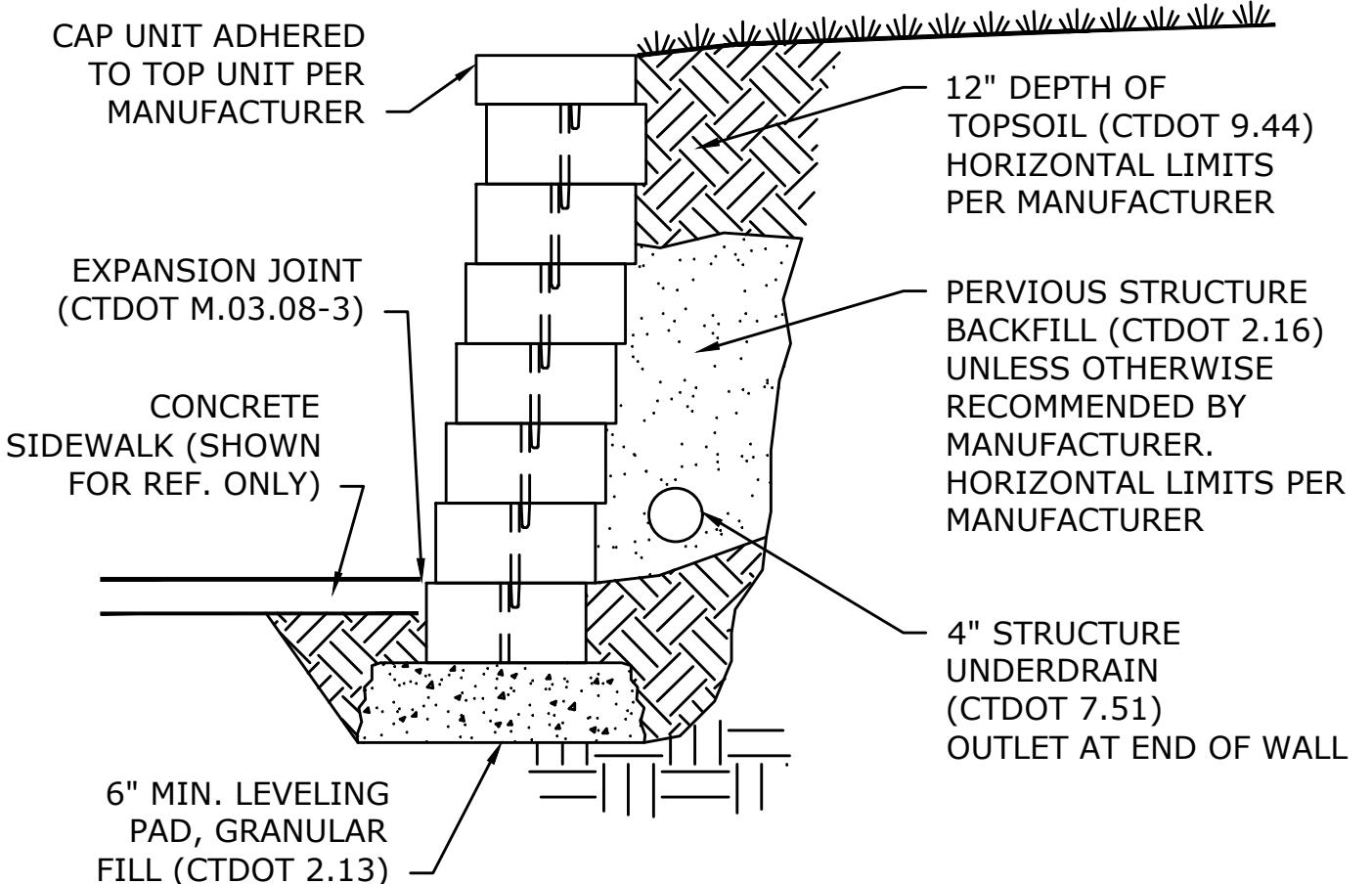
## PROGRESS PRINT - 60% DESIGN

GRAD-01



### DECORATIVE CROSSWALK

SCALE: NONE



### BIKE BOX

SCALE: NONE

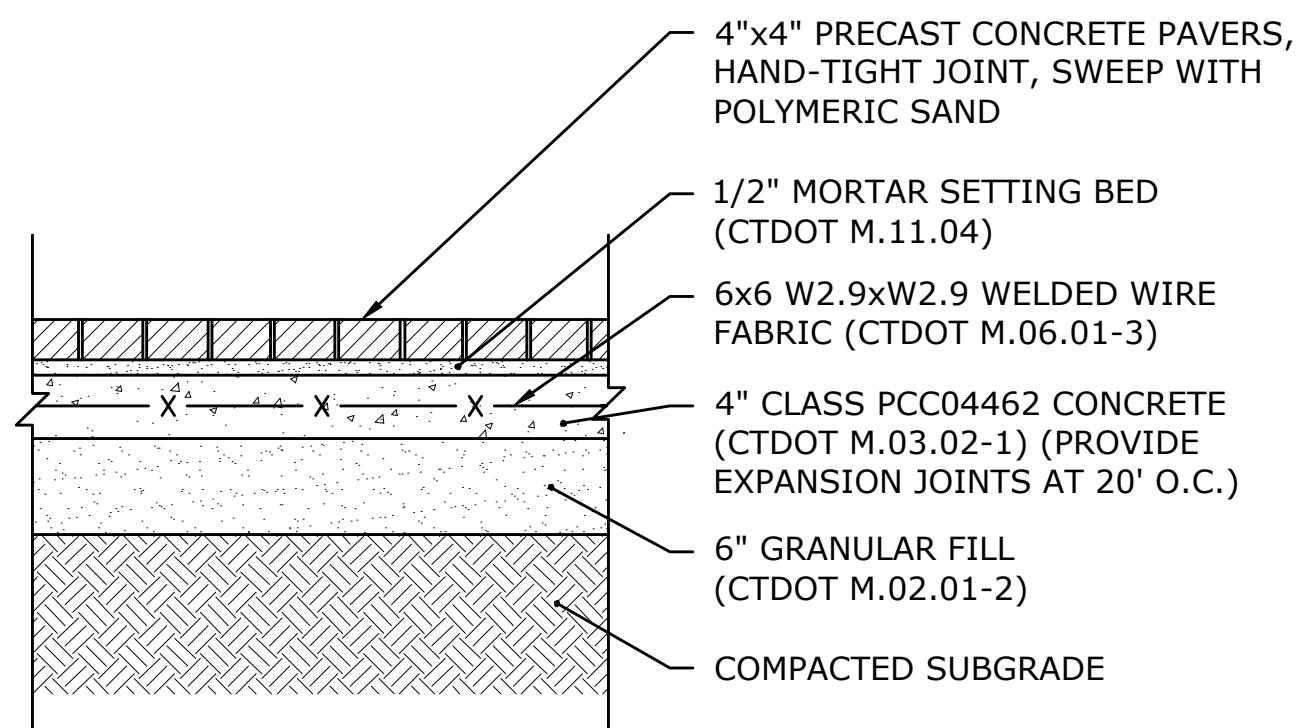
#### SEGMENTAL RETAINING WALL (SRW) NOTES:

NOTE TO CONTRACTOR: THIS IS A DELEGATED DESIGN. SEE BELOW FOR MINIMUM REQUIREMENTS AND SPECIFICATIONS. THE SELECTED SYSTEM SHALL BE A SEGMENTAL RETAINING WALL SYSTEM. SEGMENTAL BLOCK RETAINING WALL SHALL BE "STANDARD" BLOCK UNIT AS MANUFACTURED BY VERSA-LOK, "CONCORD WALL" AS MANUFACTURED BY UNILOCK OR APPROVED EQUAL.

1. DIMENSIONS AND REPORTED ELEVATIONS SHOWN ON THE PLANS ARE APPROXIMATE AND MAY BE ADJUSTED TO ACCOMMODATE THE SELECTED SRW SYSTEM. THE DESIGN INTENT IS TO PROVIDE A NOMINAL REVEAL ABOVE FINISHED GRADE ALONG THE SRW LENGTH (FAR FACE), A MINIMUM EMBEDMENT OF 12" (MEASURED FROM FINISHED GRADE AT THE NEAR FACE OF THE SRW TO THE TOP OF THE LEVELING PAD), A MINIMUM LEVELING PAD THICKNESS OF 6" AND A MINIMUM STRUCTURAL FILL THICKNESS OF 1'-0".
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND INTERNAL STABILITY OF THE SELECTED SRW SYSTEM. THE DESIGN SHALL BE COMPLETED IN ACCORDANCE WITH THE LATEST NATIONAL CONCRETE MASONRY ASSOCIATION (NCMA) DESIGN MANUAL FOR SEGMENTAL RETAINING WALLS. THE CONTRACTOR SHALL SUBMIT SMALL BLOCK SRW SHOP DRAWINGS AND DESIGN CALCULATIONS FOR APPROVAL. THE DESIGN SHALL ALSO ACCOUNT FOR ANY STRUCTURES, FOOTINGS, OR OTHER SURCHARGE LOADS LOCATED WITHIN THE VICINITY OF THE TOP OF WALL. ALL PLANS AND CALCULATIONS SHALL BE PREPARED, SIGNED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF CONNECTICUT.
3. ALL COSTS ASSOCIATED WITH THE DESIGN, FABRICATION AND CONSTRUCTION OF THE SRW SYSTEM INCLUDING THE EXCAVATION, STRUCTURAL FILL, LEVELING PAD, GRANULAR BACKFILL FOR SRW, GEOTEXTILE FABRIC, GEOMEMBRANE, FACING SEALER, TEMPORARY EXCAVATION SUPPORT, AND PERFORATED PIPE SHALL BE CONSIDERED INCIDENTAL TO THE SRW SYSTEM.
4. A MANUFACTURER APPROVED FACING SEALER SHALL BE APPLIED TO ALL EXPOSED SRW SURFACES TO APPROXIMATELY 1'-0" BELOW FINISHED GRADE.
5. THE CONTRACTOR SHALL DEVELOP THE OVERALL CONSTRUCTION SEQUENCE TO ENSURE THAT THE SRW IS NOT UNDERRUNNED, OR DAMAGED DURING CONSTRUCTION OPERATIONS. THE CONTRACTOR IS NOTIFIED THAT THE INSTALLATION OF TEMPORARY EXCAVATION SUPPORT/CASING MAY BE REQUIRED, DEPENDING ON THE FINAL LOCATION AND SELECTED CONSTRUCTION SEQUENCE.

### SEGMENTAL RETAINING WALL

SCALE: NONE



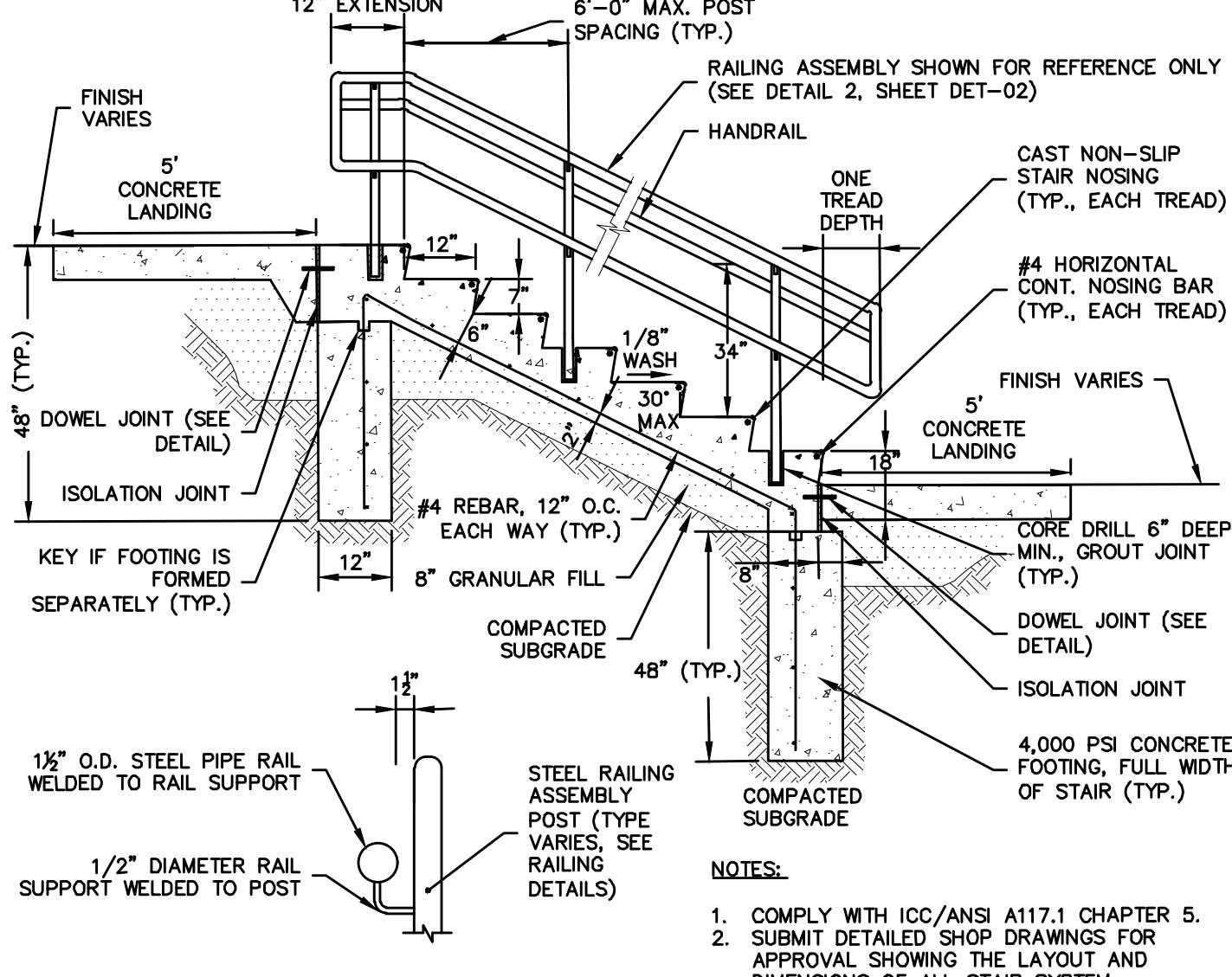
**NOTE:**  
THIS WORK SHALL BE PAID FOR UNDER CONCRETE PAVERS, ITEM NO. 0921024A. SEE SPECIAL PROVISION.

### CONCRETE PAVERS

SCALE: NONE

### CONCRETE PAD AT GBT STOP

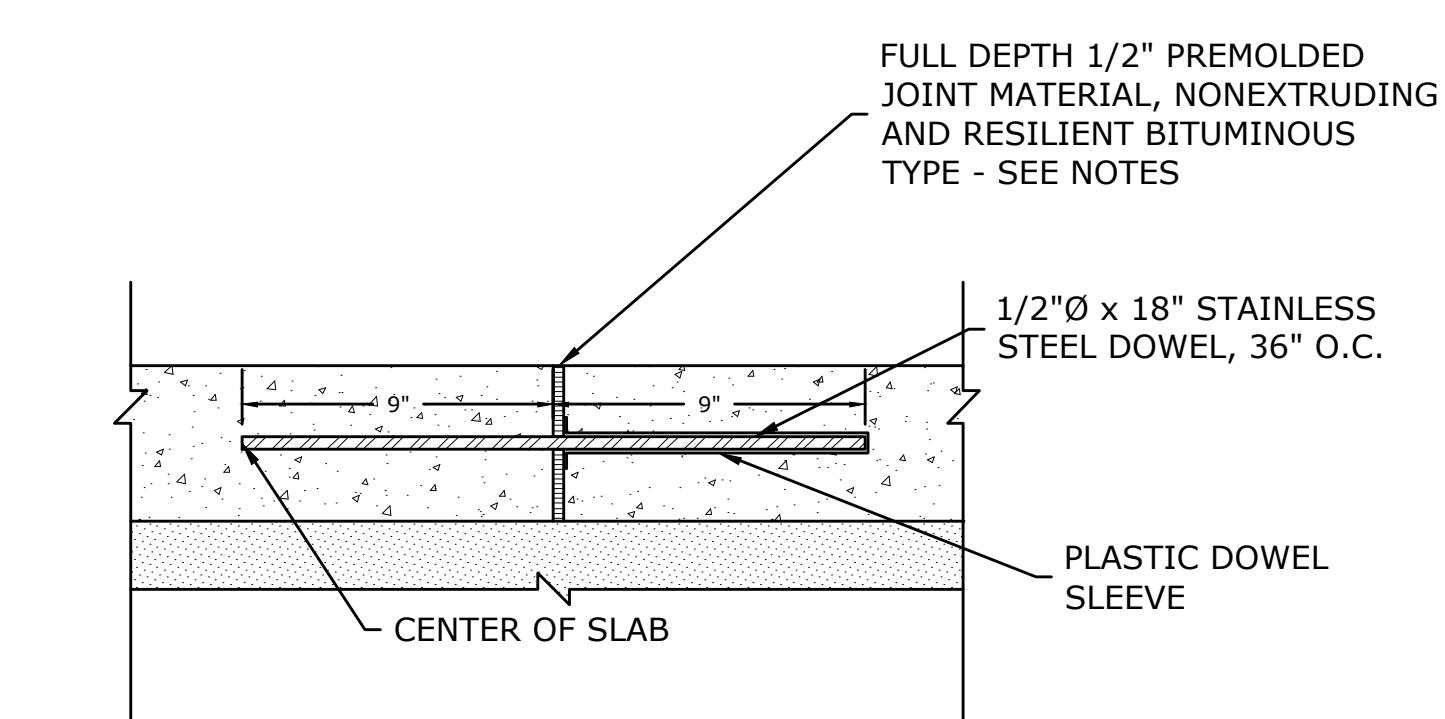
SCALE: NONE



\*THIS ITEM SHALL BE PAID FOR AS A LUMP SUM. ALL WORK SHOWN HERON SHALL BE CONSIDERED INCIDENTAL.

### CONCRETE STAIRS

SCALE: NONE

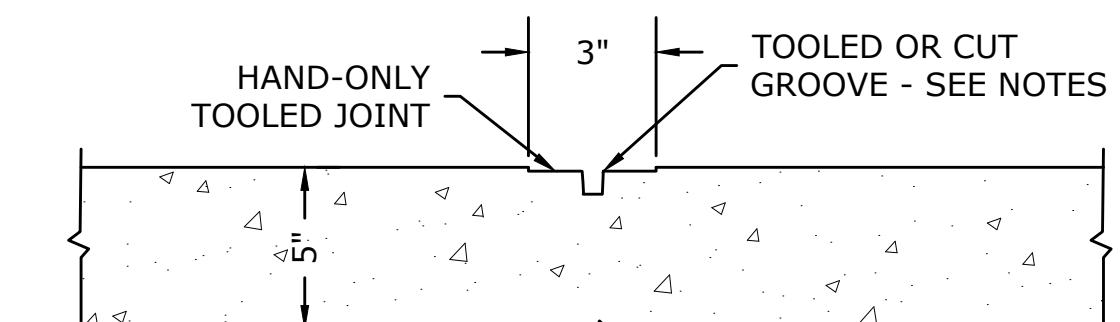


**NOTES:**

1. PRIOR TO INITIATION OF CONCRETE FLATWORK, SUBMIT PROPOSED CONSTRUCTION JOINT PLAN TO THE ENGINEER FOR REVIEW AND APPROVAL. COORDINATE SUCH PLAN WITH THE JOINT PATTERNS DEPICTED ON THE DRAWINGS.
2. FOR CONCRETE JOINTS WITH BULKHEAD/FORMWORK TO ENSURE A SMOOTH, CLEAN EDGE FREE OF SAGS AND IRREGULARITIES.
3. ENSURE PROPER CURING OF THE VERTICAL FACE AFTER REMOVAL OF BULKHEAD/FORMWORK BY APPLYING A LIQUID CURING COMPOUND OR PLASTIC SHEETING.
4. UNLESS OTHERWISE SPECIFIED, CONSTRUCTION JOINTS SHALL BE CONSTRUCTED WITH JOINT FILLER. JOINT FILLER SHALL EXTEND THE FULL LENGTH OF THE SLAB AND SHALL EXTEND THE FULL LENGTH OF THE JOINT. JOINT FILLER SHALL BE A MATERIAL OF LESSER DIMENSIONS TO MAKE UP REQUIRED DEPTH AND WIDTH OF JOINT WILL NOT BE PERMITTED.
5. WHERE JOINTS ARE TO RECEIVE FILLER, RECESS JOINT FILLER 1/4-INCH BELOW FINISH SURFACE OR AS OTHERWISE INDICATED ON THE DRAWINGS.

### DOWELED CONSTRUCTION JOINT IN CONCRETE

SCALE: NONE



**NOTES:**

1. JOINT GROOVE SHALL HAVE A MINIMUM DEPTH OF 1/4 SLAB THICKNESS.
2. TOOLED JOINTS SHALL BE RUN EARLY IN THE FINISHING PROCESS AND RERUN LATER TO ENSURE GROOVE BOND HAS NOT OCCURRED.
3. SAWCUT JOINTS MAX. WIDTH OF 1/8 INCH.

### CONTROL JOINT IN CONCRETE

SCALE: NONE

## MAIN STREET STREETSCAPE - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)  
IN  
STRATFORD CONNECTICUT

MISCELLANEOUS DETAILS

REVISIONS:

PREPARED FOR:  
METROCOC  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**

BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

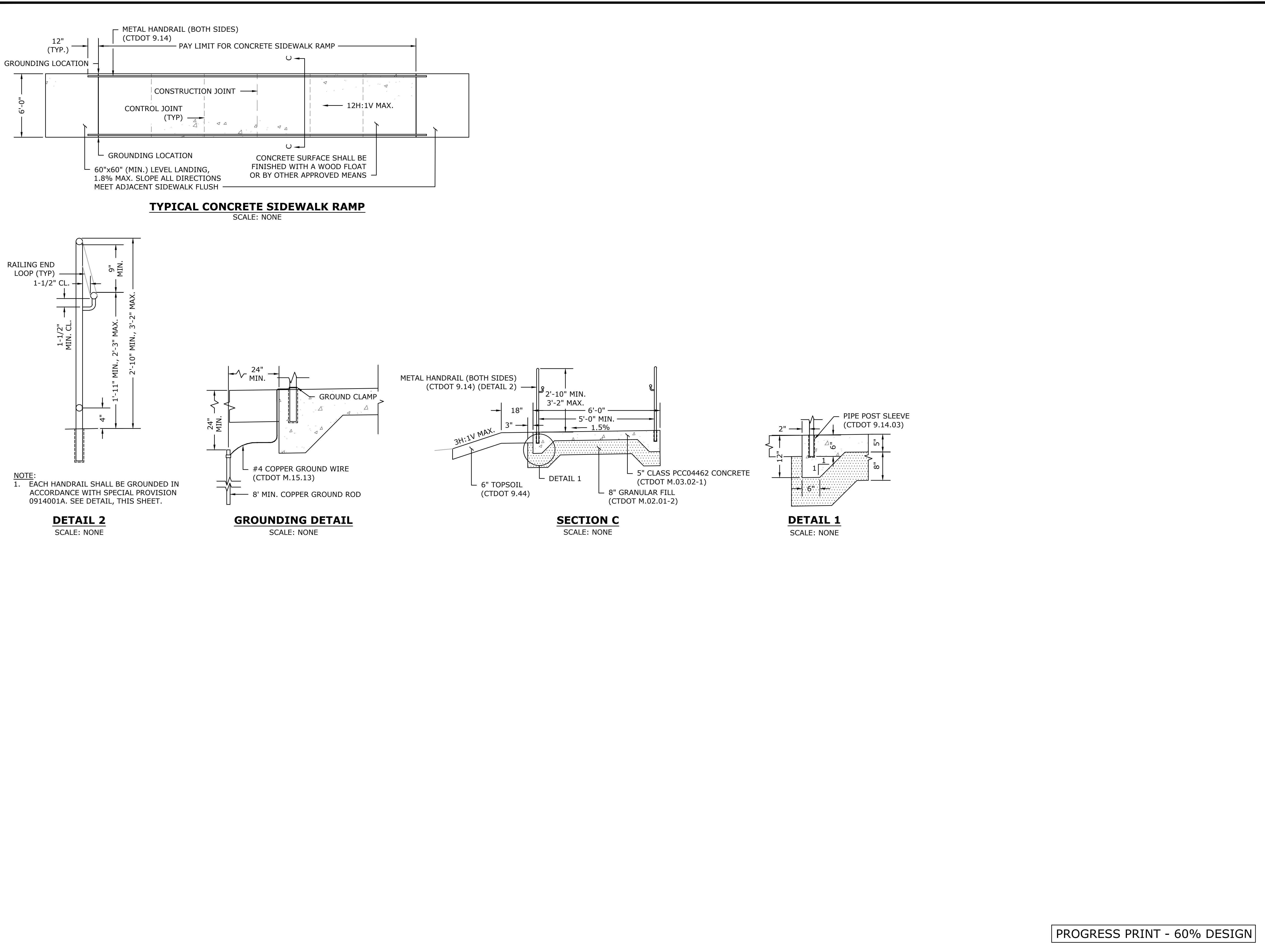
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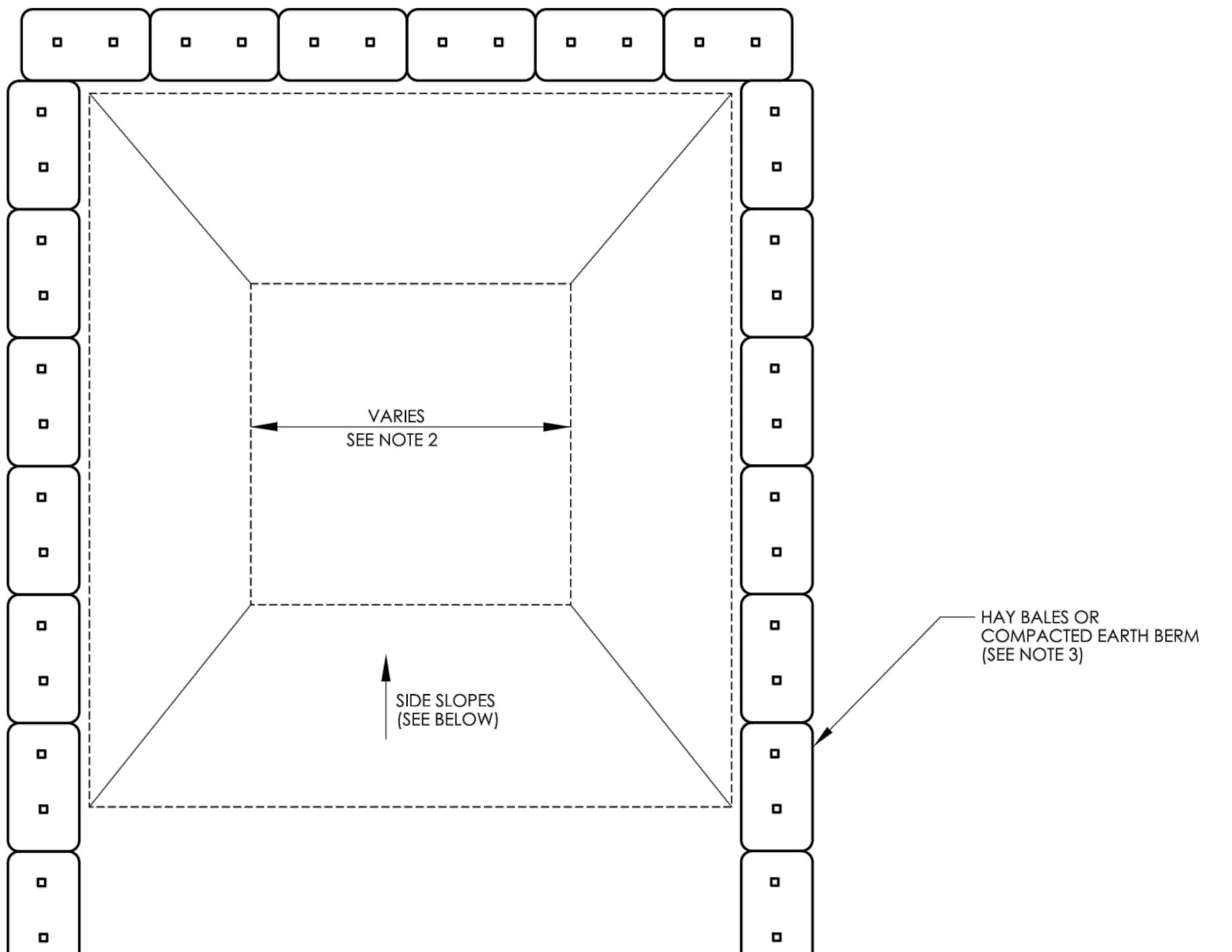
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FILE: 8387300\ CIVIL\ DRAWINGS  
DWG. NO: JOB. NO: 83873.00 DET-01

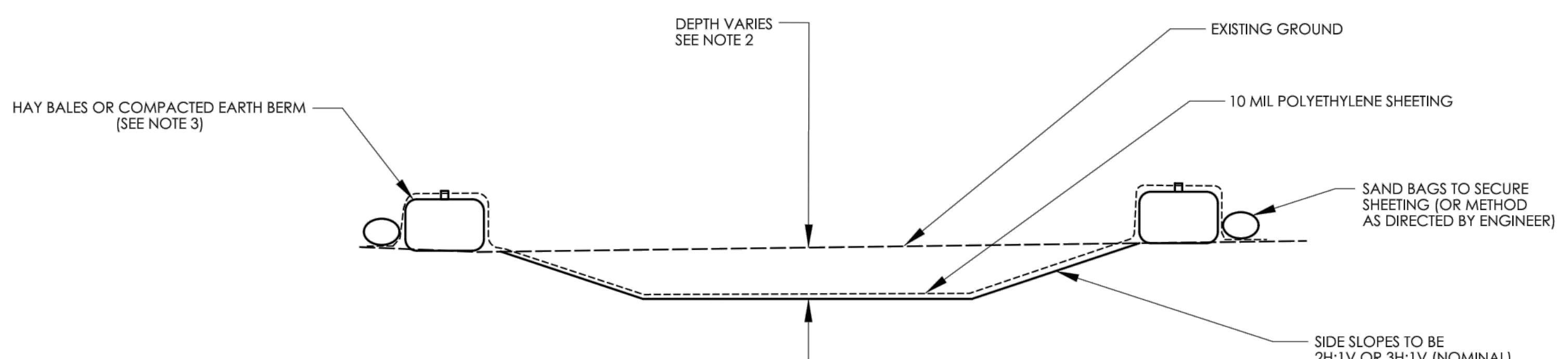
PROGRESS PRINT - 60% DESIGN

DET-01





PLAN



CONCRETE WASHOUT AREA

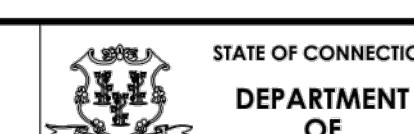
NOT TO SCALE  
(SEE NOTE 2)

REV.	DATE	REVISION DESCRIPTION

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PLOTTED DATE: 11/25/2022

SIGNATURE/  
BLOCK:  
OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

APPROVED BY:



PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE WASHOUT AREA

DRAWING NO.  
SHEET NO.

**GENERAL NOTES:**

1. CONCRETE WASHOUT AREA(S) SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE. THE CONCRETE WASHOUT AREA SHALL BE ENTIRELY SELF-CONTAINED.
2. THE CONTRACTOR SHALL SUBMIT THE DESIGN, LOCATION AND SIZING OF THE CONCRETE WASHOUT AREA(S) WITH THE PROJECT'S EROSION AND SEDIMENTATION CONTROL PLAN AND SHALL BE APPROVED BY THE ENGINEER.
3. LOCATION: WASHOUT AREA(S) ARE TO BE LOCATED AT LEAST 50 FEET FROM ANY STREAM, WETLAND, STORM DRAINS, OR OTHER SENSITIVE RESOURCE. THE FLOOD CONTINGENCY PLAN MUST ADDRESS THE CONCRETE WASHOUT IF THE WASHOUT IS TO BE LOCATED WITHIN THE FLOODPLAIN.
4. SIZE: THE WASHOUT MUST HAVE SUFFICIENT VOLUME TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS INCLUDING, BUT NOT LIMITED TO, OPERATIONS ASSOCIATED WITH GROUT AND MORTAR.
5. SURFACE DISCHARGE IS UNACCEPTABLE. THEREFORE, HAY BALES OR OTHER CONTROL MEASURES, AS APPROVED BY THE ENGINEER, SHOULD BE USED AROUND THE PERIMETER OF THE CONCRETE WASHOUT AREA FOR CONTAINMENT.
6. SIGNS SHOULD BE PLACED AT THE CONSTRUCTION ENTRANCE AT THE CONCRETE AREA(S) AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CONCRETE WASHOUT TO OPERATORS OF CONCRETE TRUCKS AND PUMP RIGS. WASHOUT AREA(S) SHOULD BE FLAGGED WITH SAFETY FENCING OR OTHER APPROVED METHOD.
7. WASHOUT AREA(S) ARE TO BE INSPECTED AT LEAST ONCE A WEEK FOR STRUCTURAL INTEGRITY, ADEQUATE HOLDING CAPACITY AND CHECKED FOR LEAKS, TEARS, OR OVERFLOWS. (AS REQUIRED BY THE CONSTRUCTION SITE ENVIRONMENTAL INSPECTION REPORT) WASHOUT AREA(S) SHOULD BE CHECKED AFTER HEAVY RAINS.
8. HARDENED CONCRETE WASTE SHOULD BE REMOVED AND DISPOSED OF WHEN THE WASTE HAS ACCUMULATED TO HALF OF THE CONCRETE WASHOUT'S DEPTH. THE WASTE CAN BE STORED AT AN UPLAND LOCATION, AS APPROVED BY THE ENGINEER. ALL CONCRETE WASTE SHALL BE DISPOSED OF IN A MANNER CONSISTENT WITH ALL APPLICABLE LAWS, REGULATIONS, AND GUIDELINES.
9. PAYMENT FOR THIS ITEM IS TO BE INCLUDED UNDER THE GENERAL COST OF THE WORK FOR THE PROJECT, INCLUDING SITE RESTORATION.

**MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2**

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD  
CONNECTICUT

**STATE GUIDE SHEET  
DETAILS**

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

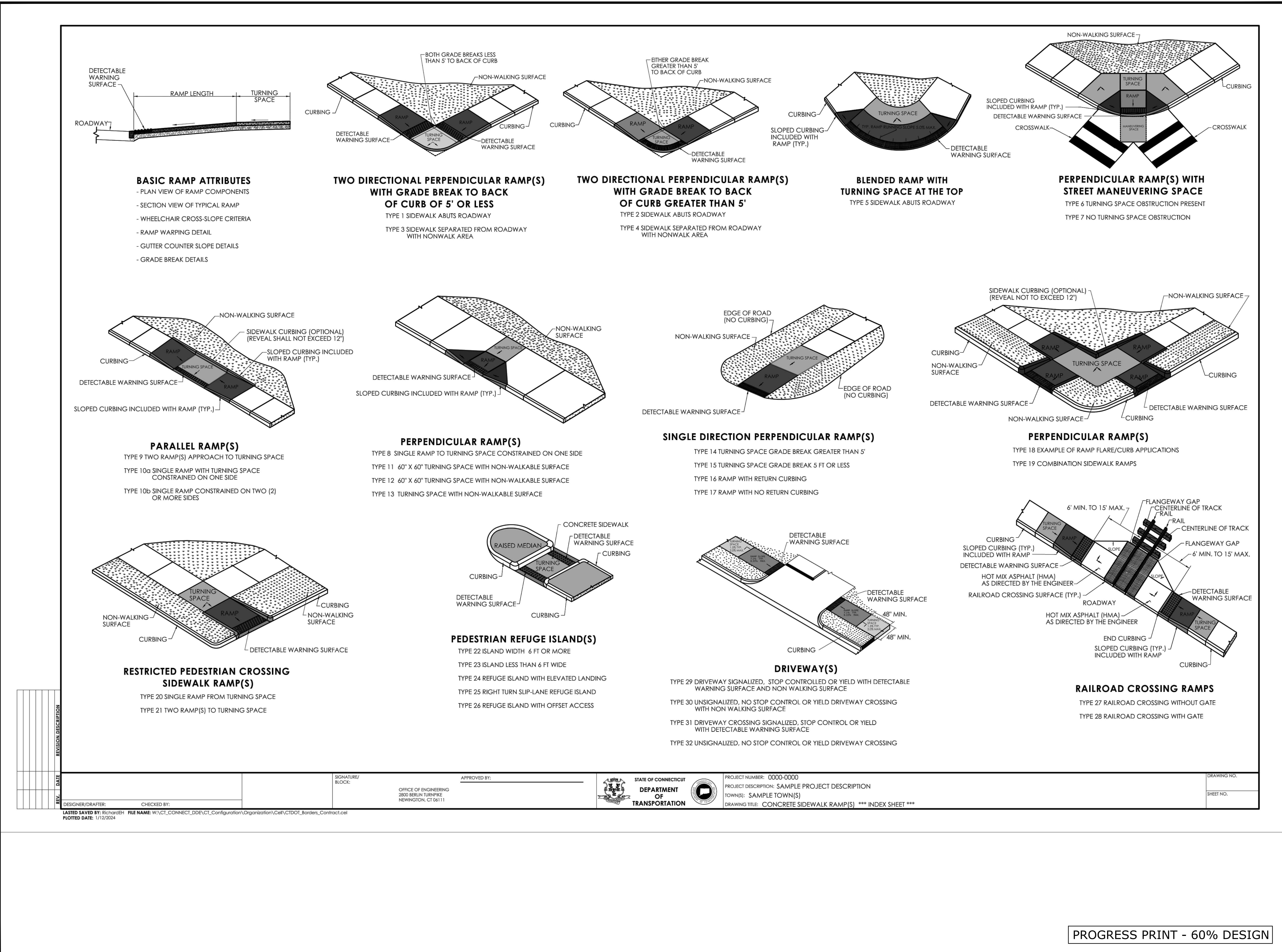
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SCALE: AS SHOWN

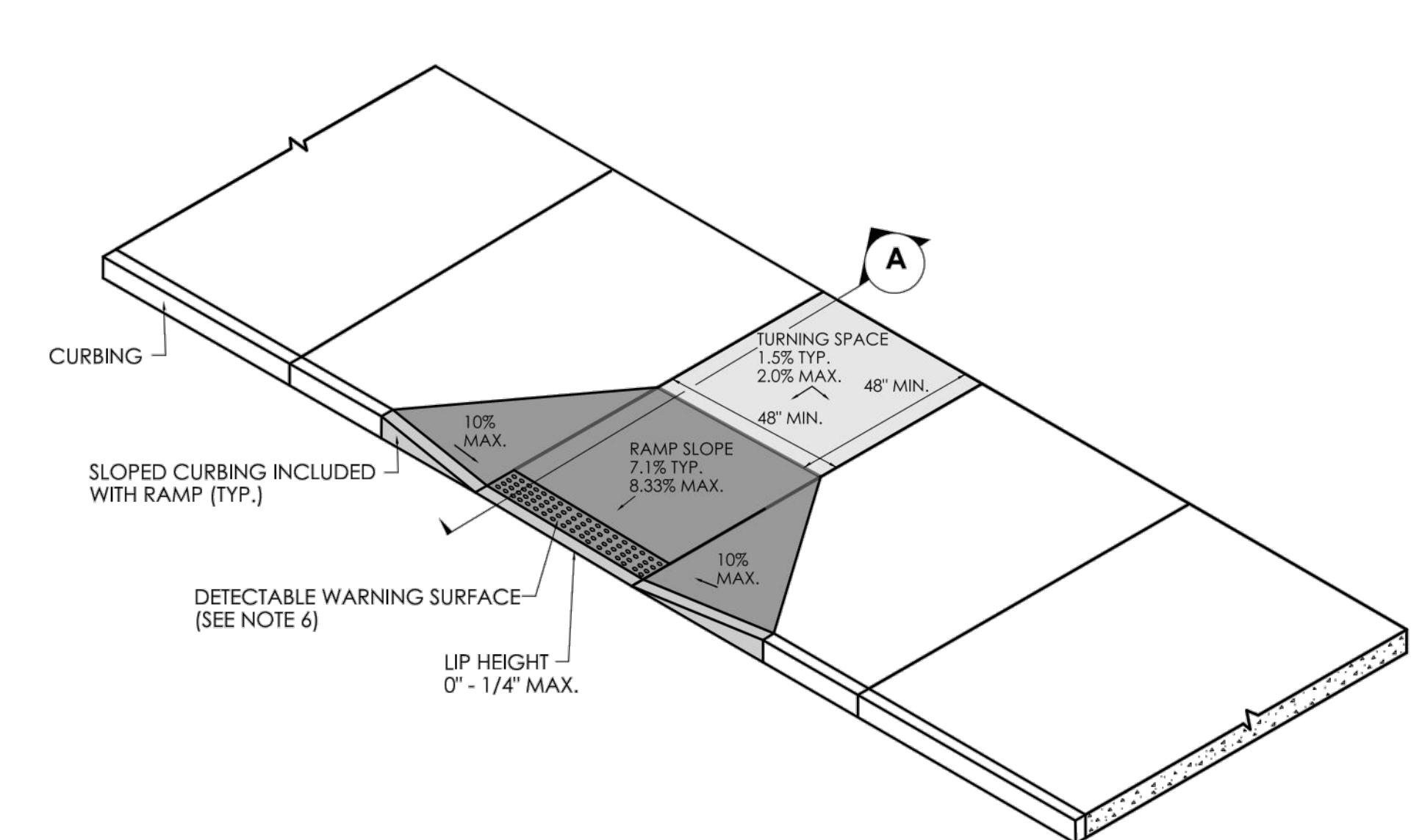
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DWG. NO:  
JOB. NO: 83873.00

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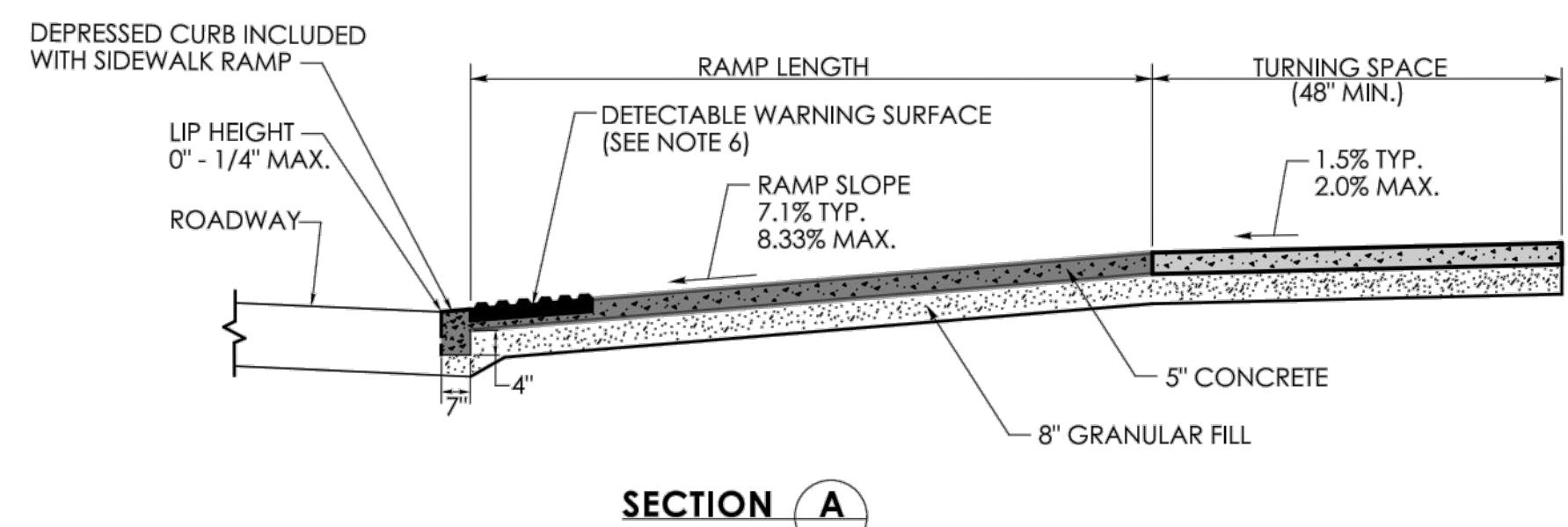
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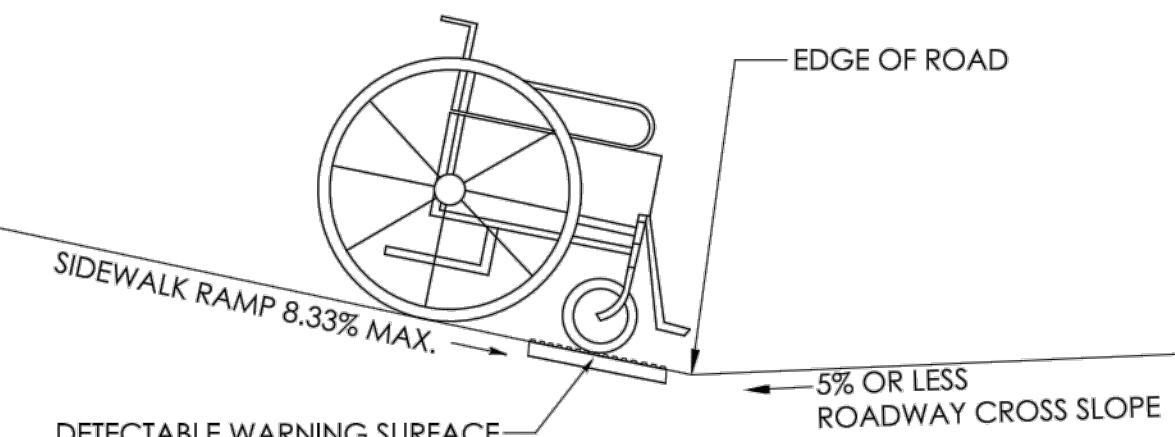




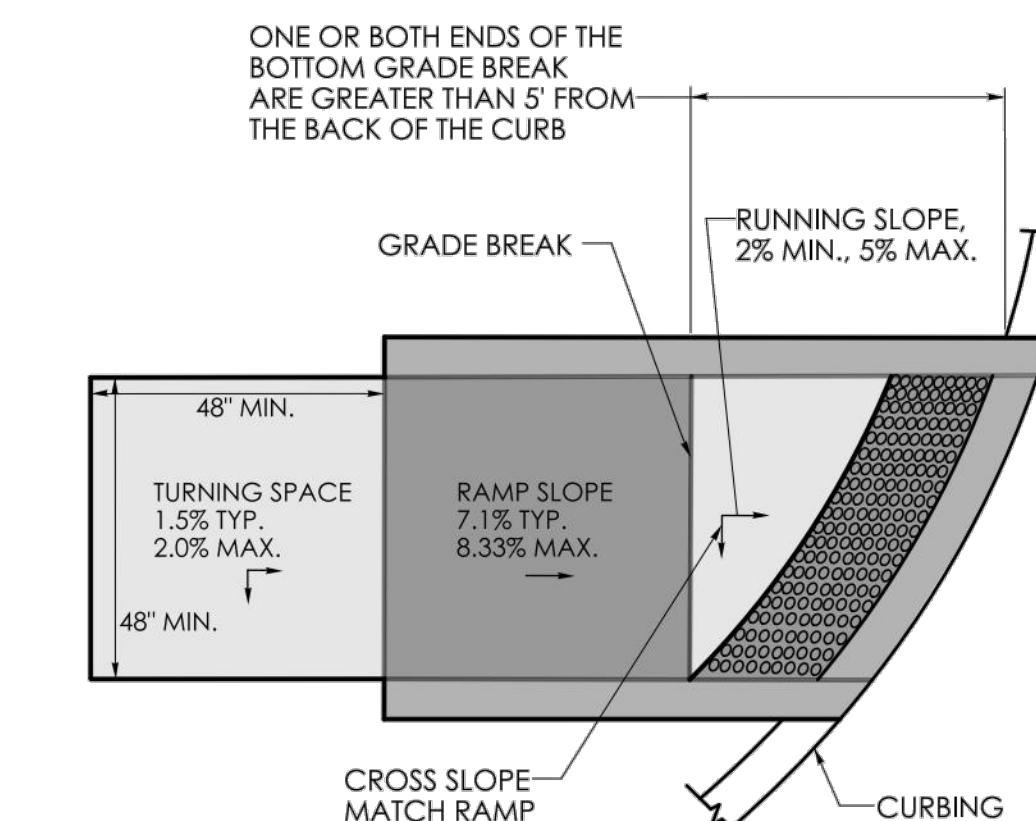
PERPENDICULAR SIDEWALK RAMP  
AND SECTION



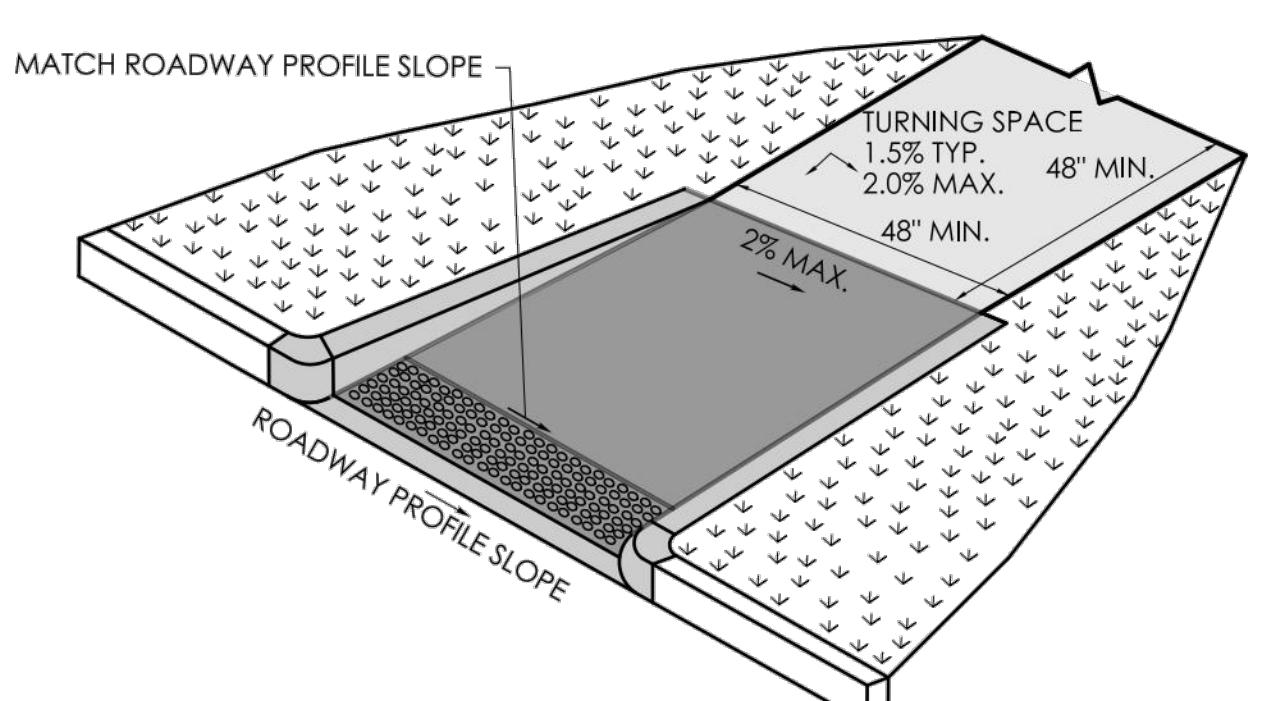
SECTION A



SIDEWALK RAMP GRADE AT  
ROADWAY CROSS SLOPE OF 5% OR LESS  
GUTTER COUNTER SLOPE

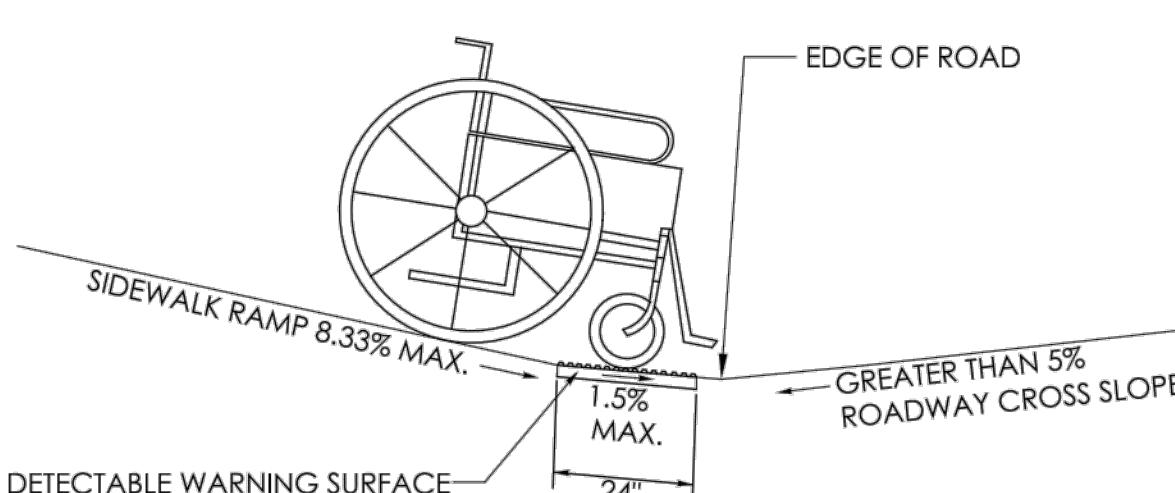


GRADE BREAK GREATER THAN 5'  
DETECTABLE WARNING SURFACE LOCATION

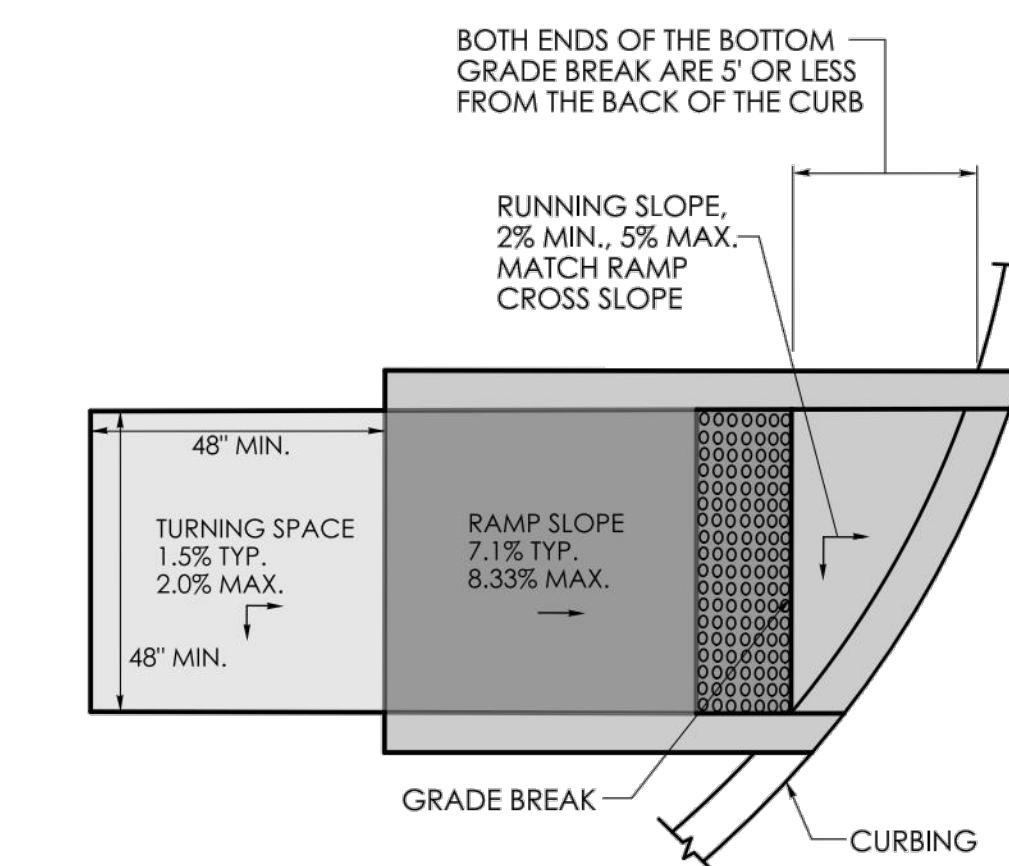


RAMP WARPING DETAIL

- TRANSITION SIDEWALK RAMP TO MATCH ROADWAY PROFILE AS GRADUALLY AS POSSIBLE. DO NOT EXCEED 3% PER FOOT CROSS SLOPE RATE OF CHANGE WHEN TRANSITIONING TO ROADWAY PROFILE.
- COMPLETE TRANSITION TO THE ROADWAY PROFILE BEHIND THE DETECTABLE WARNING SURFACE.



SIDEWALK RAMP GRADE AT  
ROADWAY CROSS SLOPE OF GREATER THAN 5%  
GUTTER COUNTER SLOPE



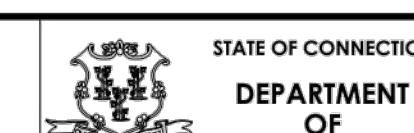
GRADE BREAK OF 5' OR LESS  
DETECTABLE WARNING SURFACE LOCATION

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PLOTTED DATE: 1/12/2024

SIGNATURE/  
BLOCK:  
OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

APPROVED BY:



PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 1 OF 11

DRAWING NO.  
SHEET NO.

GENERAL NOTES:

- SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
- VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED  $\frac{1}{4}$  INCH.
- REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
- THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
- DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
- DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
- ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
- CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
- CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

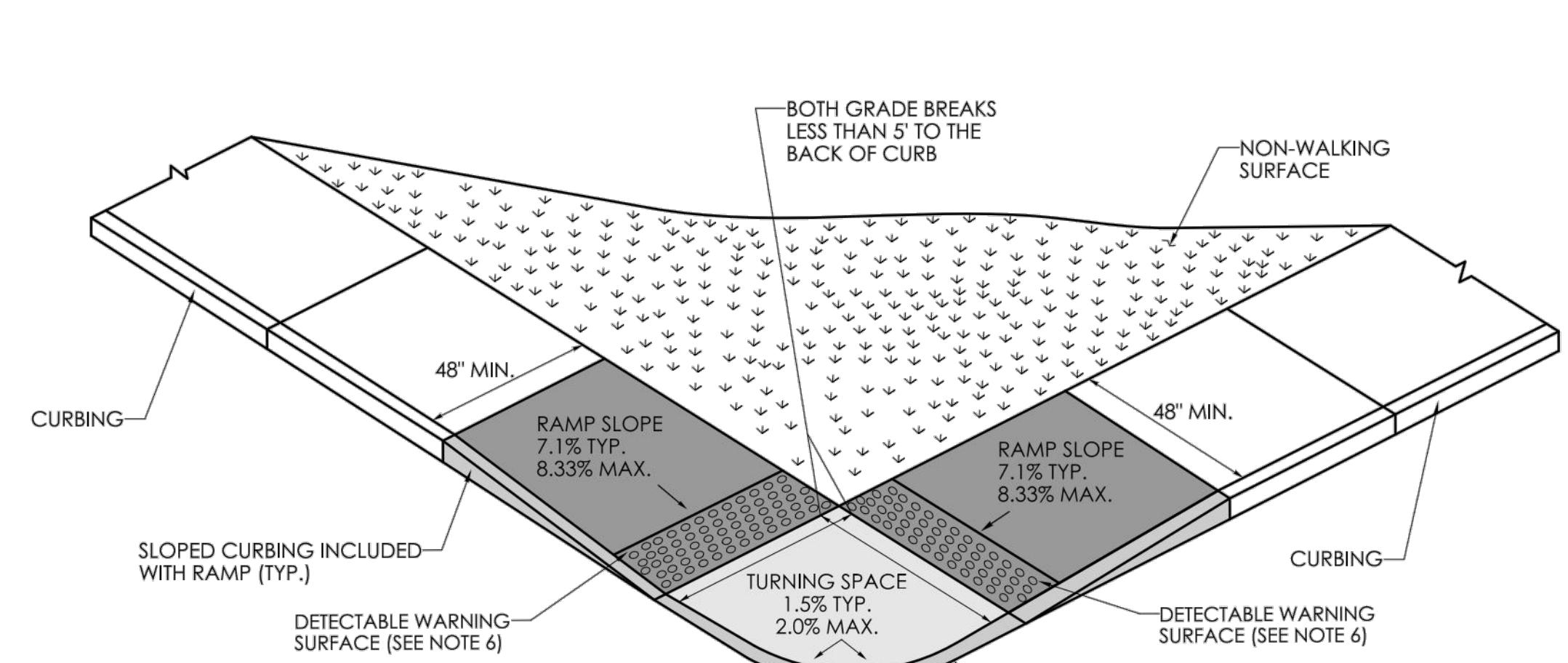
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665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

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SCALE: AS SHOWN

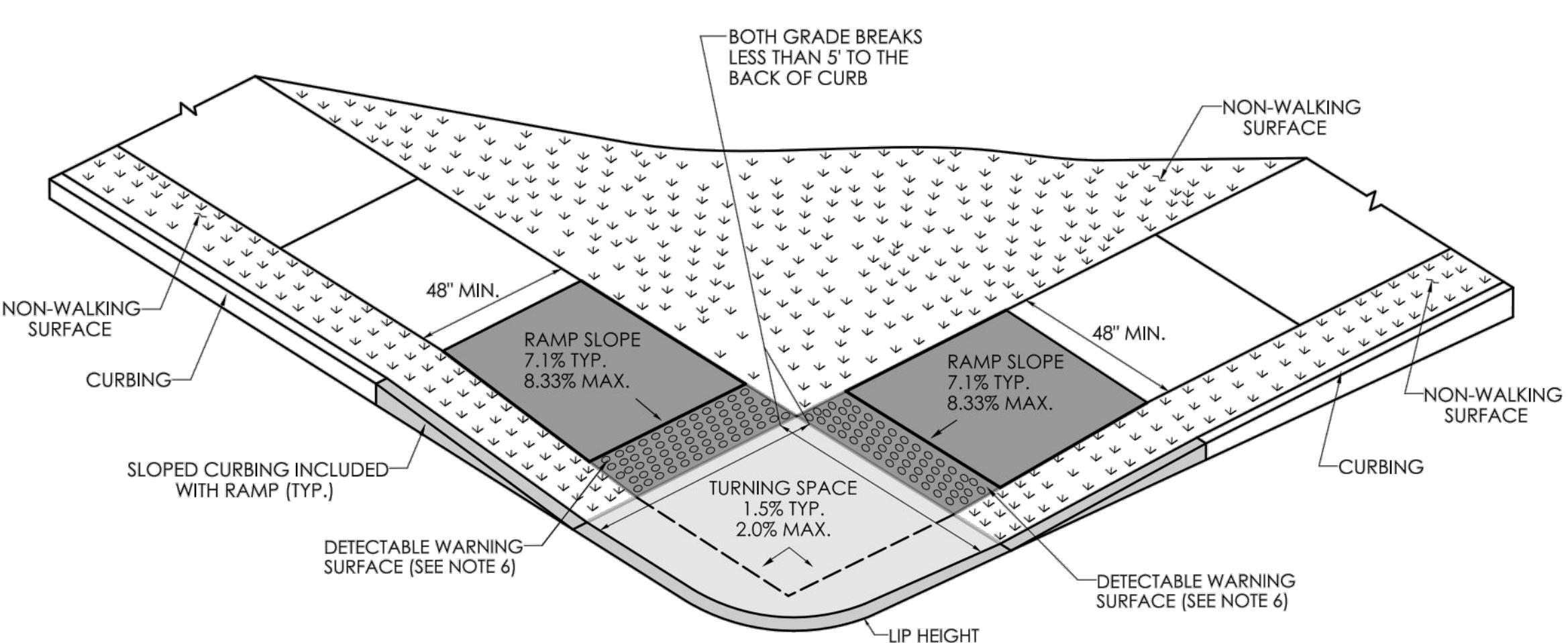
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DWG. NO:  
JOB. NO: 83873.00

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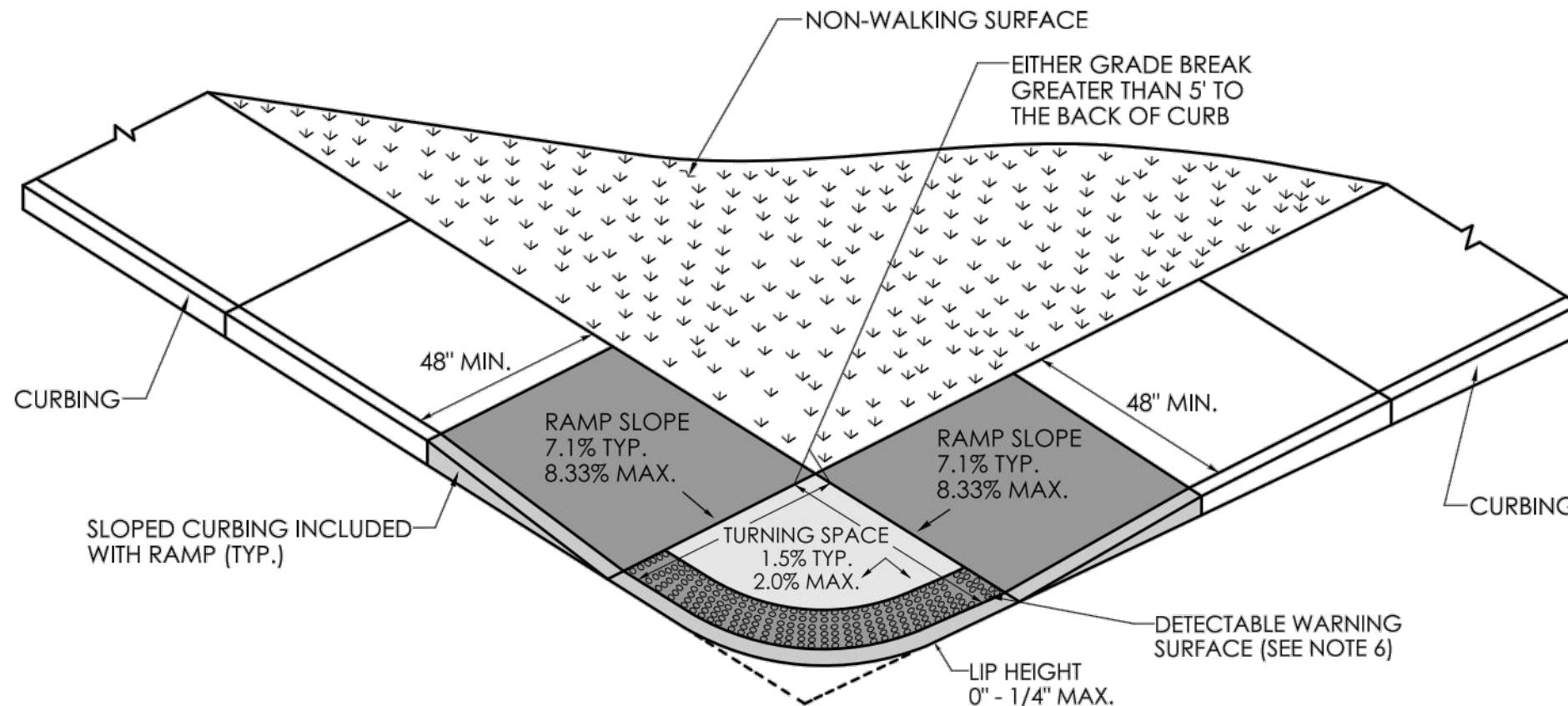
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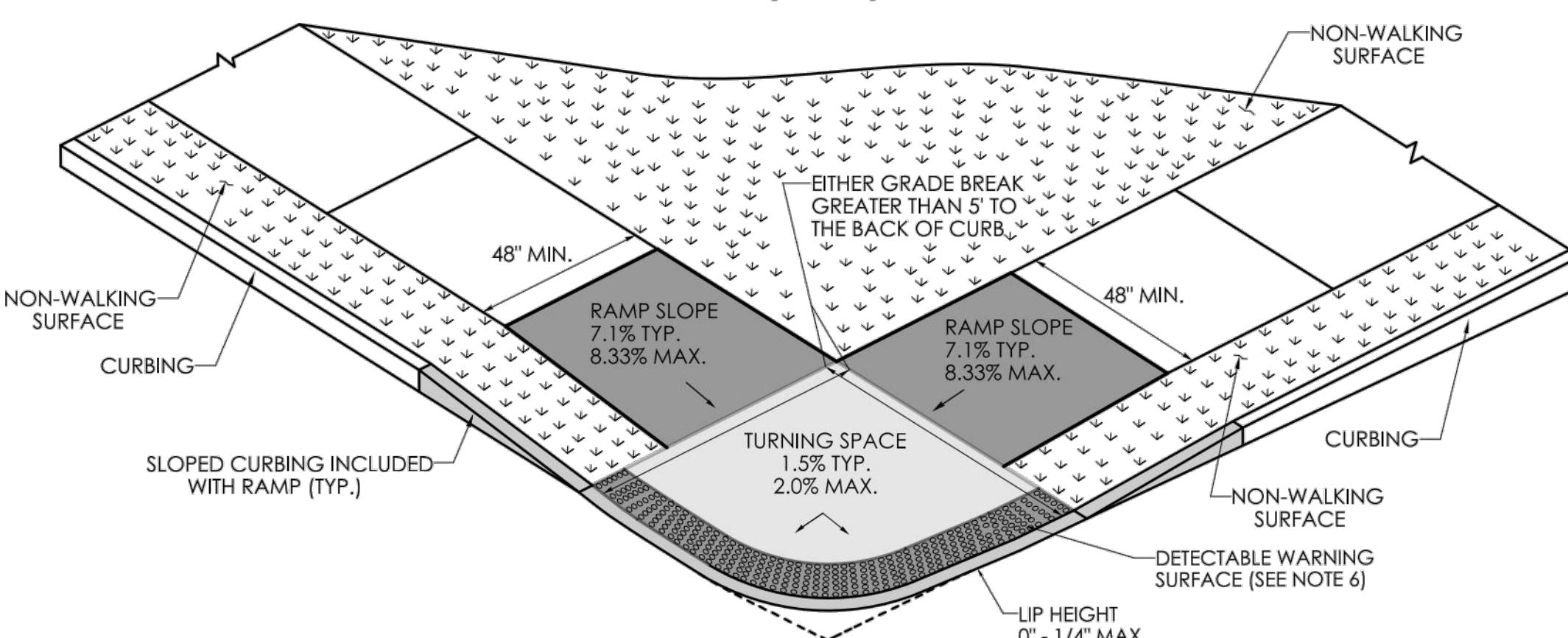
## **TWO DIRECTIONAL PERPENDICULAR RAMP WITH A GRADE BREAK TO BACK OF CURB OF 5' OR LESS (TYPE 1)**



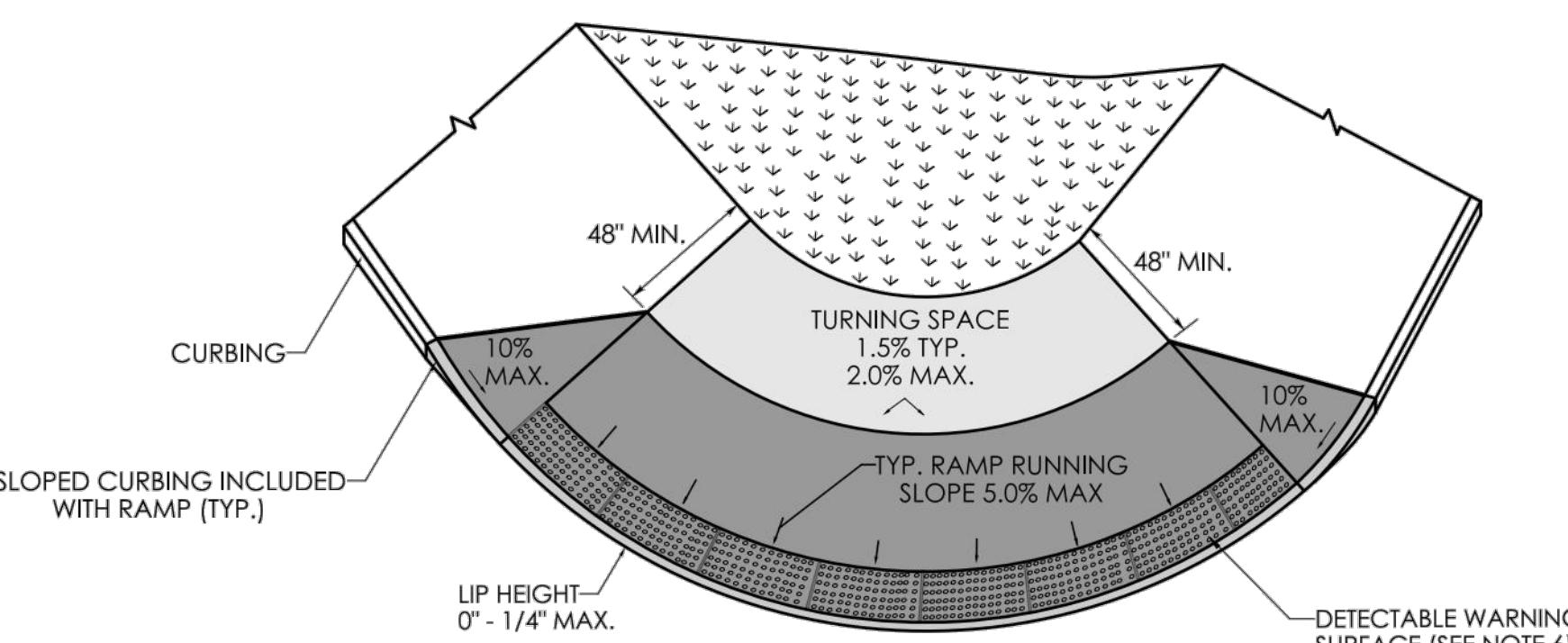
**TWO DIRECTIONAL PERPENDICULAR RAMPS  
WITH A GRADE BREAK  
TO BACK OF CURB OF 5' OR LESS  
WITH NON-WALKING SURFACE  
(TYPE 3)**



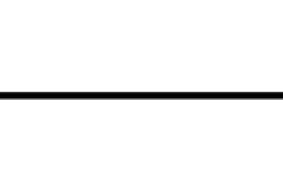
## **TWO DIRECTIONAL PERPENDICULAR RAMP WITH GRADE BREAK TO BACK OF CURB GREATER THAN 5' (TYPE 2)**



**TWO DIRECTIONAL PERPENDICULAR RAMP  
WITH A GRADE BREAK  
TO BACK OF CURB GREATER THAN 5'  
WITH NON-WALKING SURFACE  
(TYPE 4)**



## BLENDED TRANSITION WITH TURNING SPACE AT TOP (TYPE 5)

REVISION DESCRIPTION		0" - 1/4" MAX.		DETECTABLE WARNING SURFACE (SENTRY 6)	
<p><b>BLENDED TRANSITION WITH TURNING SPACE AT TOP (TYPE 5)</b></p>					
REV.	DATE	DESIGNER/DRAFTER:	CHECKED BY:	SIGNATURE/ BLOCK:	APPROVED BY:
				<p>OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111</p> <p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p> 	
<p>PROJECT NUMBER: 0000-0000 PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION TOWN(S): SAMPLE TOWN(S) DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 2 OF 11</p>					
				DRAWING NO.	
				SHET NO.	

# MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
**STRATFORD  
CONNECTICUT**

# STATE GUIDE SHEET DETAILS

OCTOBER 2024

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

# BSC GROUP

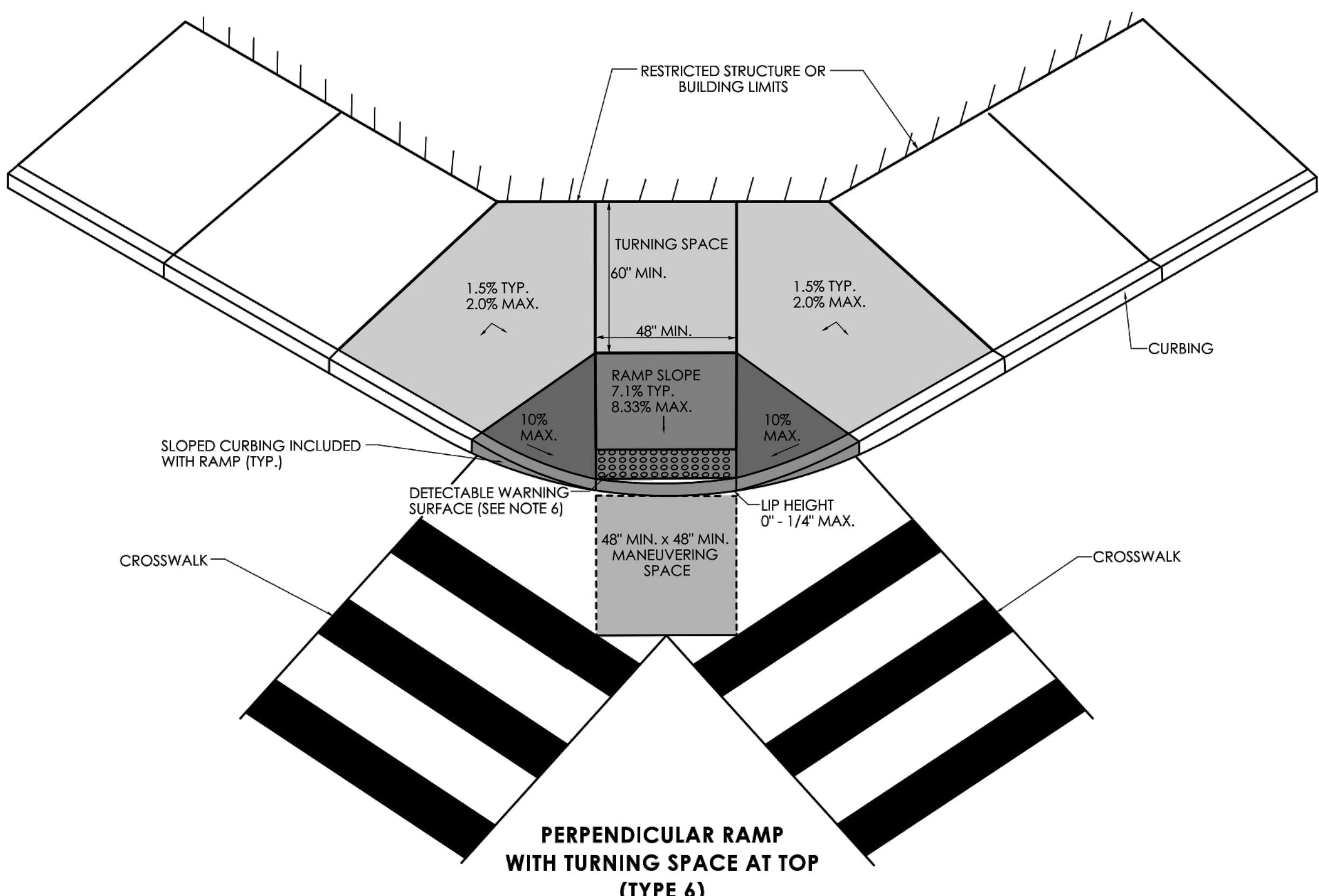
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Glastonbury, Connecticut  
06033

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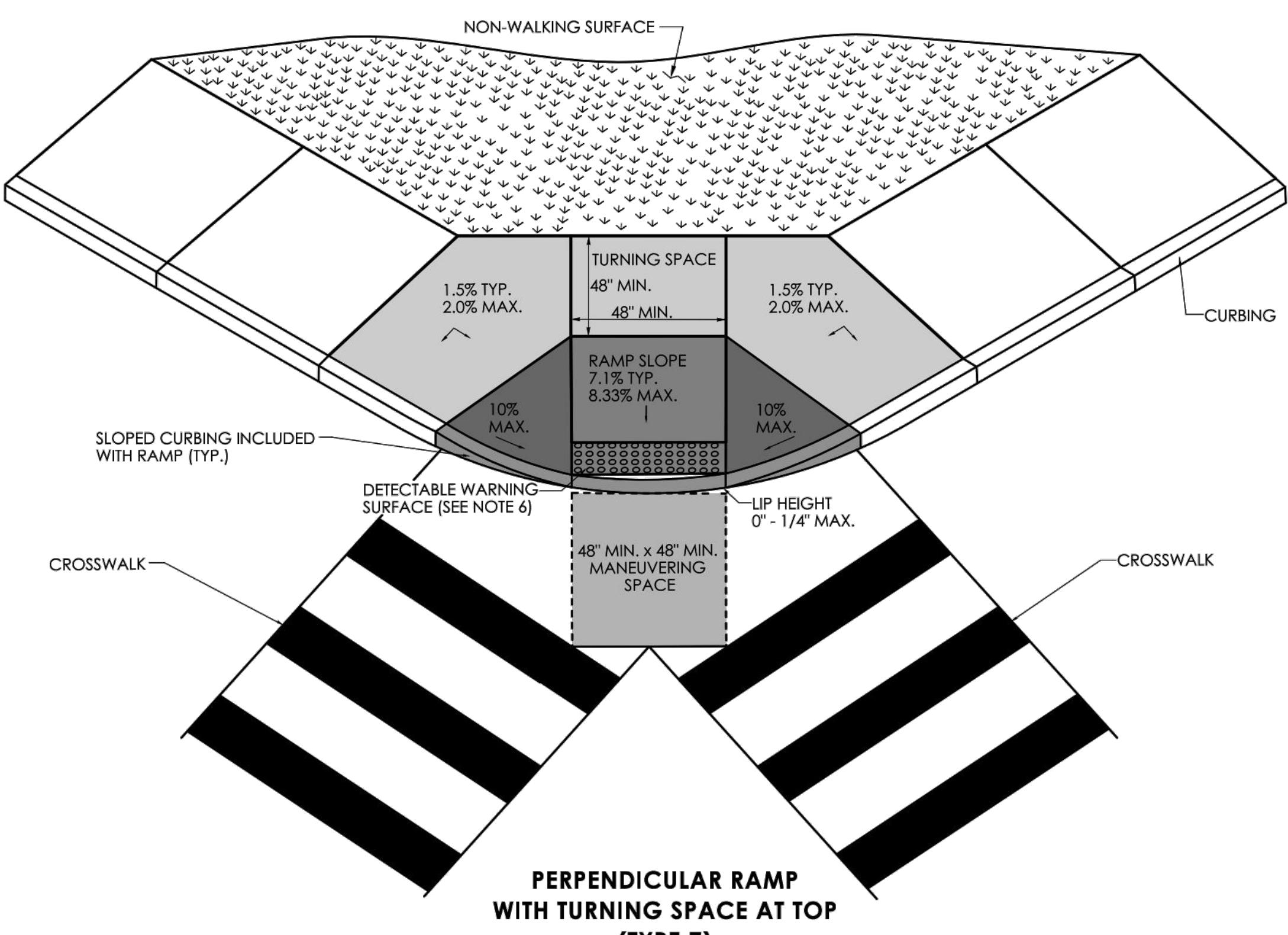
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DWG. NO: \_\_\_\_\_ GS-  
JOB. NO: 83873.00

PROGRESS PRINT - 60% DESIGN



## **GENERAL NOTES:**

1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED  $\frac{1}{4}$  INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



## PERPENDICULAR RAMP WITH TURNING SPACE AT TOP (TYPE 5)

REVISION DESCRIPTION		PERPENDICULAR RAMP WITH TURNING SPACE AT TOP (TYPE 7)		DRAWING NO.	
REV.	DATE			SHEET NO.	
DESIGNER/DRAFTER:		APPROVED BY:		PROJECT NUMBER: 0000-0000	
CHECKED BY:		SIGNATURE/		PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION	
		BLOCK:		TOWN(S): SAMPLE TOWN(S)	
		OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111		DRAWING TITLE: CONCRETE SIDEWALK RAMP SHEET 3 OF 11	
					

# MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

# STATE GUIDE SHEET DETAILS

OCTOBER 2024

PREPARED FOR:  
METROCOG  
10 LAFAYETTE BOULEVARD  
RIDGEBORO, CT 06664

# BSC GROUP

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665 Winding Brook Drive  
Glastonbury, Connecticut



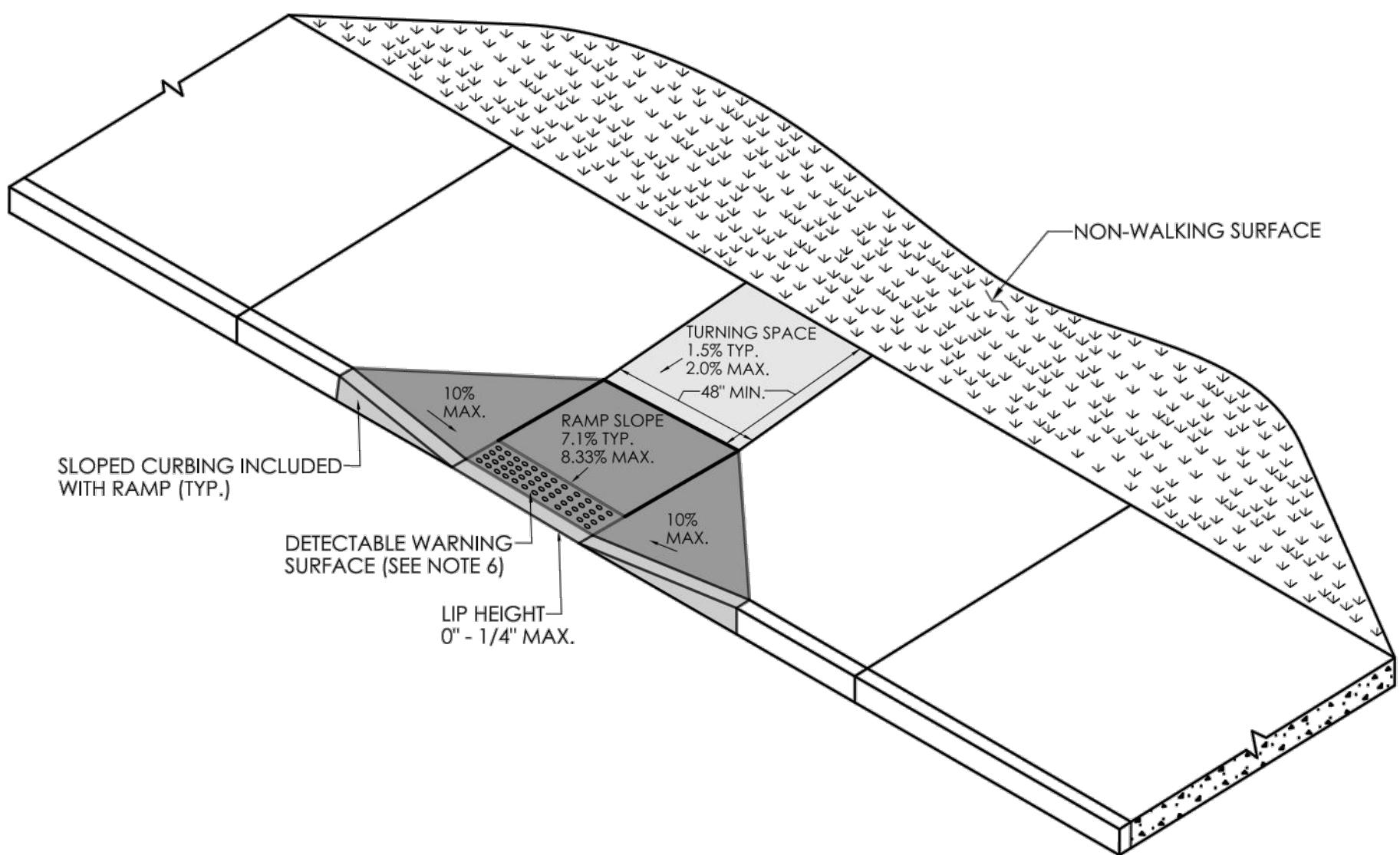
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# PROGRESS PRINT - 60% DESIGN

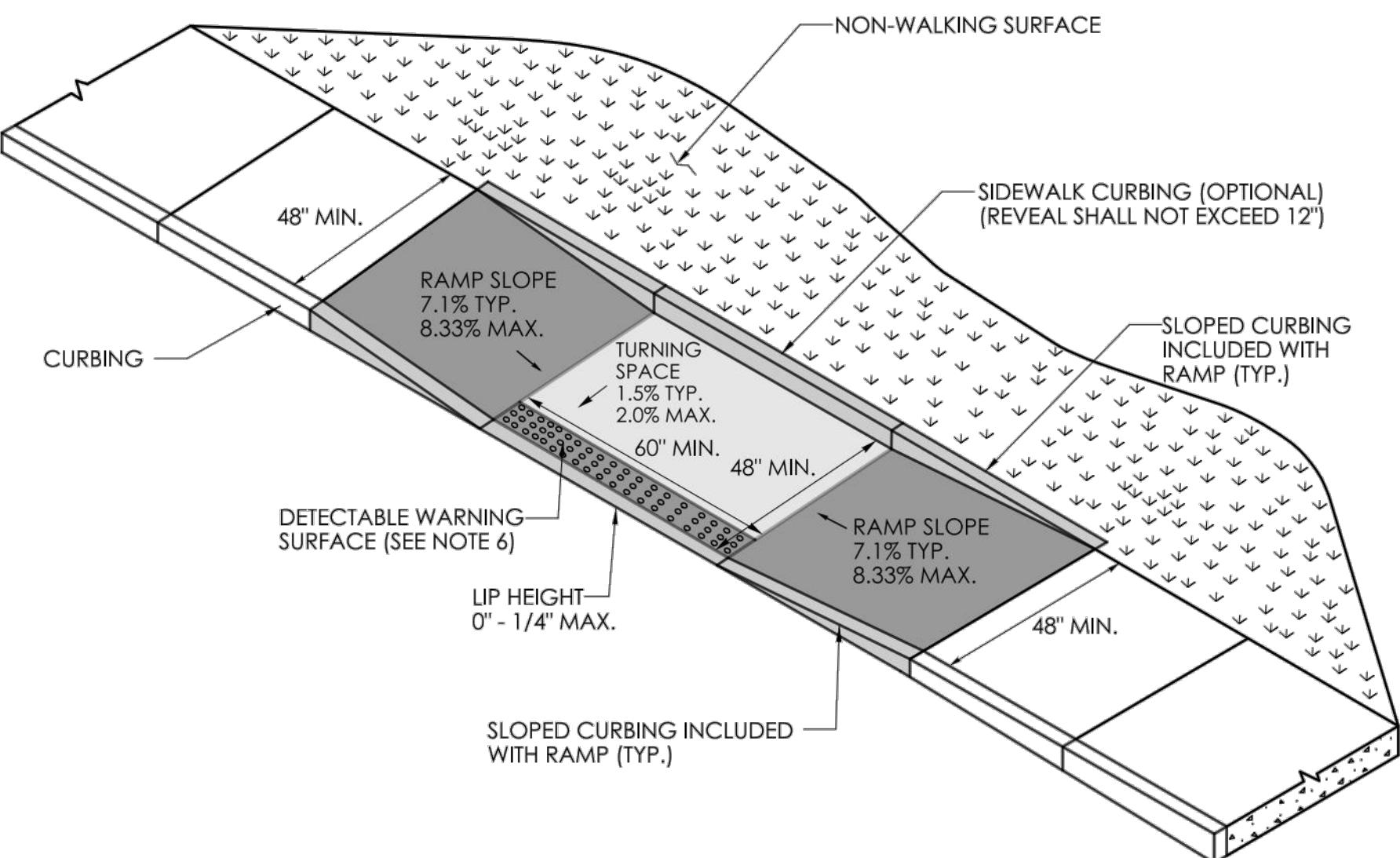
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JOB. NO: 83873.00

### GENERAL NOTES:

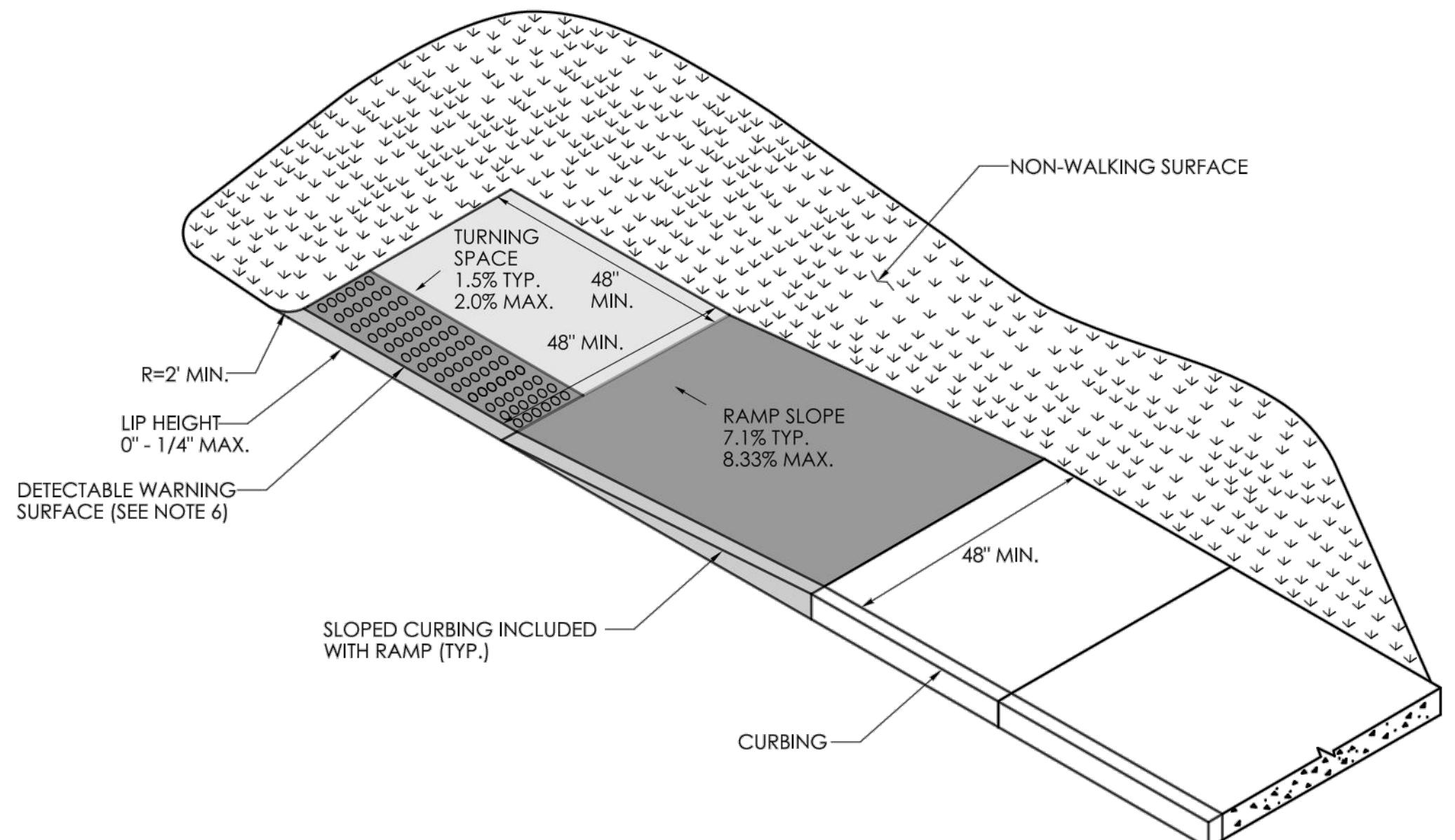
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED  $\frac{1}{4}$  INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CT DOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



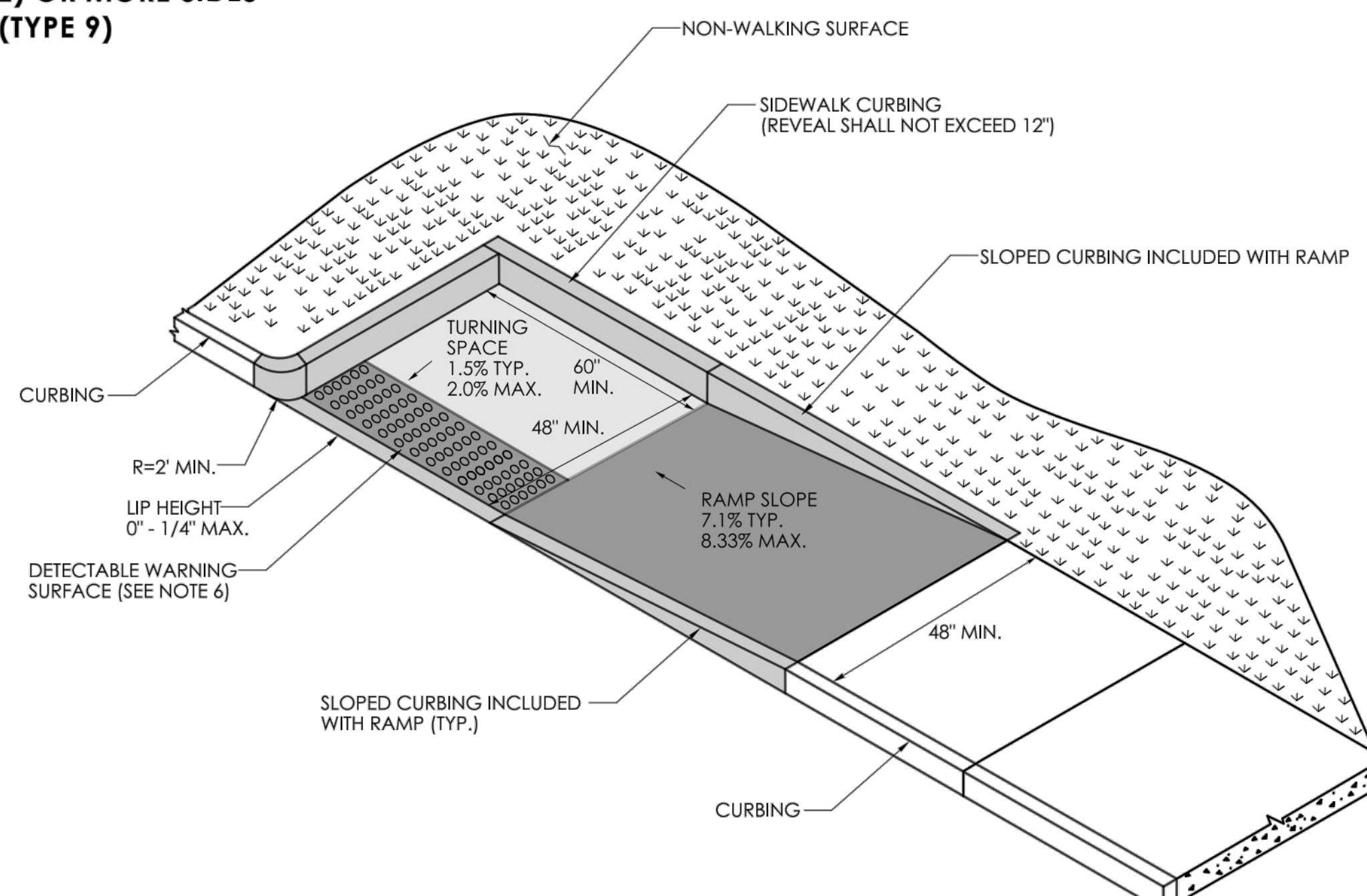
PERPENDICULAR RAMP  
WITH 48" BY-PASS  
(TYPE 8)



PARALLEL RAMP WITH  
TURNING SPACE CONSTRAINED  
ON TWO (2) OR MORE SIDES  
(TYPE 9)



PARALLEL RAMP  
WITH TURNING SPACE  
CONSTRAINED ON ONE SIDE  
(TYPE 10a)

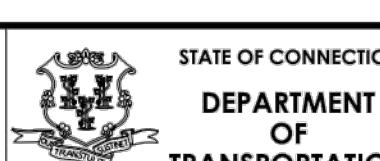


PARALLEL RAMP  
WITH TURNING SPACE  
CONSTRAINED ON TWO (2) OR MORE SIDES  
(TYPE 10b)

REV.	REVISION DESCRIPTION

DESIGNER/DRAFTER: CHECKED BY: LASTED SAVED BY: RichardEH FILE NAME: W:\CT\_CONNECT\_DDE\CT\_Configuration\Organization\Cell\CTDOT\_Borders\_Contract.cel PLOTTED DATE: 1/12/2024

SIGNATURE/ BLOCK:  
OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111



APPROVED BY:

STATE OF CONNECTICUT  
DEPARTMENT OF  
TRANSPORTATION



PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 4 OF 11

DRAWING NO.  
SHEET NO.

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

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SCALE: AS SHOWN

FILE: 8387300\CIVIL\DRAWINGS  
DWG. NO:  
JOB. NO: 83873.00

GS-07

MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PROGRESS PRINT - 60% DESIGN

MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

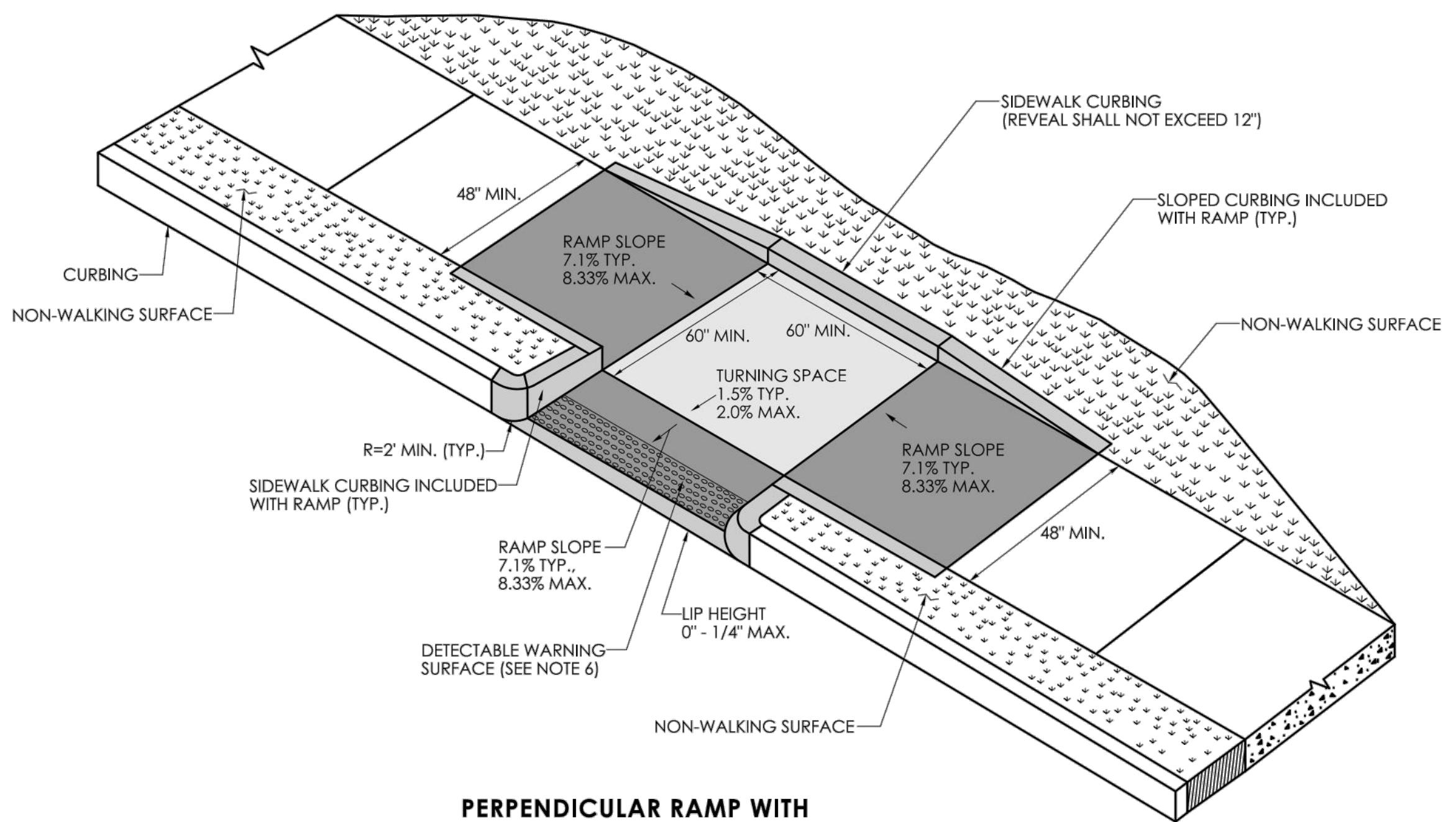
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SCALE: AS SHOWN

FILE: 8387300\CIVIL\\_DRAWINGS  
DWG. NO:    
JOB. NO: 83873.00

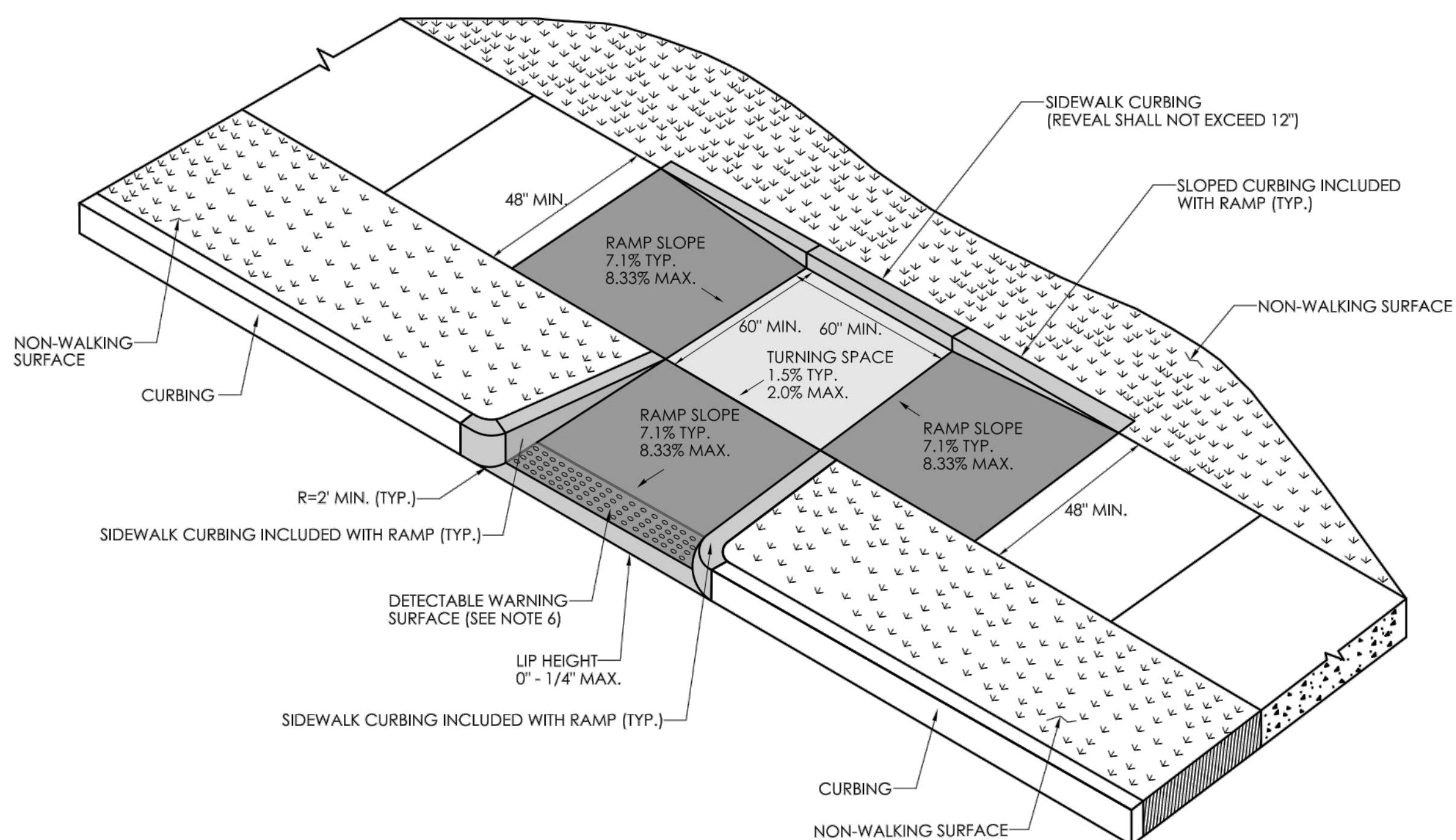
GS-08

**GENERAL NOTES:**

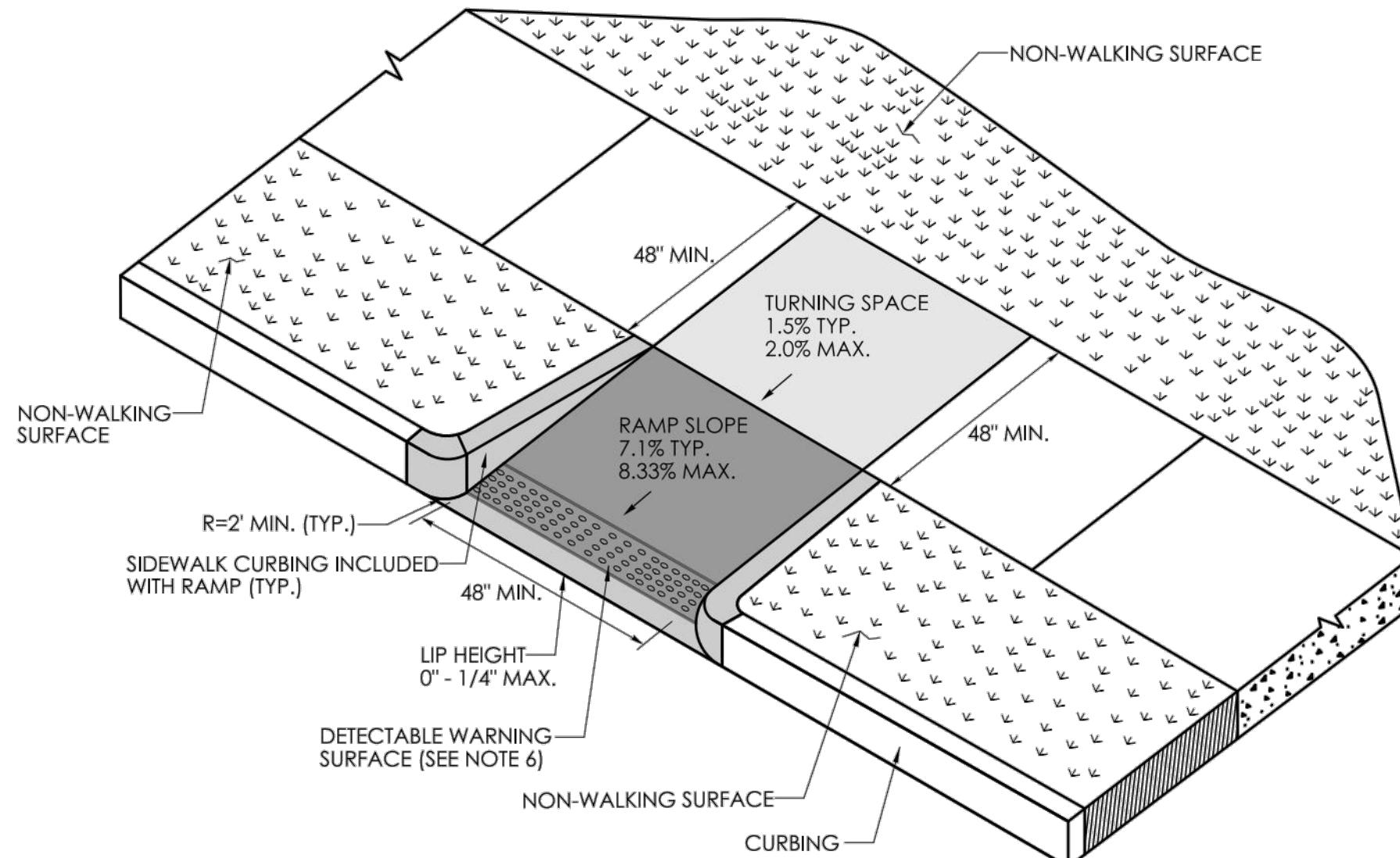
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED  $\frac{1}{4}$  INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
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6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
7. ALL GUIDE SHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CT DOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



PERPENDICULAR RAMP WITH  
NON-WALKING SURFACE  
(TYPE 11)



PERPENDICULAR RAMP WITH  
SIDEWALK CURB  
AND NON-WALKING SURFACE  
(TYPE 12)



PERPENDICULAR RAMP  
WITH NON-WALKING SURFACE  
(TYPE 13)

REV.	DATE	REVISION DESCRIPTION

DESIGNER/DRAFTER:   CHECKED BY:    
LASTED SAVED BY: RichardEH FILE NAME: W:\CT\_CONNECT\_DDE\CT\_Configuration\Organization\Cell\CTDOT\_Borders\_Contract.cel  
PLOTTED DATE: 1/12/2024

SIGNATURE/  
BLOCK:

OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

APPROVED BY:



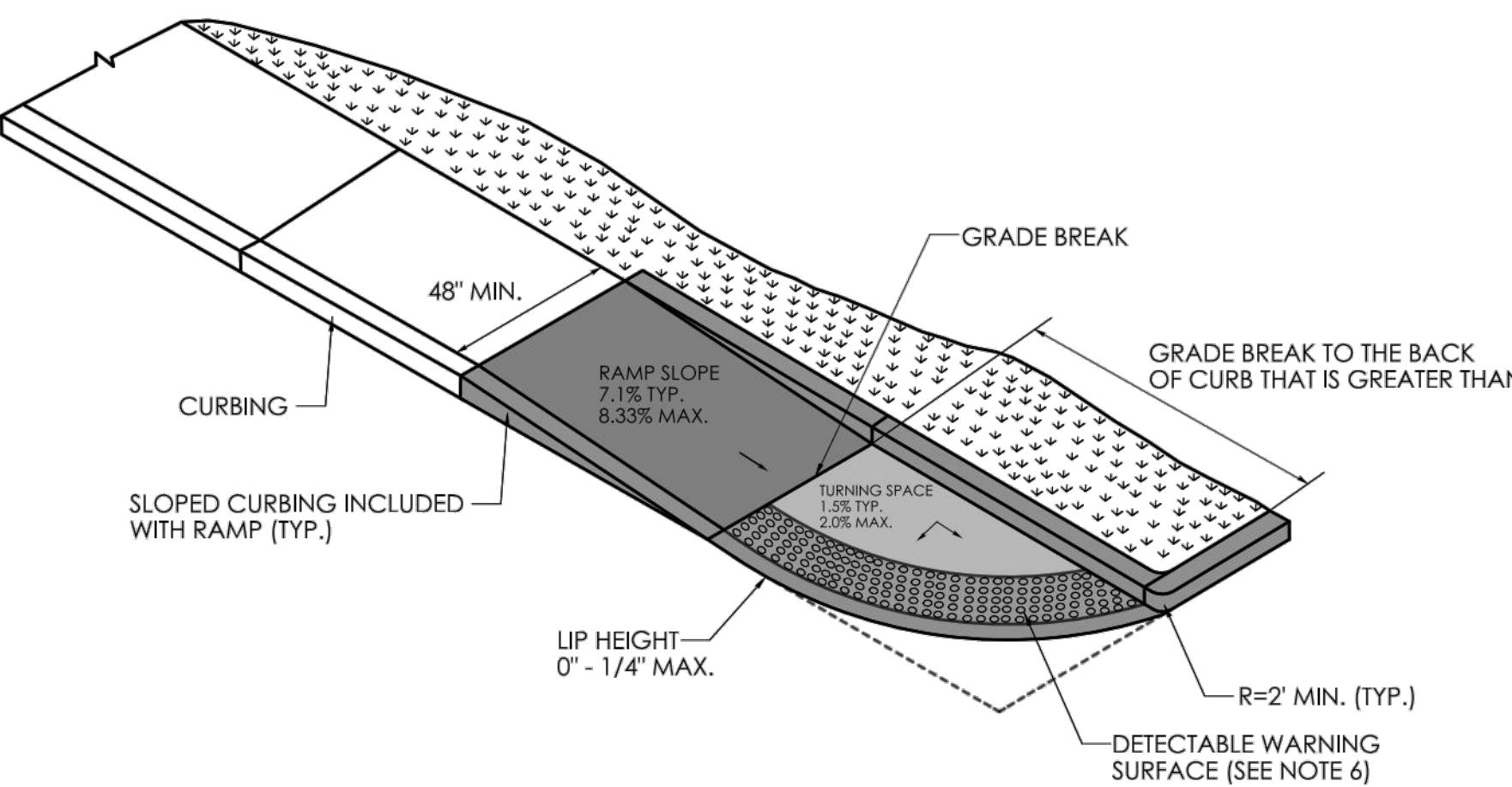
PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 5 OF 11

FILE: 8387300\CIVIL\\_DRAWINGS  
DWG. NO:    
JOB. NO: 83873.00

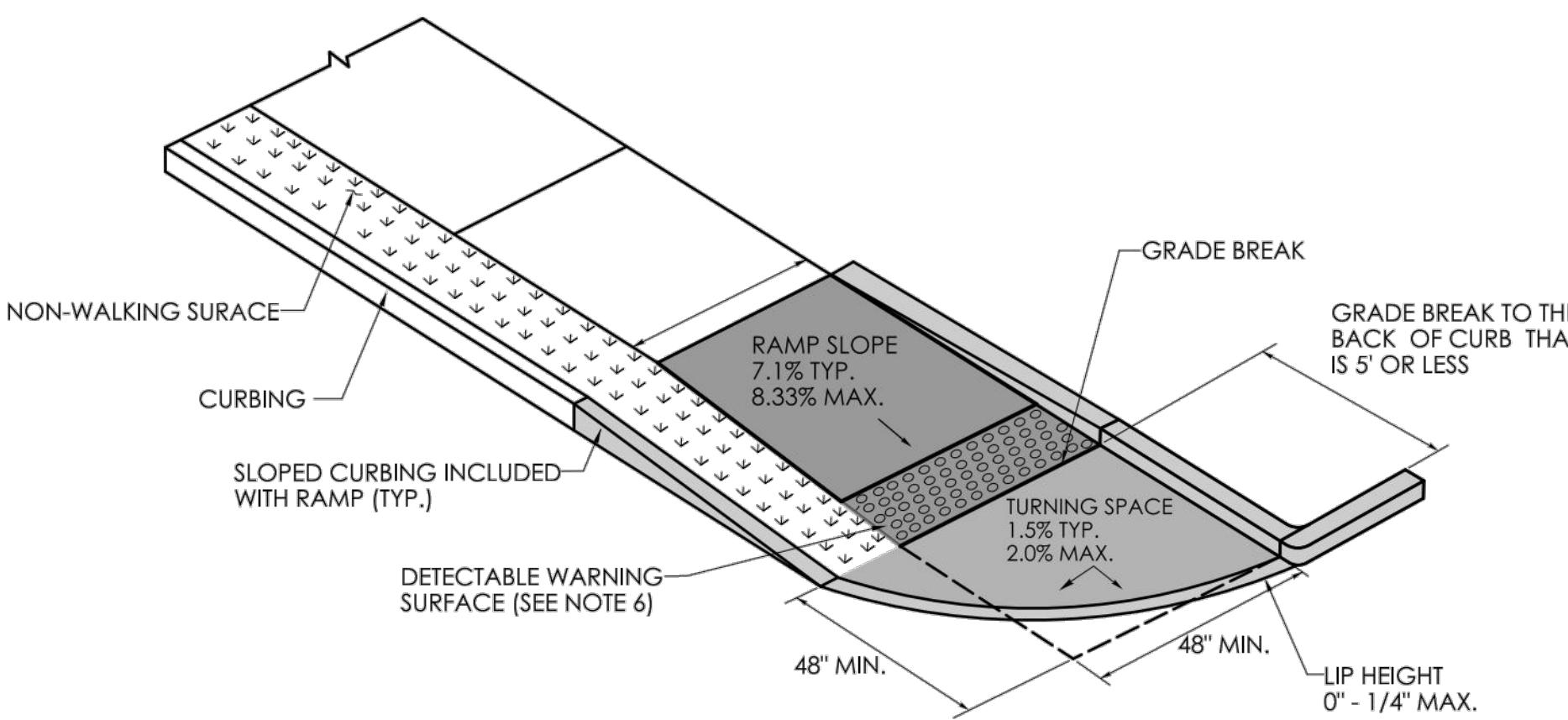
PROGRESS PRINT - 60% DESIGN

**GENERAL NOTES:**

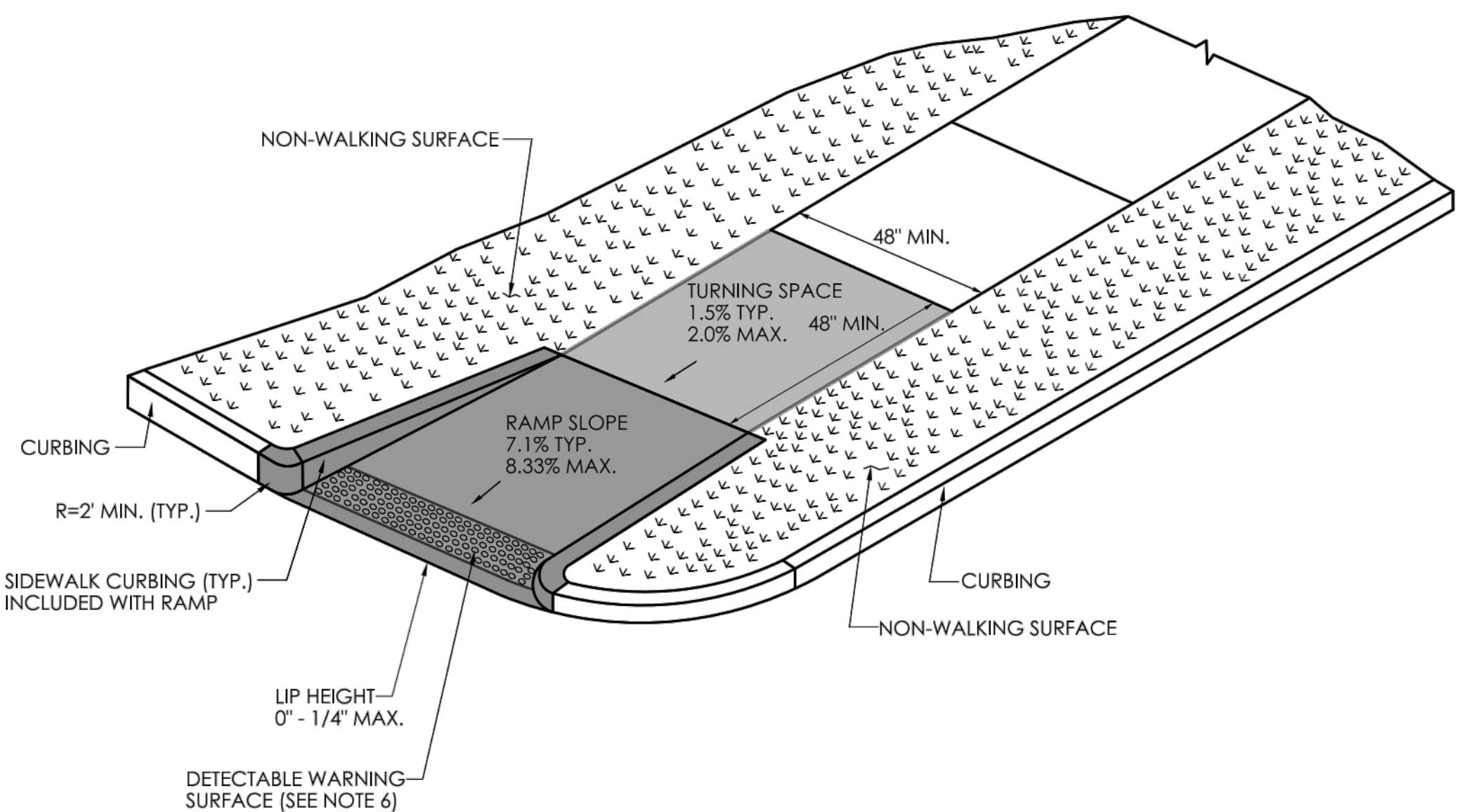
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED  $\frac{1}{4}$  INCH.
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7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CT DOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-07.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



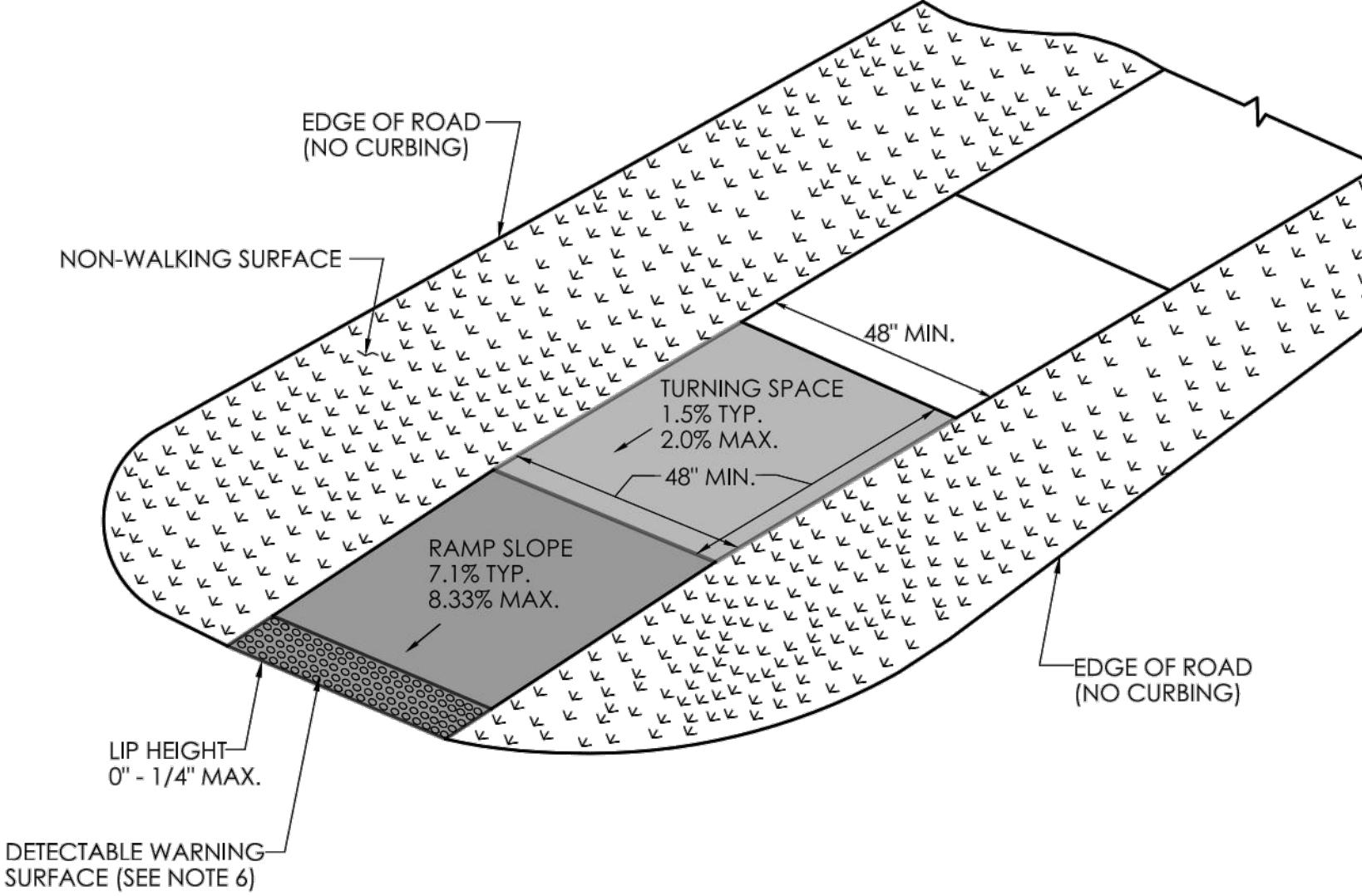
**SINGLE DIRECTION PERPENDICULAR RAMP  
GRADE BREAK TO THE BACK OF CURB  
GREATER THAN 5'  
(TYPE 14)**



**SINGLE DIRECTION PERPENDICULAR RAMP  
WITH GRADE BREAK TO THE BACK  
OF CURB THAT IS 5' OR LESS  
(TYPE 15)**



**SINGLE DIRECTION PERPENDICULAR RAMP  
RETURN CURB WITH NON-WALKING SURFACE  
(TYPE 16)**



**SINGLE DIRECTION PERPENDICULAR RAMP  
NO CURB WITH NON-WALKING SURFACE  
(TYPE 17)**

REV.	DATE	REVISION DESCRIPTION

LASTED SAVED BY: RichardEH FILE NAME: W:\CT\_CONNECT\_DDE\CT\_Configuration\Organization\Cell\CTDOT\_Borders\_Contract.cel  
PLOTTED DATE: 1/12/2024

SIGNATURE/  
BLOCK:  
OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

APPROVED BY:



PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 6 OF 11

DRAWING NO.  
SHEET NO.

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

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SCALE: AS SHOWN

FILE: 8387300\CIVIL\\_DRAWINGS  
DWG. NO: GS-09  
JOB. NO: 83873.00

PROGRESS PRINT - 60% DESIGN

**MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2**

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

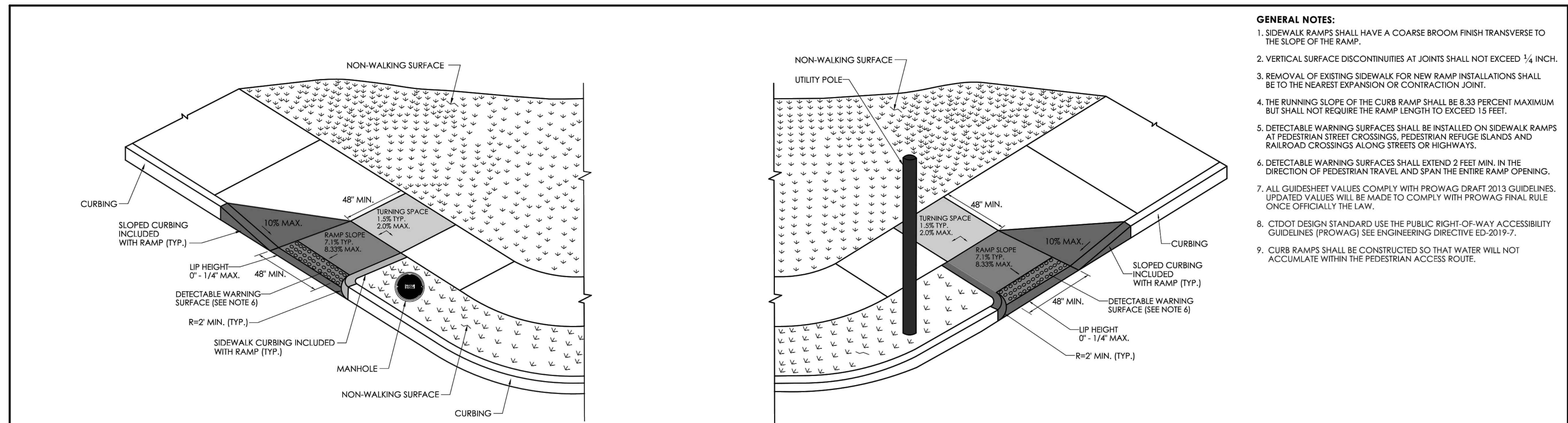
REVISIONS:

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

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BUILD | SUPPORT | CONNECT  
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Glastonbury, Connecticut  
06033  
860 652 8227

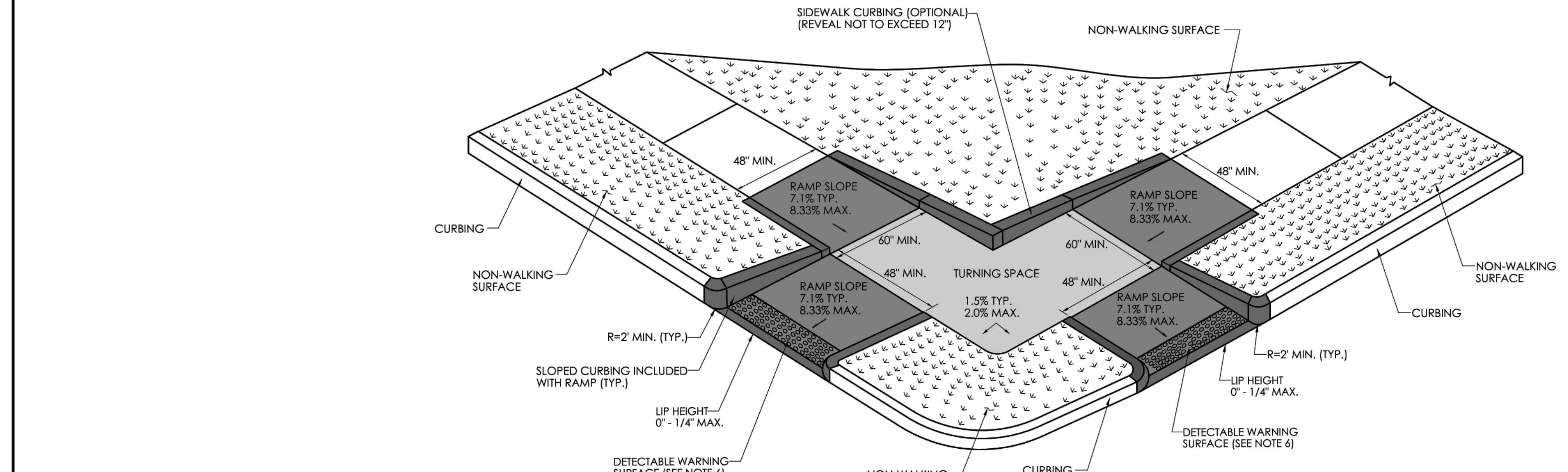
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SCALE: AS SHOWN

FILE: 8387300\CIVIL\\_DRAWINGS  
DWG. NO: GS-09  
JOB. NO: 83873.00



PERPENDICULAR RAMP  
WITH NON-WALKING SURFACE  
(TYPE 18 LEFT)

PERPENDICULAR RAMP  
WITH NON-WALKING SURFACE  
(TYPE 18 RIGHT)



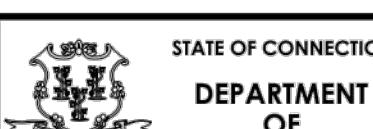
PERPENDICULAR RAMP  
WITH NON-WALKING SURFACE  
(TYPE 19)

REV.	DATE	REVISION DESCRIPTION

LASTED SAVED BY: RichardEH FILE NAME: W:\CT\_CONNECT\_DDE\CT\_Configuration\Organization\Cell\CTDOT\_Borders\_Contract.cel  
PLOTTED DATE: 1/12/2024

SIGNATURE/  
BLOCK:

APPROVED BY:



PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 7 OF 11

## MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
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665 Winding Brook Drive  
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06033  
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SCALE: AS SHOWN

FILE: 8387300\CIVIL\\_DRAWINGS  
DWG. NO:    
JOB. NO: 83873.00

PROGRESS PRINT - 60% DESIGN

GS-10

MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

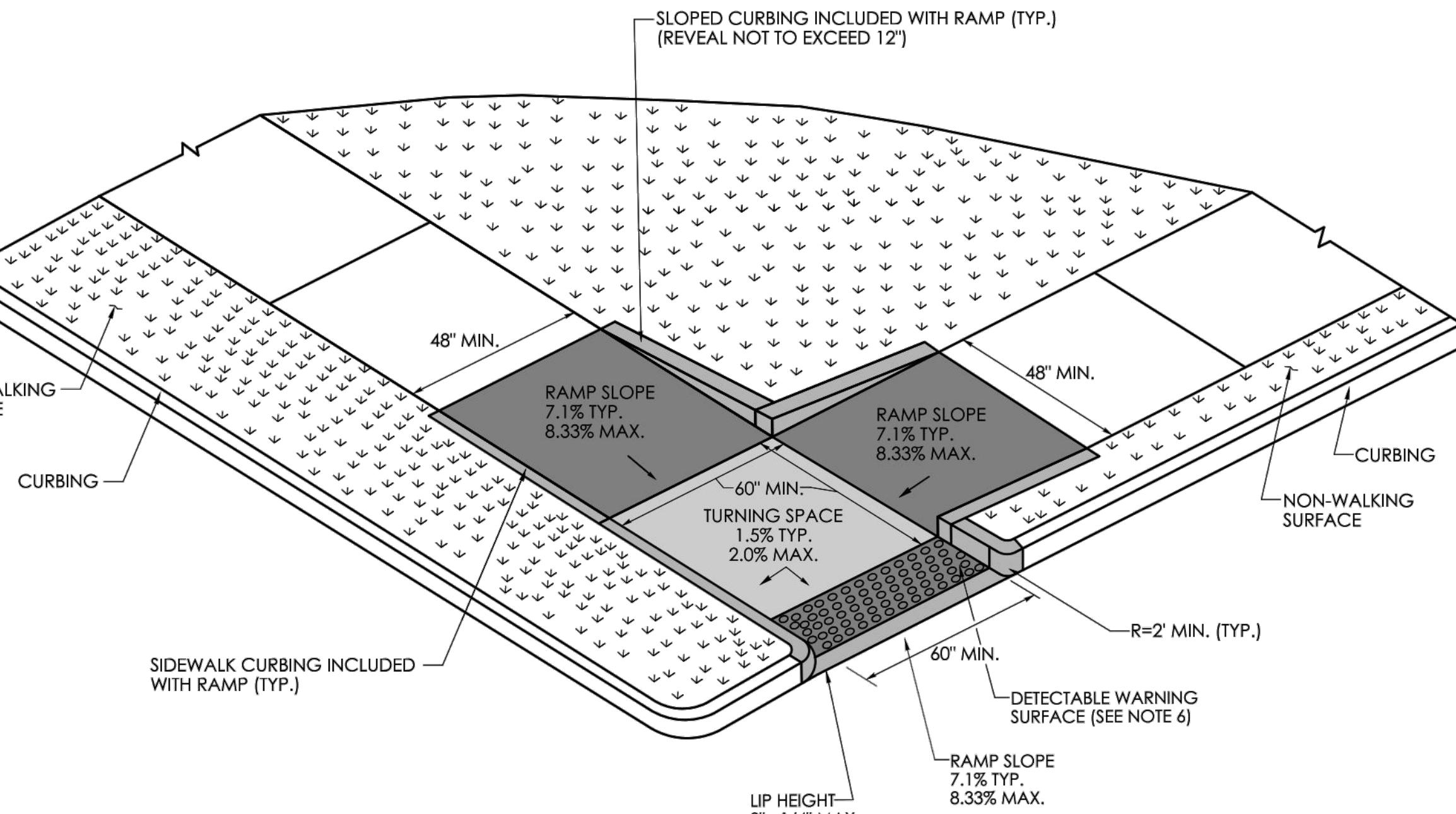
**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

© 2024 BSC GROUP, INC.  
SCALE: AS SHOWN

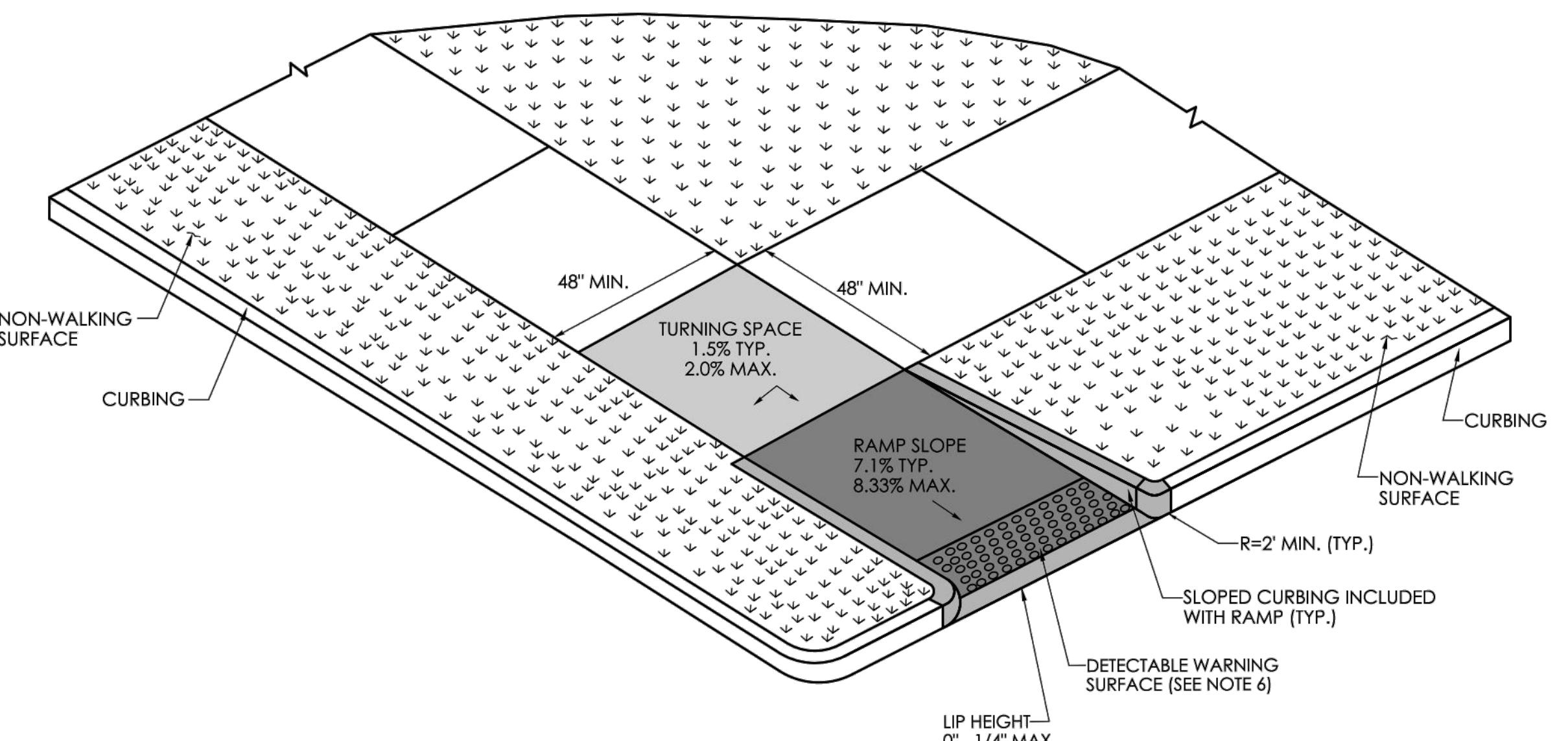
FILE: 8387300\CIVIL\DRAWINGS  
DWG. NO:   GS-11  
JOB. NO: 83873.00

**GENERAL NOTES:**

1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED  $\frac{1}{4}$  INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
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8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



RESTRICTED PEDESTRIAN CROSSING  
WITH LANDING AT BOTTOM AND NON-WALKING SURFACE  
(TYPE 21)



RESTRICTED PEDESTRIAN CROSSING SIDEWALK RAMP  
WITH NON-WALKING SURFACE  
(TYPE 20)

REV.	DATE	REVISION DESCRIPTION



PROJECT NUMBER: 0000-0000  
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION  
TOWN(S): SAMPLE TOWN(S)  
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 8 OF 11

DRAWING NO.

SHEET NO.

DESIGNER/DRAFTER:   CHECKED BY:    
LASTED SAVED BY: RichardSH FILE NAME: W:\CT\_CONNECT\_DDE\CT\_Configuration\Organization\Cell\CTDOT\_Borders\_Contract.cel  
PLOTTED DATE: 1/12/2024

SIGNATURE/  
BLOCK:   APPROVED BY:  

OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

PROGRESS PRINT - 60% DESIGN

MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

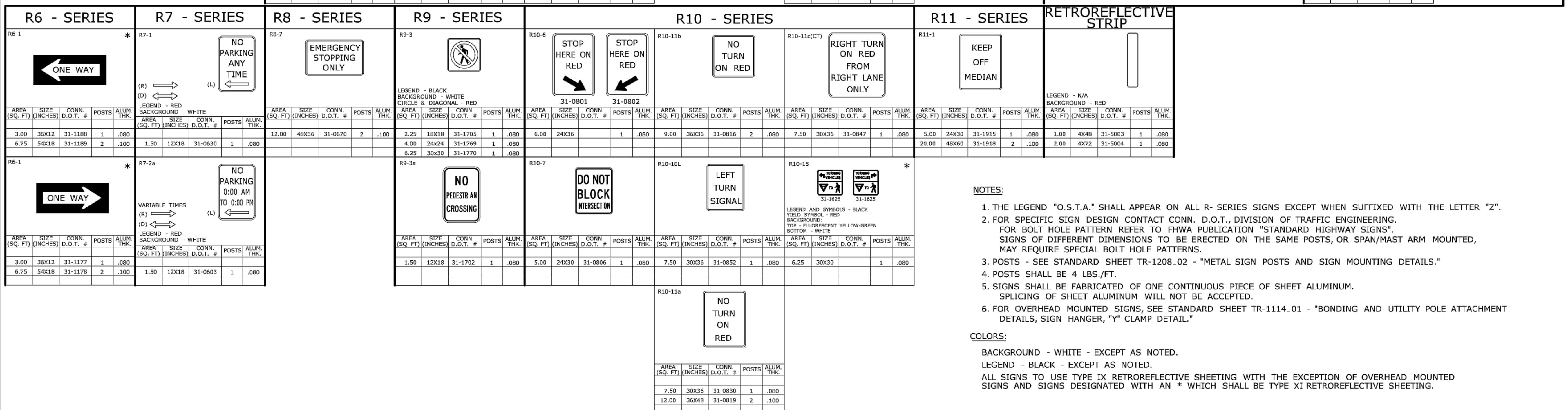
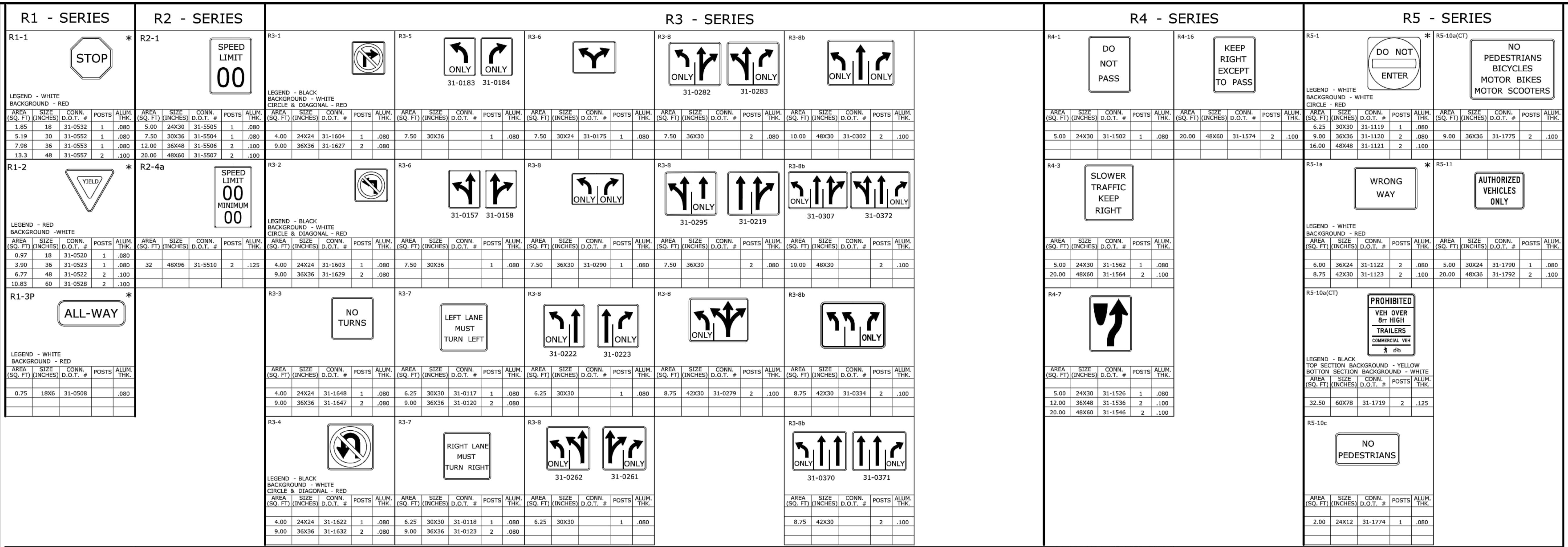
**BSC GROUP**  
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033

860 652 8227

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FILE: 8387300\ CIVIL\ DRAWINGS  
DWG. NO: GS-12  
JOB. NO: 83873.00

PROGRESS PRINT - 60% DESIGN



NOTES:  
1. THE LEGEND "O.S.T.A." SHALL APPEAR ON ALL R- SERIES SIGNS EXCEPT WHEN SUFFIXED WITH THE LETTER "Z".  
2. FOR SPECIFIC SIGN DESIGN CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING.  
FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS".  
SIGNS OF DIFFERENT DIMENSIONS TO BE ERECTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED,  
MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.  
3. POSTS - SEE STANDARD SHEET TR-1208-02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."  
4. POSTS SHALL BE 4 LBS./FT.  
5. SIGNS SHALL BE FABRICATED OF ONE CONTINUOUS PIECE OF SHEET ALUMINUM.  
SPlicing OF SHEET ALUMINUM WILL NOT BE ACCEPTED.  
6. FOR OVERHEAD MOUNTED SIGNS, SEE STANDARD SHEET TR-1114-01 - "BONDING AND UTILITY POLE ATTACHMENT  
DETAILS, SIGN HANGER, "Y" CLAMP DETAIL."

COLORS:

BACKGROUND - WHITE - EXCEPT AS NOTED.

LEGEND - BLACK - EXCEPT AS NOTED.

ALL SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING WITH THE EXCEPTION OF OVERHEAD MOUNTED  
SIGNS AND SIGNS DESIGNATED WITH AN \* WHICH SHALL BE TYPE XI RETROREFLECTIVE SHEETING.

4 10-2021 REVISED SHEETING TYPE	5 12-2018 REVISED RETROREFLECTIVE STRIPS	6 10-2018 REVISED R5-11 SIGNS PER MUTCD	7 6-2018 NEW SHEET ISSUED
TR-GS_01	TR-GS_01	TR-GS_01	TR-GS_01
REV. DATE	REVISION DESCRIPTION	SHEET NO.	Plotted Date: 10/14/2021

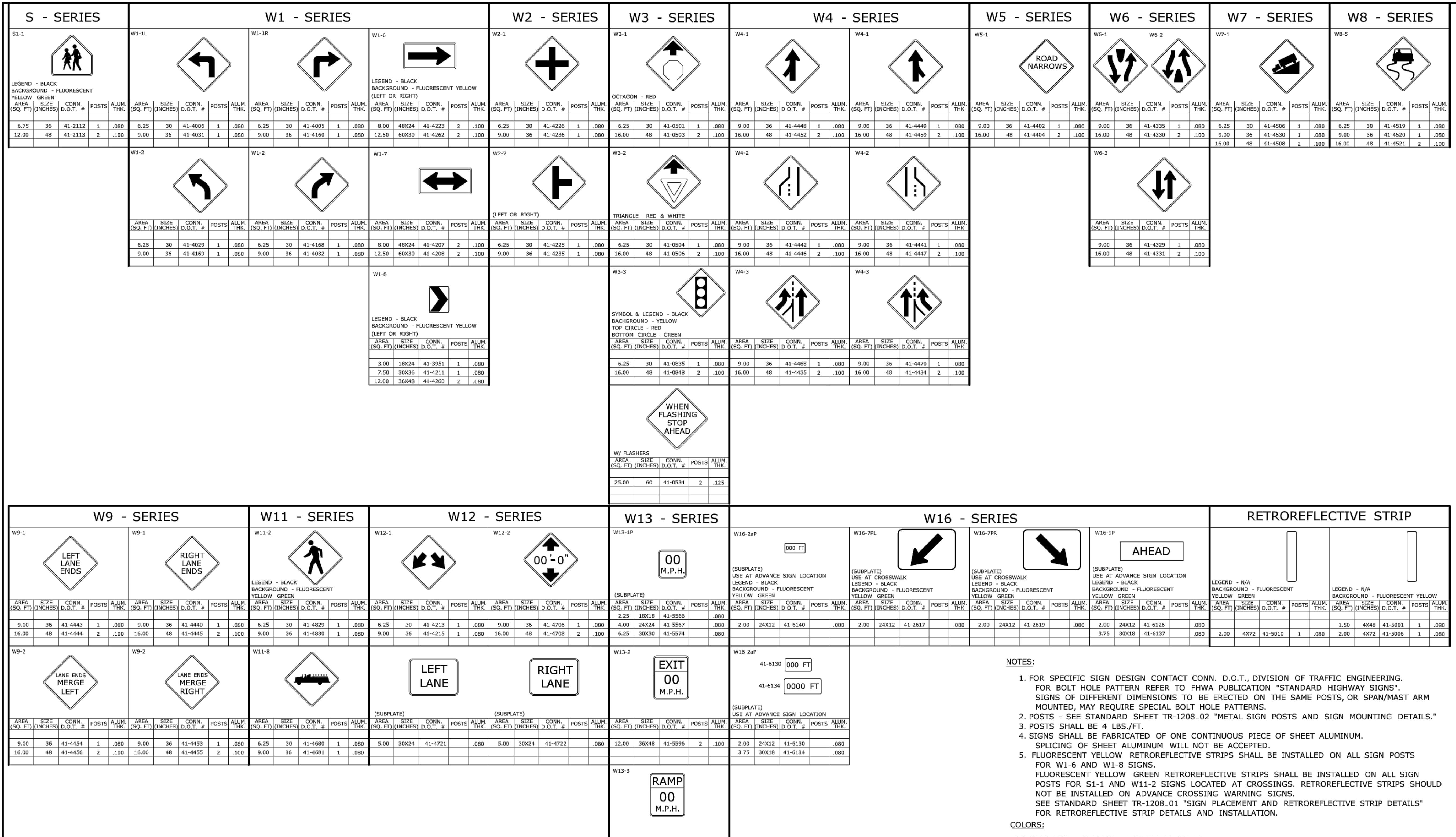
DESIGNER/DRAFTER: **A. MERMELSTEIN**  
CHECKED BY: **B. SCHILLING**  
NOT TO SCALE

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

SIGNATURE/ BLOCK:  
OFFICE OF ENGINEERING  
APPROVED BY: DATE: -

PROJECT TITLE: -  
TOWN: -  
PROJECT NO. -  
DRAWING NO. TR-GS\_01  
SHEET NO. -

DRAWING TITLE: SIGN FACE SHEET ALUMINUM  
R-SERIES TYPICAL SIGN DETAILS



MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2  
STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)  
IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

PREPARED FOR:

# MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

# STATE GUIDE SHEET DETAILS

OCTOBER 2024

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

The image shows the BSC Group logo. The word "BSC" is in a large, bold, dark blue sans-serif font. The word "GROUP" is in a large, bold, dark blue sans-serif font, with a teal square icon to its right. Below "BSC" and "GROUP" is a teal horizontal bar with the words "BUILD | SUPPORT | CONNECT" in white, separated by vertical teal lines. Underneath this bar, the address "665 Winding Brook Drive" is in a large, dark blue sans-serif font. Below that, "Glastonbury, Connecticut" is in a slightly smaller, dark blue sans-serif font. At the bottom, the zip code "06033" is in a dark blue sans-serif font. A teal horizontal bar is at the very bottom.

2021 BDC GROUP, INC.

SCALE: AS SHOWN

---

E: 8387300\ CIVIL \\_DRAWINGS \

G. NO:	GS-14
B. NO: 83873.00	

# PROGRESS PRINT - 60% DESIGN

## MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCOG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
860 652 8227

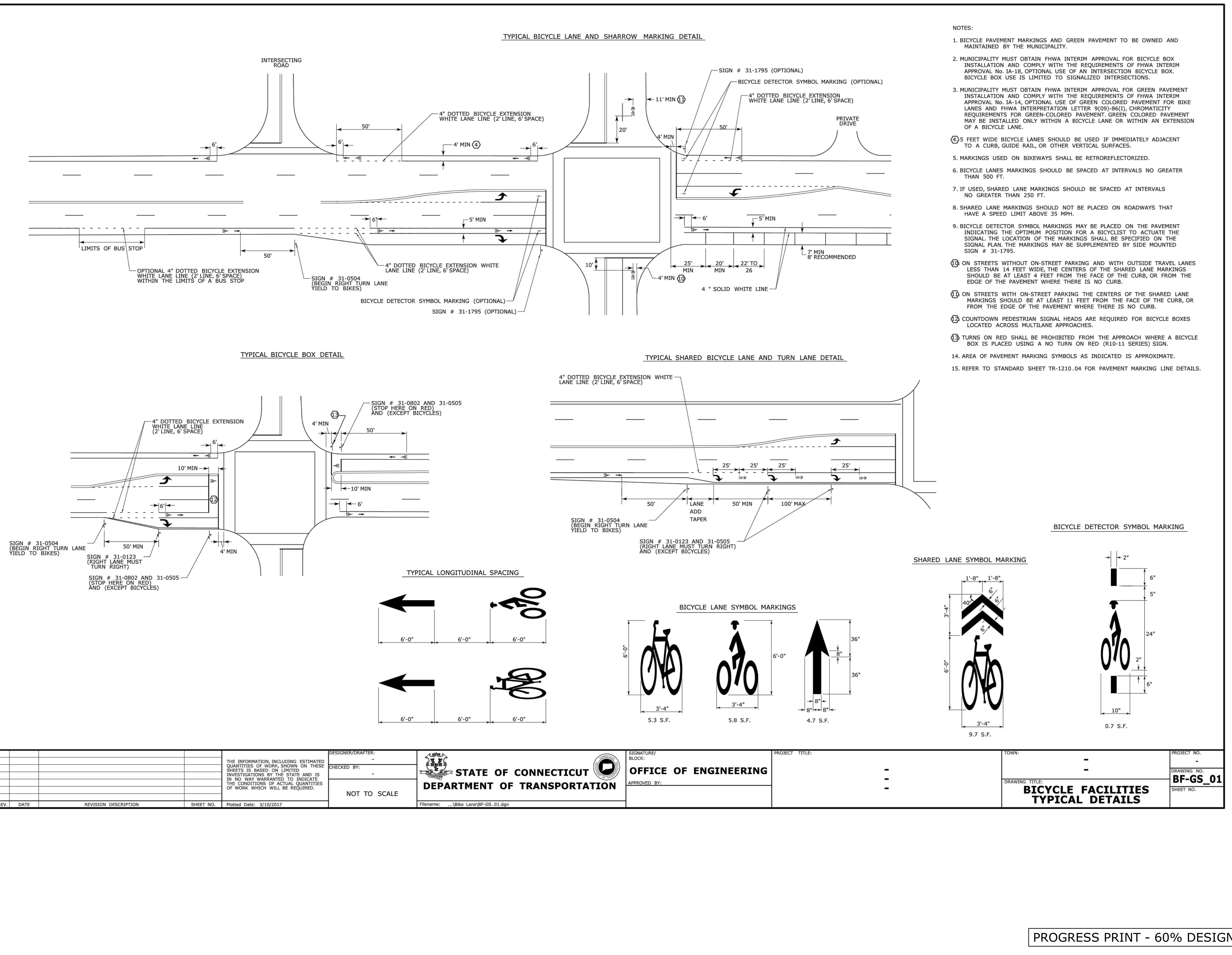
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FILE: 8387300\ CIVIL \ DRAWINGS  
DWG. NO:   GS-15  
JOB. NO: 83873.00

PROGRESS PRINT - 60% DESIGN

**NOTES:**

1. BICYCLE PAVEMENT MARKINGS AND GREEN PAVEMENT TO BE OWNED AND MAINTAINED BY THE MUNICIPALITY.
2. MUNICIPALITY MUST OBTAIN FHWA INTERIM APPROVAL FOR BICYCLE BOX INSTALLATION AND COMPLY WITH THE REQUIREMENTS OF FHWA INTERIM APPROVAL No. IA-18, OPTIONAL USE OF AN INTERSECTION BICYCLE BOX. BICYCLE BOX USE IS LIMITED TO SIGNALIZED INTERSECTIONS.
3. MUNICIPALITY MUST OBTAIN FHWA INTERIM APPROVAL FOR GREEN PAVEMENT INSTALLATION AND COMPLY WITH THE REQUIREMENTS OF FHWA INTERIM APPROVAL No. IA-14, OPTIONAL USE OF GREEN COLORED PAVEMENT FOR BIKE LANES AND FHWA INTERPRETATION LETTER 9(09)-86(I), CHROMATICITY REQUIREMENTS FOR GREEN-COLORED PAVEMENT. GREEN COLORED PAVEMENT MAY BE INSTALLED ONLY WITHIN A BICYCLE LANE OR WITHIN AN EXTENSION OF A BICYCLE LANE.
4. 5 FEET WIDE BICYCLE LANES SHOULD BE USED IF IMMEDIATELY ADJACENT TO A CURB, GUIDE RAIL, OR OTHER VERTICAL SURFACES.
5. MARKINGS USED ON BIKEWAYS SHALL BE RETROREFLECTORIZED.
6. BICYCLE LANES MARKINGS SHOULD BE SPACED AT INTERVALS NO GREATER THAN 500 FT.
7. IF USED, SHARED LANE MARKINGS SHOULD BE SPACED AT INTERVALS NO GREATER THAN 250 FT.
8. SHARED LANE MARKINGS SHOULD NOT BE PLACED ON ROADWAYS THAT HAVE A SPEED LIMIT ABOVE 35 MPH.
9. BICYCLE DETECTOR SYMBOL MARKINGS MAY BE PLACED ON THE PAVEMENT INDICATING THE OPTIMUM POSITION FOR A BICYCLIST TO ACTIVATE THE SIGNAL. THE LOCATION OF THE MARKINGS SHALL BE SPECIFIED ON THE SIGNAL PLAN. THE MARKINGS MAY BE SUPPLEMENTED BY SIDE MOUNTED SIGN # 31-1795.
10. ON STREETS WITHOUT ON-STREET PARKING AND WITH OUTSIDE TRAVEL LANES LESS THAN 14 FEET WIDE, THE CENTERS OF THE SHARED LANE MARKINGS SHOULD BE AT LEAST 4 FEET FROM THE FACE OF THE CURB, OR FROM THE EDGE OF THE PAVEMENT WHERE THERE IS NO CURB.
11. ON STREETS WITH ON-STREET PARKING THE CENTERS OF THE SHARED LANE MARKINGS SHOULD BE AT LEAST 11 FEET FROM THE FACE OF THE CURB, OR FROM THE EDGE OF THE PAVEMENT WHERE THERE IS NO CURB.
12. COUNTDOWN PEDESTRIAN SIGNAL HEADS ARE REQUIRED FOR BICYCLE BOXES LOCATED ACROSS MULTILANE APPROACHES.
13. TURNS ON RED SHALL BE PROHIBITED FROM THE APPROACH WHERE A BICYCLE BOX IS PLACED USING A NO TURN ON RED (R10-11 SERIES) SIGN.
14. AREA OF PAVEMENT MARKING SYMBOLS AS INDICATED IS APPROXIMATE.
15. REFER TO STANDARD SHEET TR-1210.04 FOR PAVEMENT MARKING LINE DETAILS.



MAIN STREET  
STREETSCAPE  
IMPROVEMENTS -  
PHASE 2

STATE PRJ. NO. L138-0003  
MAIN STREET (CT-113)

IN  
STRATFORD  
CONNECTICUT

STATE GUIDE SHEET  
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:  
METROCG  
1000 LAFAYETTE BOULEVARD  
BRIDGEPORT, CT 06604

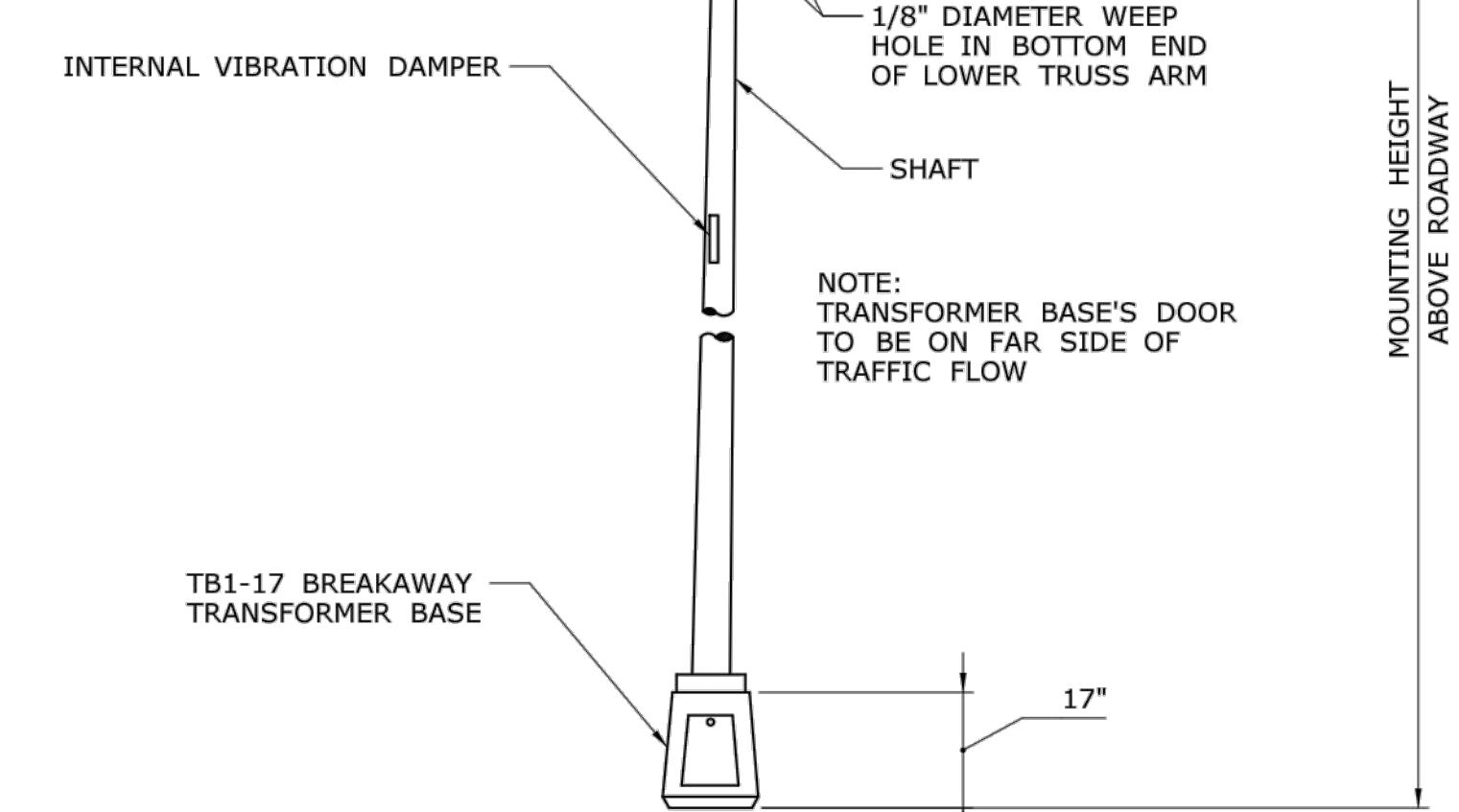
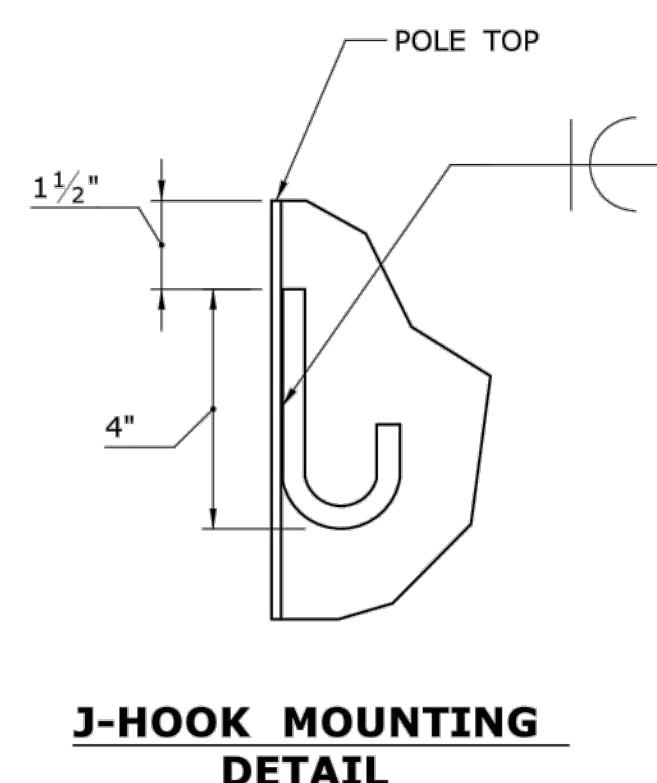
**BSC GROUP**   
BUILD | SUPPORT | CONNECT  
665 Winding Brook Drive  
Glastonbury, Connecticut  
06033  
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SCALE: AS SHOWN

FILE: 8387300\ CIVIL\ DRAWINGS  
DWG. NO:   GS-16  
JOB. NO: 83873.00

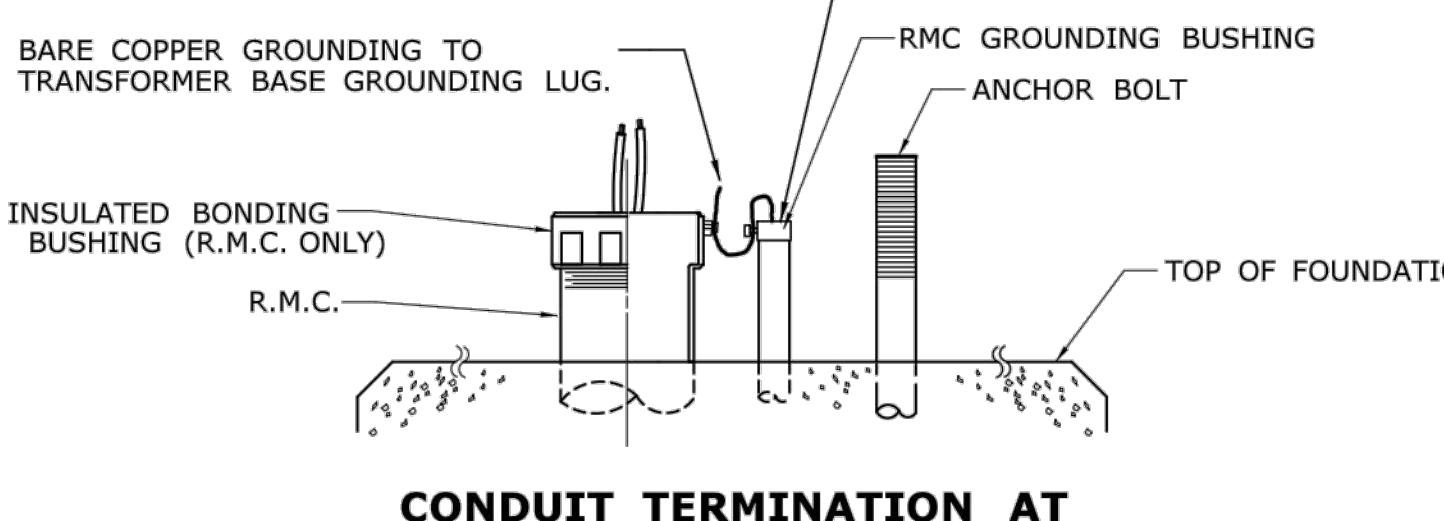
PROGRESS PRINT - 60% DESIGN

ALUMINUM LIGHT STANDARD - DIMENSION TABLE								
ITEM NO.	MOUNTING HEIGHT	BRACKET LENGTH	SHAFT DIAMETER		SHAFT WALL THICKNESS	BASE TYPE	ANCHOR BOLT SIZE	BOLT CIRCLE DIAMETER
			BOTTOM	TOP				
1003206	30'	15'	8"	6"	0.188"	TRANSFORMER	1"X40"	15"



**ALUMINUM LIGHT STANDARD FOR VEHICLE DETECTOR**

NOTE:  
TRANSFORMER BASE DOOR TO BE ON FAR SIDE OF TRAFFIC FLOW.  
TOP 2'-6" OF SHAFT IS NOT TAPERED.  
REFER TO SIGNAL PLANS FOR BRACKET LENGTHS.



**CONDUIT TERMINATION AT VEHICLE DETECTOR LIGHT STANDARD BASE**

**LIGHT STANDARD NOTES:**

- 1) ALUMINUM ALLOY SHALL BE 6063, T6 TEMPER.
- 2) BOLT CIRCLE SHOWN IS FOR TRANSFORMER BASE BOTTOM.
- 3) TO BE DESIGNED TO AASHTO "STANDARD SPECIFICATION FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" FOR 90 M.P.H. WINDS.
- 4) WELDING DESIGN AND FABRICATION SHALL CONFORM TO THE LATEST EDITION OF THE ANSI/AWS D1.2, STRUCTURAL WELDING CODE - ALUMINUM.
- 5) FOR BASE CONNECTION WELDS, FABRICATION INSPECTION AND TESTING SHALL BE PERFORMED AS NECESSARY PRIOR TO ASSEMBLY, DURING ASSEMBLY, AND AFTER WELDING, TO ENSURE THAT MATERIALS AND WORKMANSHIP MEET THE REQUIREMENTS OF THE CONTRACT DOCUMENTS. FABRICATION INSPECTION AND TESTING IS THE RESPONSIBILITY OF THE CONTRACTOR. VERIFICATION INSPECTION AND TESTING IS THE PREROGATIVE OF THE ENGINEER (CONNDOT).
- 6) NON-DESTRUCTIVE TESTING FOR ALUMINUM SHALL BE AS FOLLOWS: A RANDOM 25% OF ALL BASE CONNECTION WELDS SHALL BE INSPECTED IN ACCORDANCE WITH ASTM E-165 STANDARD PRACTICE FOR LIQUID PENETRANT INSPECTION METHOD.

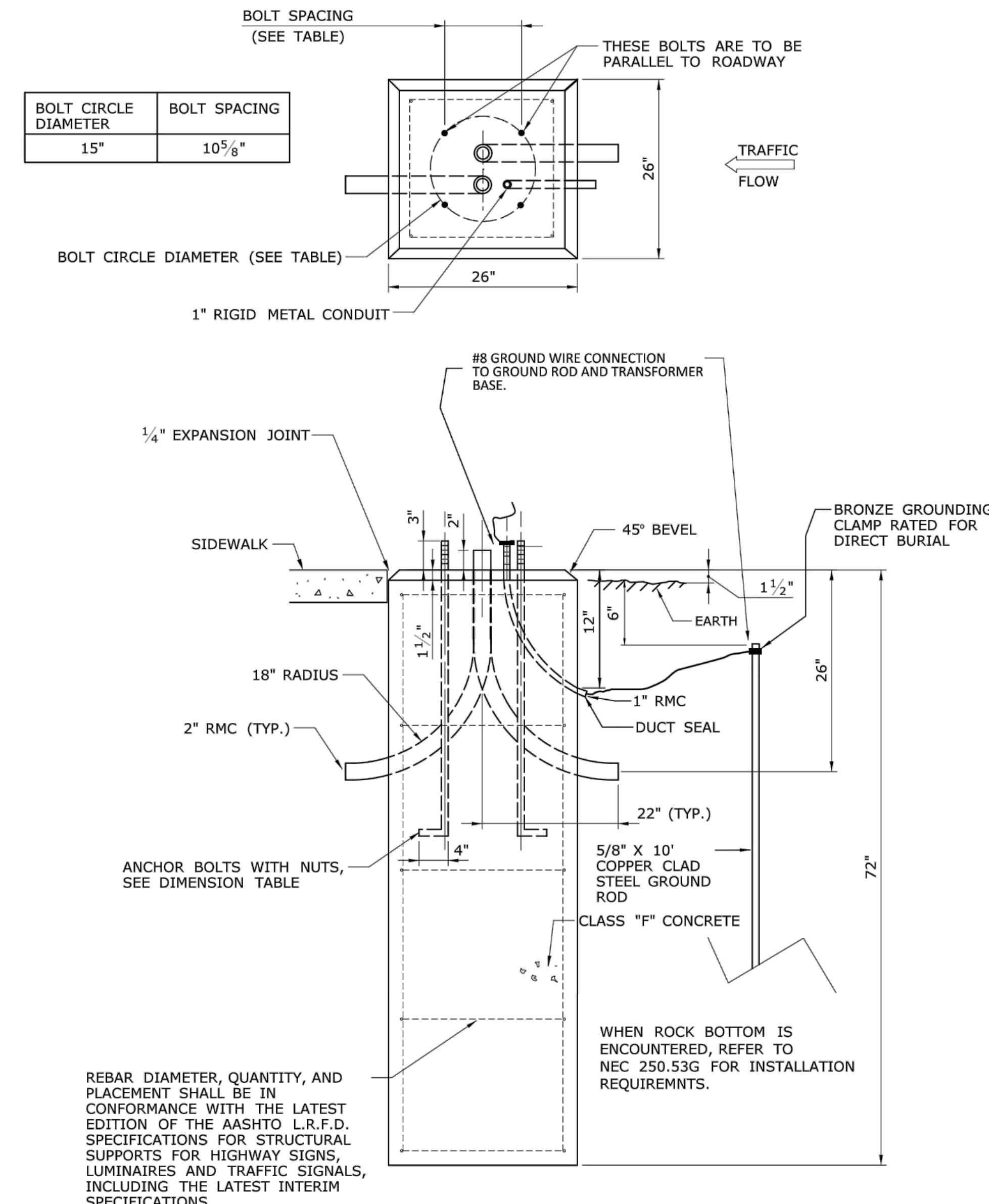
REV.	DATE	REVISION DESCRIPTION	SHEET NO.
		Plotted Date: 11/14/2019	

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS FOR INFORMATION PURPOSES ONLY. NO INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE ACTUAL ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DESIGNER/DRAFTER:	CHECKED BY:	SIGNATURE/BLOCK:	PROJECT TITLE:	TOWN:	PROJECT NO.
NO SCALE		OFFICE OF ENGINEERING			DRAWING NO.
		APPROVED BY: DATE:			SHEET NO.

Filename: ...\\CTDOT TRAFFIC GS-LS AND FOUND\\10-11-2019.dwg

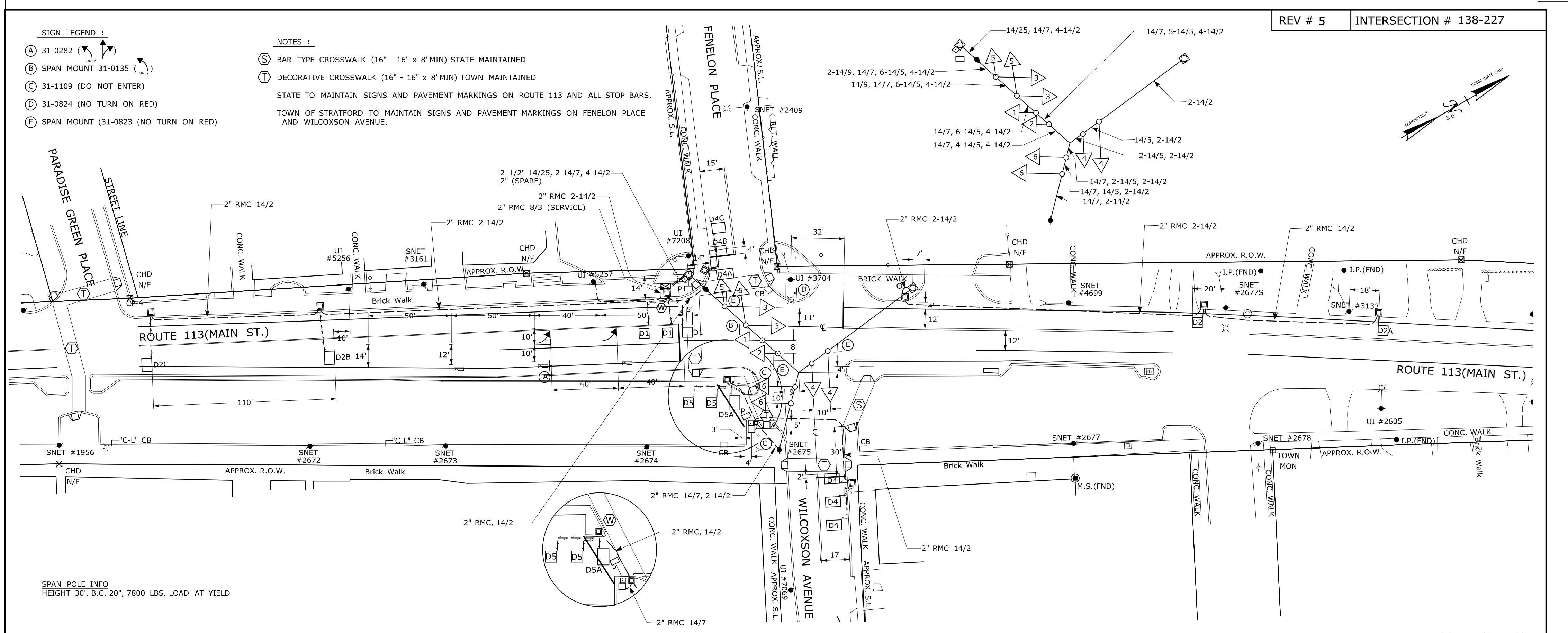
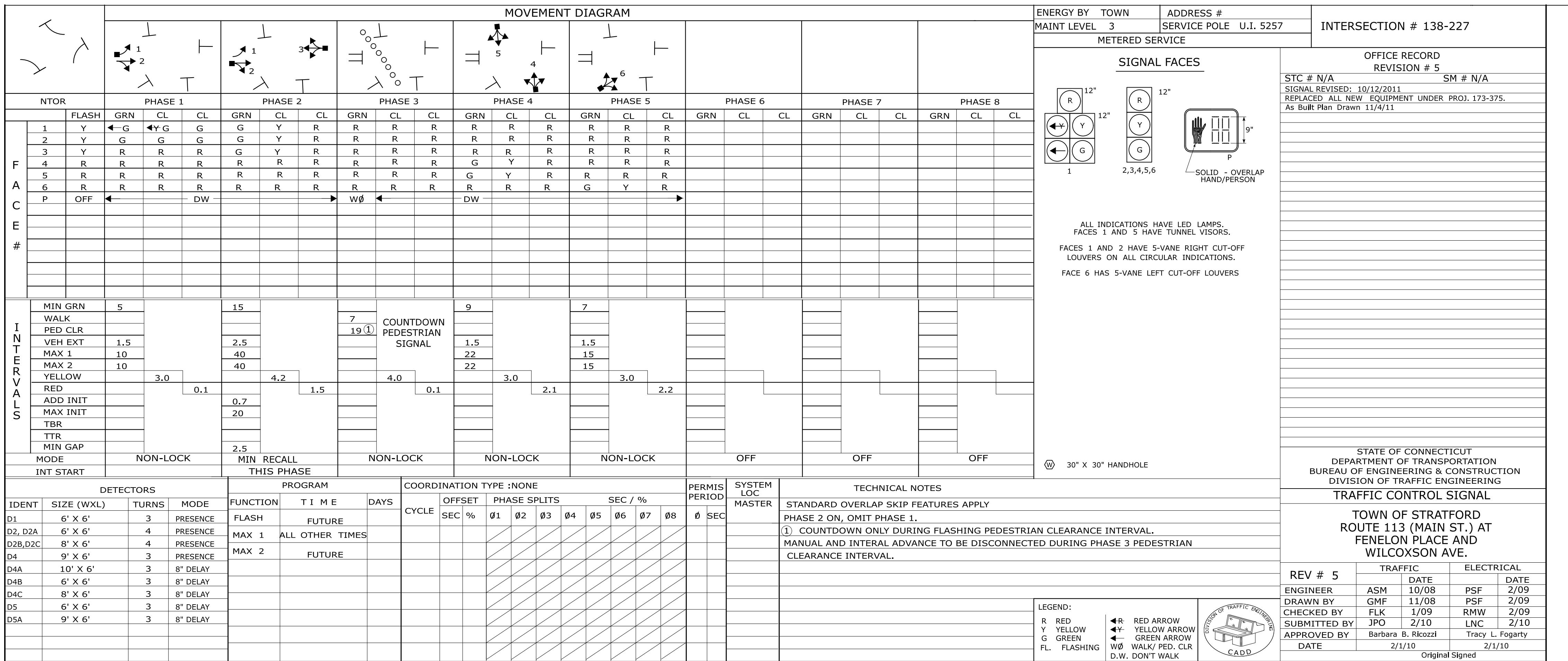
Plotted Date: 11/14/2019



**LIGHT STANDARD FOUNDATION - TYPE I**

REBAR DIAMETER, QUANTITY, AND PLACEMENT SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE AASHTO LRFD. SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, INCLUDING THE LATEST INTERIM SPECIFICATIONS

PROGRESS PRINT - 60% DESIGN



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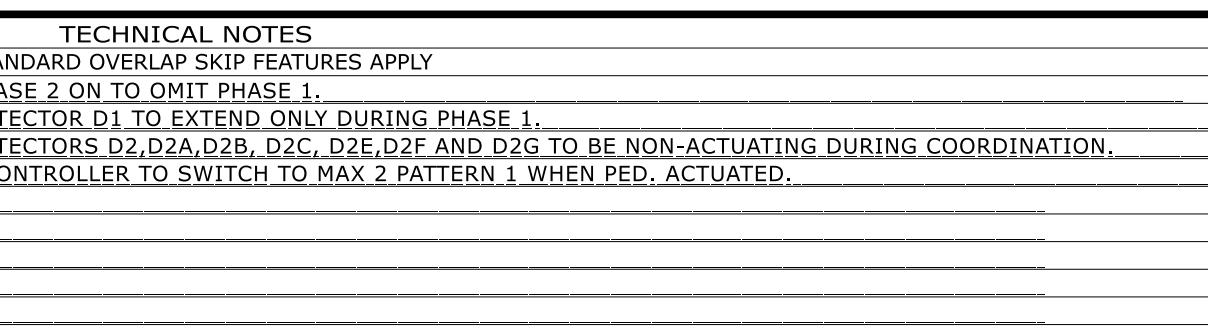
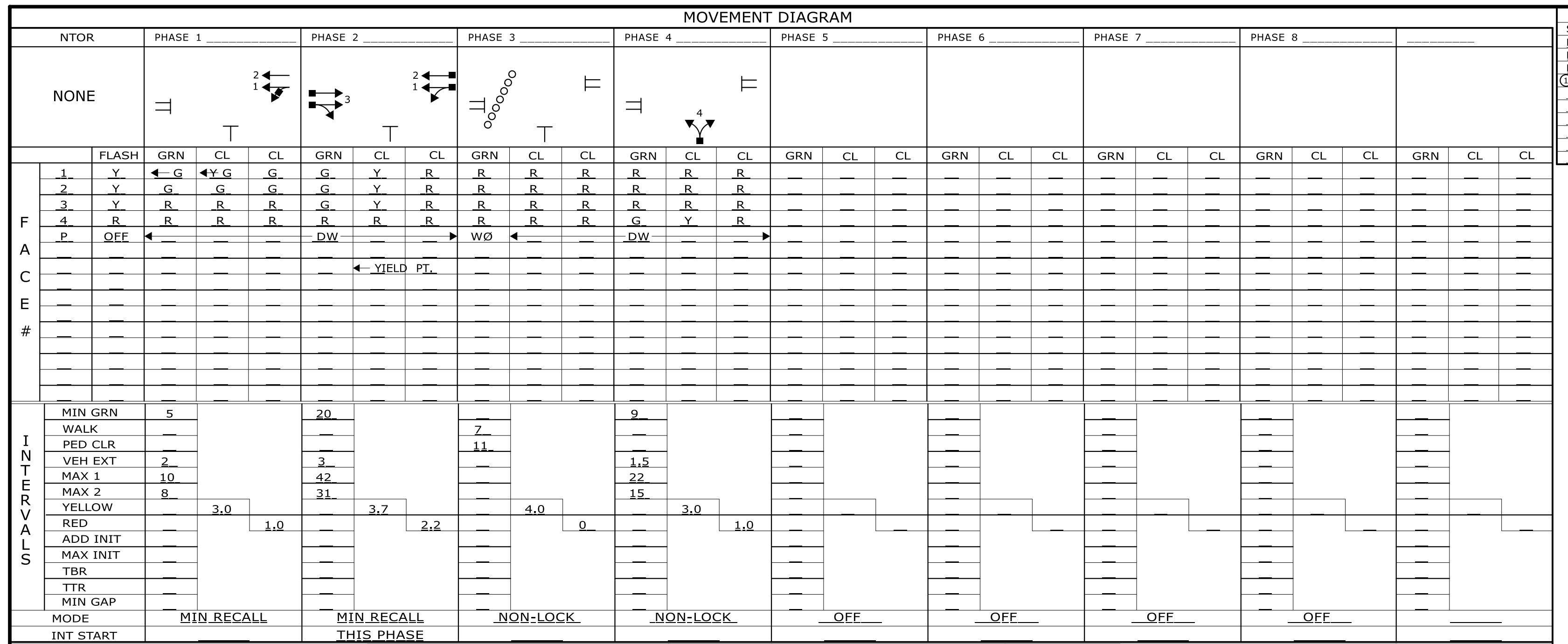
○ PROPOSED WOOD SPAN POLE	□ PEDESTRIAN SIGNAL FACE	— — - PROPOSED R.M.C. (RIGID METAL CONDUIT)	◀ VIDEO DETECTOR	□ SIDEWALK RAMP	DRAWN BY : GMF
● EXISTING WOOD SPAN POLE	■ AUDIBLE PEDESTRIAN SIGNAL	—●— EXISTING R.M.C. (RIGID METAL CONDUIT)	—●— OPTICAL DETECTOR	—●— SONIC DETECTOR	
□ PROPOSED STEEL SPAN POLE	☒ PROPOSED CONTROLLER	→ DET. LEADS IN SAW CUT	—●— SYSTEM DETECTOR	—●— GUY WIRE	
■ EXISTING STEEL SPAN POLE	☒ TRAFFIC SIGNAL FACE	☒ AUXILIARY TERMINATION CABINET	☒ EXISTING CONTROLLER	□ LOOP DETECTOR	
—○— PROPOSED UTILITY POLE	□ PEDESTRIAN PUSH BUTTON & SIGN	☒ PEDESTAL MOUNTING	□ PROPOSED HANDHOLE	■ MAGNETIC DETECTOR	DESIGNED BY : AM
● EXISTING UTILITY POLE		☒ AUXILIARY EQUIPMENT CABINET	□ EXISTING HANDHOLE	△ CABLE CLOSURE	

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

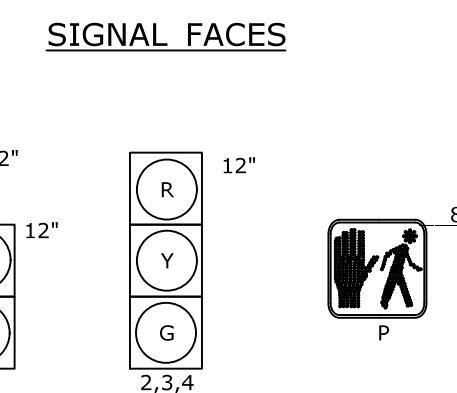
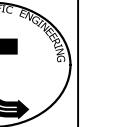
TOWN OF STRATFORD  
ROUTE 113 (MAIN ST.) AT  
FENELON PLACE AND  
WILCOXSON

## CONSTRUCTION NOTES :

STRATFORD	173-375
	DRAWING NO.
DRAWING TITLE: <b>TRAFFIC CONTROL SIGNAL PLAN</b>	SHEET NO.



OFFICE RECORD  
REV # 9 TIR # N/A SM # 101155 SIGNAL REVISED: 01/09/2019  
REVISED TIMINGS UNDER PROJECT #173-454



## CONSTRUCTION NOTES:

EN-1 ALL MATERIAL AND CONSTRUCTION METHODS SHALL CONFORM TO THE FOLLOWING CURRENT D.O.T. DOCUMENTS WHICH CAN BE ACCESSED ON THE D.O.T. WEBSITE:  
STANDARD SPECIFICATIONS FOR ROADS, BRIDGES AND  
INCIDENTAL CONSTRUCTION,  
SUPPLEMENTAL SPECIFICATION TO THE STANDARD SPECIFICATION,  
SPECIAL PROVISIONS TO THE STANDARD SPECIFICATIONS,  
STANDARD INSTALLATION AND GUIDE DETAIL SHEETS.  
EN-4 ALL TRAFFIC SIGNAL EQUIPMENT IS EXISTING EXCEPT AS NOTED.  
EN-5 STAKE ALL R.O.W. PRIOR TO EXCAVATION.  
EN-7 ANY PROPOSED REVISION TO THE LOCATION OF THE APPURTENANCES SHOWN ON THE PLAN MUST BE SUBMITTED FOR REVIEW AND APPROVAL BY THE DIVISION OF TRAFFIC ENGINEERING PRIOR TO INSTALLATION.

THE LOCATION OF TRAFFIC SIGNAL FOUNDATIONS WHEN IN OR  
ADJACENT TO SIDEWALKS SHALL BE VERIFIED PRIOR TO INSTALLATION  
TO PROVIDE A CLEAR PATH OF NOT LESS THAN 4 FEET. IF A MINIMUM  
4 FOOT FREE PATH IS UNAVAILABLE, NOTIFY THE ENGINEER AND  
CONTACT THE DIVISION OF TRAFFIC ENGINEERING.

REMOVE ALL ABANDONED TRAFFIC SIGNAL EQUIPMENT PER SPECIAL

SEN-9 REMOVE ALL ABANDONED TRAFFIC SIGNAL EQUIPMENT PER SPECIAL PROVISIONS.

FOUNDATION NOTES

OUN-1      INSTALL PEDESTAL FOUNDATIONS ADJACENT TO LANDING AREA.

CON-1 INSTALL FEDERAL FOUNDATIONS ADJACENT TO LANDING AREA.  
CONTROLLER NOTES

ONT-2 MODIFY CONTROLLER FOUNDATION TO ACCEPT NEW CONDUIT.

ONT-3 MODIFY EXISTING CONTROLLER TO ACCOMMODATE CHANGES. SUPPLY 3 PAPER COPIES OF REVISED CABINET WIRING DIAGRAMS AND EMAIL A PDF COPY TO DOT.SIGNALLAB@CT.GOV

ONT-7 COORDINATE THIS REVISION WITH CONNECTICUT D.O.T. SIGNAL LAB AT DOT.SIGNALLAB@CT.GOV AT LEAST 45 DAYS PRIOR TO REVISION.

## CONDUIT AND HANHOLE NOTES

COND. 1 LOCATE EXISTING RIGID METAL CONDUIT, EXTEND INTO NEW

OND-1 LOCATE EXISTING RIGID METAL CONDUIT. EXTEND INTO NEW  
HANDHOLE.

OND-3 W INSTALL 30 $\frac{9}{32}$  X 30 $\frac{9}{32}$  HANDHOLE. ALL OTHERS ARE TYPE II UNLESS OTHERWISE NOTED.

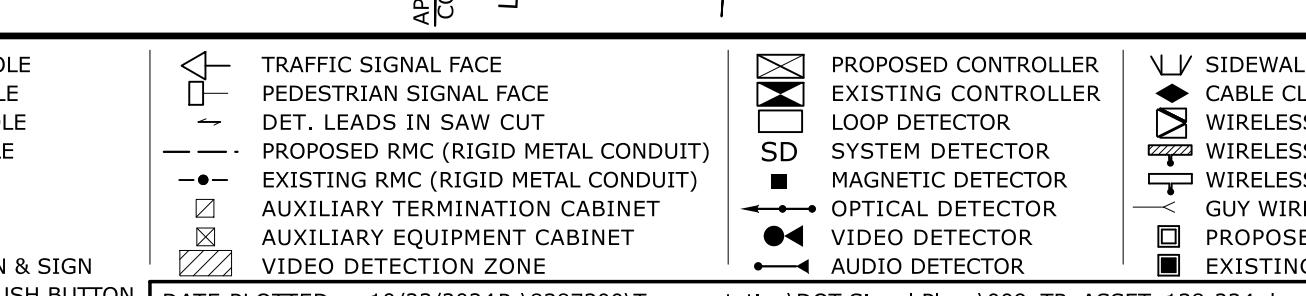
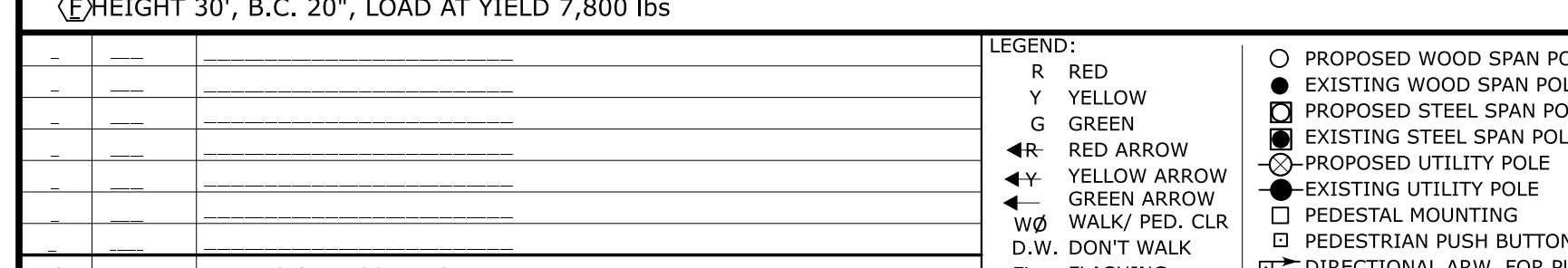
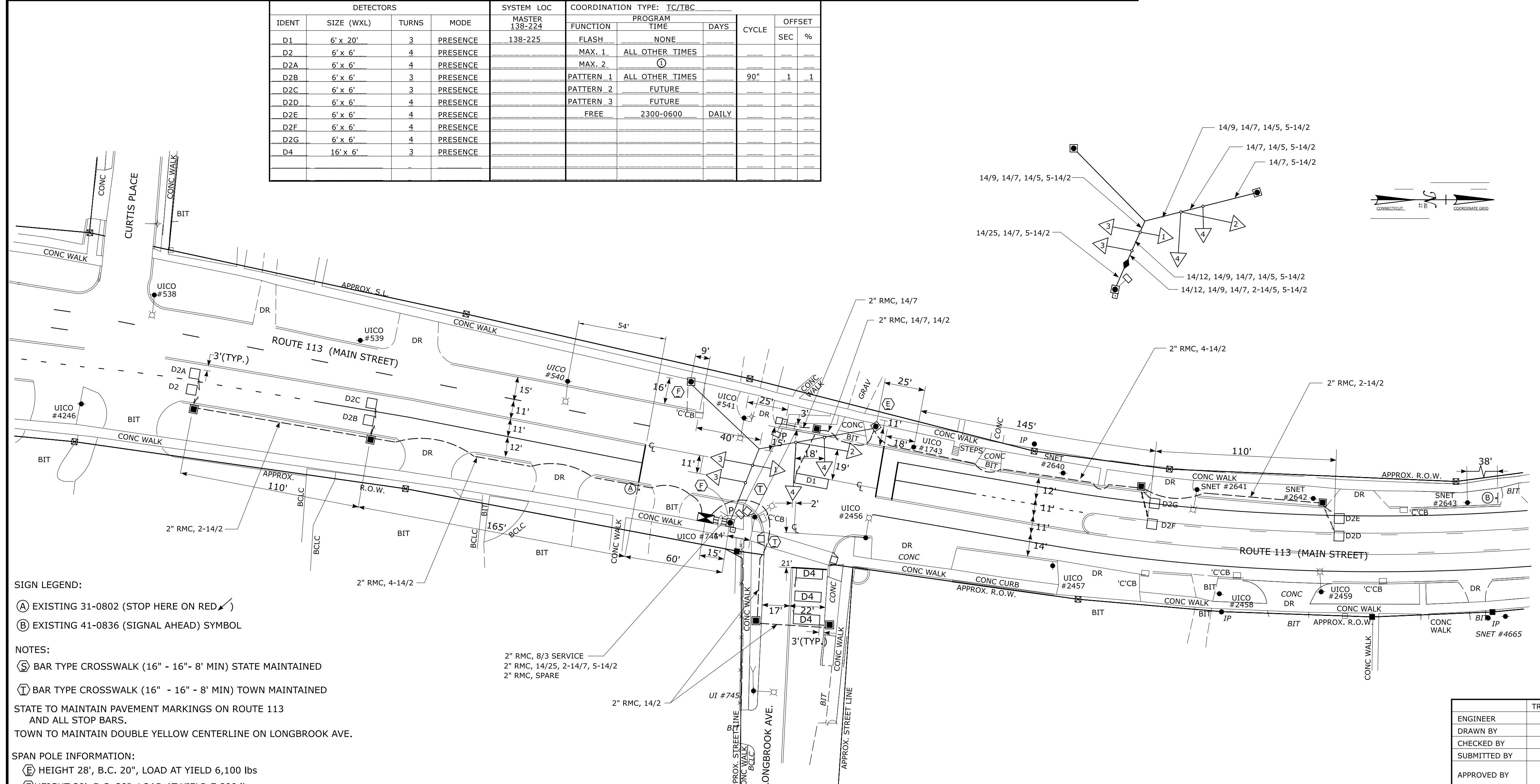
#### EQUESTRIAN/PEDESTAL NOTES

ED-1                    INSTALL NEW 8 FOOT ALUMINUM PEDESTAL WITH COUNTDOWN  
PEDESTRIAN SIGNAL AND ABS PUSHBUTTON ON EXISTING FOUNDATION

ED-2                   INSTALL APS AND COUNTDOWN PEDESTRIAN SIGNAL ON EXISTING PEDESTAL.  
ED-3                   REPLACE PEDESTRIAN SIGNAL FACE AND PUSH BUTTON. USE

ED-3 REPLACE PEDESTRIAN SIGNAL FACE AND PUSH BUTTON. USE EXISTING PEDESTAL FOUNDATION AND PEDESTAL.

IISC-1 INSTALL A TOGGLE BOLT AND WASHER TO EFFECTIVELY SEAL EACH HOLE LEFT IN THE (SPAN POLE/MAST ARM/PEDESTAL) WHERE ANY TRAFFIC SIGNAL EQUIPMENT HAS BEEN REMOVED.





# STATE OF CONNECTICUT



## DEPARTMENT OF TRANSPORTATION

	TRAFFIC	DATE	ELECTRICAL	DATE	REV # 9	INTERSECTION # 138-224
	---	---	---	---		
	---	---	---	---		
BY	---	---	---	---		
D BY	---	---	---	---	ENERGY BY - TOWN	ADDRESS #
D BY					MAINT LEVEL 5	SERVICE POLE - U.I. 746
D DATE						METERED SERVICE - 016019554

ROUTE 113 (MAIN ST.) AT  
LONGBROOK AVE.

' # 9	INTERSECTION # 138-224
ENERGY BY - TOWN	ADDRESS #
MAINT LEVEL 5	SERVICE POLE - U.I. 746

<b>STRATFORD</b>		173-454
		DRAWING NO.
		SHEET NO.





\*ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

\*\*REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-211_01	ANTI-TRACKING PAD	11-09-22
	HW-286_01	DRAINAGE TRENCH EXCAVATION	11-09-22
	HW-505_01a	STRAIGHT ENDWALLS	02-28-24
	HW-505_01b	STEEL REINFORCING FOR STRAIGHT ENDWALLS (2" DIFF BASE TO FLOW LINE)	01-05-24
	HW-505_01c	STEEL REINFORCING FOR STRAIGHT ENDWALLS (STANDARD RIPRAP APPLICATION)	01-05-24
	HW-505_02	TYPE "D-G" & "L" ENDWALLS	01-05-24
	HW-586_01	CATCH BASIN AND DROP INLET TYPES "C" AND "C-L" STRUCTURES	01-05-24
	HW-586_02	CATCH BASIN ( TYPES "C" AND "C-L" ) FOR DOUBLE GRATE TYPE I STRUCTURES	01-05-24
	HW-586_03	CATCH BASIN ( TYPES "C" AND "C-L" ) FOR DOUBLE GRATE TYPE II STRUCTURES	01-05-24
	HW-586_04	PRECAST CATCH BASIN AND ROUND STRUCTURE	11-08-22
	HW-586_05	PRECAST CATCH BASIN TYPES FOR DOUBLE GRATE TYPE I	11-08-22
	HW-586_06	PRECAST CATCH BASIN TYPES FOR DOUBLE GRATE TYPE II	11-08-22
	HW-586_07a	CATCH BASIN TYPE "C" AND "C-L" TOPS	01-05-24
	HW-586_07b	CATCH BASIN TYPE "C" AND "C-L" DOUBLE GRATE TYPE I TOPS	11-09-22
	HW-586_07c	CATCH BASIN TYPE "C" AND "C-L" DOUBLE GRATE TYPE II TOPS	11-08-22
	HW-586_07d	CATCH BASIN TYPE "C-G" AND "C-M" BARRIER CURB TOPS	11-09-22
	HW-586_08	CATCH BASIN FRAMES AND GRATES	11-09-22
	HW-586_09	CATCH BASIN LOCK DOWN TOPS	11-09-22
	HW-586_10a	MANHOLE FRAME AND COVER	01-05-24
	HW-586_10b	MANHOLE FRAME AND GRATE	01-05-24
	HW-586_10c	REINFORCED PRECAST CONCRETE MANHOLE	11-08-22
	HW-586_10d	MANHOLE NON-PRECAST CONCRETE UNIT	11-08-22
	HW-686_01a	CONCRETE PIPE CONNECTION SHEET 1	11-08-22
	HW-686_01b	CONCRETE PIPE CONNECTION SHEET 2	11-08-22
	HW-686_02a	DRAINAGE PIPE ENDS SHEET 1 [ CORRUGATED METAL PIPE ]	11-08-22
	HW-686_02b	DRAINAGE PIPE ENDS SHEET 2 [ CONCRETE PIPE ]	11-08-22
	HW-751_01	UNDERDRAINS AND UNDERDRAIN OUTLETS	02-28-24
	HW-803_01	PAVED APRONS	11-08-22
	HW-811_01	CONCRETE CURBING	11-08-22
	HW-813_01	GRANITE STONE TRANSITION CURBING	11-08-22
	HW-813_02	STONE CURBING	11-08-22
	HW-815_01	BITUMINOUS CONCRETE CURBING	11-08-22

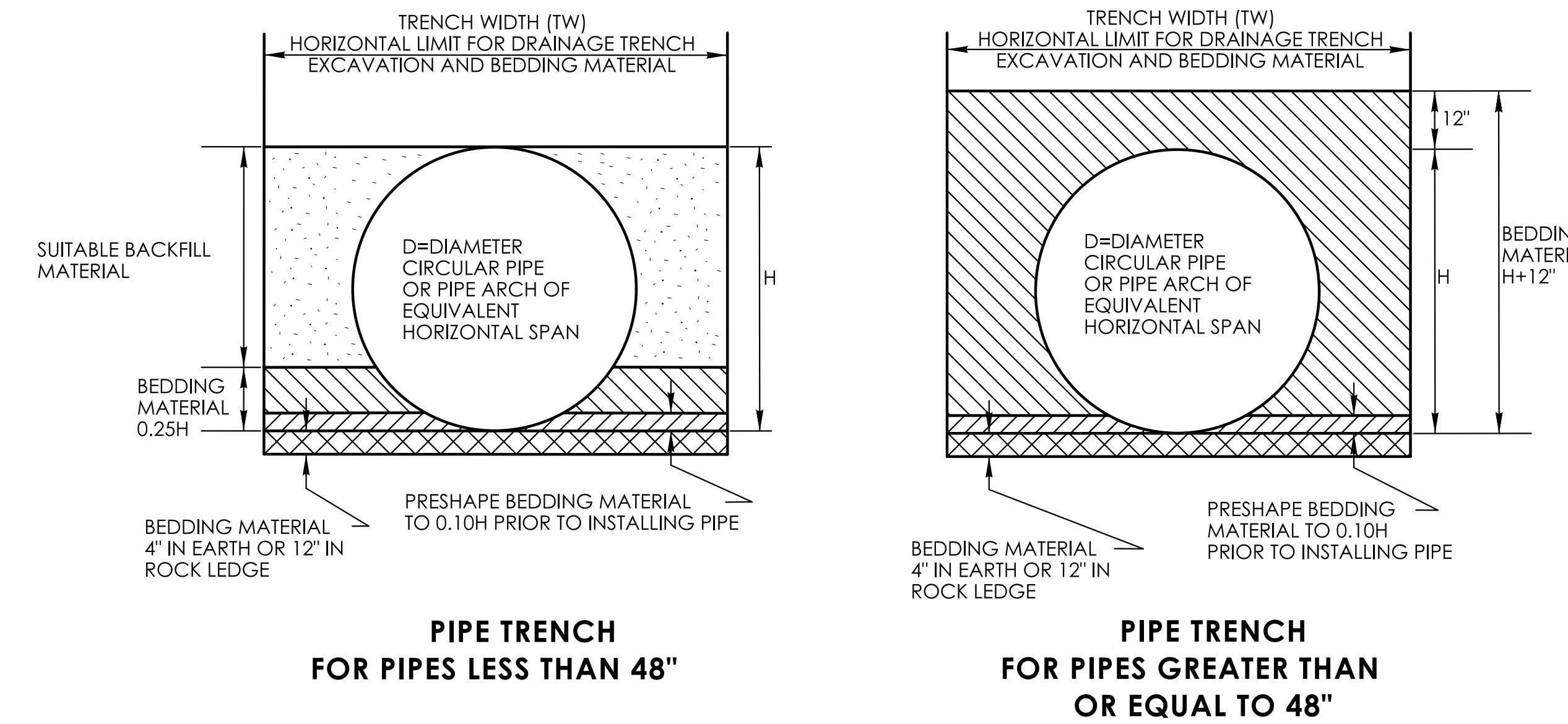
✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-821_01a	TRANSITION - 45" F-SHAPE TO 45" VERTICAL SHAPE SHEET 1	11-08-22
	HW-821_01b	TRANSITION - 45" F-SHAPE TO 45" VERTICAL SHAPE SHEET 2	11-08-22
	HW-821_01c	TRANSITION - 45" F-SHAPE TO 45" VERTICAL SHAPE SHEET 3	11-08-22
	HW-821_02a	45" F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 1	11-08-22
	HW-821_02b	45" F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 2	11-08-22
	HW-821_03a	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 1	11-08-22
	HW-821_03b	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 2	11-08-22
	HW-821_03c	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 3	11-08-22
	HW-821_03d	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 4	11-08-22
	HW-821_03e	TRANSITION - 32" JERSEY SHAPE TO 45" F-SHAPE	11-08-22
	HW-821_04a	MERRITT PARKWAY NARROW MEDIAN BARRIER	11-08-22
	HW-821_04b	MERRITT PARKWAY - 2' WIDE MEDIAN BARRIER AND ROADSIDE BARRIER	11-08-22
	HW-821_05a	TRANSITION - 45" F-SHAPE TO 54" VERTICAL SHAPE SHEET 1	11-08-22
	HW-821_05b	TRANSITION - 45" F-SHAPE TO 54" VERTICAL SHAPE SHEET 2	11-08-22
	HW-821_06	54" VERTICAL SHAPE BARRIER	11-08-22
	HW-821_07	MISCELLANOUS DETAILS FOR BARRIER TRANSITIONS	11-08-22
	HW-821_08a	F-SHAPE CONC. BARRIER CURB (21"x45") TRANSITION FOR THRIE-BEAM	11-08-22
	HW-821_08b	F-SHAPE CONC. BARRIER CURB (21"x45") TRANSITION FOR THRIE-BEAM - REINF.	11-08-22
	HW-821_09a	SINGLE SLOPE CONC. BARRIER CURB (20"x42") TRANS. FOR THRIE-BEAM	11-08-22
	HW-821_09b	SINGLE SLOPE CONC. BARRIER CURB (20"x42") TRANS. FOR THRIE-BEAM - REINF.	11-08-22
	HW-821_10a	VERTICAL FACE CONC. (21"x54") TRANSITION FOR THRIE-BEAM	11-08-22
	HW-821_10b	VERTICAL FACE CONC. (21"x54") TRANSITION FOR THRIE-BEAM REINF.	11-08-22
	HW-821_11a	42" SINGLE SLOPE PRECAST CONCRETE BARRIER CURB -SHEET 1	01-05-24
	HW-821_11b	42" SINGLE SLOPE PRECAST CONCRETE BARRIER CURB -SHEET 2	01-05-24
	HW-822_01	TEMPORARY PRECAST CONCRETE BARRIER CURB	11-08-22
	HW-822_02a	TEMPORARY TRAFFIC BARRIER - DETAILS	11-08-22
	HW-822_02b	TEMPORARY TRAFFIC BARRIER (BOLTED)	02-02-24
	HW-822_02c	TEMPORARY TRAFFIC BARRIER & TEMPORARY TRAFFIC BARRIER (PINNED)	11-08-22
	HW-905_01	STONE WALL FENCE	11-09-22
	HW-906_01	WIRE FENCE	11-08-22

\*ONLY STANDARD SHEETS MARKED WITH AN " ✓ " ARE IN THIS PROJECT #

\*\*REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-910_01	W-BEAM METAL BEAM RAIL HARDWARE	11-08-22
	HW-910_02	METAL BEAM RAIL (TYPE R-B 350) GUIDERAIL	11-08-22
	HW-910_03	METAL BEAM RAIL (TYPE MD-B 350) GUIDERAIL	11-08-22
	HW-910_04	METAL BEAM RAIL (TYPE R-B 350) SYSTEMS 5, 5A, & 6	11-08-22
	HW-910_05	METAL BEAM RAIL R-B 350 SPAN TYPE I, II, III SECTIONS	11-08-22
	HW-910_06	R-B 350 BRIDGE ATTACHMENT SAFETY SHAPE PARAPET	11-08-22
	HW-910_07	R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET	11-08-22
	HW-910_09a	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 1	11-08-22
	HW-910_09b	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 2	11-08-22
	HW-910_10	METAL BEAM RAIL 8" x 6" BOX BEAM	11-08-22
	HW-910_11	CURVED GUIDERAIL TREATMENT DETAIL	11-08-22
	HW-910_12a	MERRITT PARKWAY GUIDERAIL LEADING END ATTACHMENTS AND SYSTEMS 2&3	11-08-22
	HW-910_12b	MERRITT PARKWAY GUIDERAIL HARDWARE DETAILS	11-08-22
	HW-910_12c	MERRITT PARKWAY GUIDERAIL TRAILING END ATTACHMENTS	11-02-22
	HW-910_12d	MERRITT PARKWAY MEDIAN GUIDERAIL AND END ANCHOR	11-08-22
	HW-910_13a	THRIE-BEAM METAL BEAM RAIL HARDWARE	11-08-22
	HW-910_13b	THRIE-BEAM TRANSITIONS	11-08-22
	HW-910_14a	THRIE-BEAM 350 BRIDGE ATTACHMENT	11-08-22
	HW-910_14b	THRIE-BEAM 350 GUIDERAIL TRANSITION TO R-B 350 GUIDERAIL	11-08-22
	HW-910_15	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE I	11-08-22
	HW-910_16	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE II	11-08-22
	HW-910_17	R-B TERMINAL SECTION	11-08-22
	HW-910_18	METAL BEAM RAIL (TYPE MD-I) GUIDERAIL	11-08-22
	HW-910_19a	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE I	11-09-22
	HW-910_19b	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE II	11-08-22
	HW-910_19c	METAL BEAM RAIL (MODIFIED TYPE R-I) SYSTEMS 2 AND 3	11-08-22
	HW-910_20	MASH W-BEAM HARDWARE	01-05-24
	HW-910_21	METAL BEAM RAIL ( R-B MASH ) GUIDERAIL	01-05-24
	HW-910_22	METAL BEAM RAIL ( MD-B MASH ) GUIDERAIL	11-08-22
	HW-910_23	METAL BEAM RAIL (R-B MASH) HALF & QUARTER POST SPACING GUIDERAIL	11-08-22
	HW-910_24	METAL BEAM RAIL SPAN SECTION TYPES II AND III	11-08-22

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-910_25a	METAL BEAM RAIL TRANSITION 350 TO MASH	01-05-24
	HW-910_25b	METAL BEAM RAIL MEDIAN APPLICATION TRANSITION 350 TO MASH GUIDERAIL	01-05-24
	HW-910_26	THRIE-BEAM ATTACHMENT HARDWARE	11-08-22
	HW-910_27	THRIE-BEAM ATTACHMENT	11-08-22
	HW-910_29	THRIE-BEAM BRIDGE ATTACHMENT TRAILING END	02-02-24
	HW-911_01	R-B END ANCHORAGE TYPE I AND II	02-28-24
	HW-911_02	MD-B END ANCHORAGE TYPE I	02-28-24
	HW-911_03	ANCHOR IN EARTH CUT SLOPE & ANCHOR IN ROCK CUT SLOPE	01-05-24
	HW-911_05	MERRITT PARKWAY GUIDERAIL END ANCHORS	11-08-22
	HW-913_01a	CHAIN LINK FENCE	11-08-22
	HW-913_01b	CHAIN LINK FENCE HARDWARE	11-08-22
	HW-913_02	CHAIN LINK FENCE GATES	11-08-22
	HW-918_01a	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 1	11-08-22
	HW-918_01b	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 2	11-08-22
	HW-918_01c	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 3	11-08-22
	HW-921_01	CONCRETE SIDEWALKS	11-08-22
	HW-922_01	BITUMINOUS CONCRETE SIDEWALK AND BITUMINOUS CONCRETE DRIVEWAY	11-08-22
	HW-924_01	CONCRETE DRIVEWAY RAMPS	11-09-22
	HW-930_01	OBJECT MARKER ( MAINTENANCE )	02-28-24
	HW-949_01a	LANDSCAPE PLANTING	11-09-22
	HW-949_01b	TREE STAKING	11-02-22
	HW-1800_01	GRADING PLAN FOR IMPACT ATTENUATION SYSTEMS (FLARED AND TANGENTIAL)	11-02-22
	HW-1800_02	GRADING PLAN FOR IMPACT ATTENUATION SYSTEMS (MEDIAN/GORE)	11-02-22



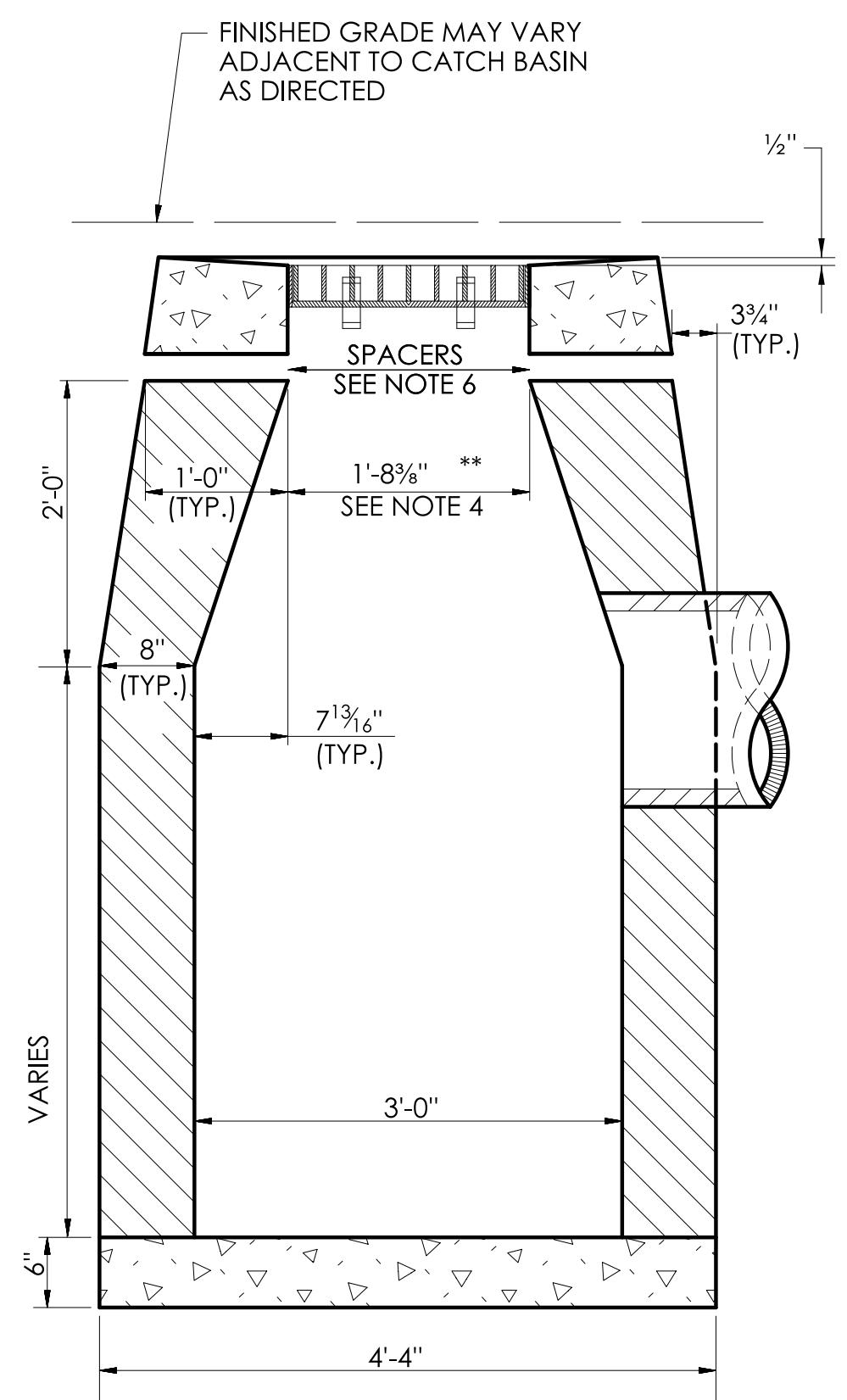
**TRENCH WIDTH (TW) CHART**

PIPE, PIPE-ARCH, OR DRAINAGE STRUCTURE	TRENCH WIDTH
PIPE OR PIPE-ARCH WITH NOMINAL INSIDE HORIZONTAL SPAN LESS THAN 30"	2' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
PIPE OR PIPE-ARCH WITH NOMINAL INSIDE HORIZONTAL SPAN GREATER THAN OR EQUAL TO 30"	3' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
PIPE OR PIPE-ARCH FABRICATED FROM STRUCTURAL PLATES	4' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
DRAINAGE STRUCTURES	2' BEYOND ALL EXTERIOR OR FOUNDATION WALLS

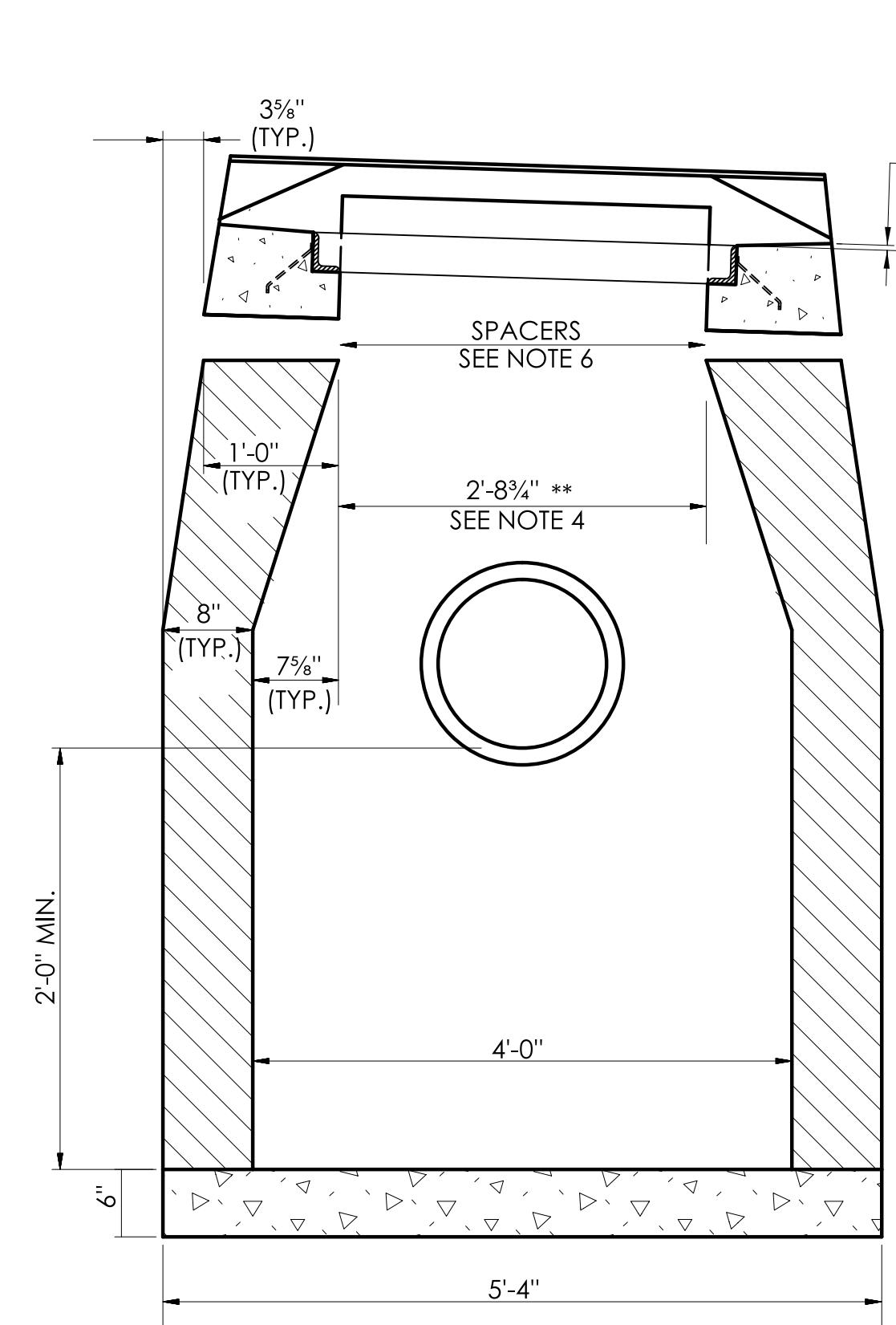
NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: <b>DRAINAGE TRENCH EXCAVATION</b>	STANDARD SHEET NO.: <b>HW-286_01</b>
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### GENERAL NOTES:

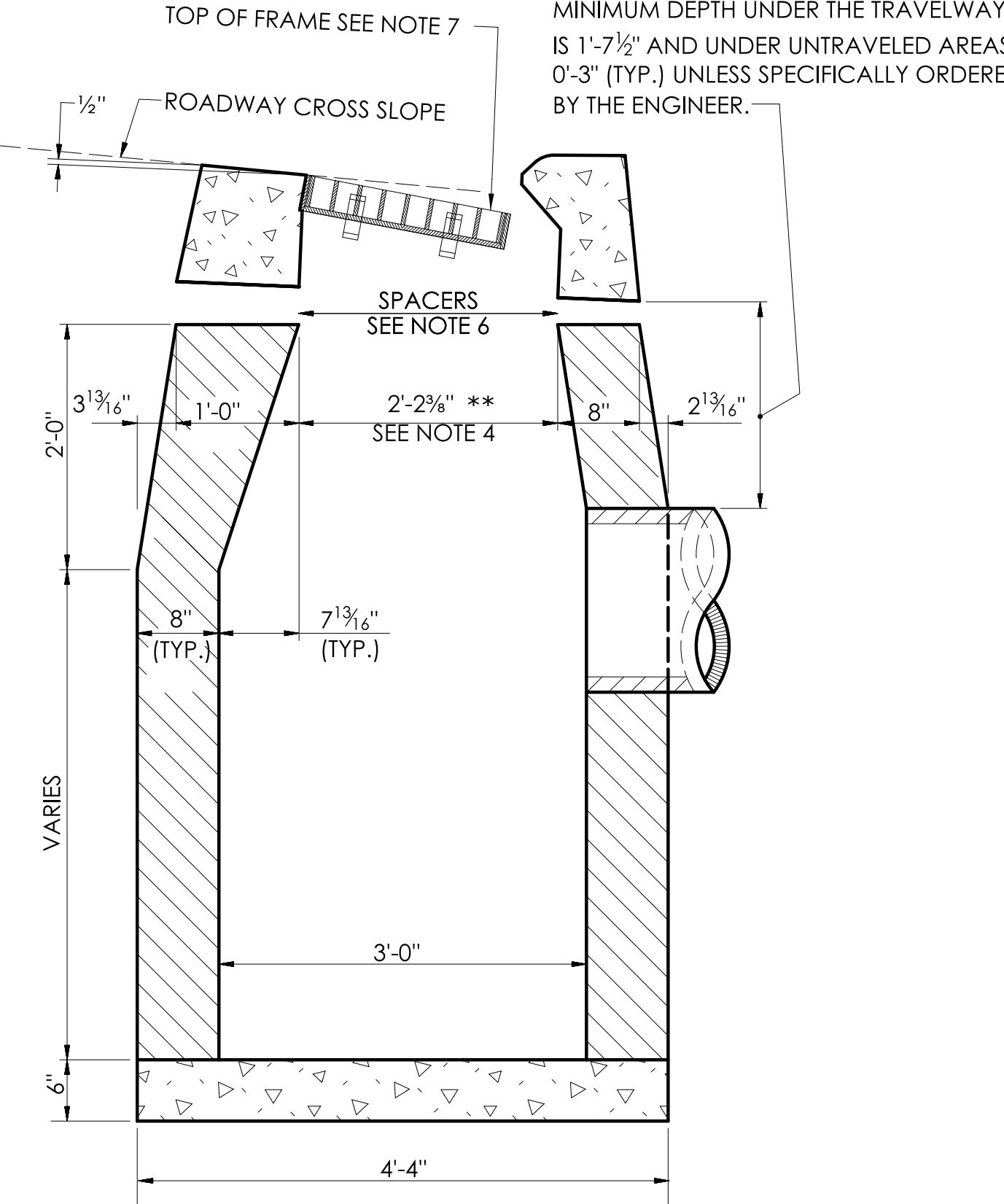
1. FOR CATCH BASIN TOPS, SEE SHEET NO. HW-586\_07.
2. ALL FACES OF STRUCTURES IN CONTACT WITH CONCRETE PAVEMENT SHALL BE COVERED WITH A LAYER OF TAR PAPER OR APPROVED EQUAL.
3. USE 6'-0" ON UPGRADE SIDE (SEE PLAN VIEW) OF CONTINUOUS GRADE AND 1'-0" ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED BY THE ENGINEER.
4. IF MASONRY UNITS ARE REQUIRED, THE BASIN SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE DIMENSIONS SHOWN. CORBELLING SHALL BE PERMITTED TO A MAXIMUM OF 3'. NO PROJECTION SHALL EXTEND INSIDE THE LIMITS FOR THE CATCH BASIN OPENINGS SHOWN IN THE SECTION VIEWS \*\*.
5. WALL THICKNESS OF ALL CATCH BASINS OVER 10' DEEP SHALL BE INCREASED TO 12" THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. 12" THICKNESS SHALL START AFTER THE FIRST 10'.
6. FOR GRADE ADJUSTMENT OF THE CATCH BASIN TOP (TO MATCH THE ROADWAYS' PROFILE AND CROSS-SLOPE SHOWN ON THE PLANS) USE SPACERS EITHER CONCRETE MASONRY UNIT OR PRECAST WITH THE REQUIRED REINFORCING (RECOMMENDED BY THE MANUFACTURER) COMBINED WITH MORTAR AS NEEDED TO PROVIDE THE PROPER GRADE ADJUSTMENTS.
7. TOP OF FRAME ELEVATION SHALL BE MEASURED IN THE CENTER OF GRATE AT GUTTER LINE.



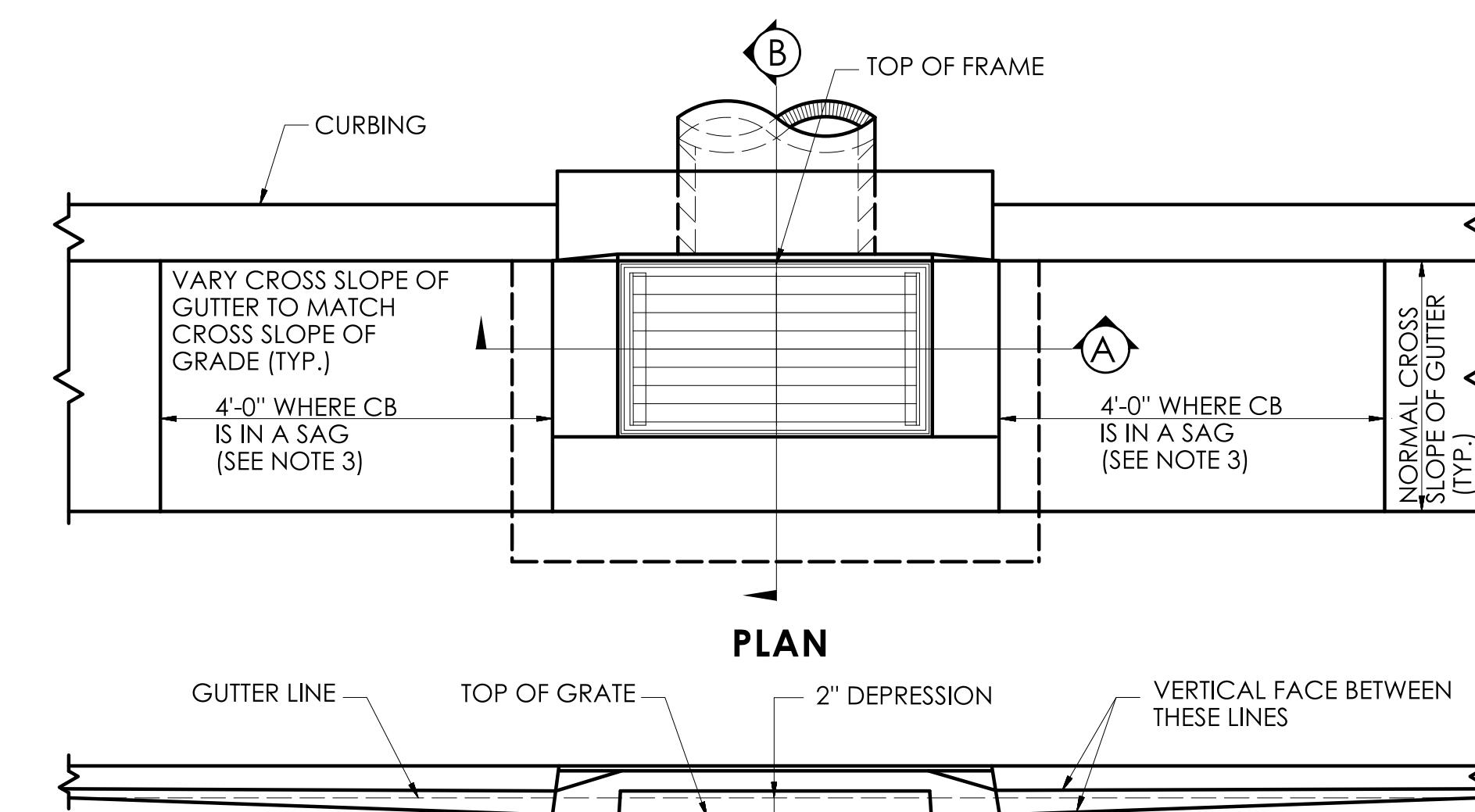
SECTION B  
TYPE "C-L" CATCH BASIN



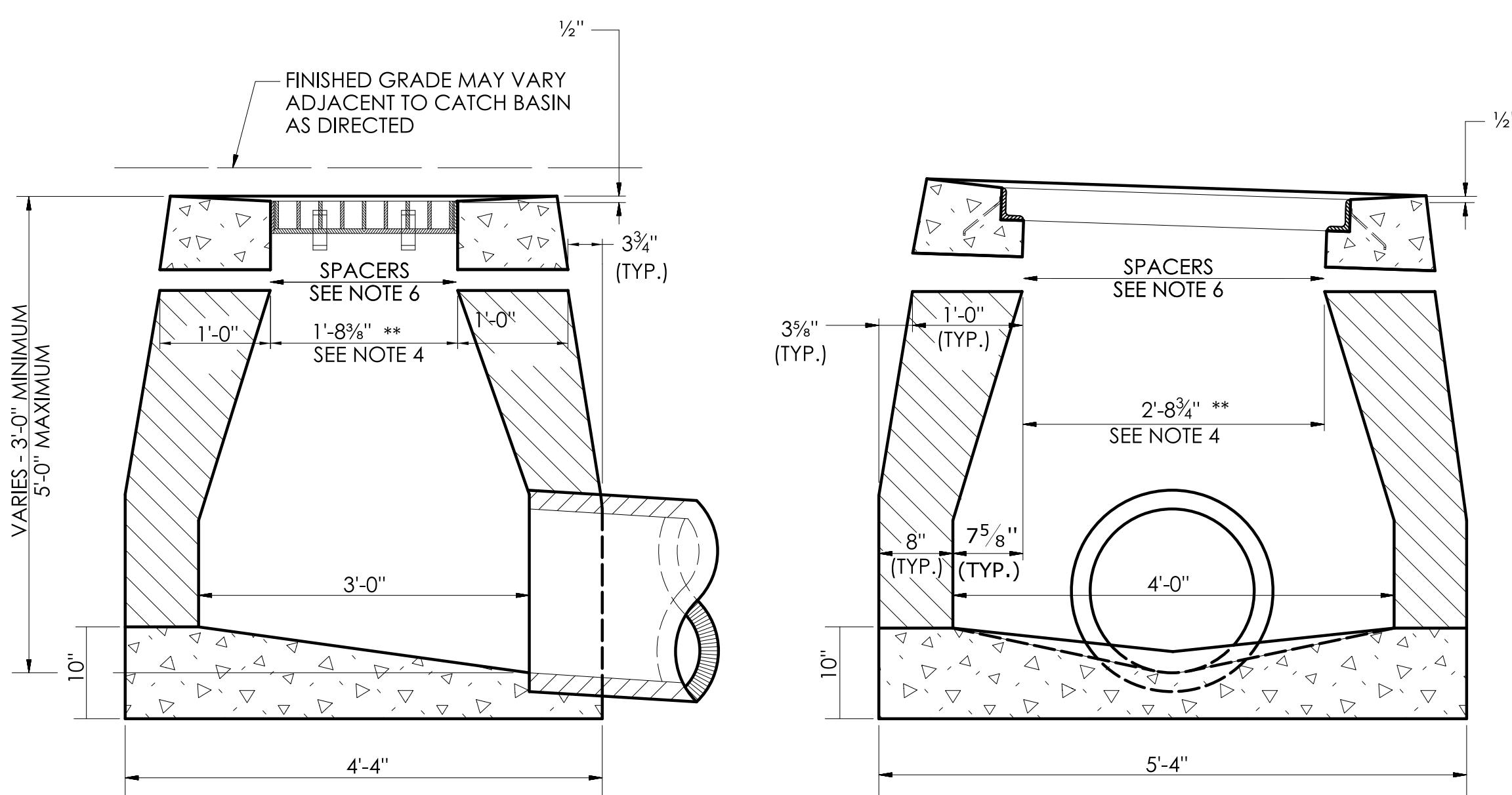
SECTION A  
TYPE "C" & "C-L" CATCH BASIN  
(TYPE "C" TOP SHOWN)



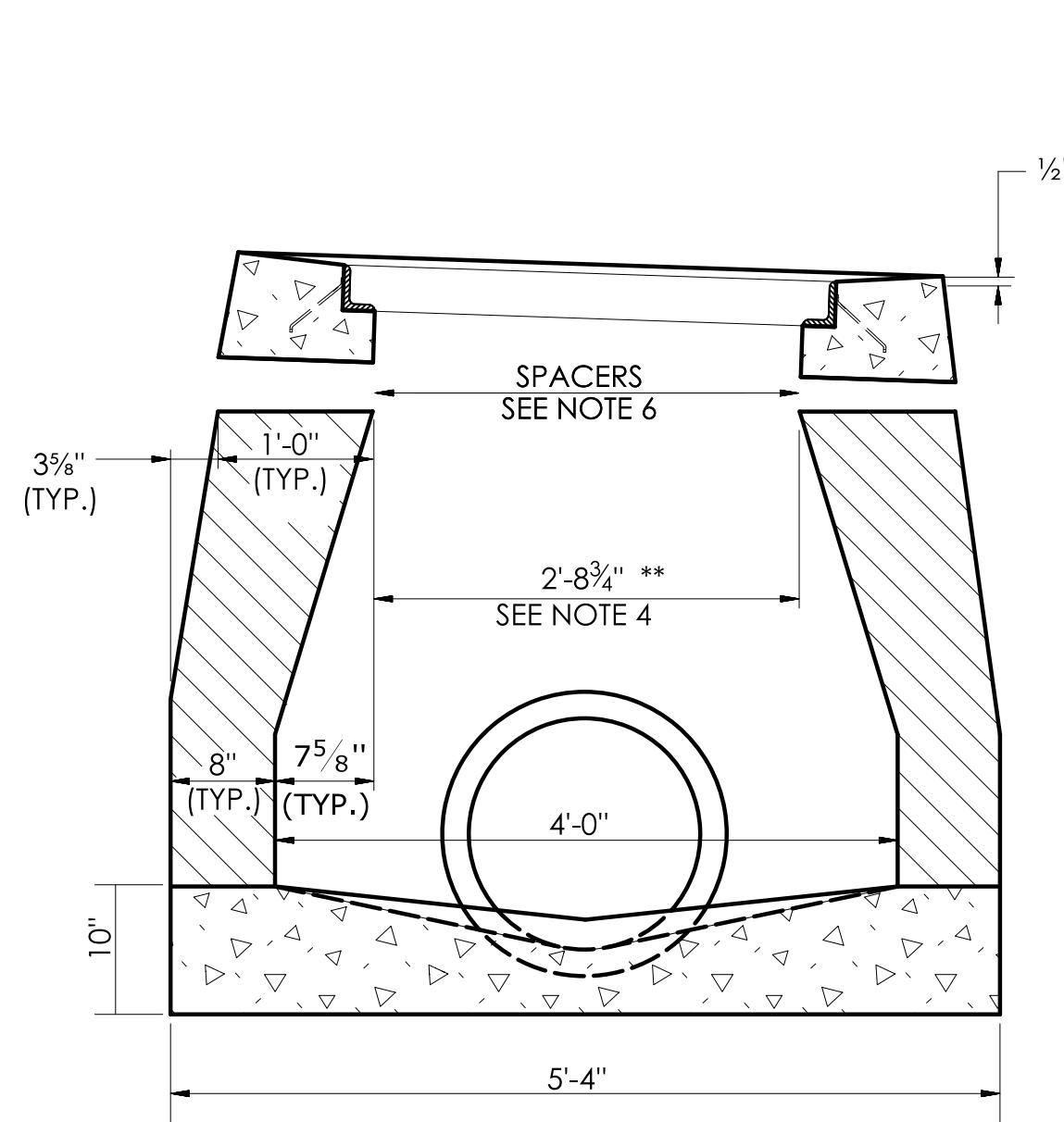
SECTION B  
TYPE "C" CATCH BASIN



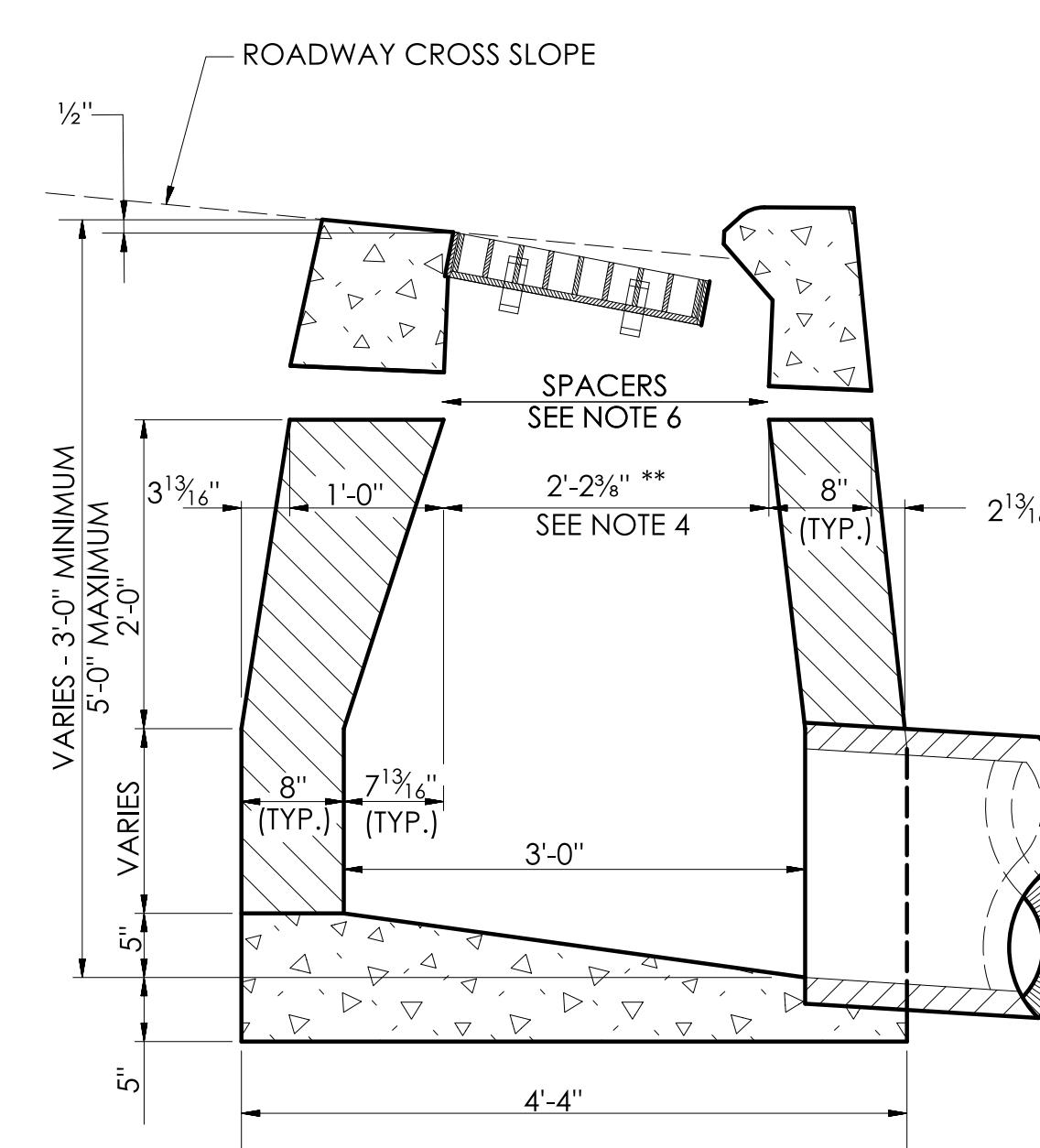
CATCH BASINS IN A LINE WITH 4" CONCRETE PARK CURBING OR 4" BITUMINOUS CONCRETE PARK CURBING



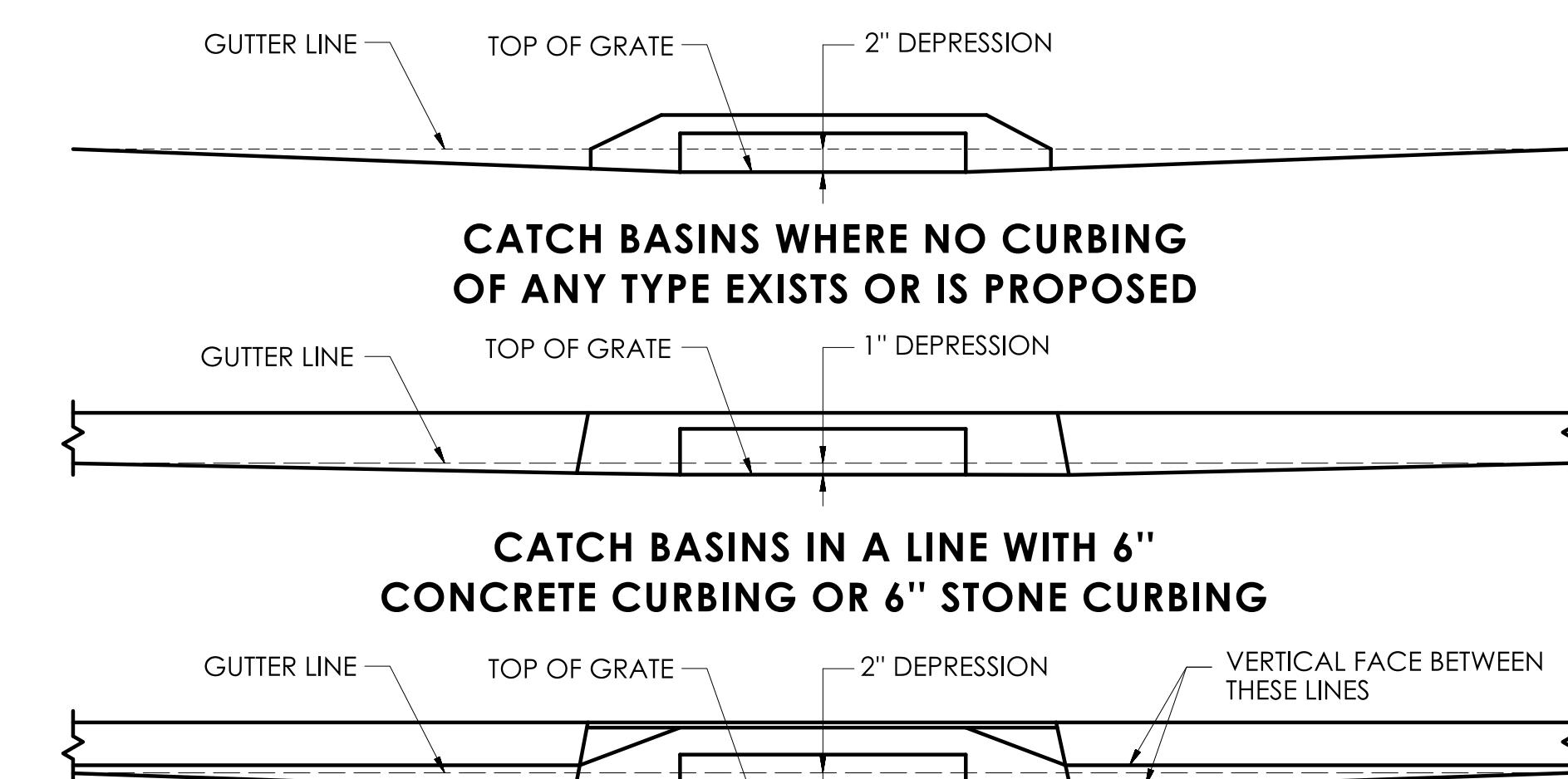
SECTION B  
TYPE "C-L" DROP INLET



SECTION A  
TYPE "C" & "C-L" DROP INLET  
(TYPE "C-L" TOP SHOWN)

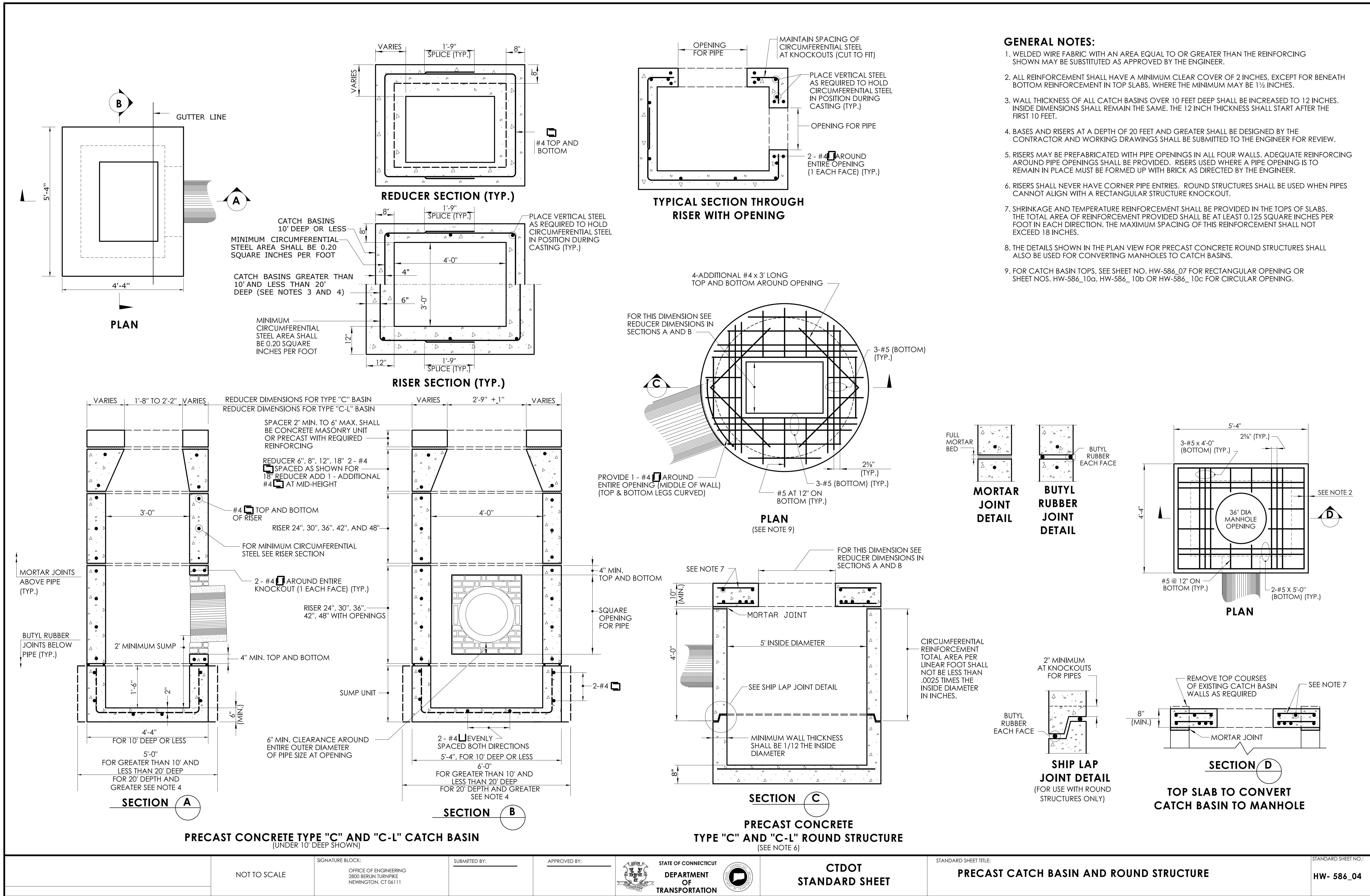


SECTION B  
TYPE "C" DROP INLET



DETAILS OF DEPRESSED GUTTER STRIP  
FOR TYPE "C" CATCH BASIN

NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: <b>CATCH BASIN AND DROP INLET TYPES "C" AND "C-L" STRUCTURES</b>	STANDARD SHEET NO.: <b>HW- 586_01</b>
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**GENERAL NOTES:**

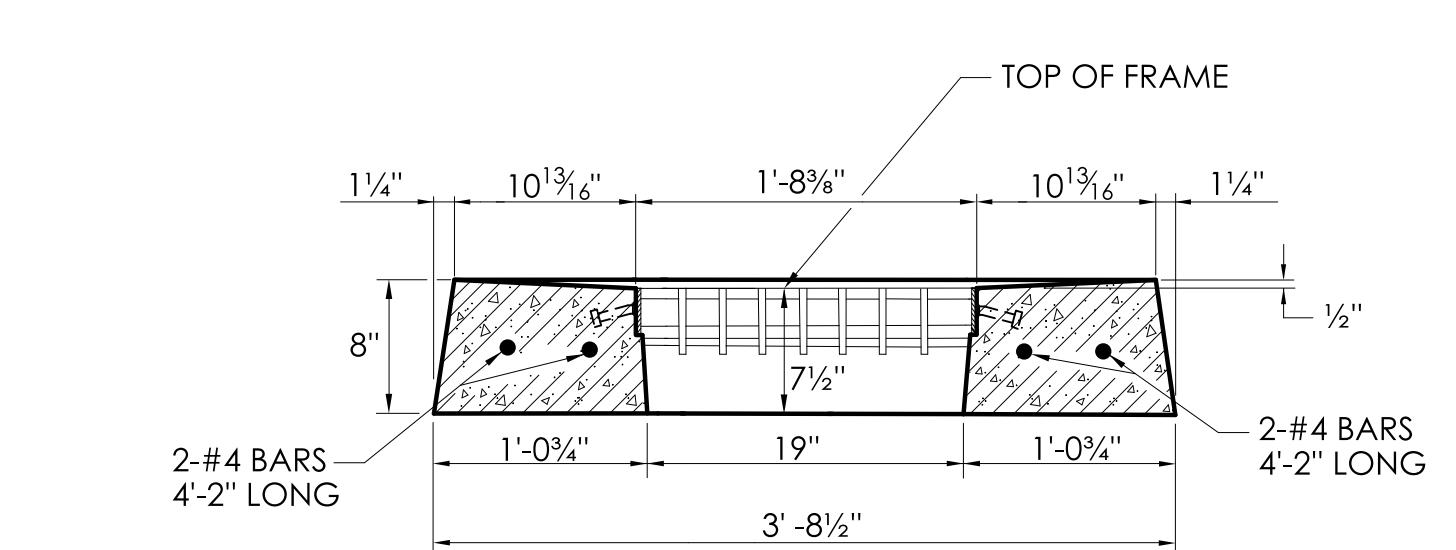
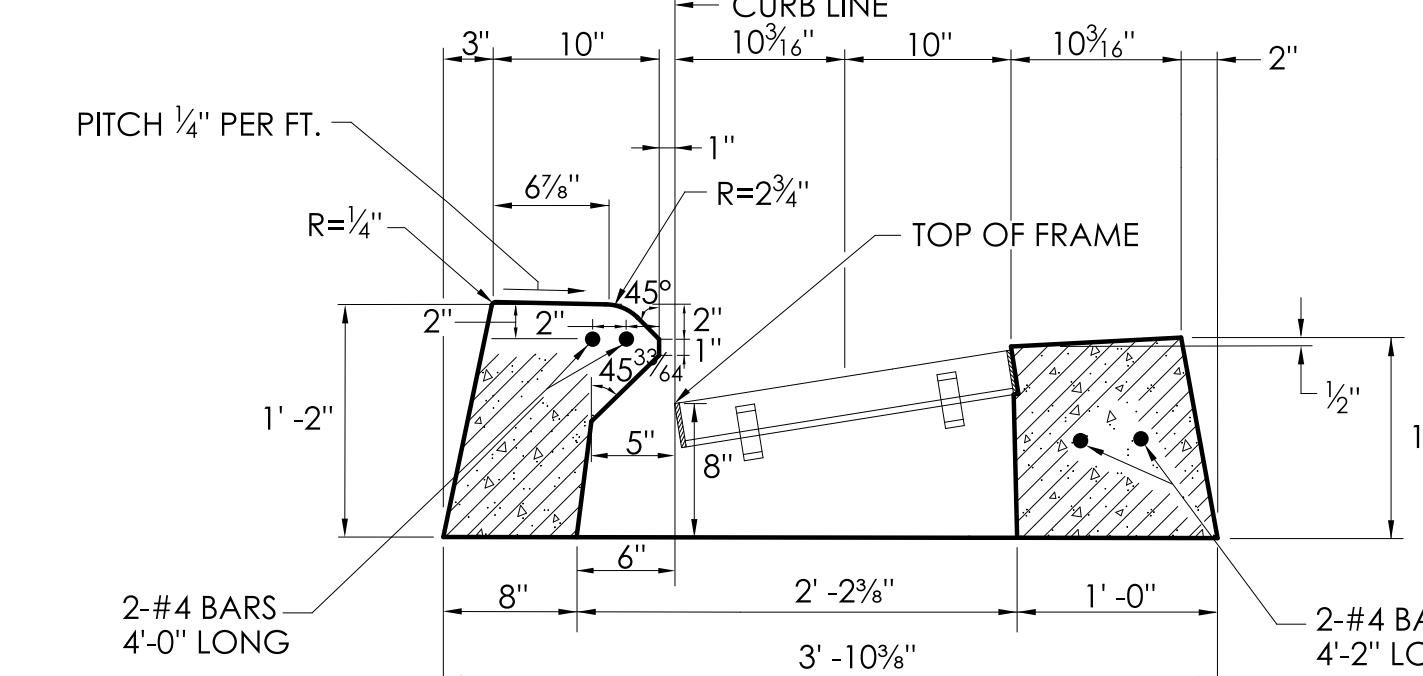
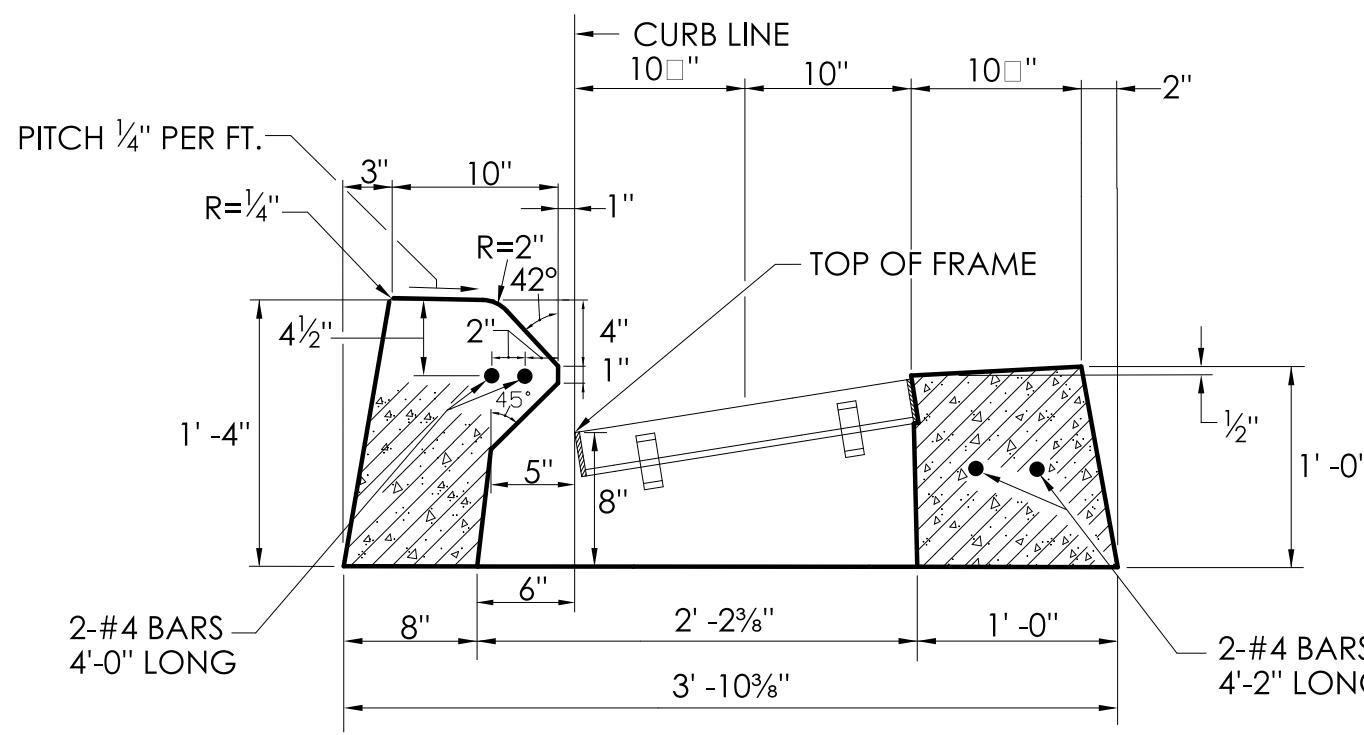
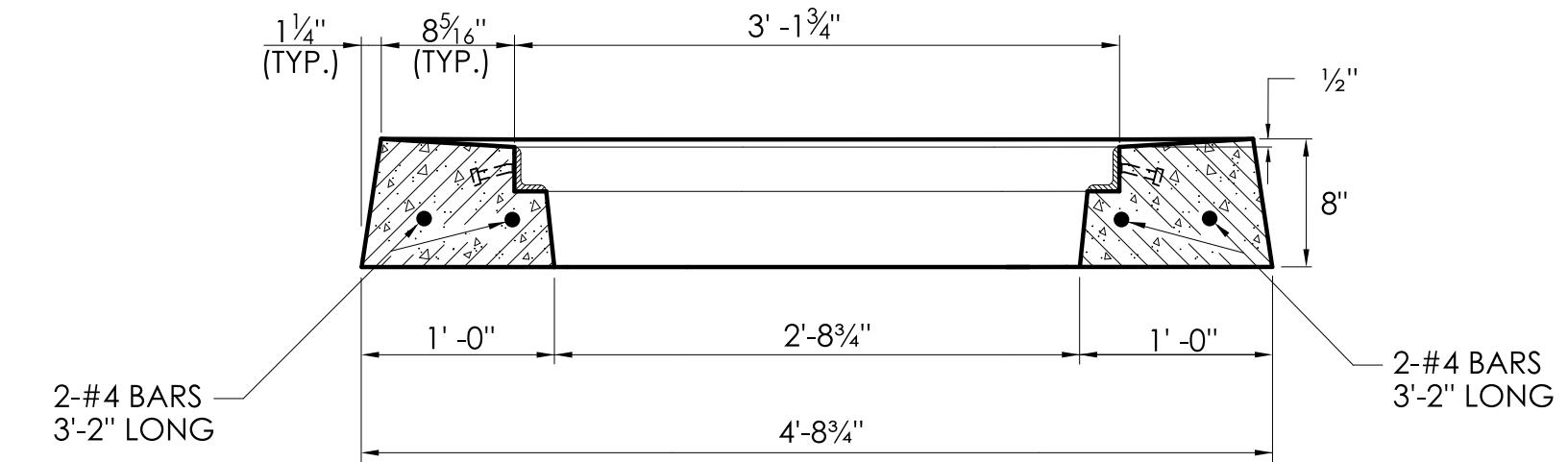
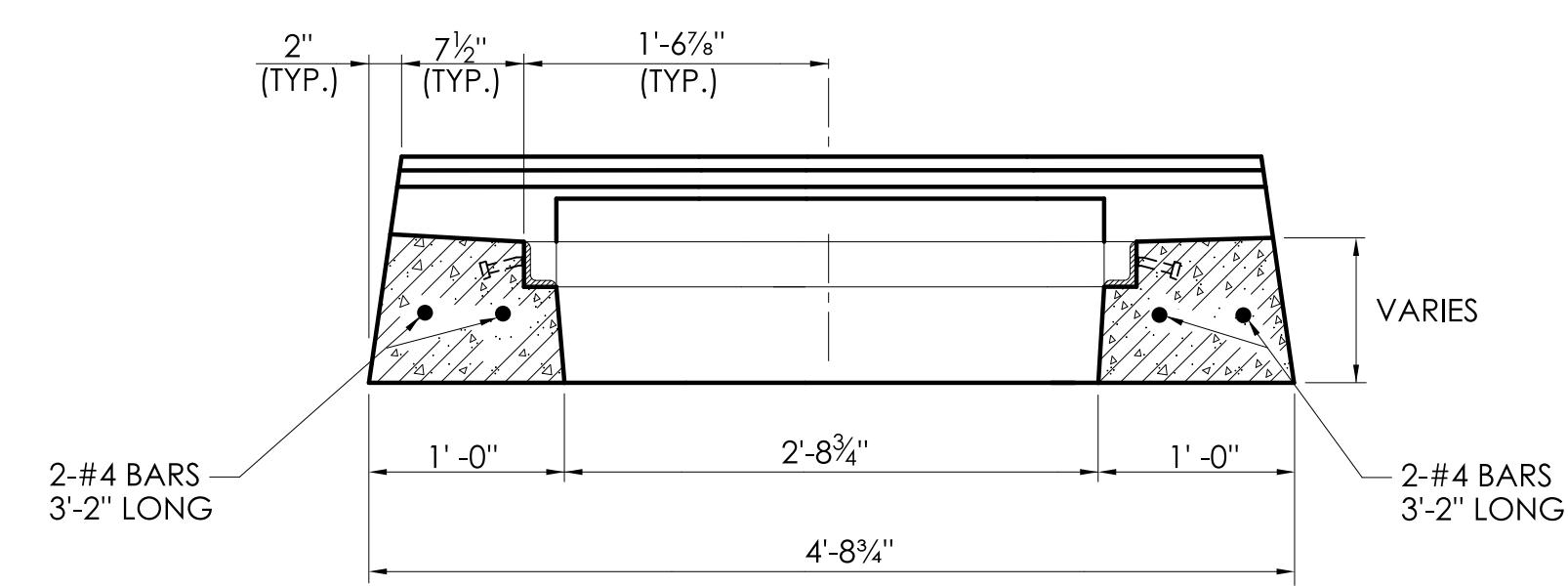
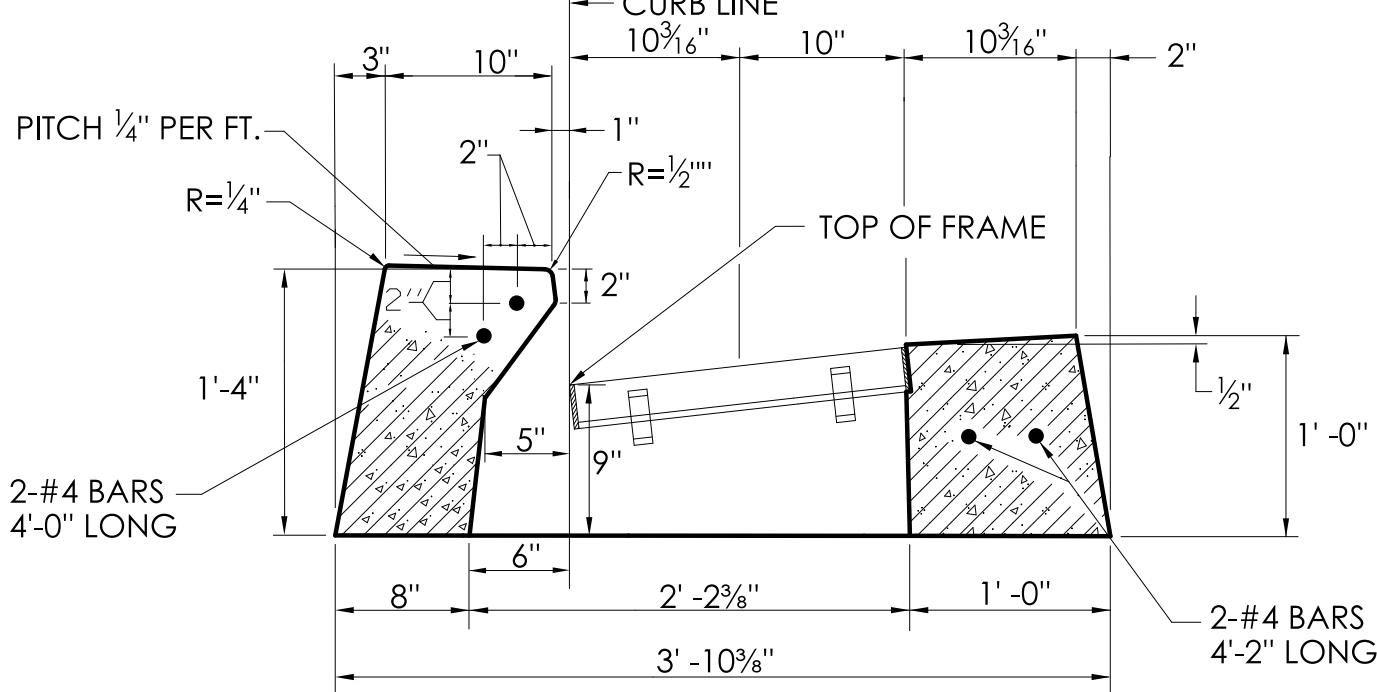
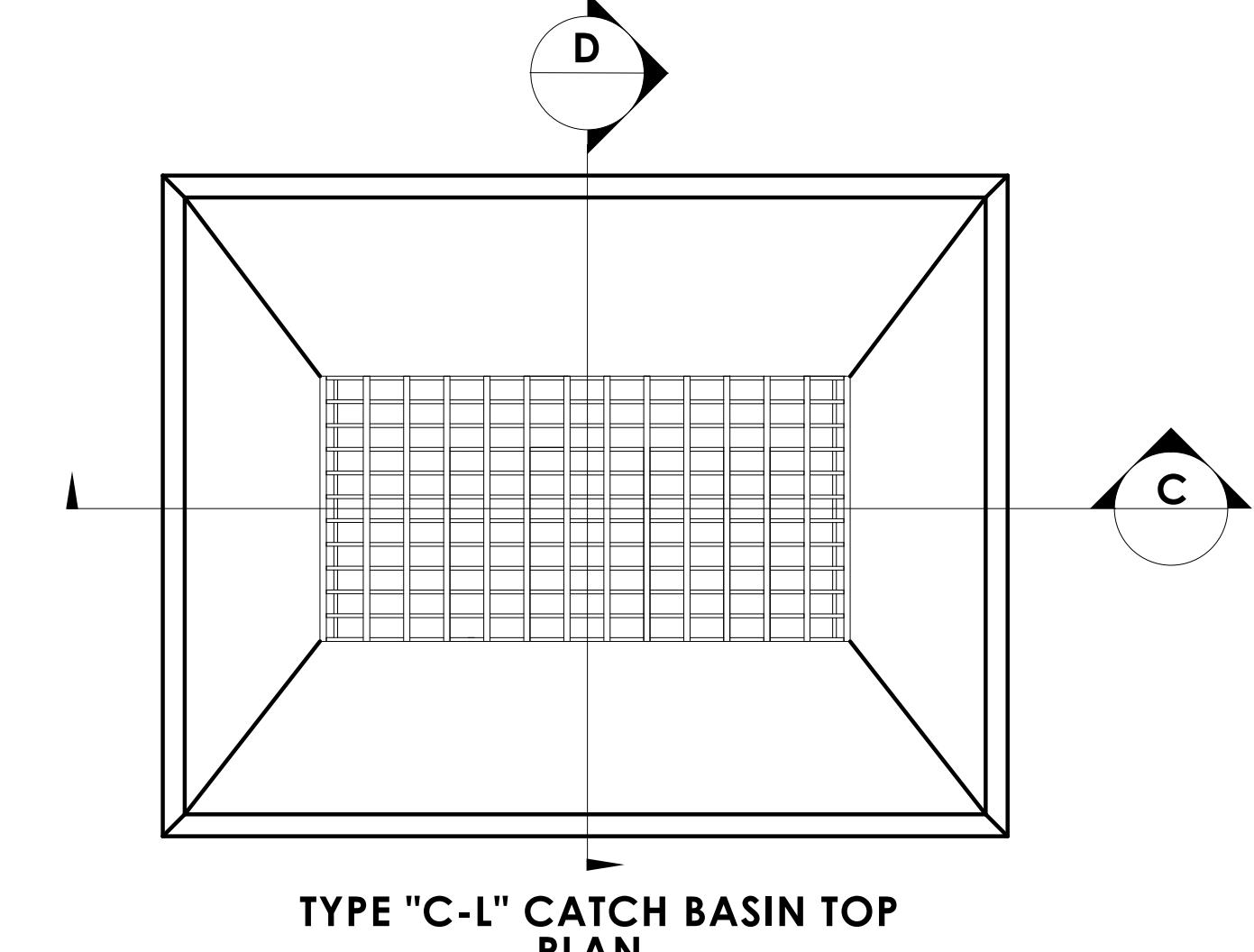
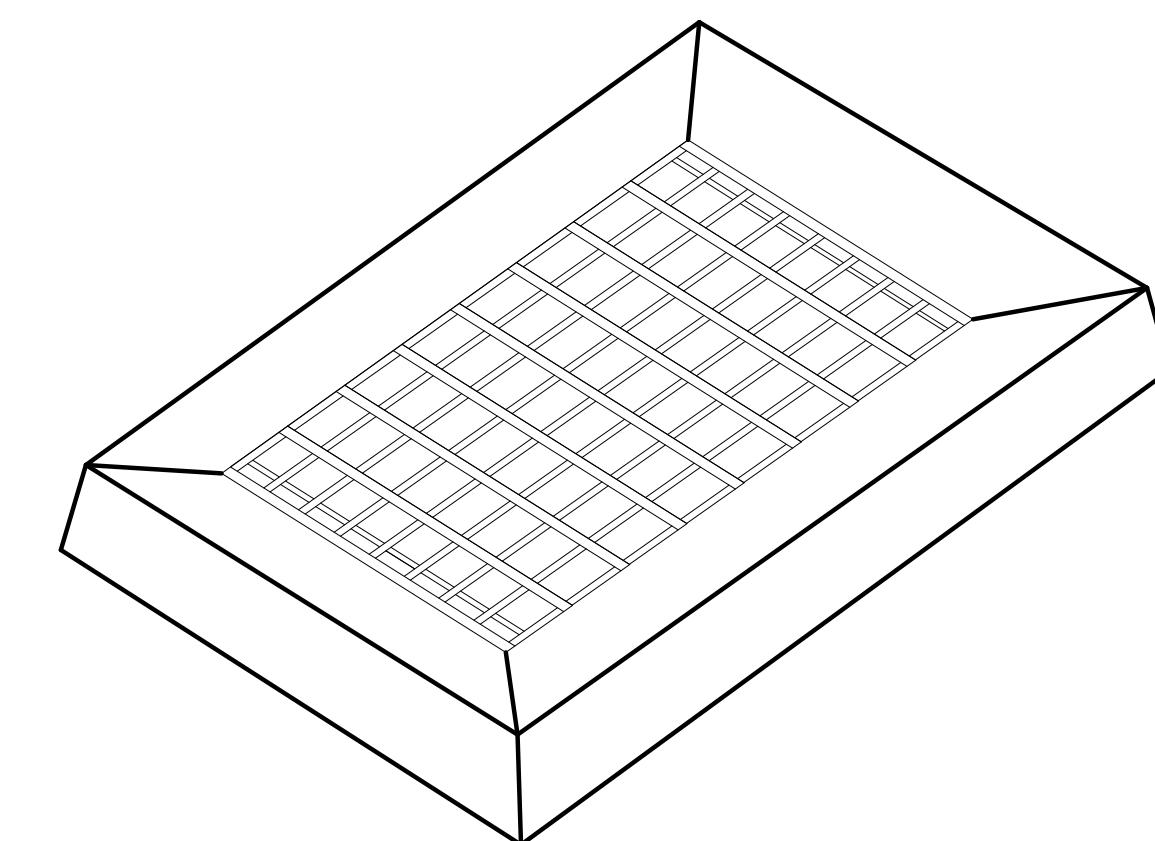
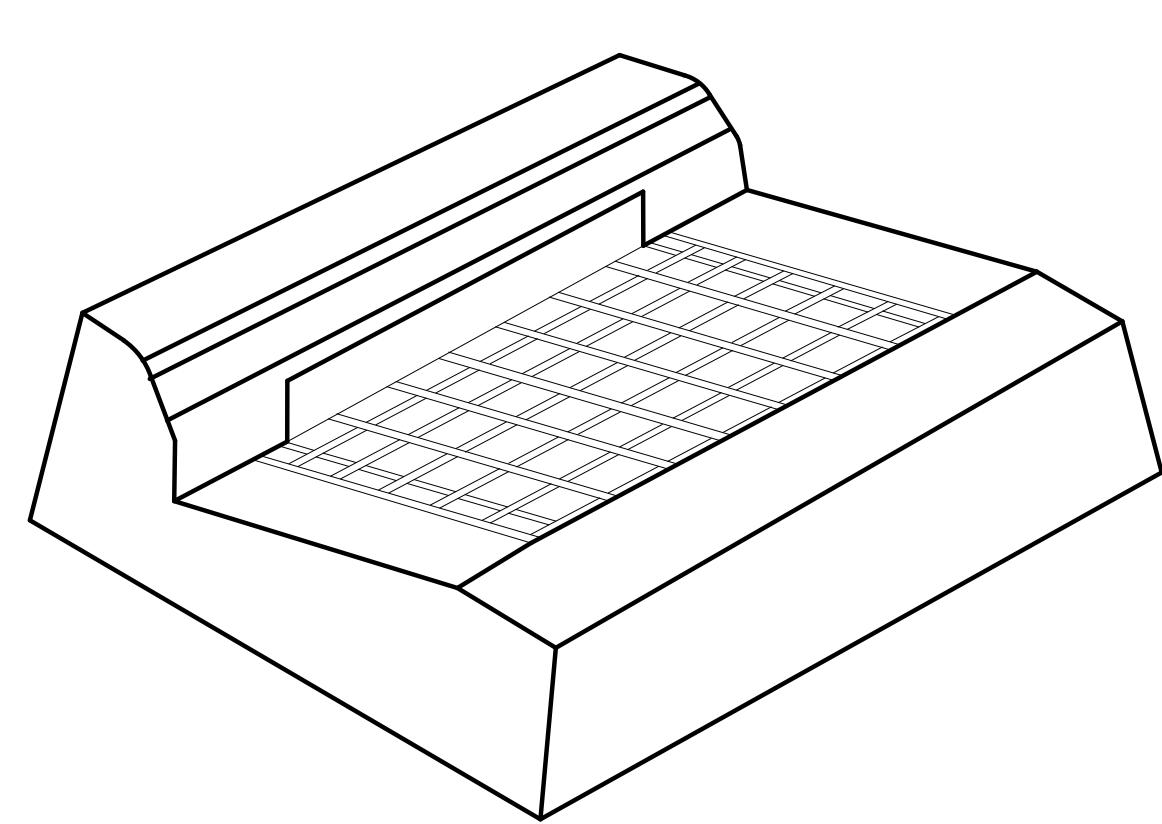
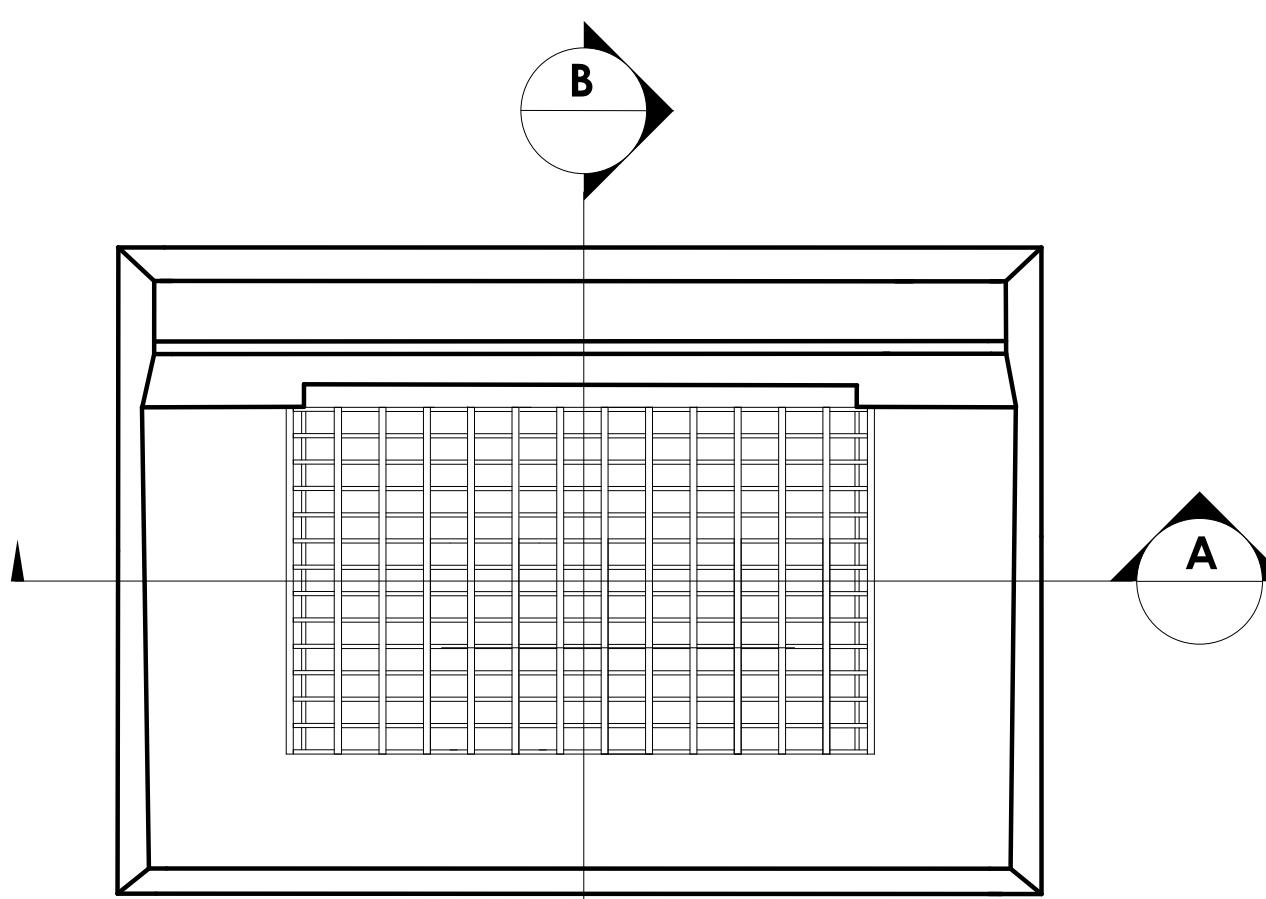
1. SEE SHEET HW-586\_08, FOR CATCH BASIN FRAMES AND GRATES AND HW-586\_09 FOR CATCH BASIN LOCK DOWN TOPS.

2. SEE SHEET HW-586\_01, CATCH BASIN AND DROP INLET TYPES "C" AND "C-L" TO DETERMINE THE TOP OF FRAME DEPRESSION AT THE GUTTER.

3. ALL BARS SHALL HAVE A MINIMUM 2" COVER.

4. Manufacturing Dimensional Tolerance Table

Any Dimension (D)	Allowable Tolerance
D < 5"	$\pm \frac{1}{8}$ "
5" $\leq$ D $\leq$ 10"	$\pm \frac{1}{16}$ "
D > 10"	$\pm \frac{1}{16}$ "



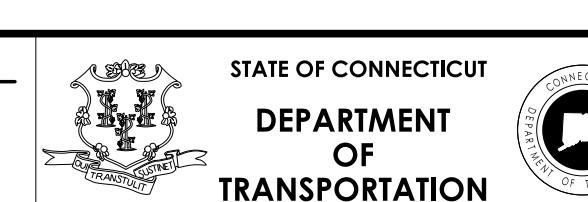
NOT TO SCALE

NOT TO SCALE

SIGNATURE BLOCK:  
OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

SUBMITTED BY: \_\_\_\_\_

APPROVED BY: \_\_\_\_\_



CTDOT  
STANDARD SHEET

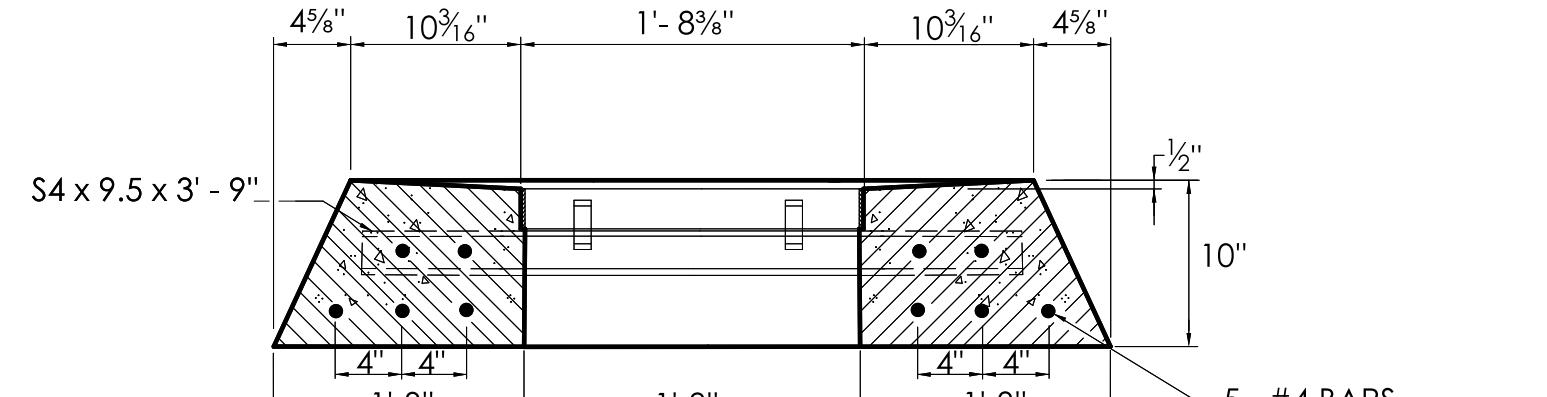
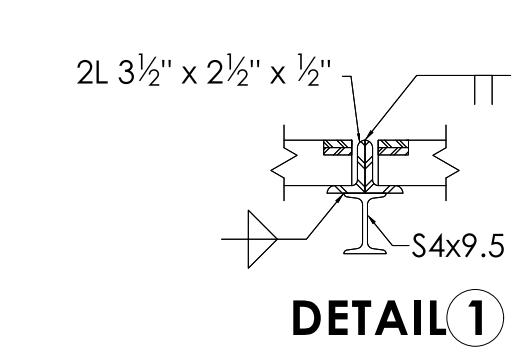
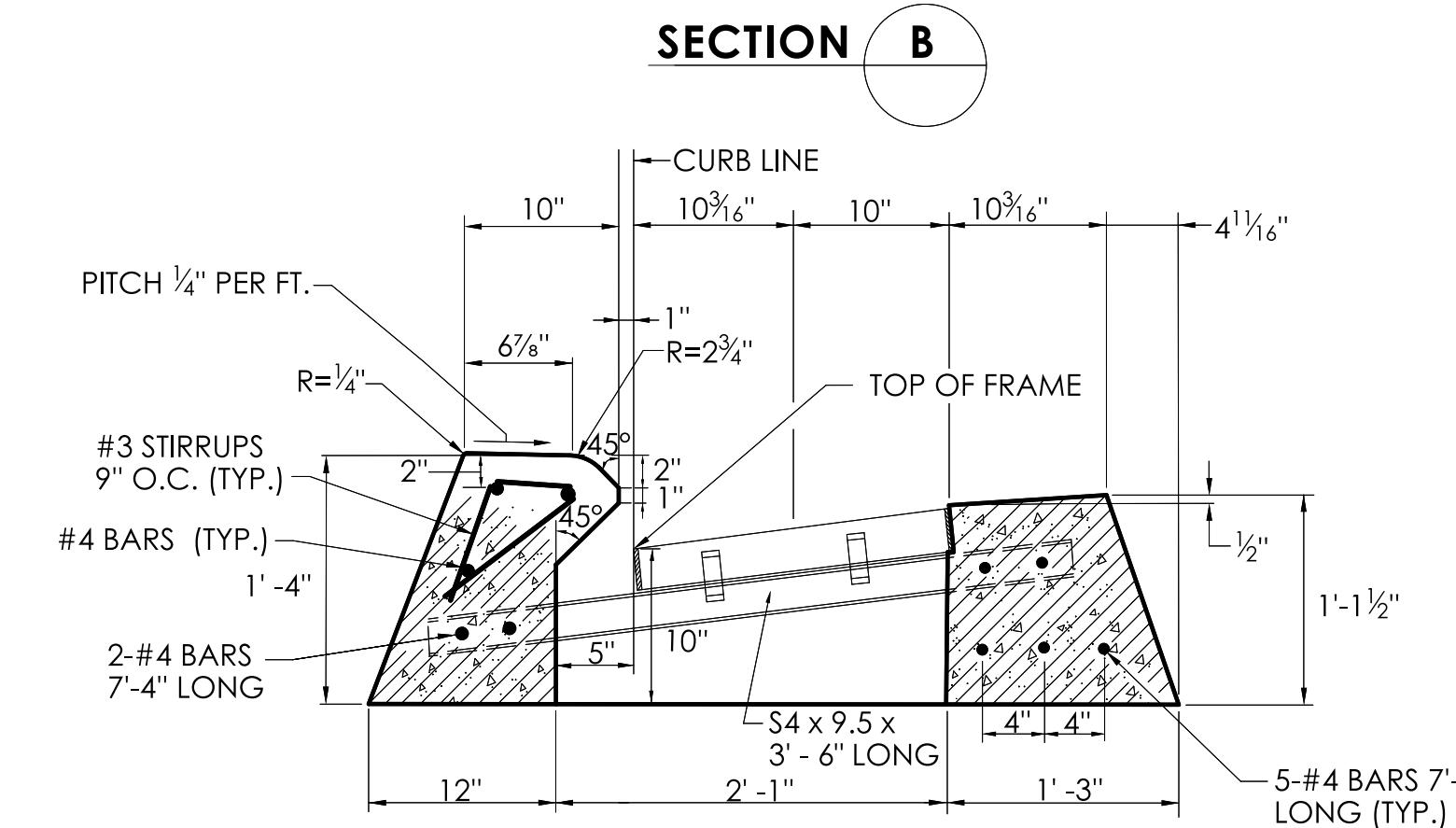
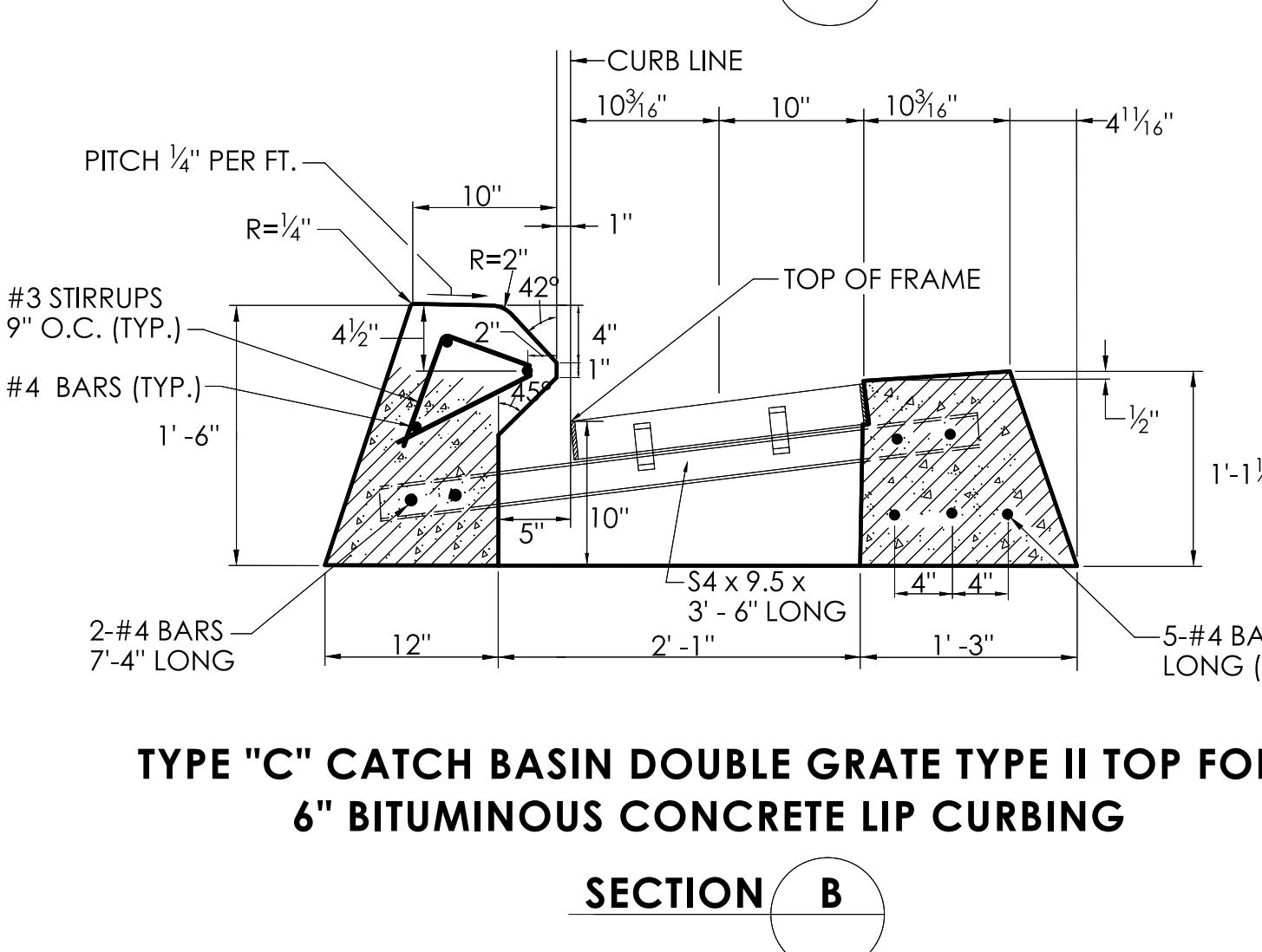
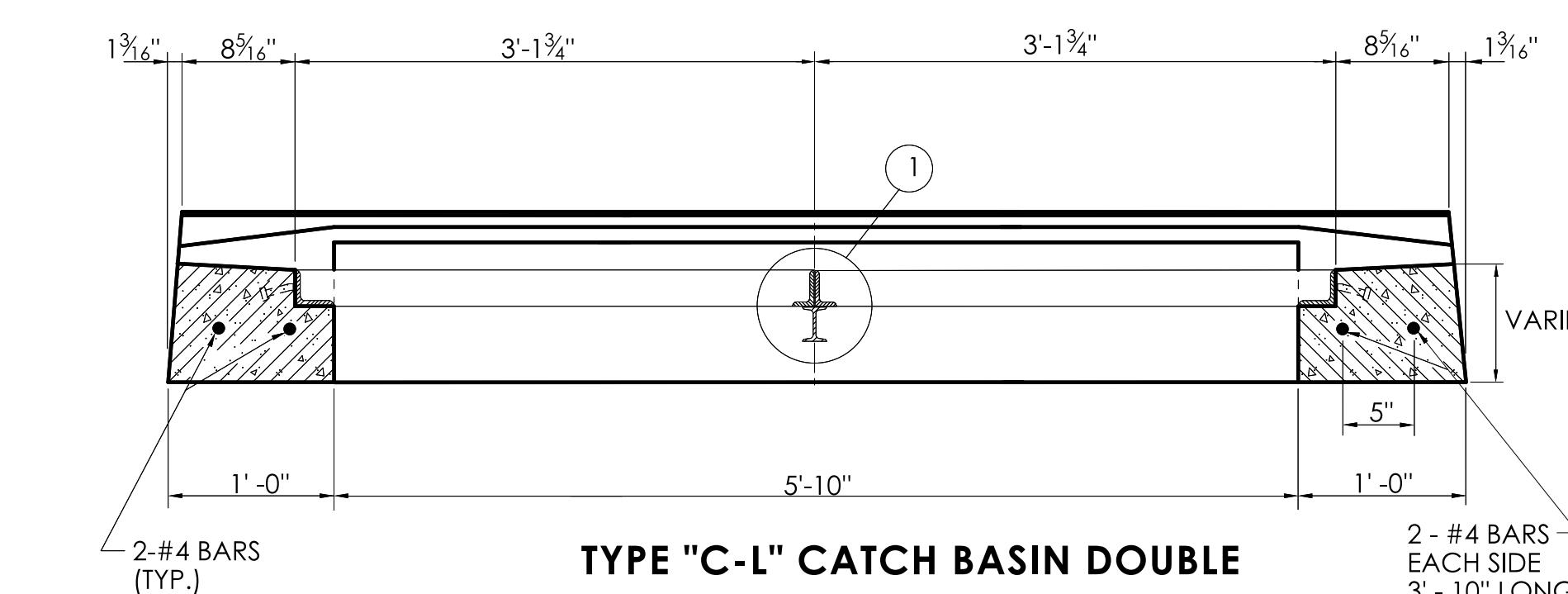
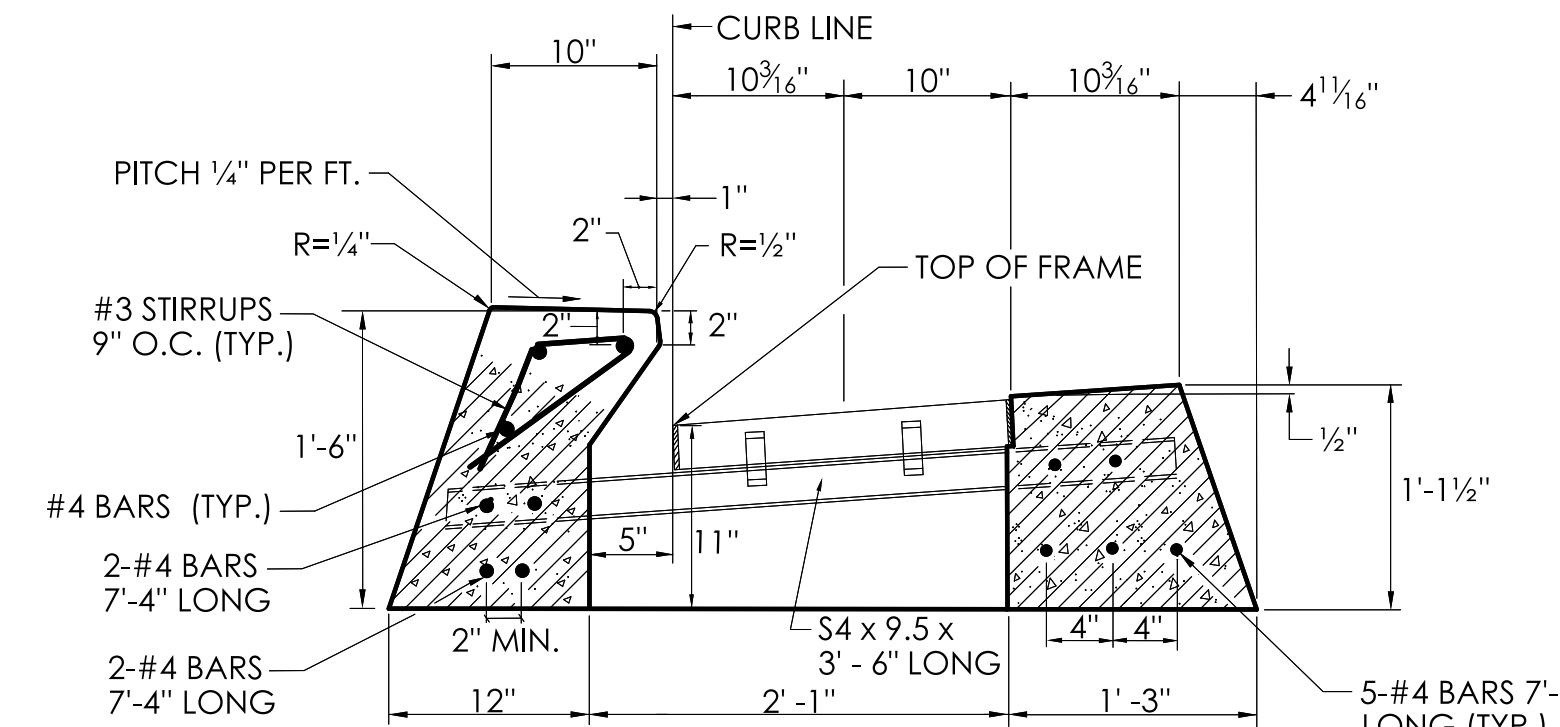
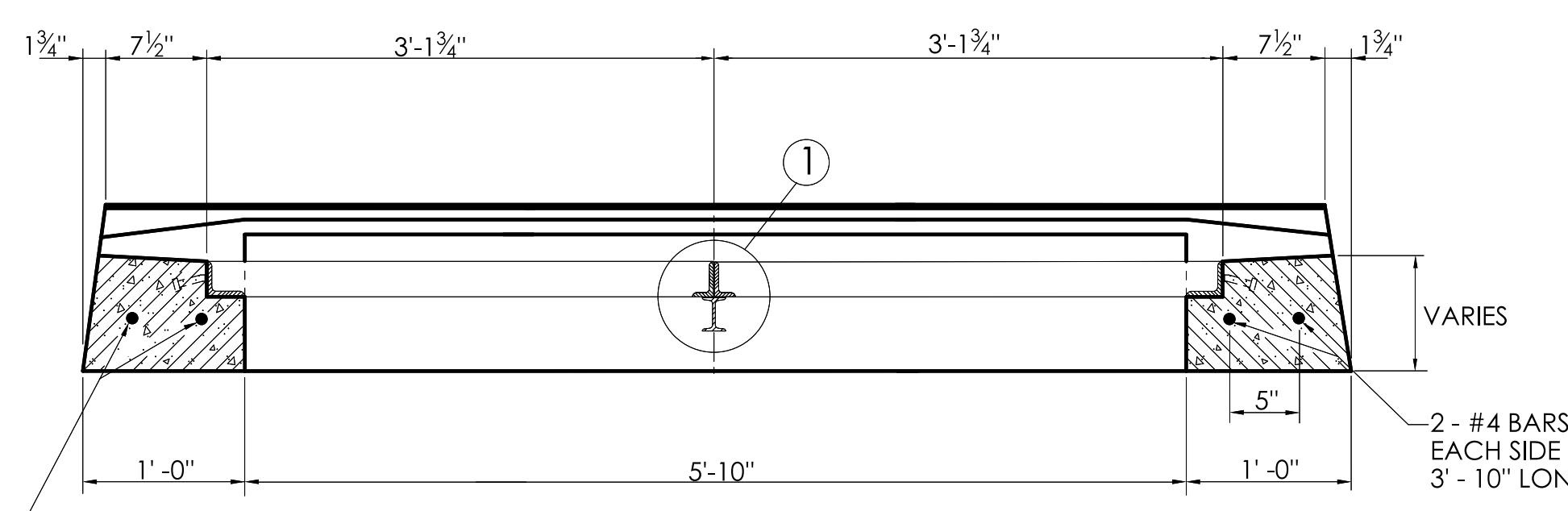
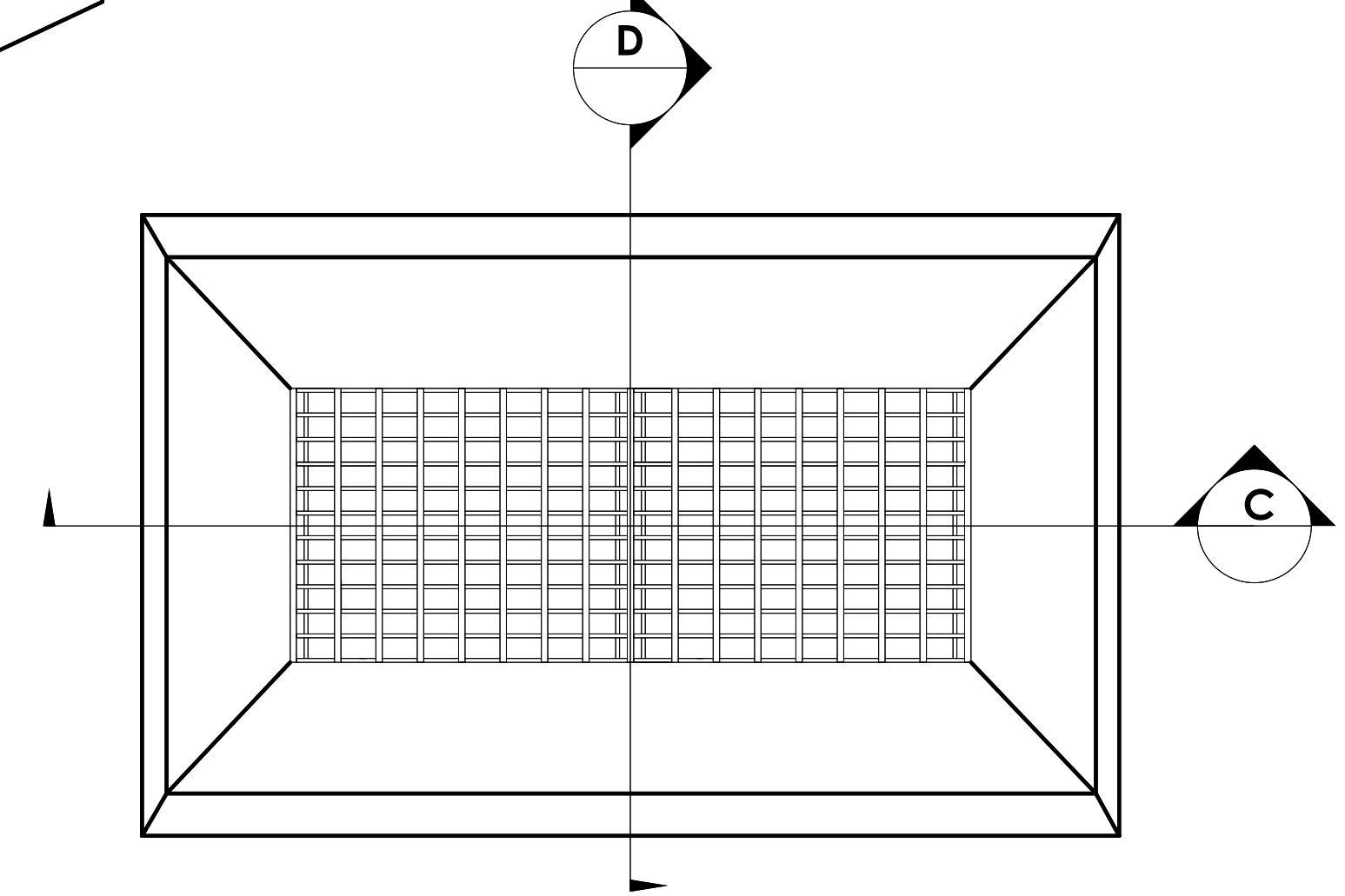
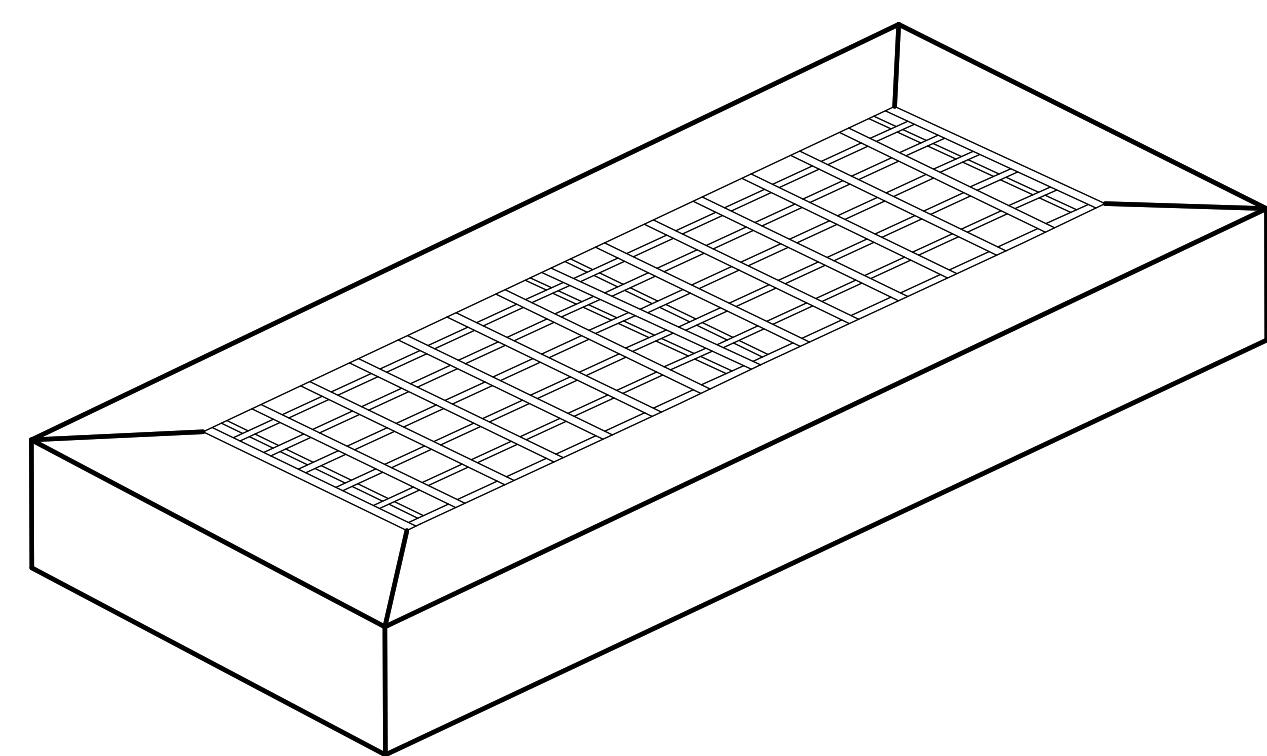
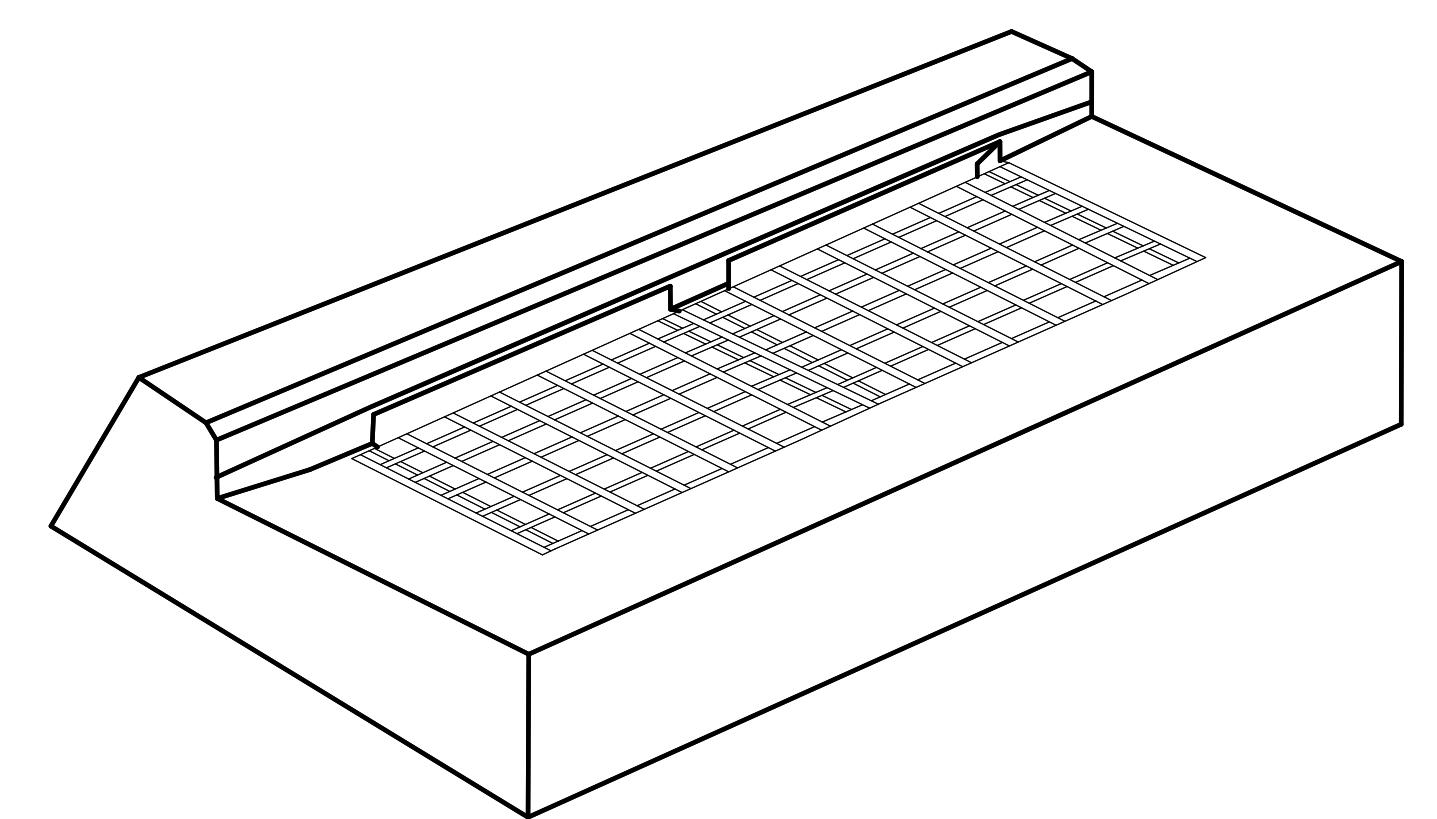
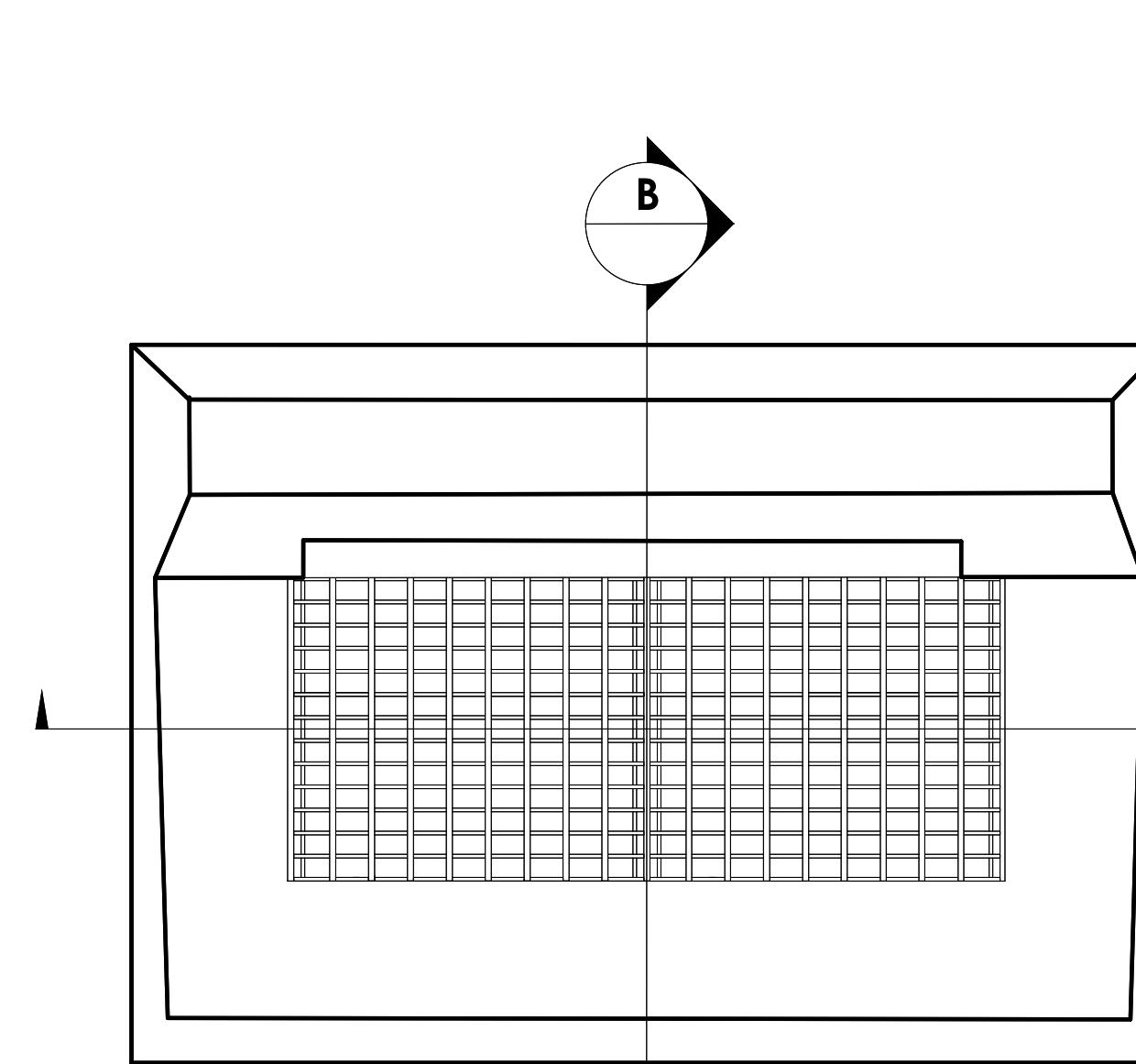
STANDARD SHEET TITLE:  
**CATCH BASIN TYPE "C" AND "C-L" TOPS**

STANDARD SHEET NO.:  
**HW- 586\_07a**

### GENERAL NOTES:

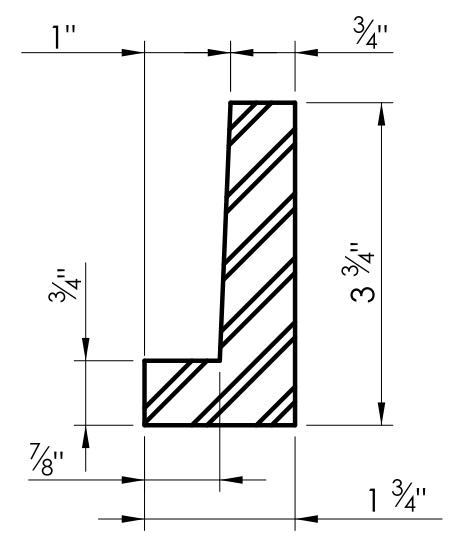
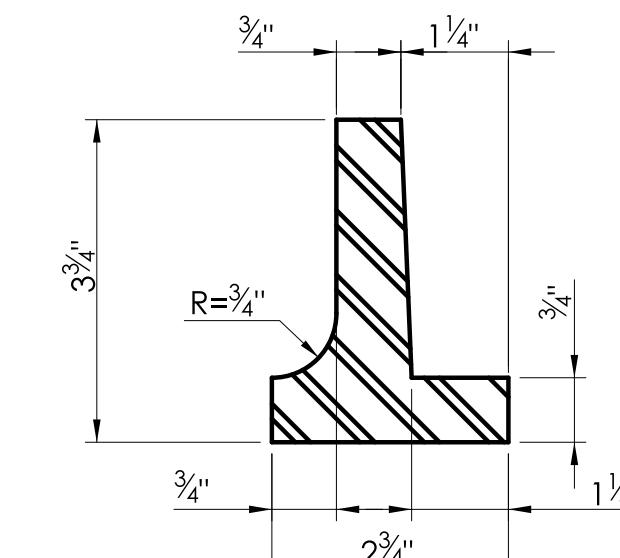
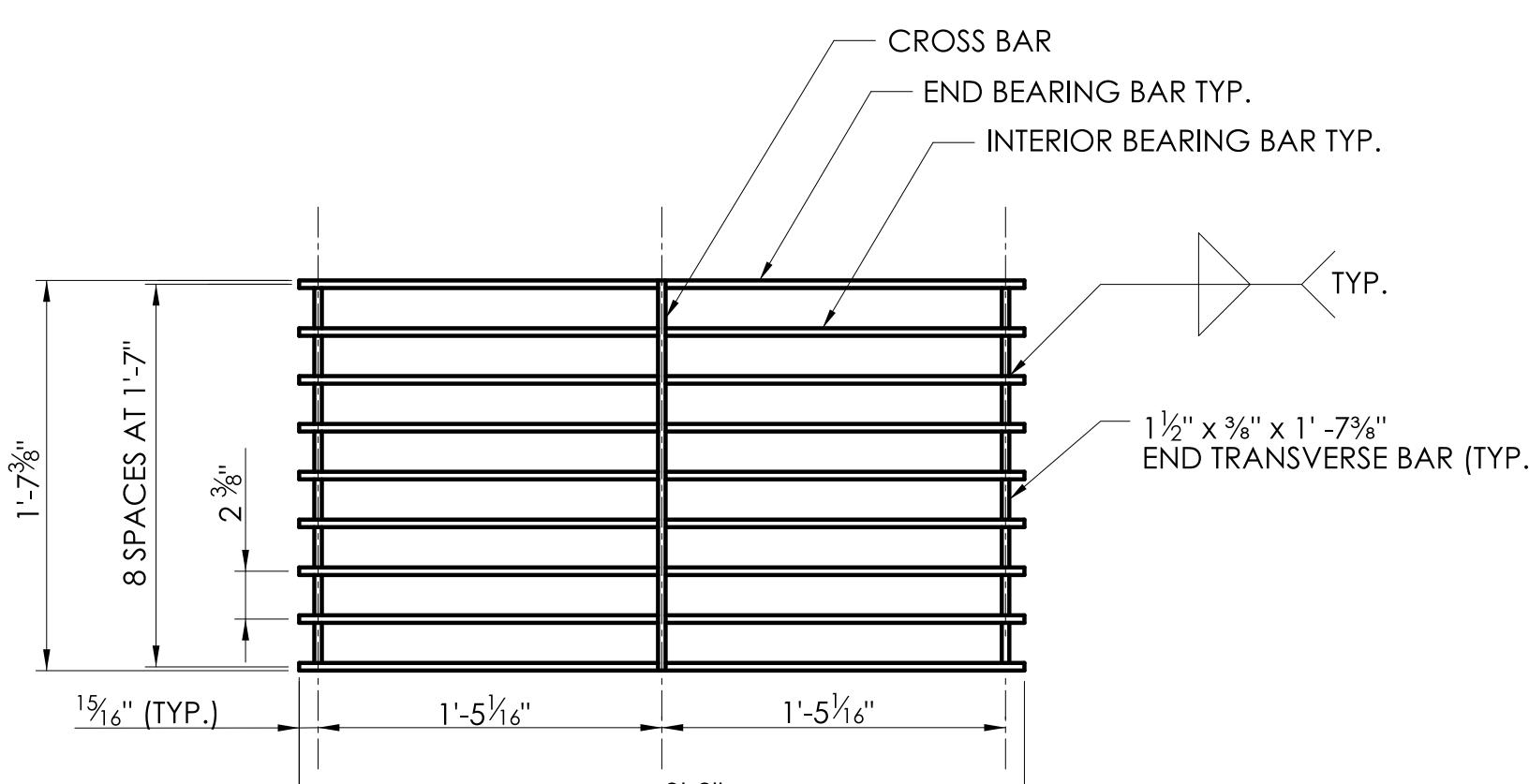
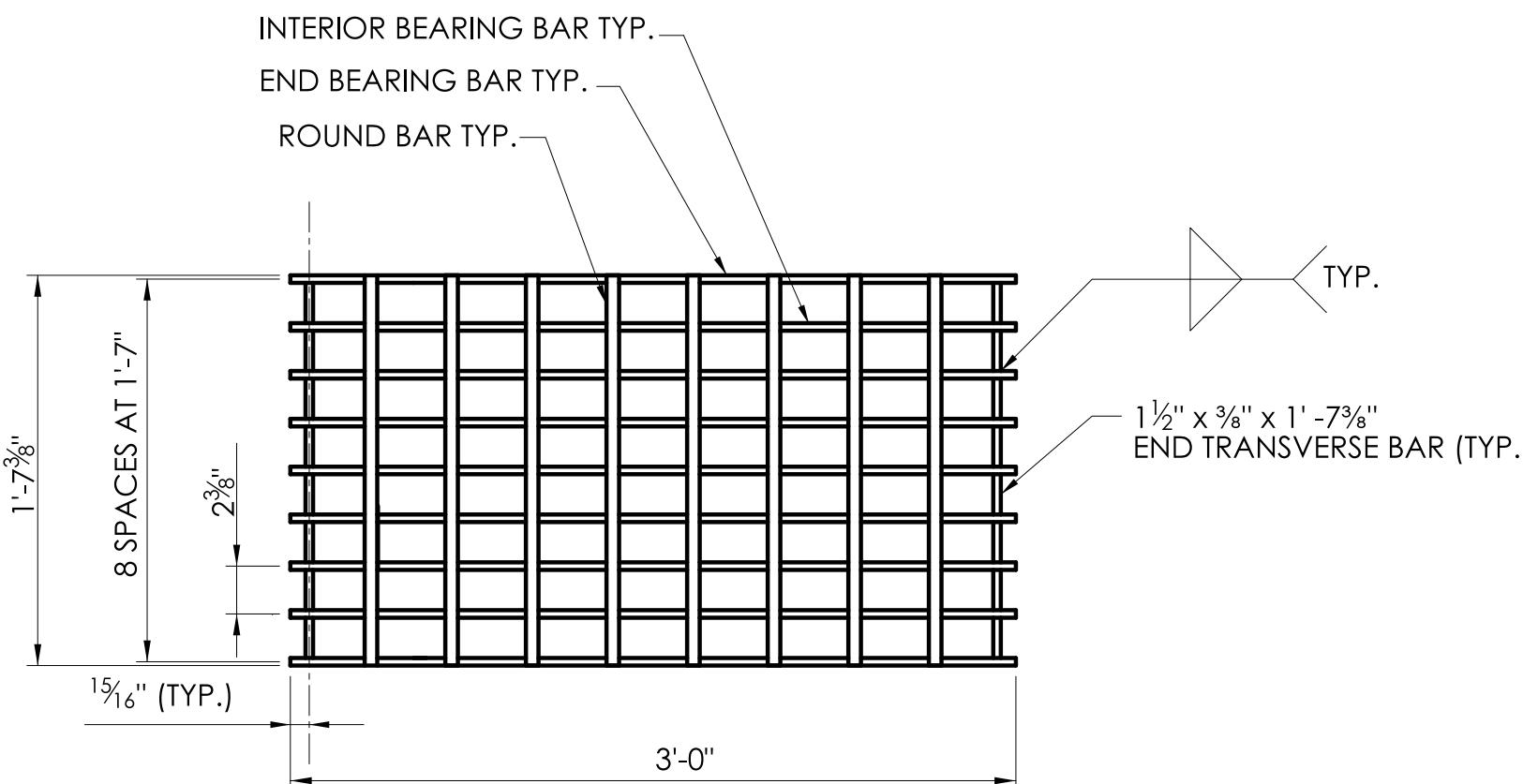
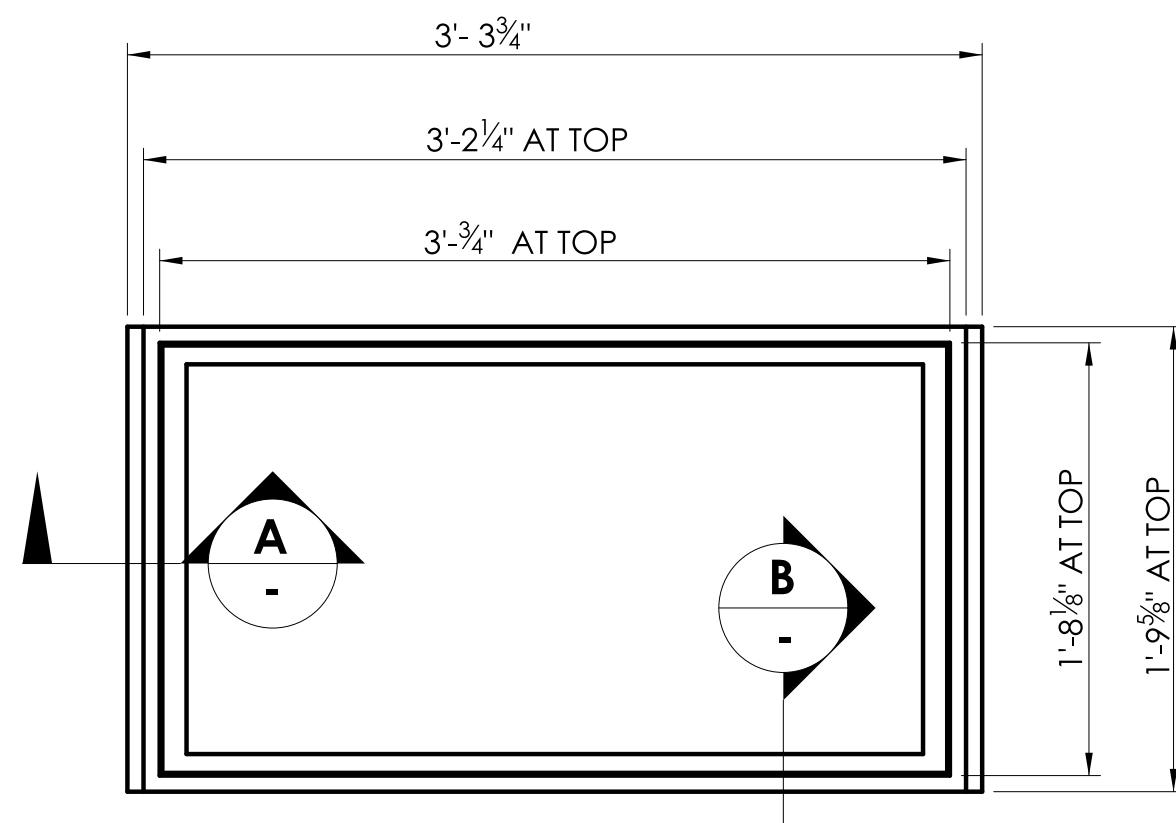
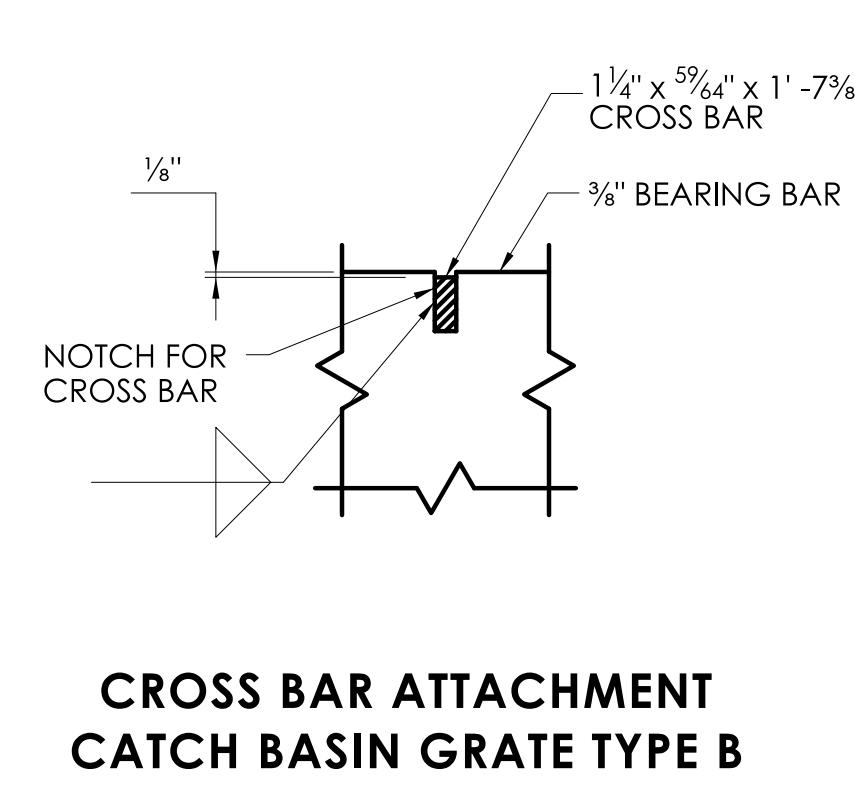
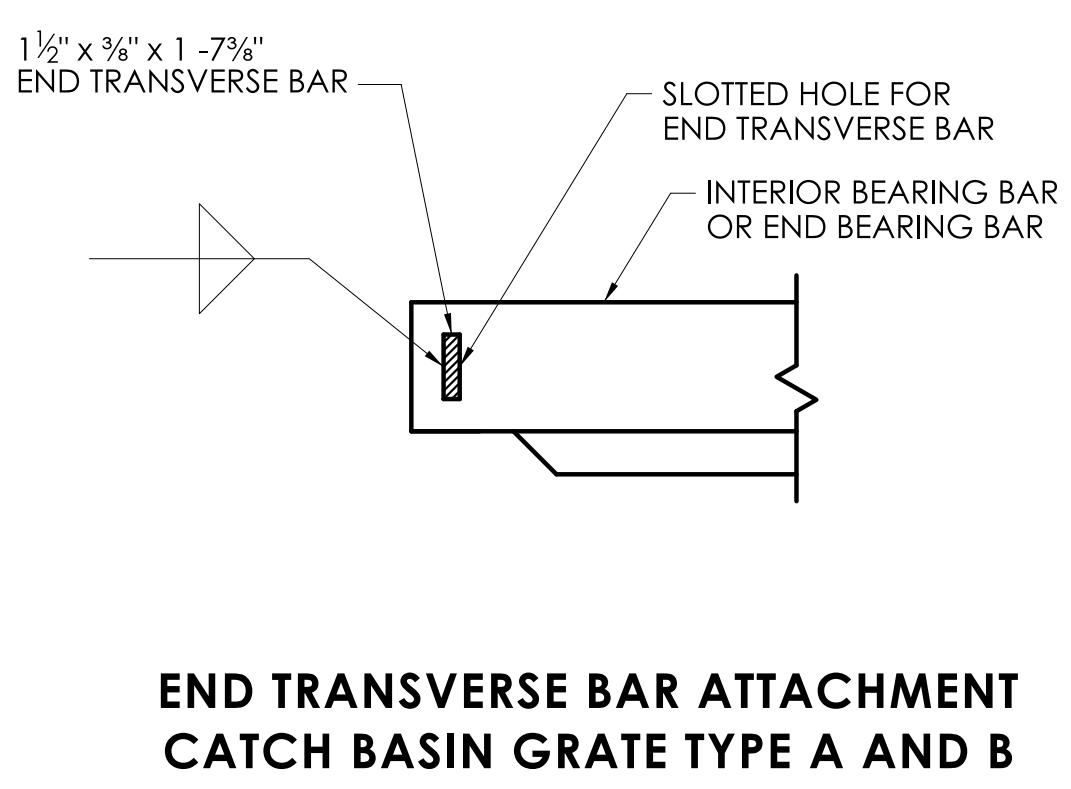
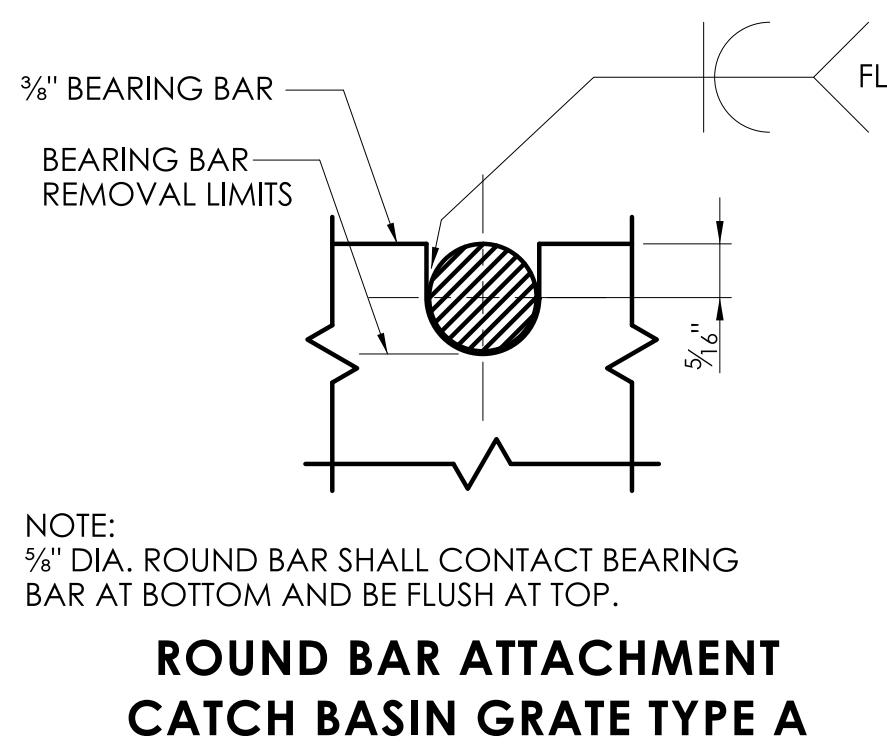
1. SEE SHEET HW-586\_08 FOR CATCH BASIN FRAMES AND GRATES, AND HW-586\_09 CATCH BASIN LOCK DOWN TOPS.
2. SEE SHEET HW-586\_03 CATCH BASIN TOPS ( TYPE "C" AND "C-L" ) FOR DOUBLE GRATE TYPE II, TO DETERMINE THE TOP OF FRAME AT THE GUTTER.
3. ALL BARS SHALL HAVE A MINIMUM 2" COVER
4. MANUFACTURING DIMENSIONAL TOLERANCE TABLE

Any Dimension (D)	Allowable Tolerance
D < 5'	$\pm \frac{1}{4}''$
5' ≤ D ≤ 10'	$\pm \frac{1}{8}''$
D > 10'	$\pm 1''$

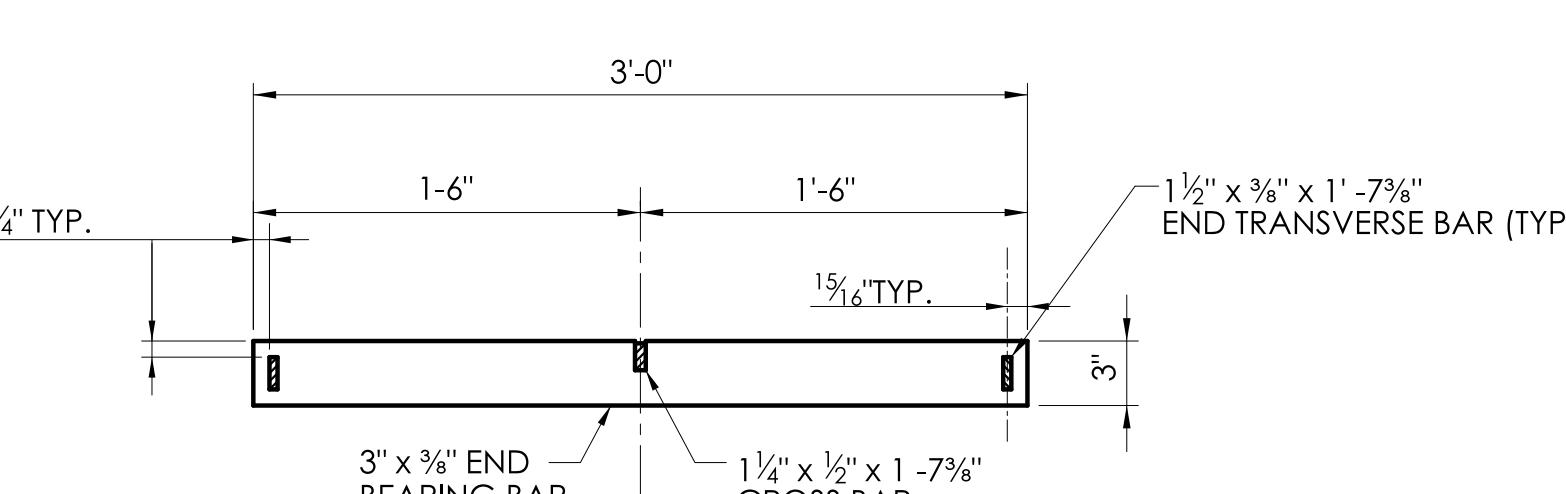
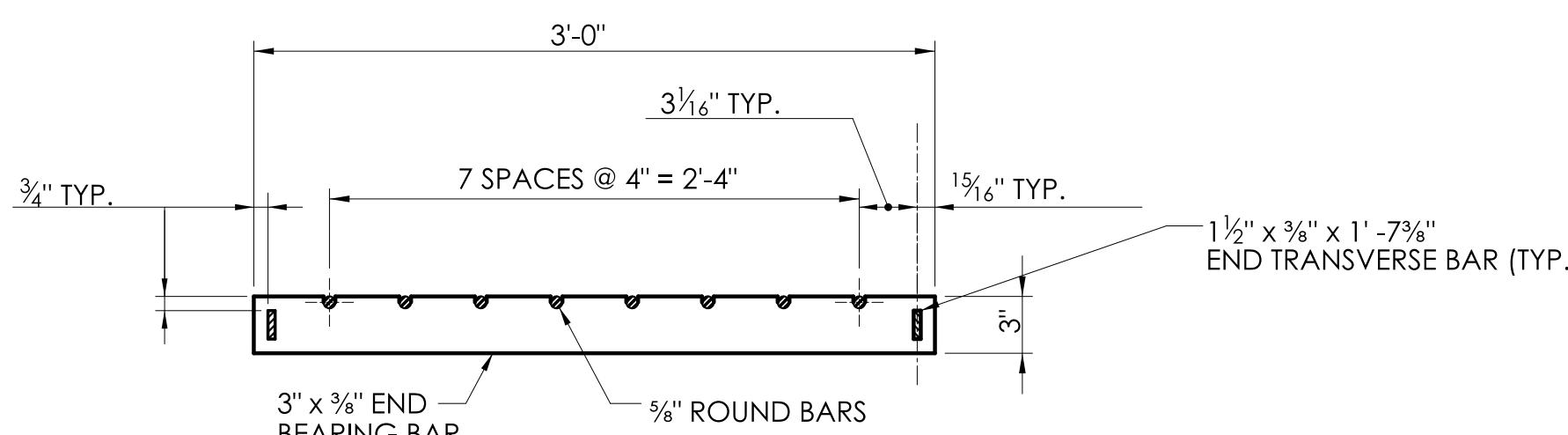
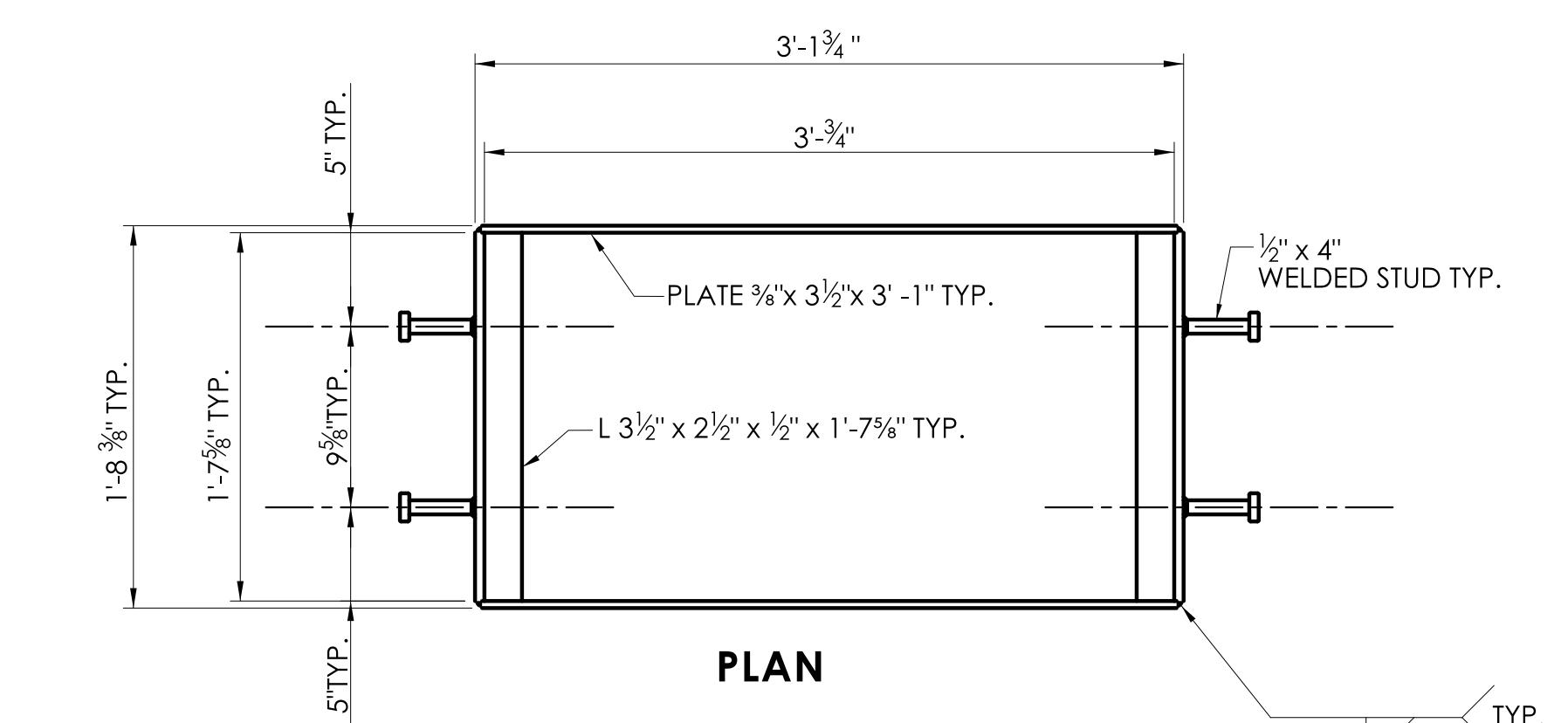
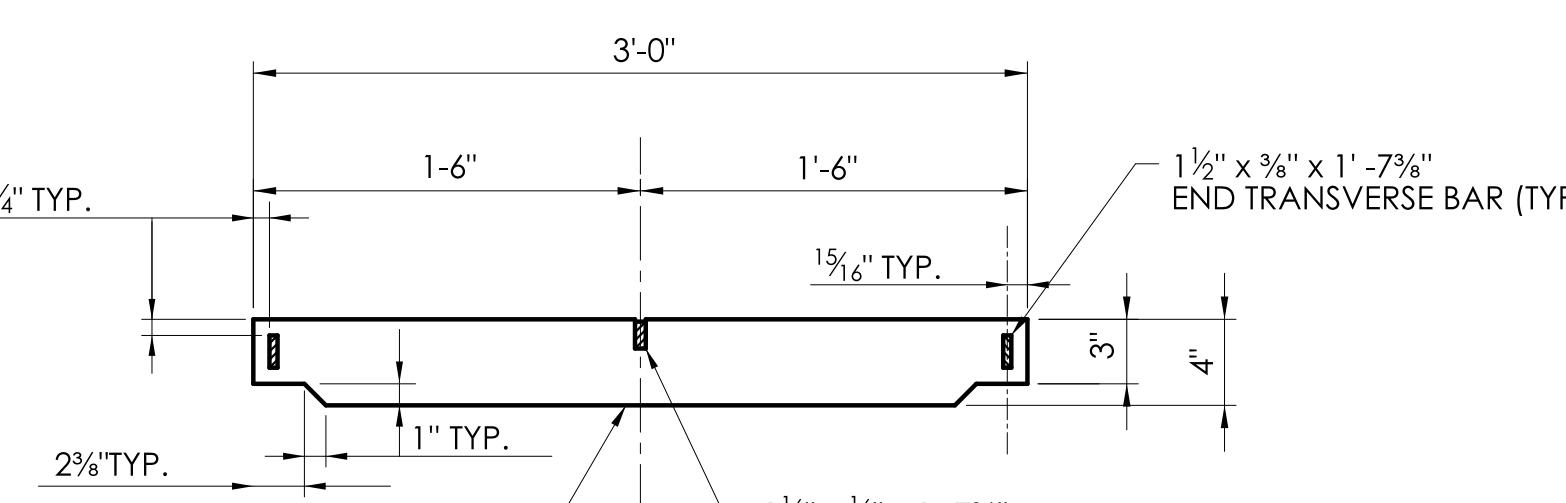
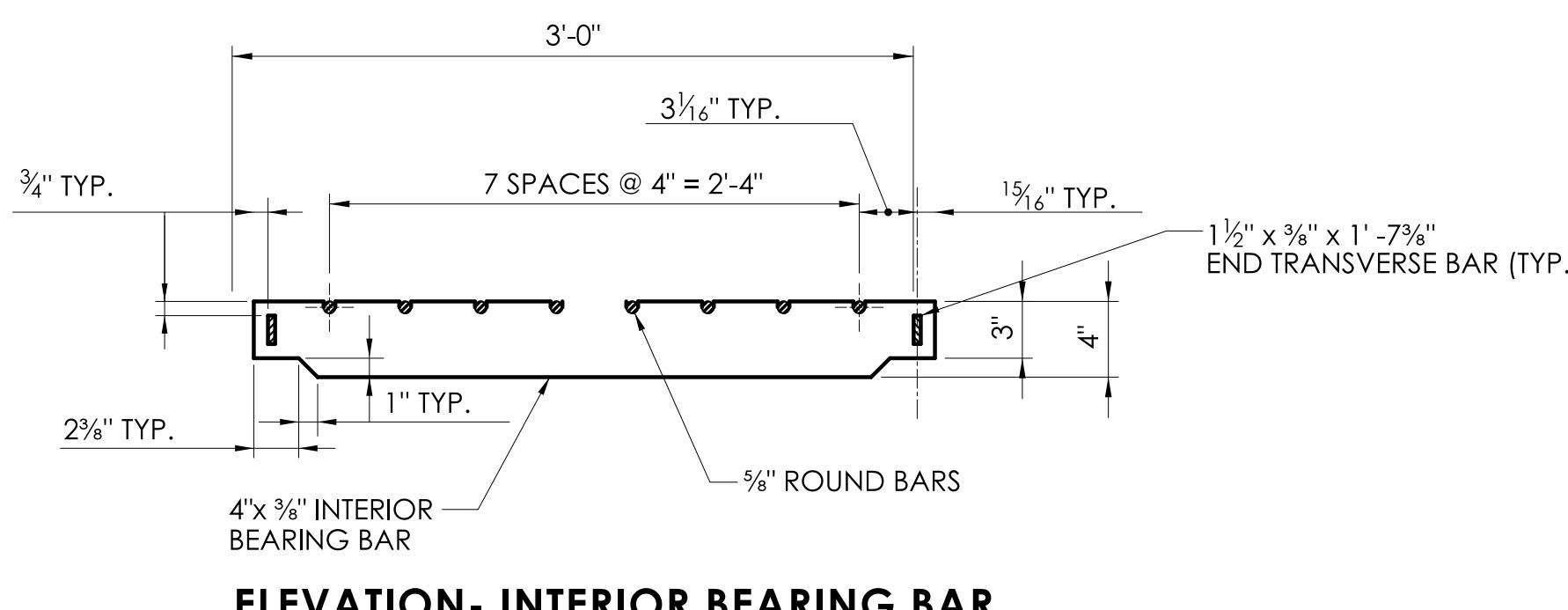


### GENERAL NOTES:

1. STEEL OR CAST IRON SHALL BE USED FOR FRAMES. STEEL SHALL BE USED FOR TYPE "A" AND "B" GRATES.
2. TYPE "A" GRATES SHALL BE USED ON ALL ROADWAYS WHERE BICYCLE TRAFFIC IS ALLOWED OR ON HEAVY DUTY LOCK DOWN TOPS AS DIRECTED BY THE ENGINEER.
3. TYPE "B" GRATES SHALL BE USED ON ALL LIMITED ACCESS HIGHWAYS, RAMPS AND WHERE BICYCLE TRAFFIC IS NOT ALLOWED OR AS DIRECTED BY THE ENGINEER.
4. DO NOT GALVANIZE CAST IRON FRAMES.
5. DIMENSIONAL TOLERANCES SHALL BE  $\pm \frac{3}{16}$ ".
6. ALL STEEL BARS SHALL BE WELDED AT ALL INTERSECTIONS.



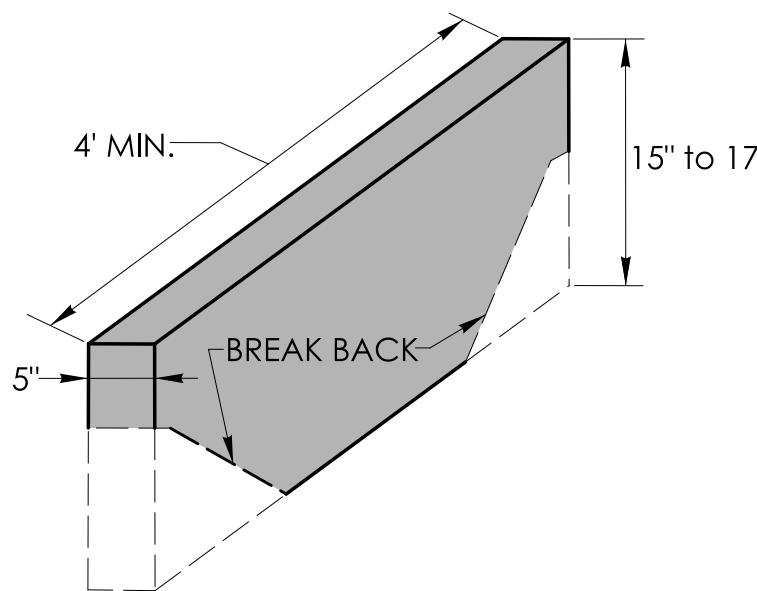
### CAST IRON FRAME ALTERNATE



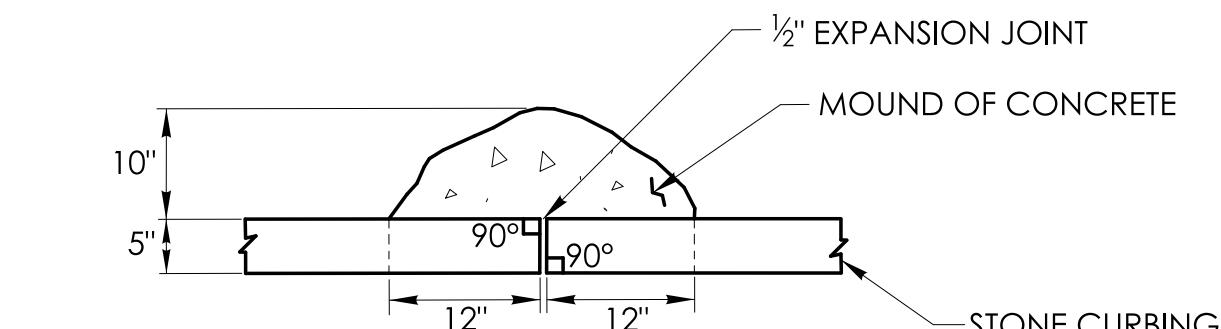
### WELDED STUD ANCHOR DETAILS STEEL FRAME

2" MIN.      15°      1/2" x 4" WELDED STUD (TYP.)

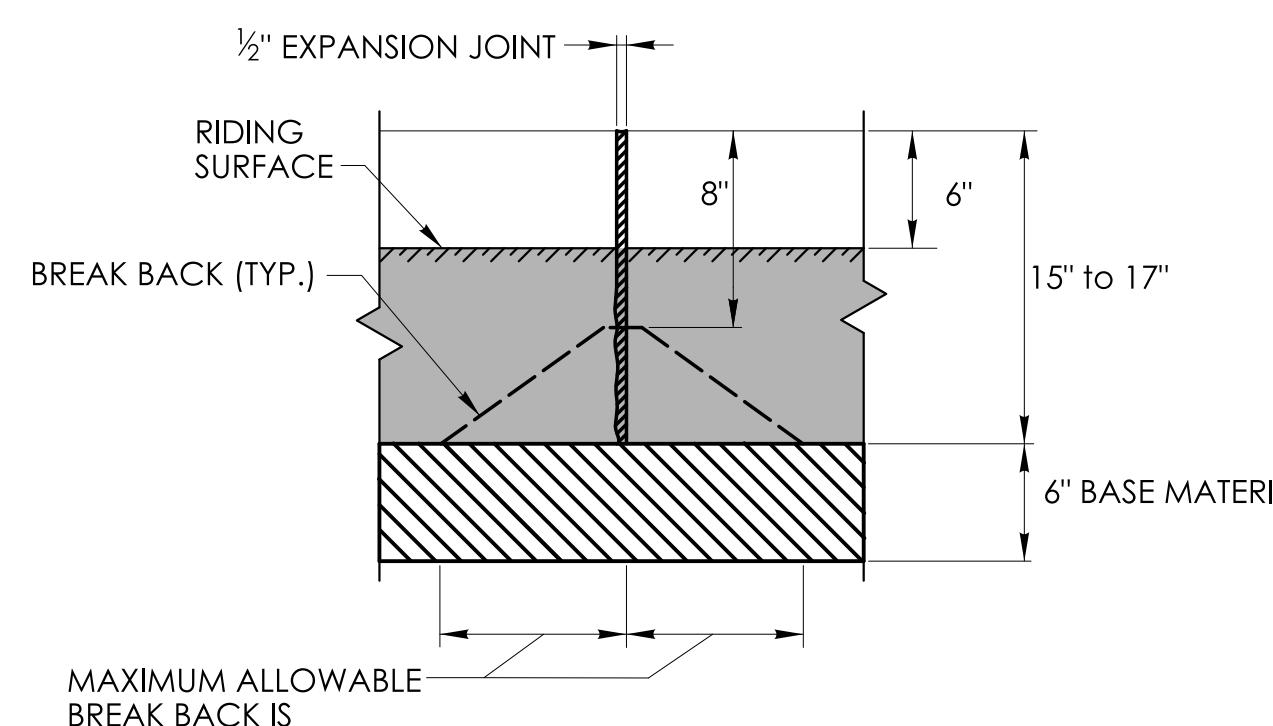
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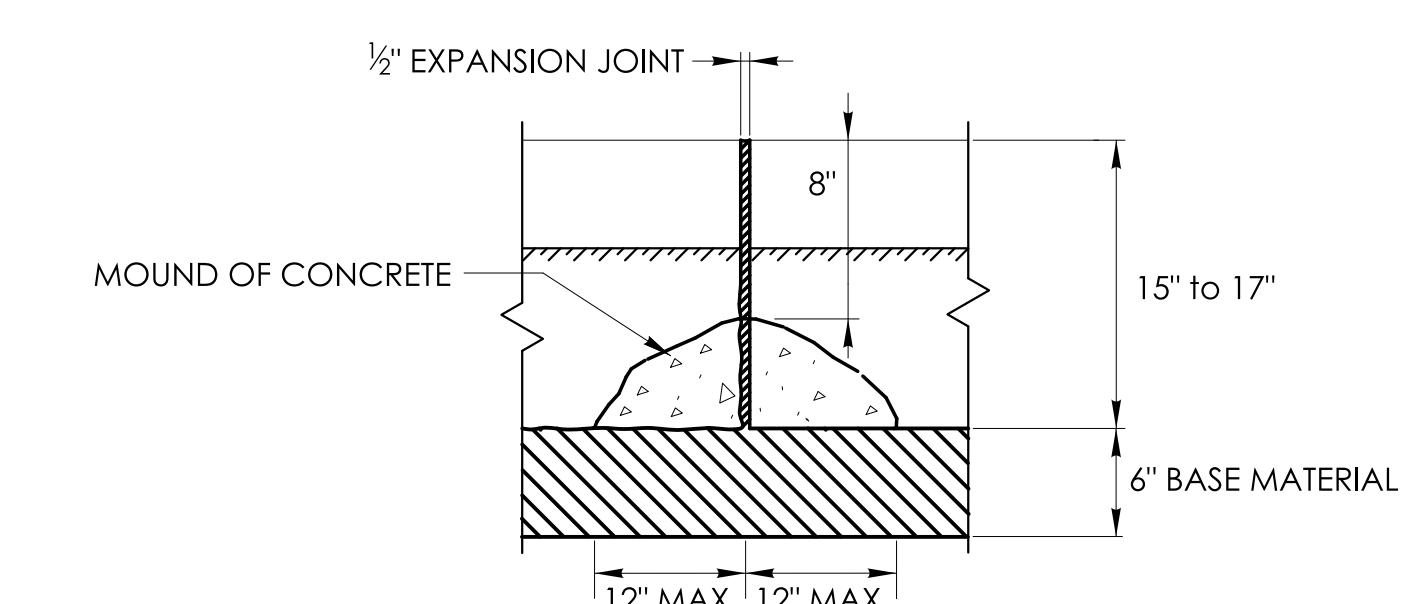
**STONE CURBING**



**PLAN**

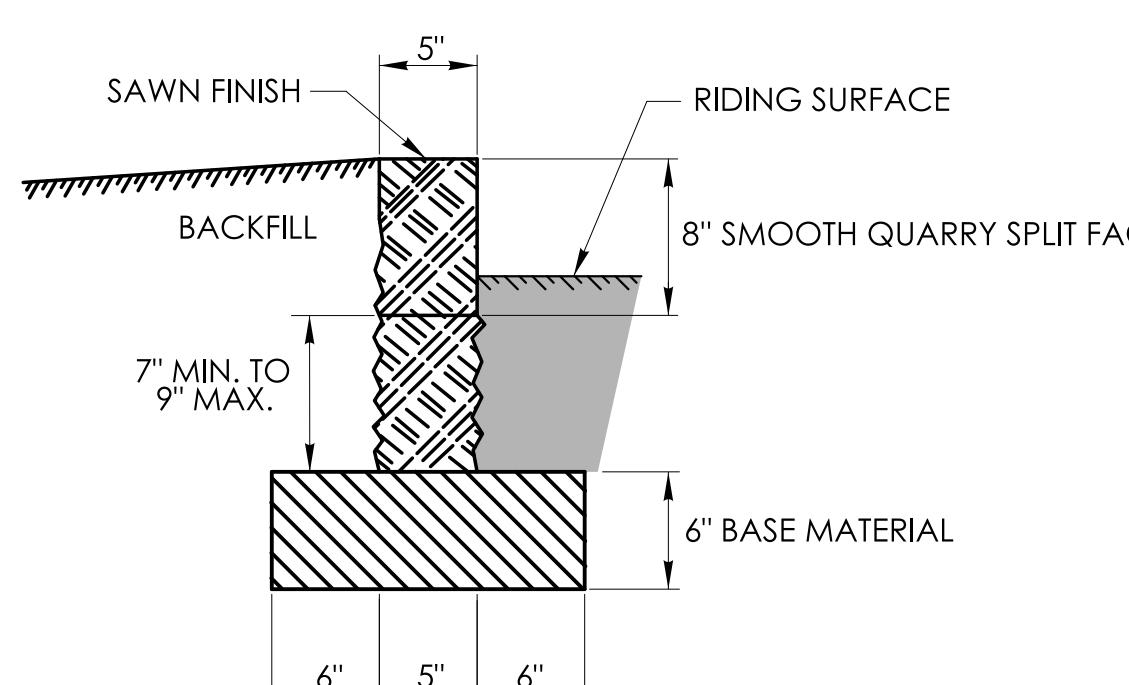


**FRONT ELEVATION**



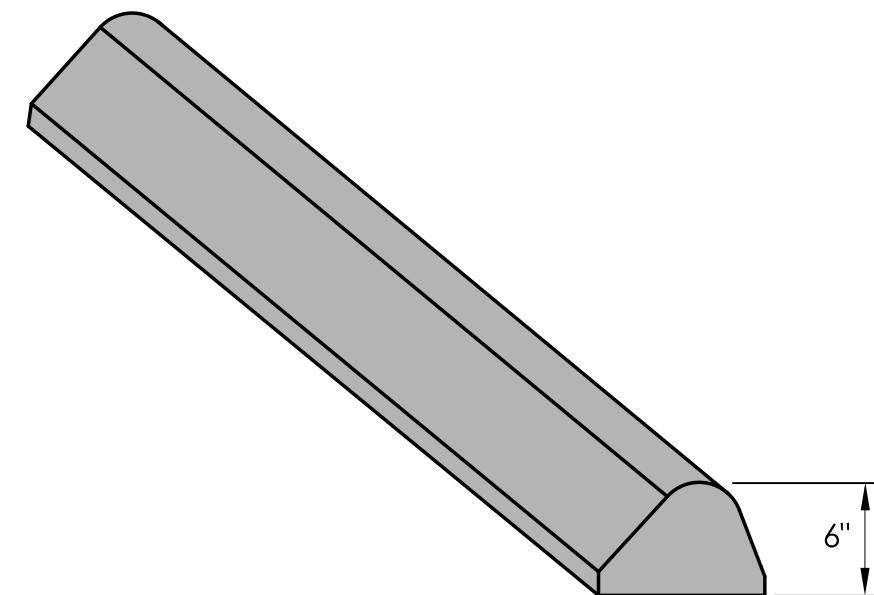
**BACK ELEVATION**

**MOULD OF CONCRETE AT ALL JOINTS  
FOR STONE CURBING**

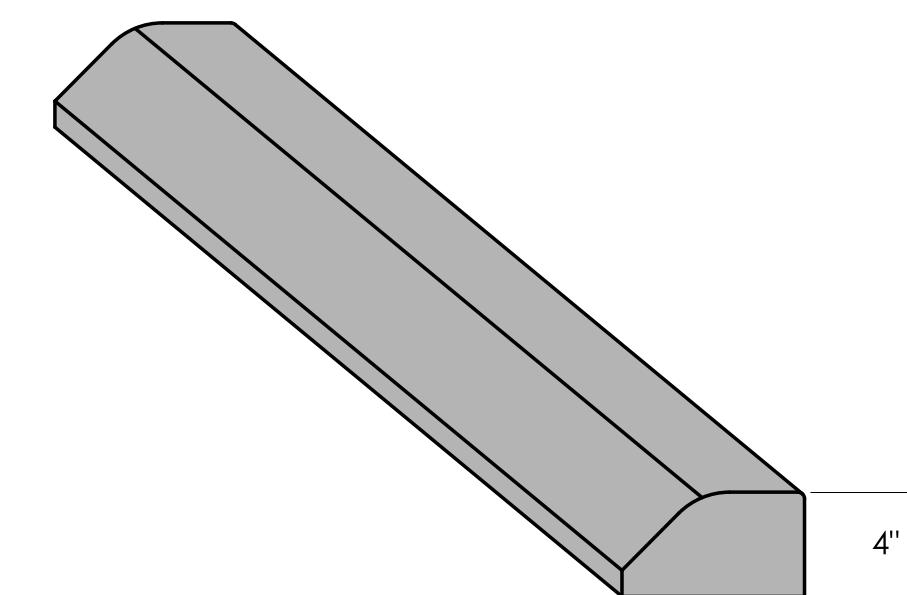


**SECTION**

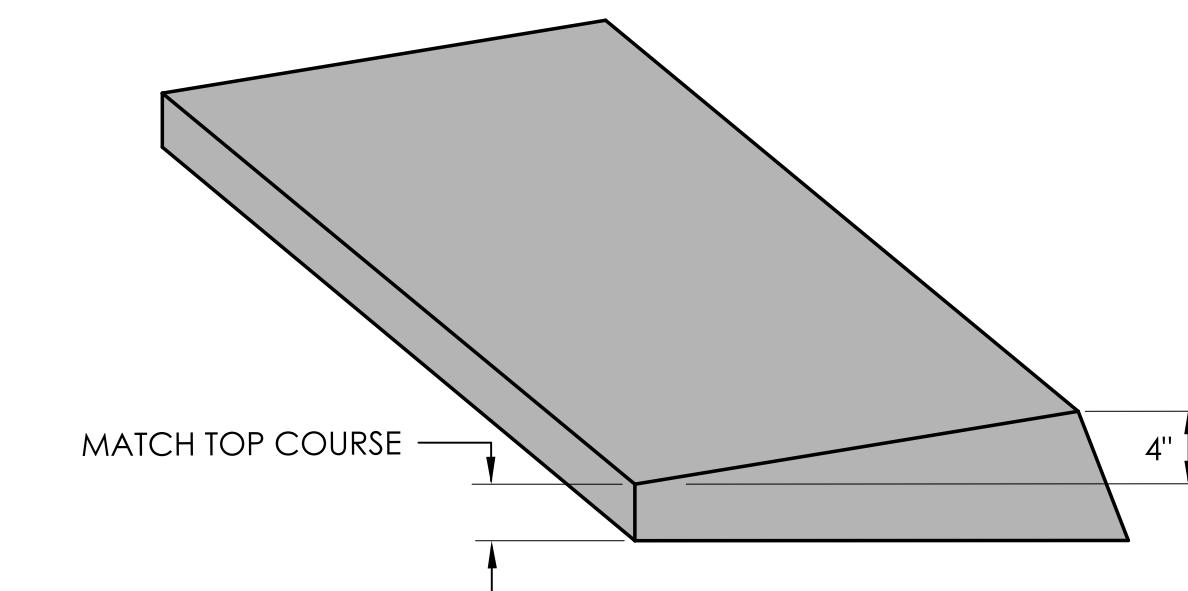
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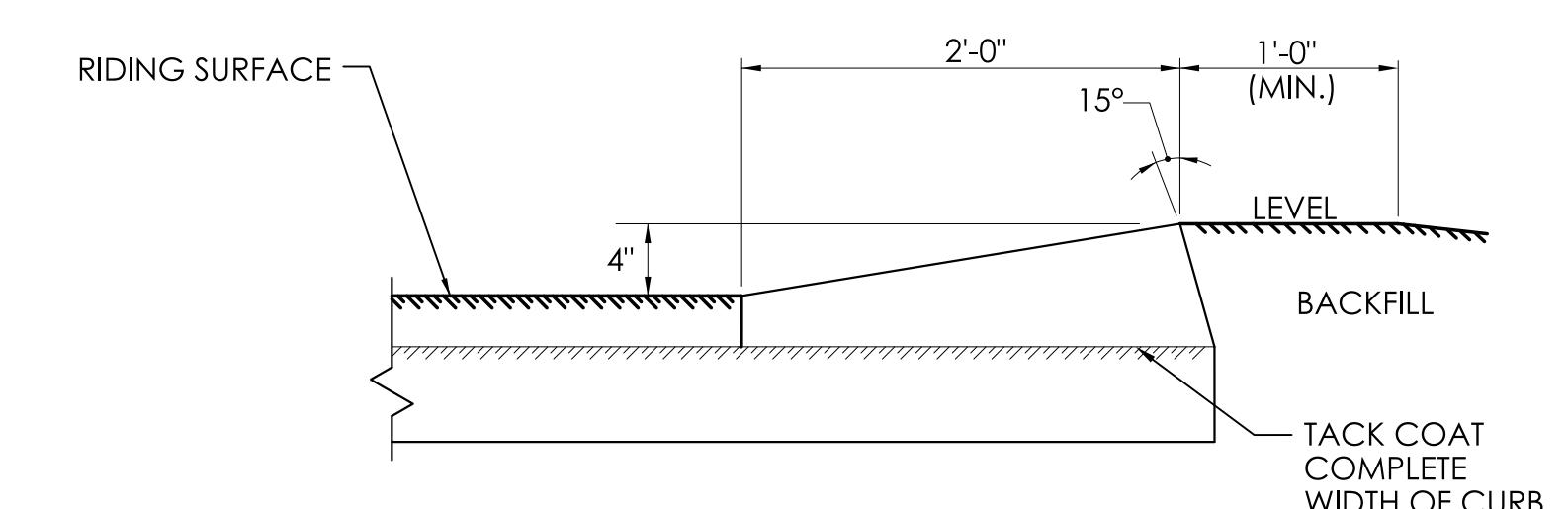
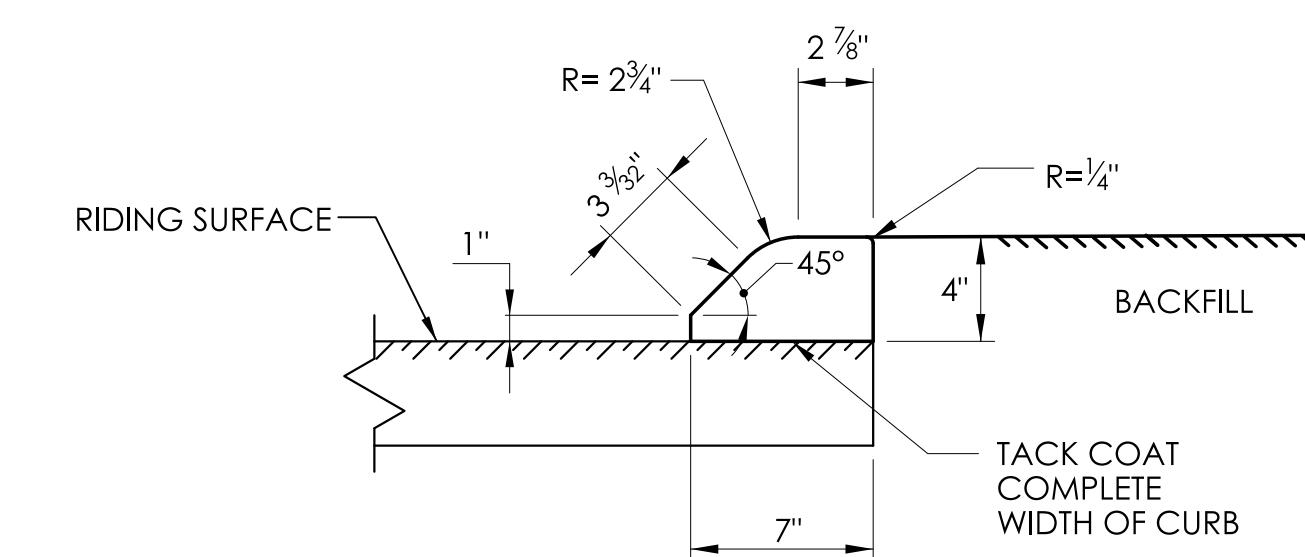
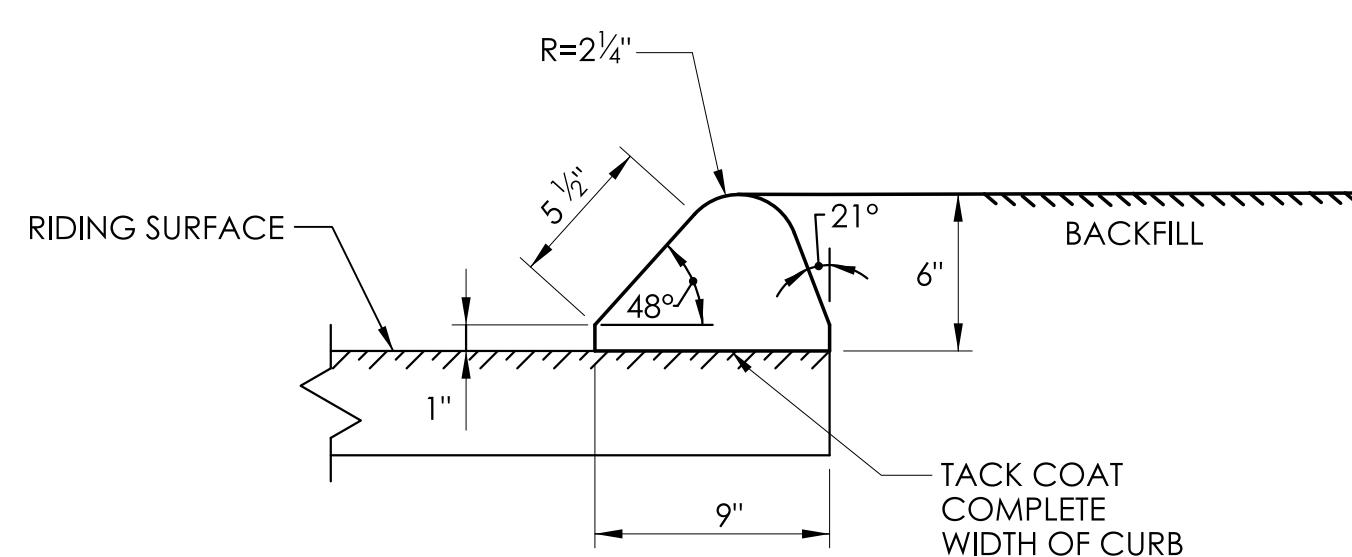
**BITUMINOUS CONCRETE LIP CURBING  
(6" HIGH)**



**BITUMINOUS CONCRETE PARK CURBING  
(4" HIGH)**



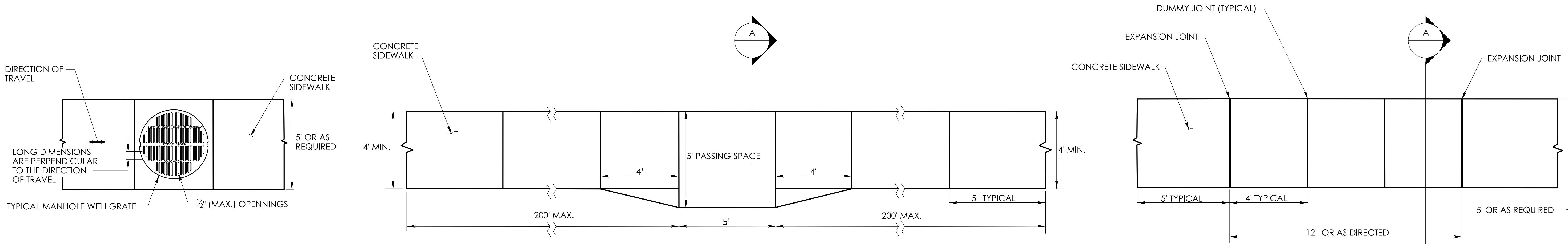
**BITUMINOUS CONCRETE BERM CURBING  
(4" HIGH)**



NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: <b>BITUMINOUS CONCRETE CURBING</b>	STANDARD SHEET NO.: <b>HW-815_01</b>
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**GENERAL NOTES:**

1. SEE CONCRETE SIDEWALK RAMPS GUIDE SHEETS FOR PEDESTRIAN RAMP TYPES.
2. ALL CURBING SHALL BE INSTALLED AS EITHER PRECAST OR CAST IN PLACE AS DIRECTED.



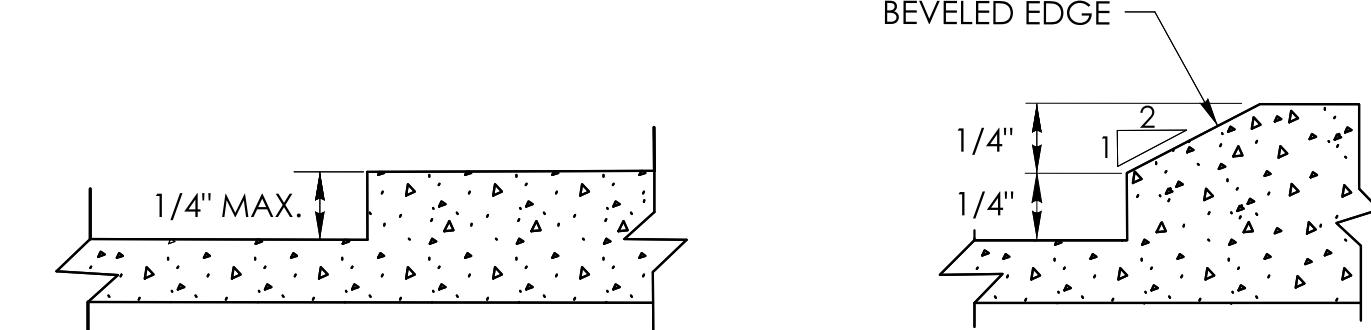
**PEDESTRIAN ACCESS ROUTE  
OVER A MANHOLE WITH GRATE**

1. HORIZONTAL OPENINGS IN GRATES AND JOINTS MUST NOT BE MORE THAN  $\frac{1}{2}$  INCH
2. ELONGATED OPENINGS IN GRATES MUST BE PLACED SO THAT THE LONG DIMENSION IS PERPENDICULAR TO THE DIRECTION OF TRAVEL

**5' PASSING SPACE FOR 4' WIDE SIDEWALK  
PLAN**

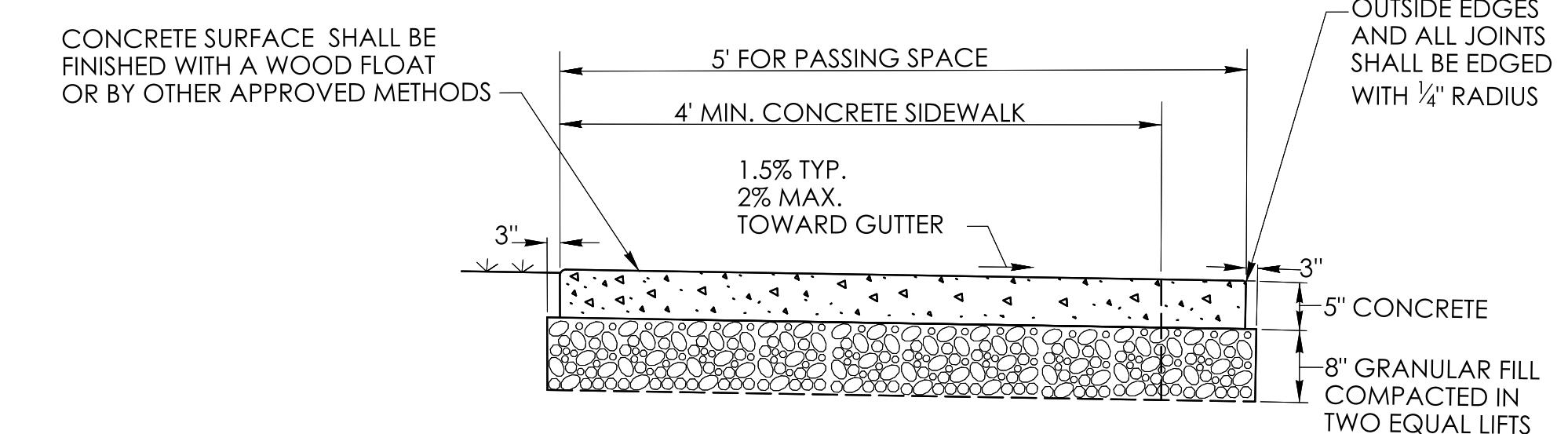
PASSING SPACES SHALL BE PROVIDED AT INTERVALS OF 200' MAXIMUM FOR SIDEWALKS LESS THAN 5' IN WIDTH

**5' WIDE SIDEWALK  
PLAN**



**VERTICAL SURFACE DISCONTINUITIES**

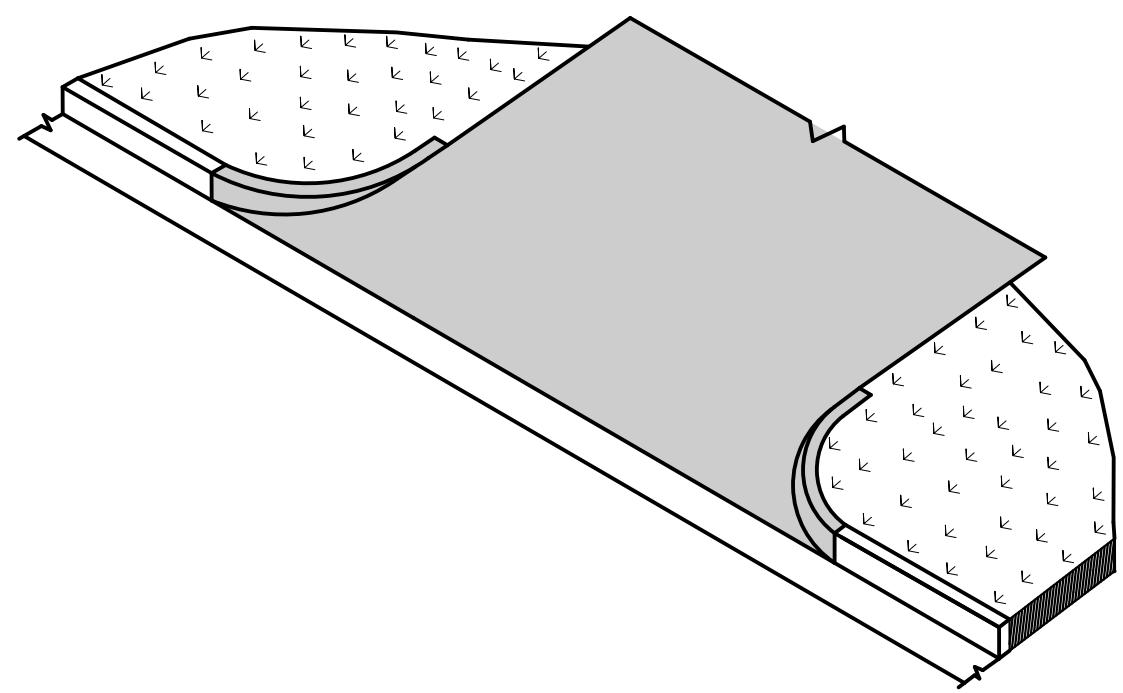
VERTICAL SURFACE DISCONTINUITIES MUST BE BEVELED TO A HEIGHT NOT GREATER THAN  $\frac{1}{4}$  INCH. THE BEVEL MUST BE THE ENTIRE WIDTH OF THE DISCONTINUITY



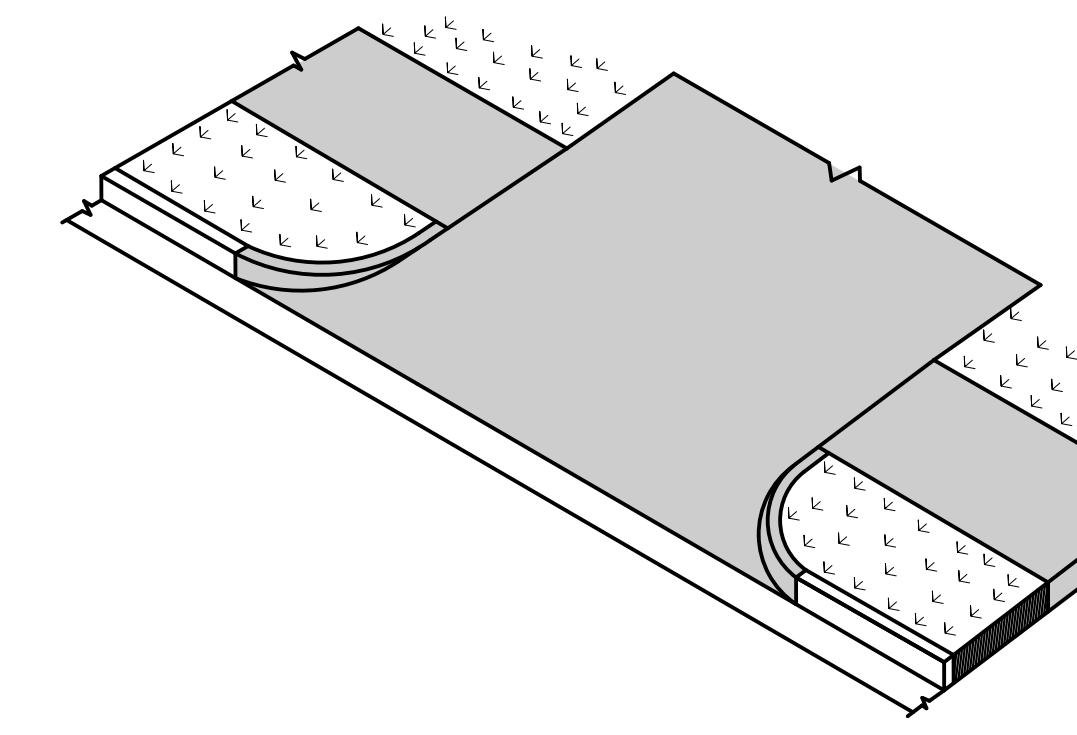
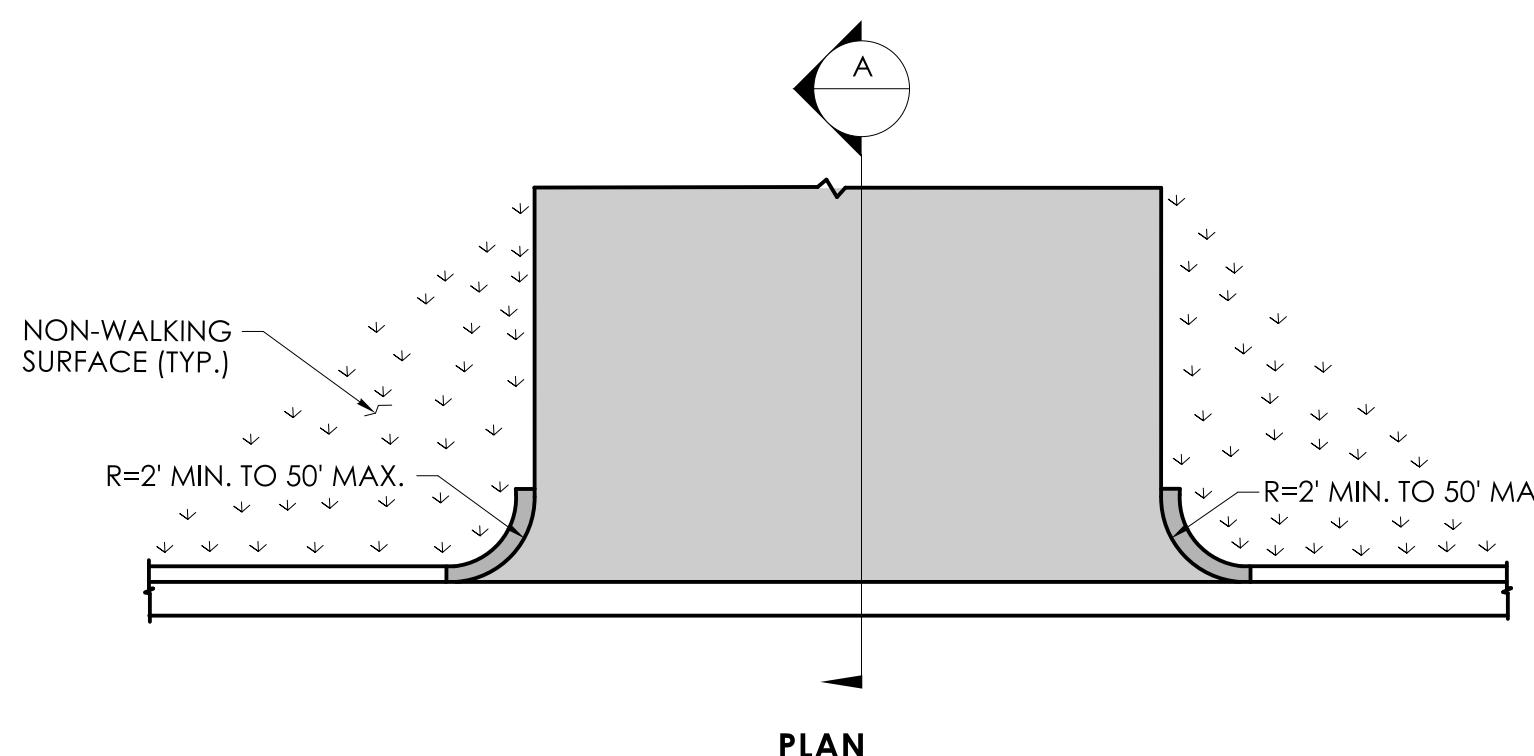
**5' PASSING SPACE FOR 4' WIDE SIDEWALK**

**SECTION A**

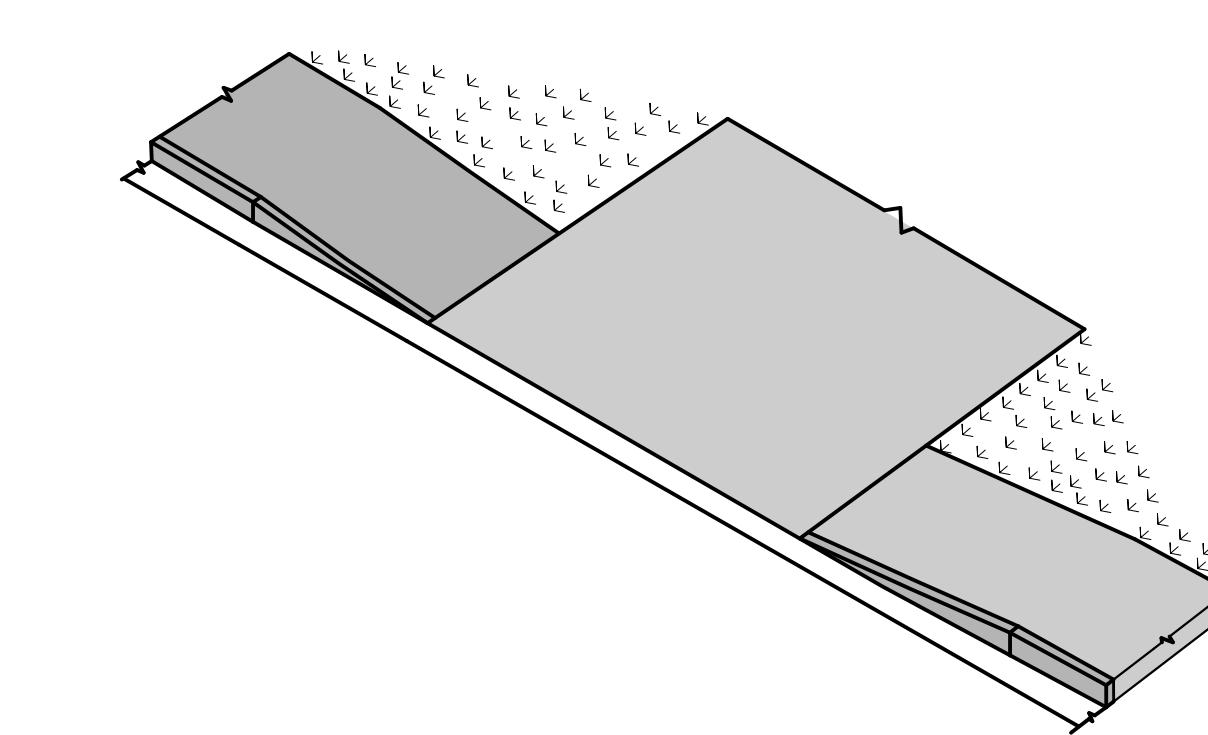
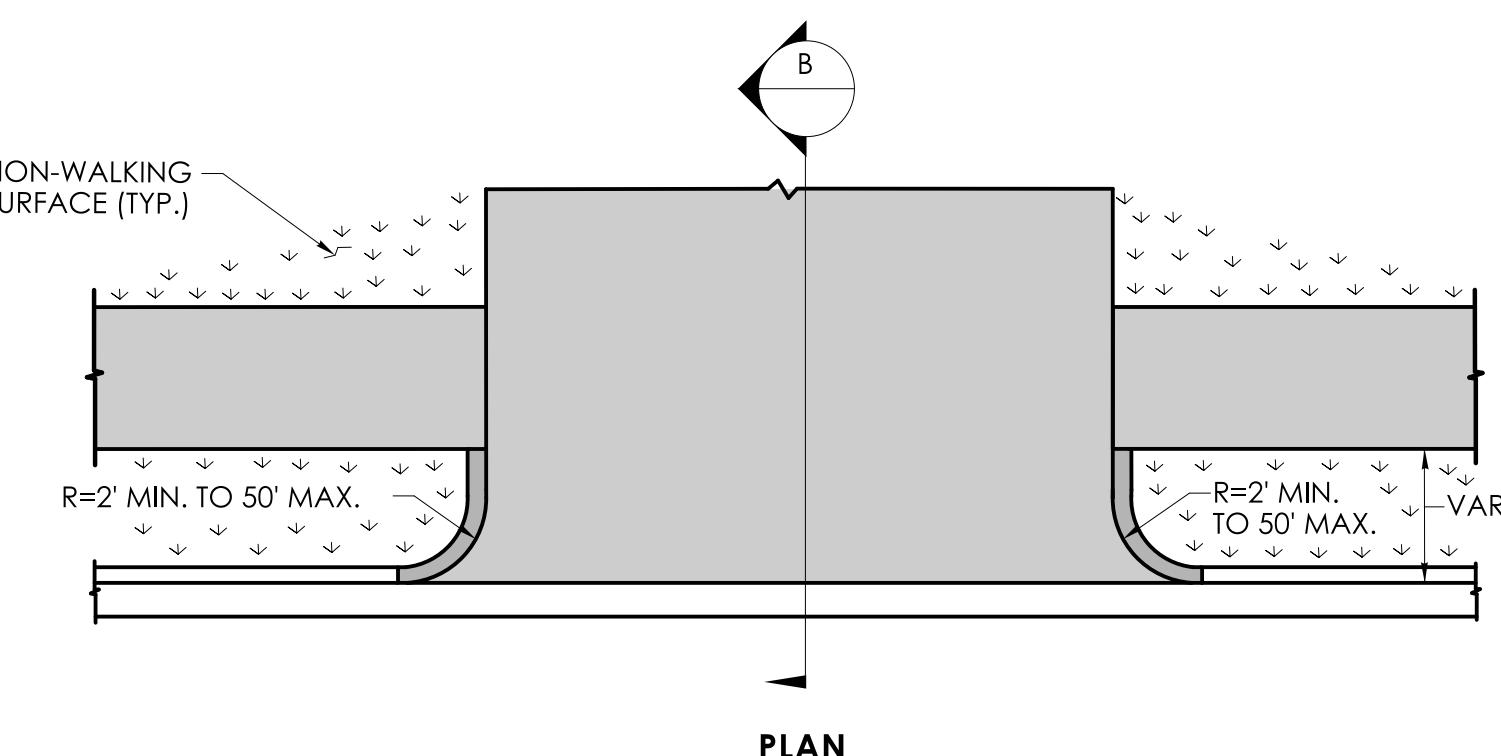
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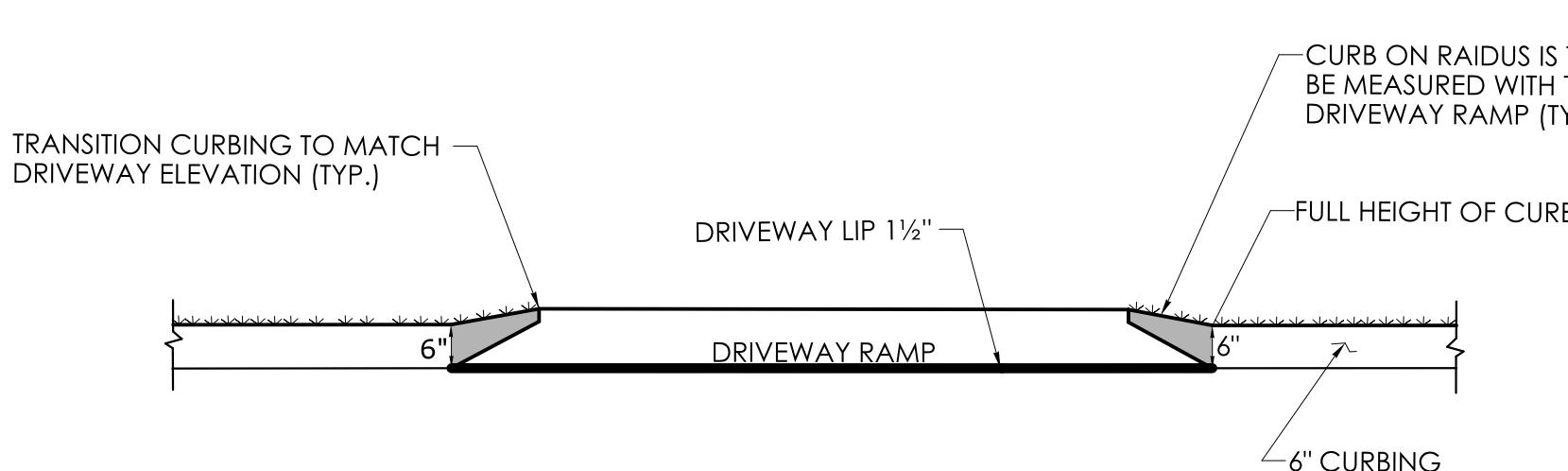
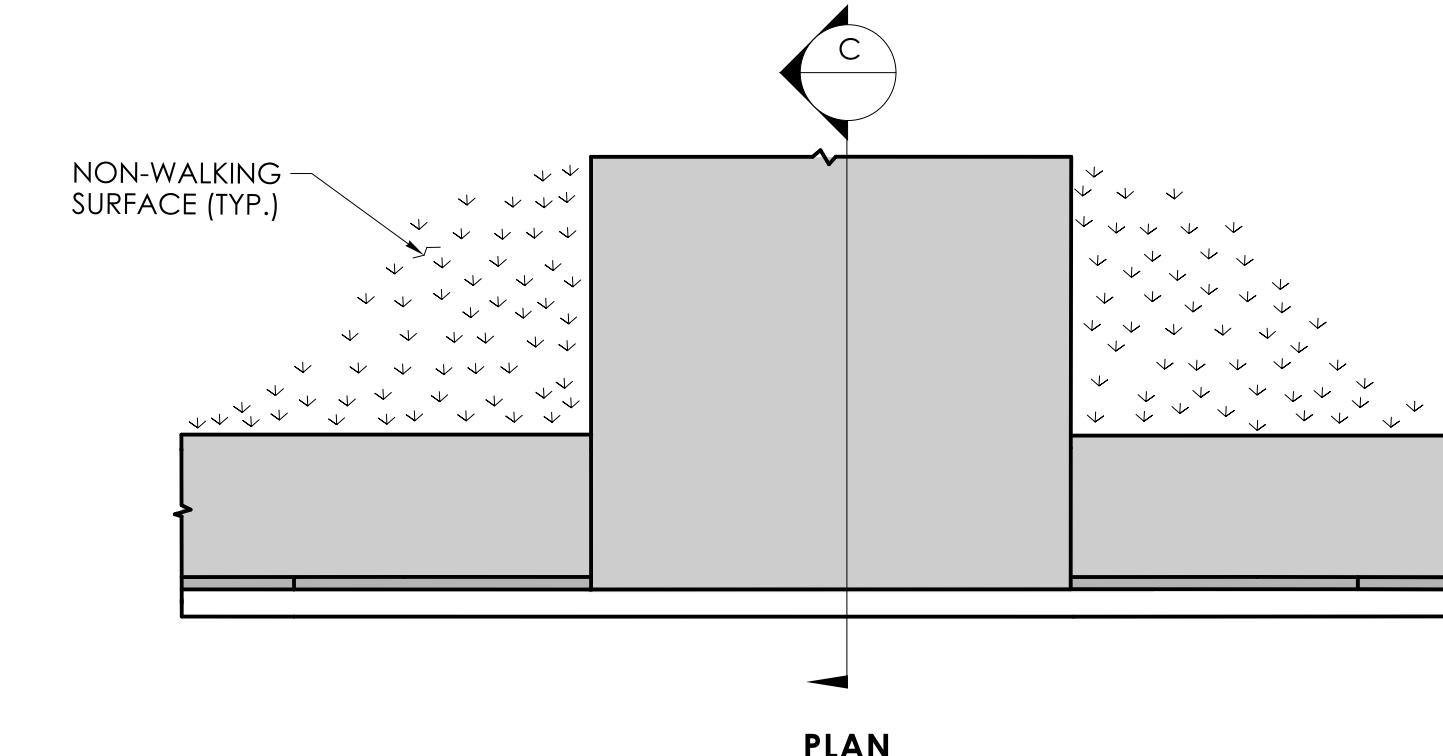
BITUMINOUS CONCRETE DRIVEWAY



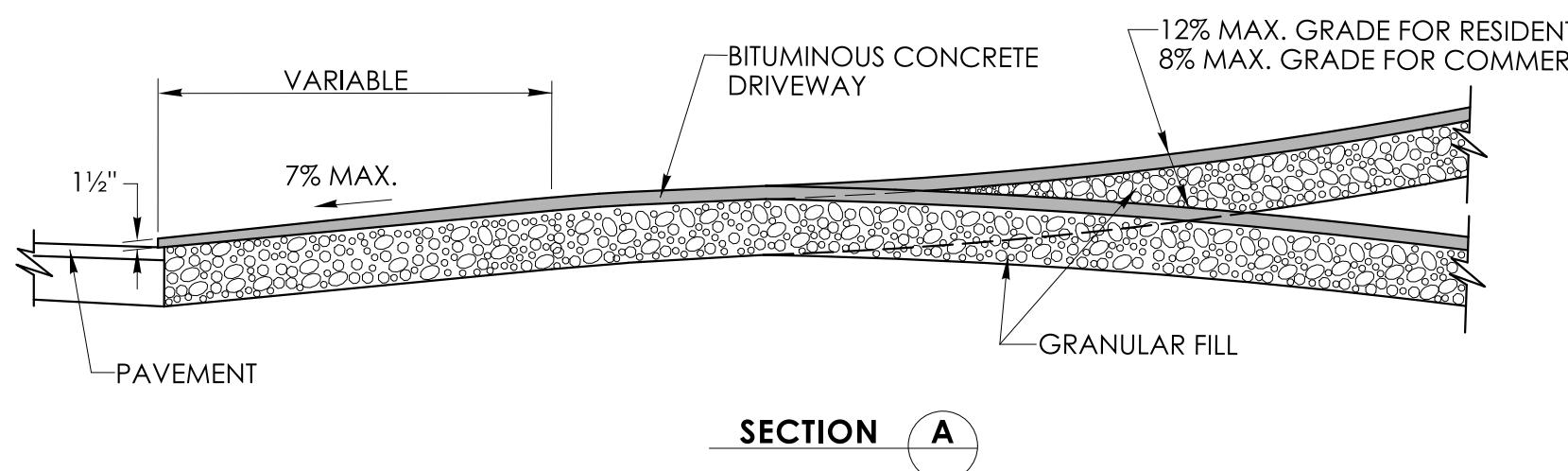
BITUMINOUS CONCRETE DRIVEWAY  
WITH A PEDESTRIAN SIDEWALK SETBACK



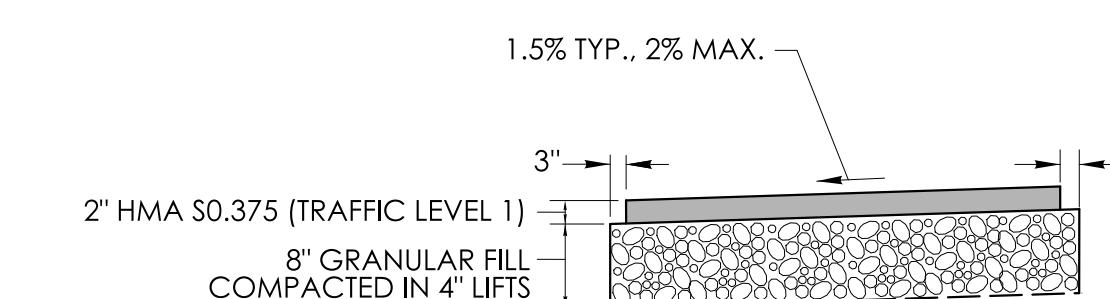
BITUMINOUS CONCRETE DRIVEWAY  
WITH PEDESTRIAN SIDEWALK AT THE CURB



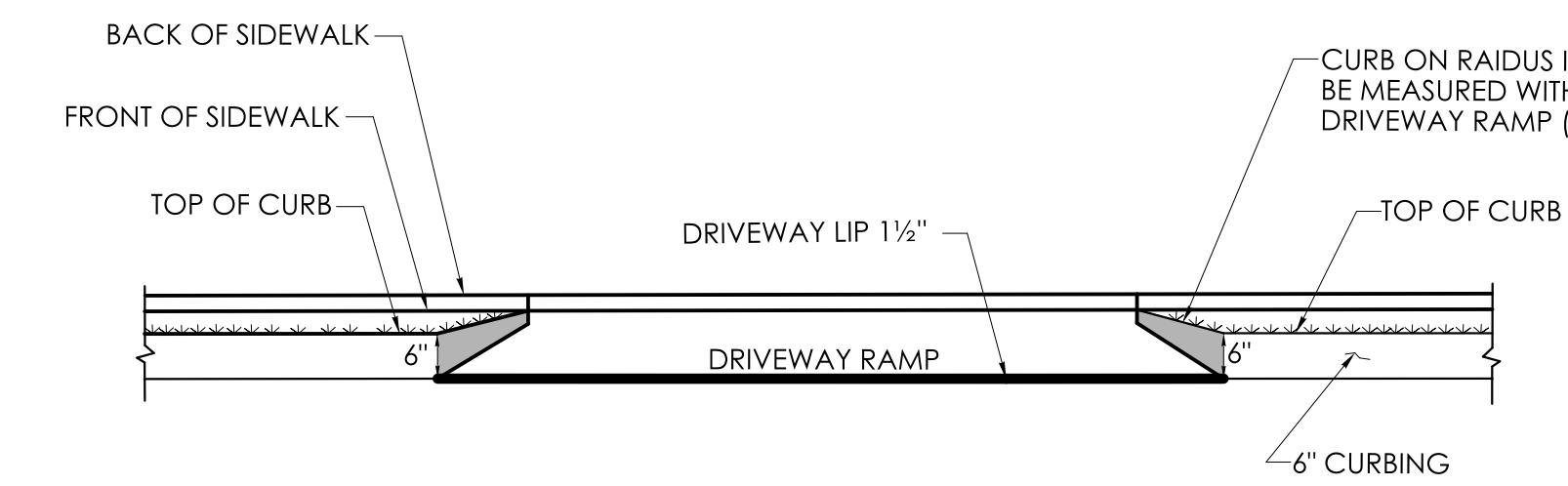
ELEVATION



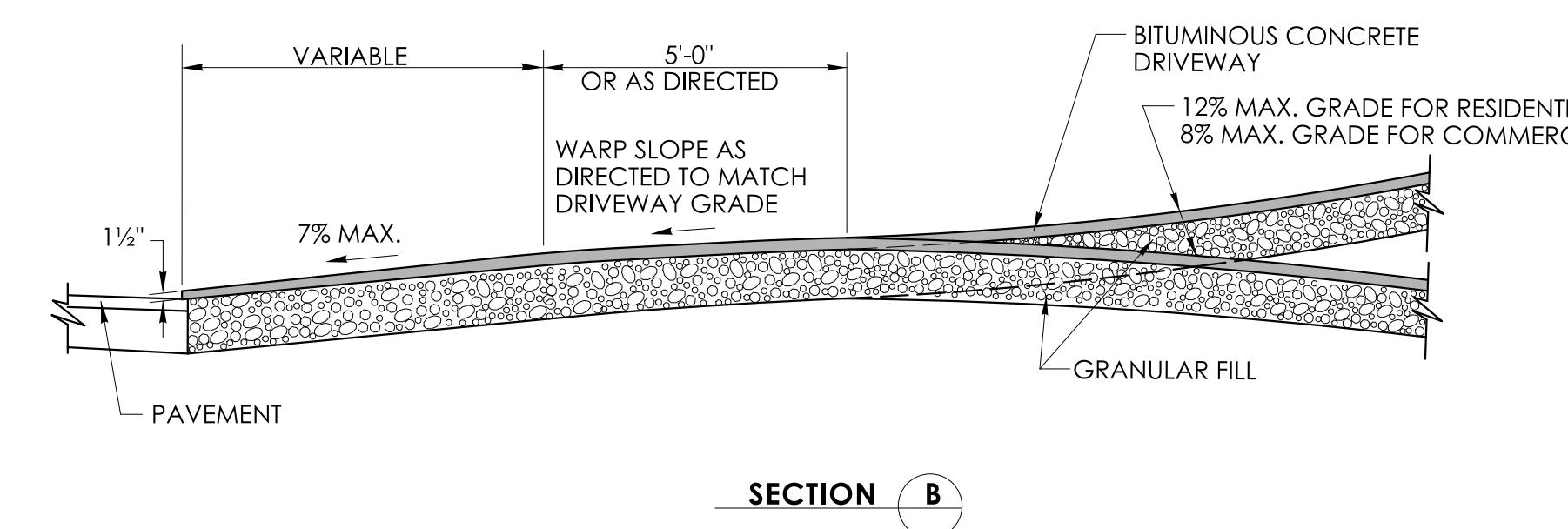
SECTION A



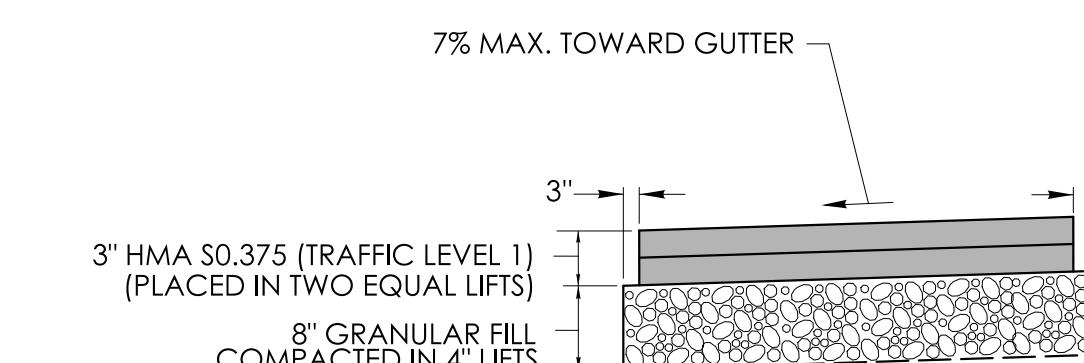
BITUMINOUS CONCRETE SIDEWALK



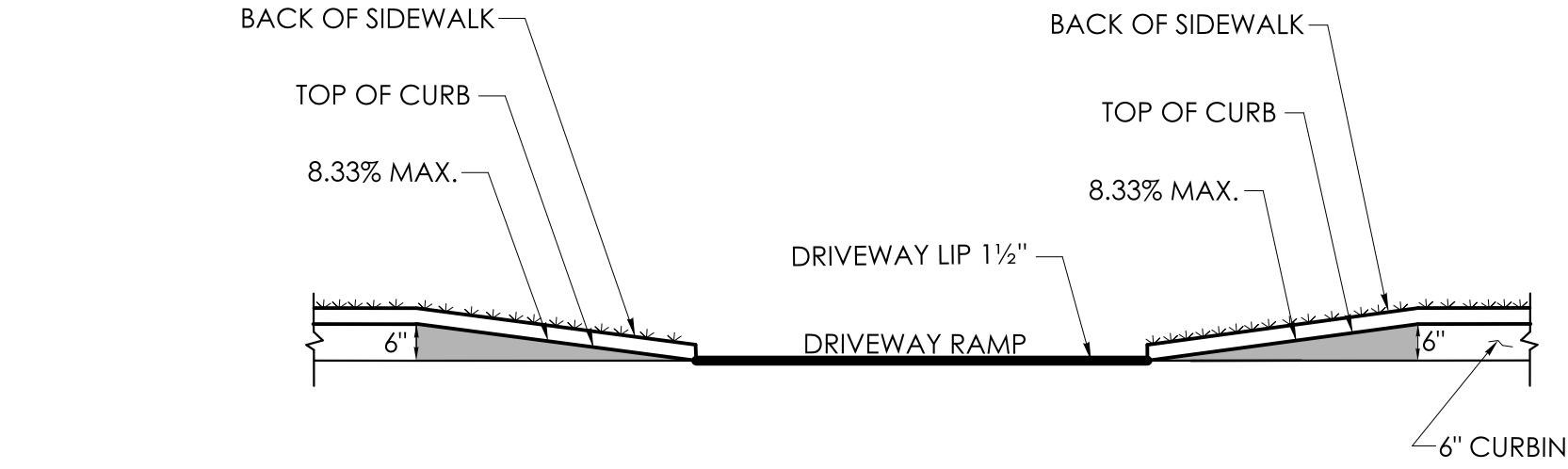
ELEVATION



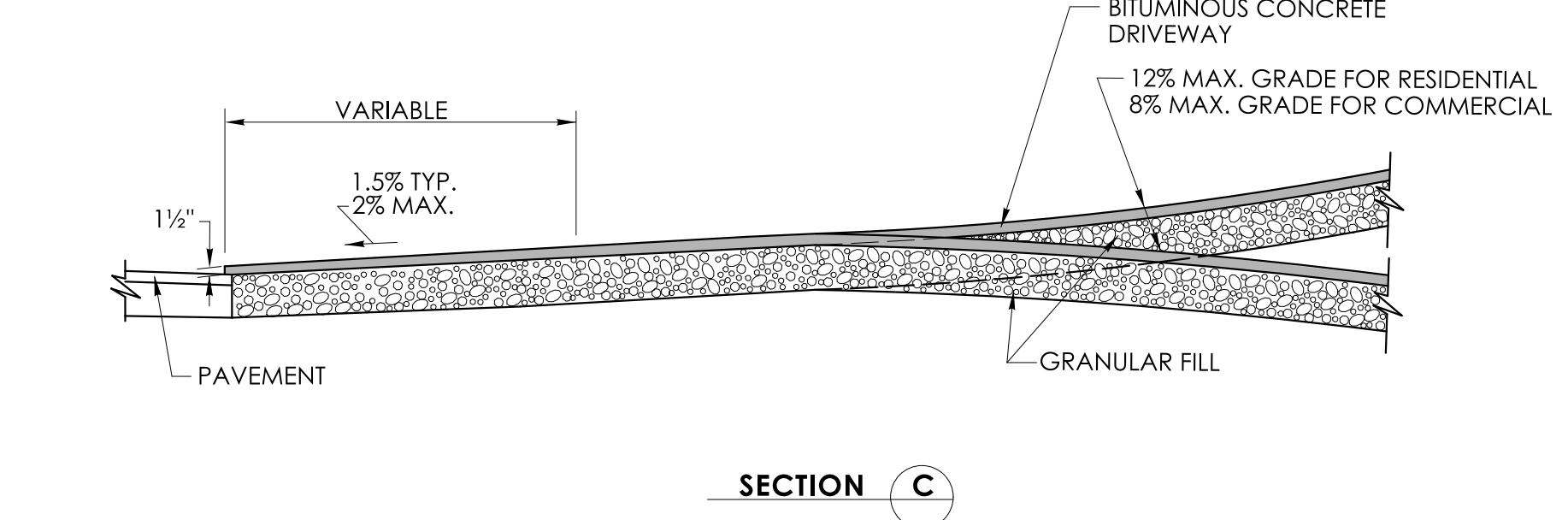
SECTION B



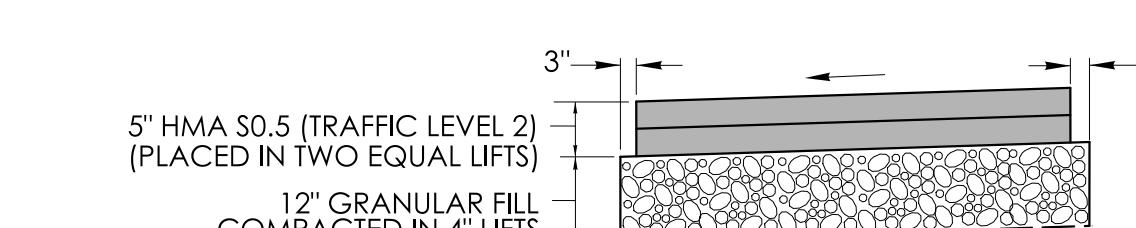
BITUMINOUS CONCRETE DRIVEWAY



ELEVATION



SECTION C



COMMERCIAL  
BITUMINOUS CONCRETE DRIVEWAY

NOT TO SCALE
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NOT TO SCALE

SIGNATURE BLOCK:  
OFFICE OF ENGINEERING  
2800 BERLIN TURNPIKE  
NEWINGTON, CT 06111

SUBMITTED BY: \_\_\_\_\_

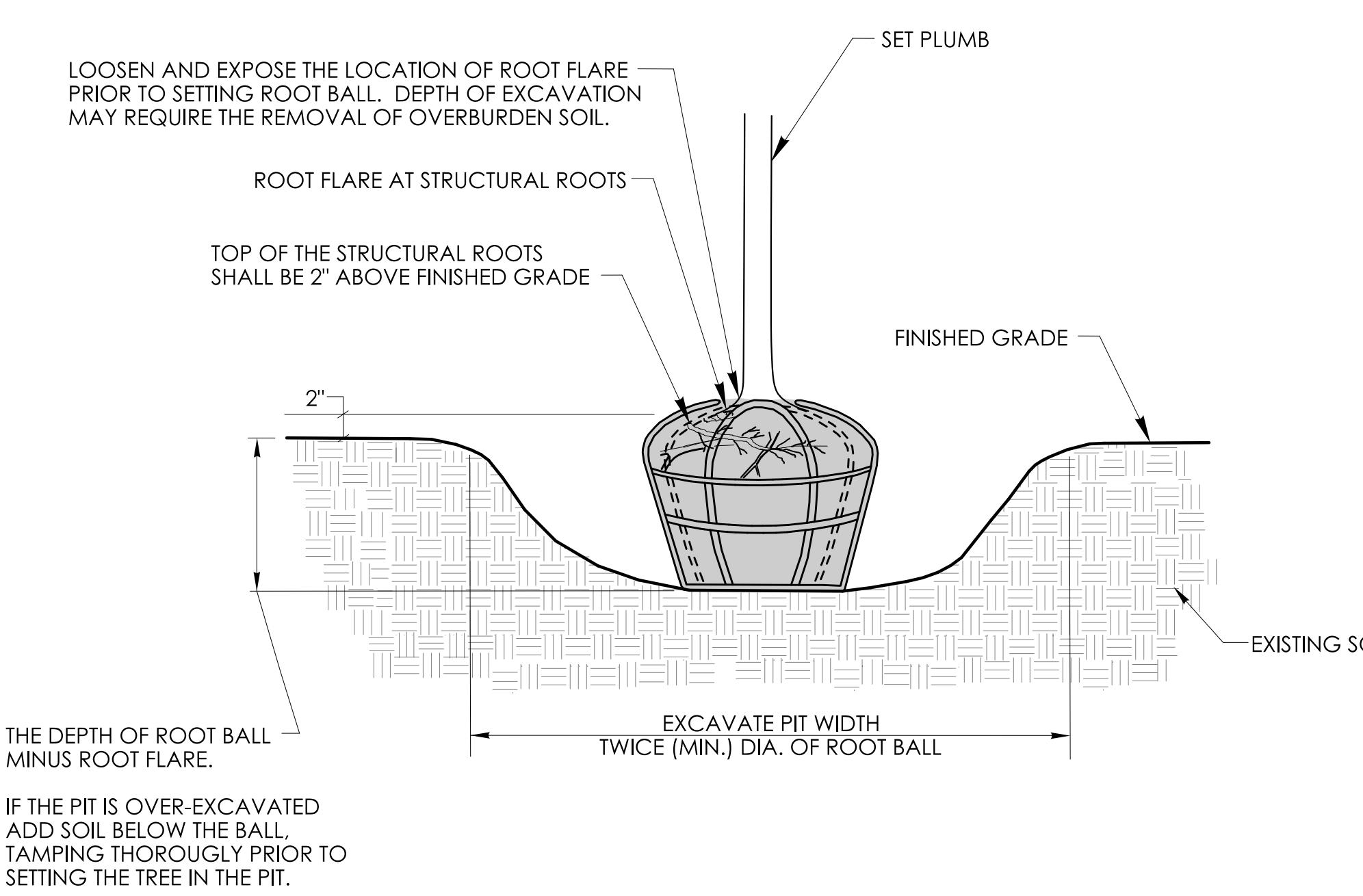
APPROVED BY: \_\_\_\_\_



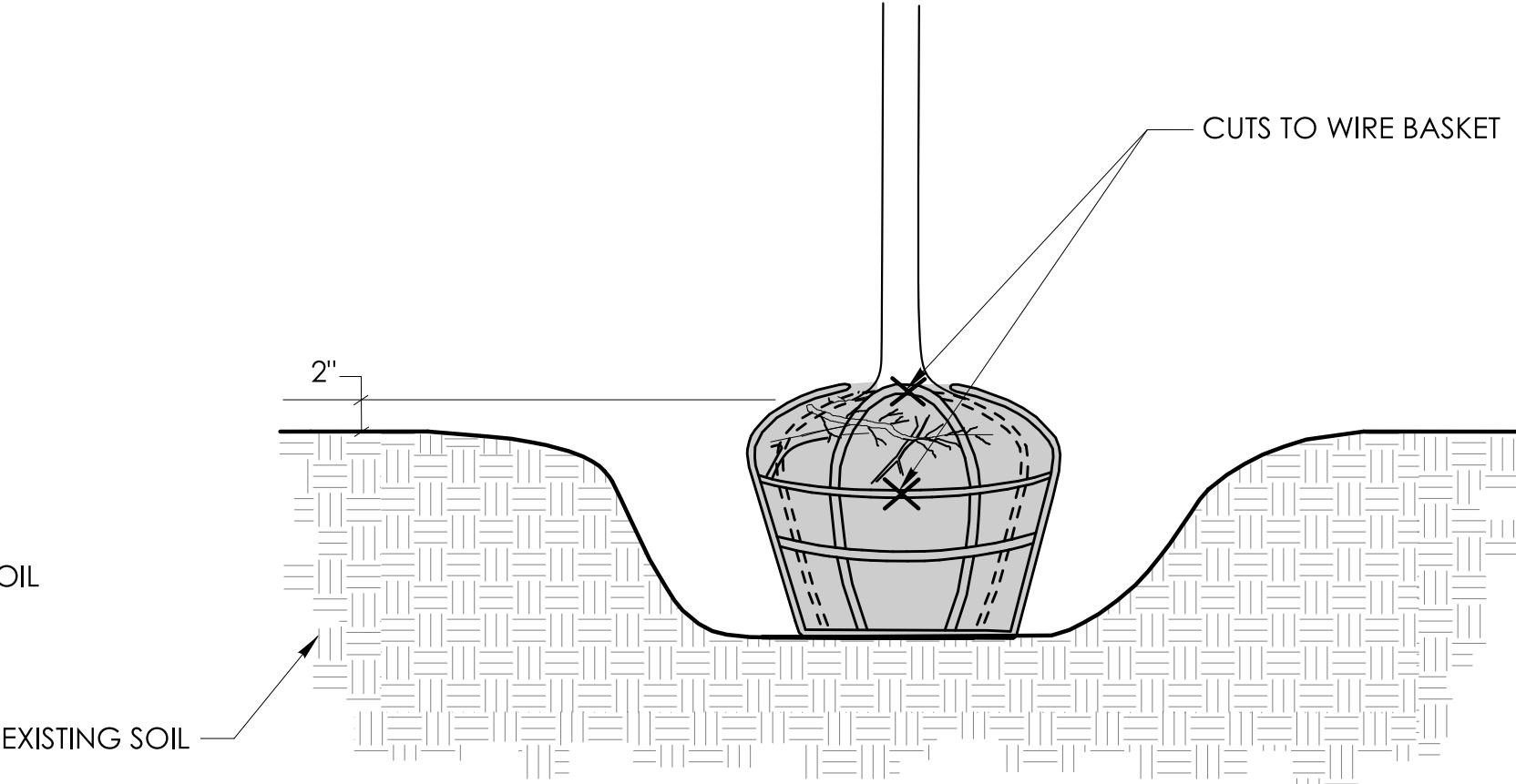
CTDOT  
STANDARD SHEET

STANDARD SHEET TITLE:  
**BITUMINOUS CONCRETE SIDEWALK AND  
BITUMINOUS CONCRETE DRIVEWAY**

STANDARD SHEET NO.:  
**HW-922\_01**

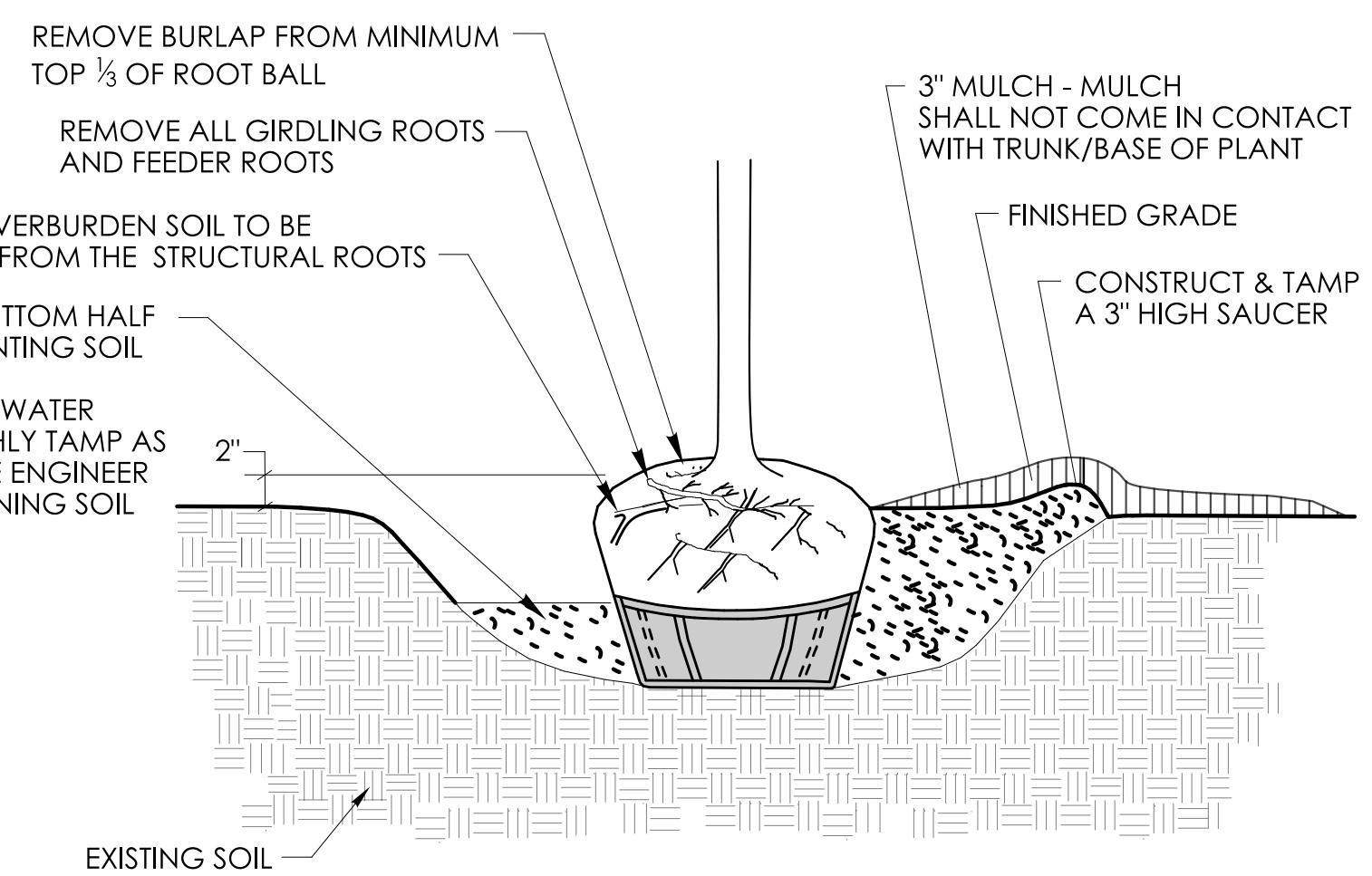


### PIT EXCAVATION AND SETTING OF PLANTING

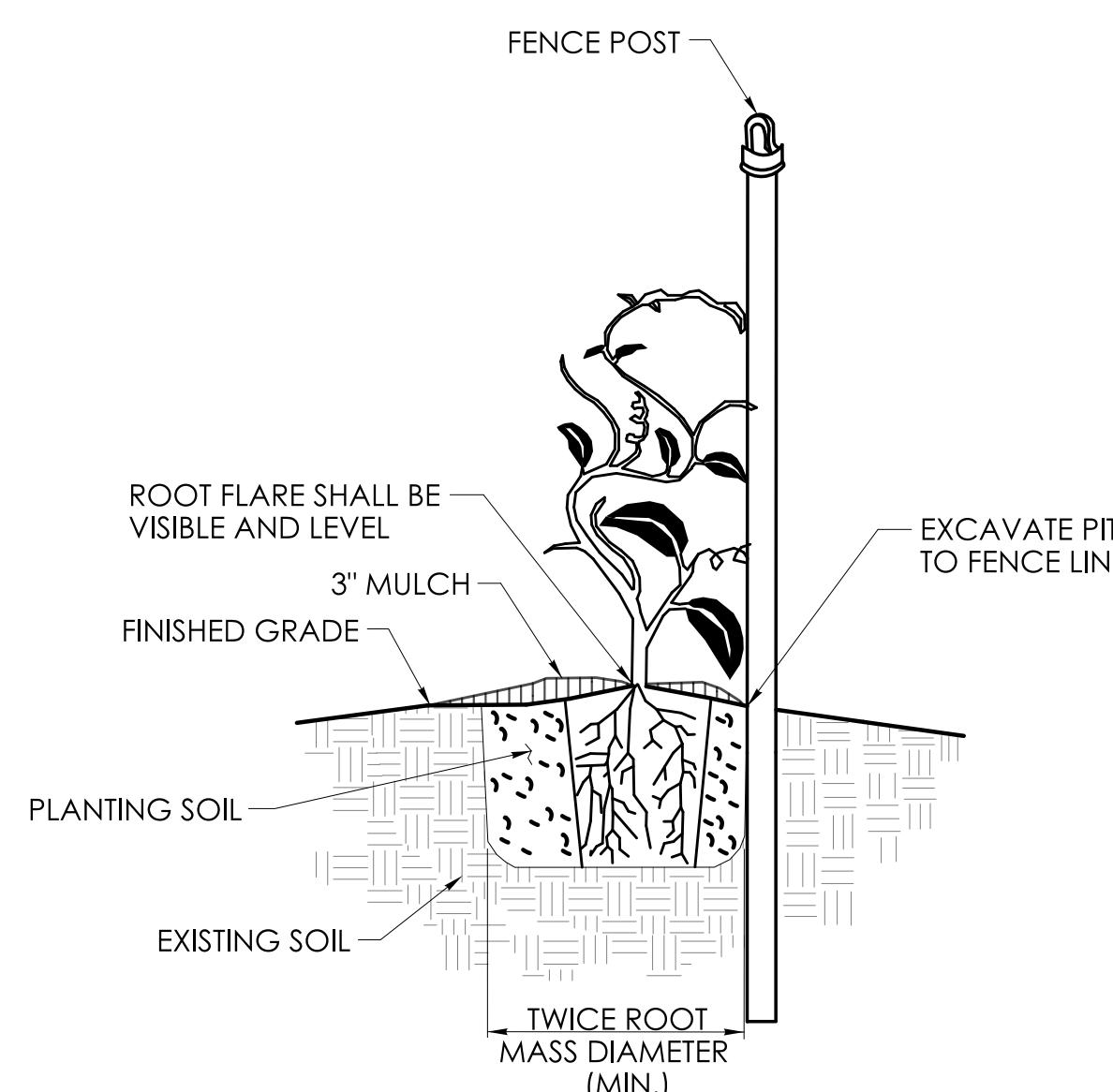


### WIRE BASKET REMOVAL

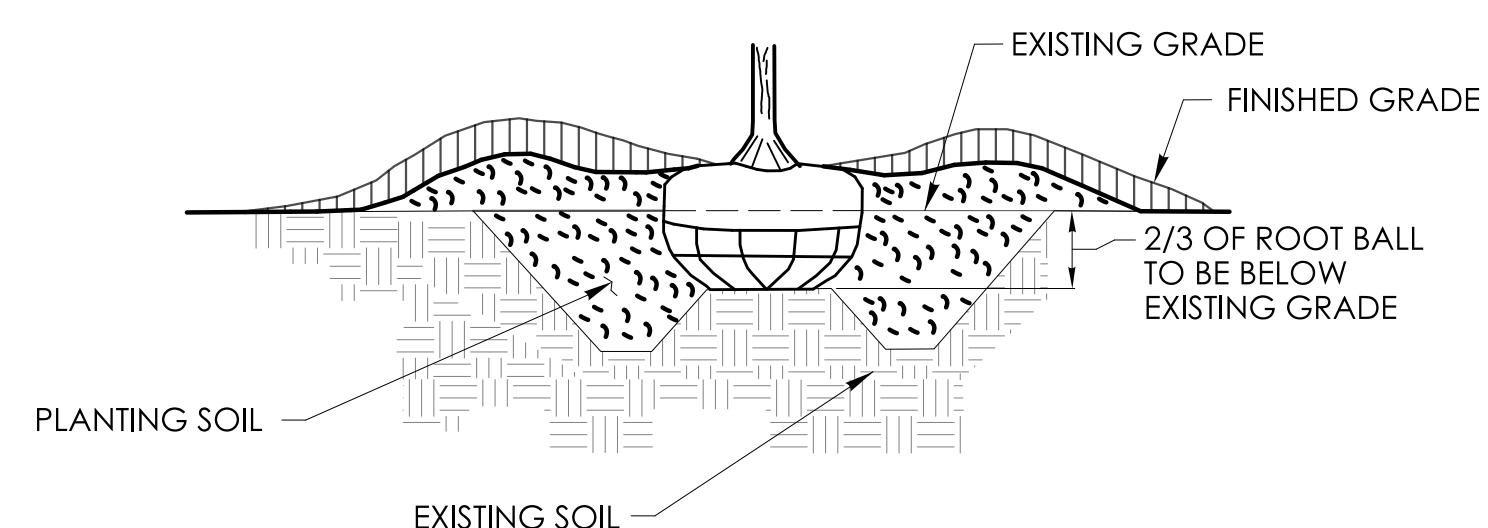
NOTE: IF WIRE BASKETS ARE USED, THE CONTRACTOR SHALL CUT ALL OF THE HORIZONTAL WIRES IN THE TOP  $\frac{1}{3}$  OF THE ROOT BALL AND BEND DOWN OR REMOVE THE TOP  $\frac{1}{3}$  OF THE WIRE BASKET



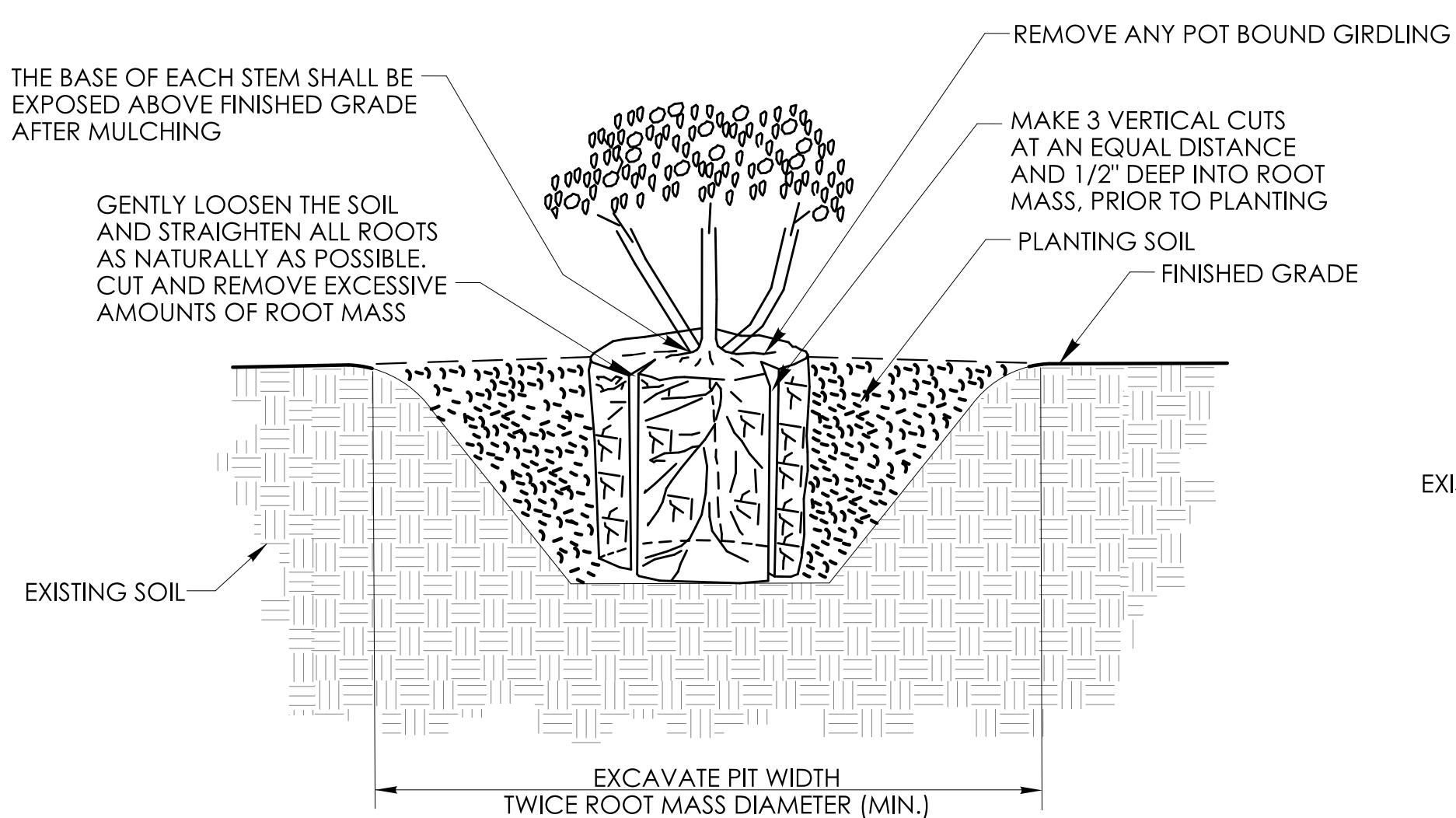
### BACKFILL AND MULCH FOR PLANTING



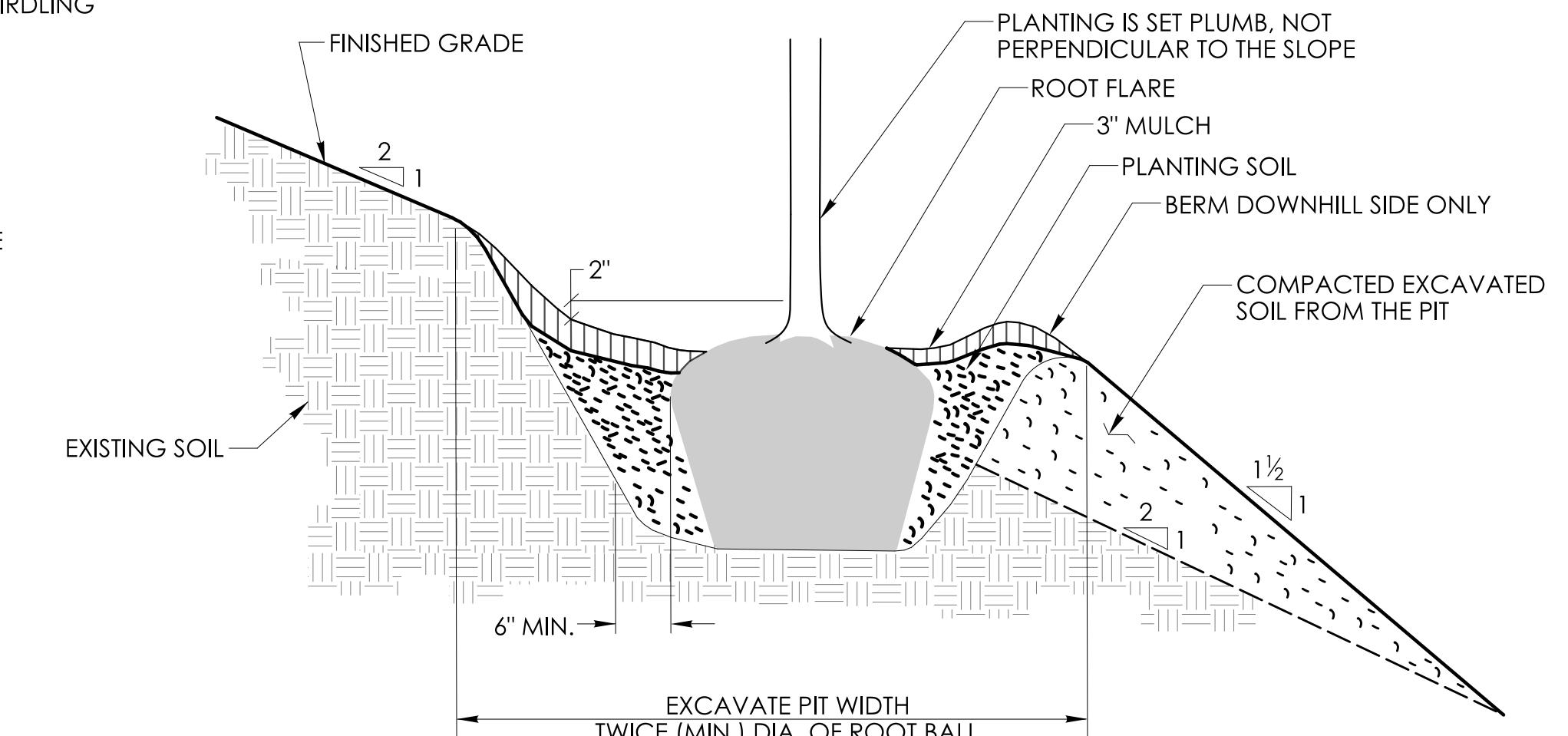
### VINE PLANTING



### HEAVY CLAY PLANTINGS



### CONTAINER GROWN PLANTING



### SLOPE PLANTING

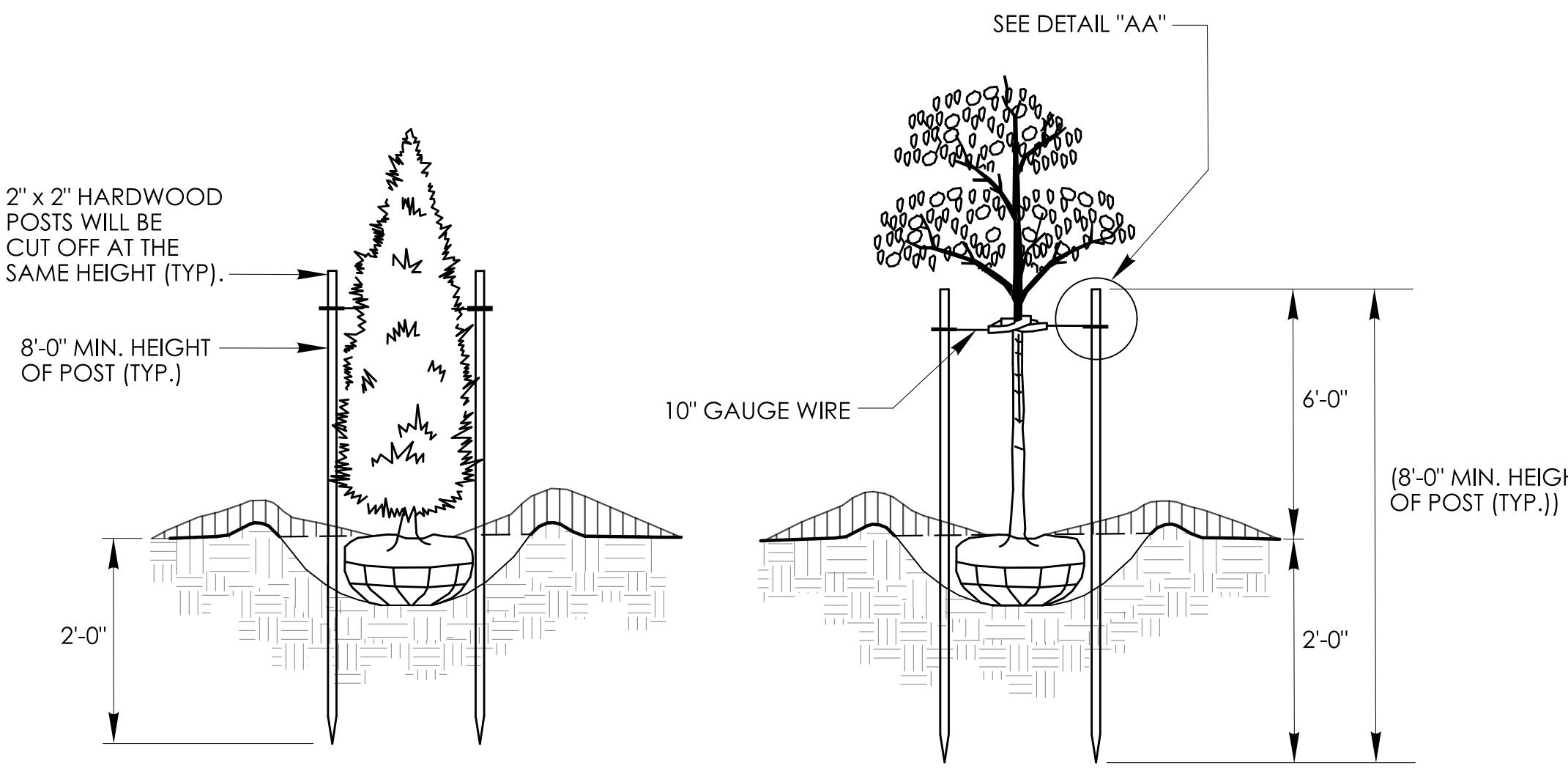
### GENERAL NOTES:

1. ALL EXTERIOR PACKAGING MATERIAL APPLIED TO PLANTS SHALL BE REMOVED AFTER THE PLANT IS LOCATED IN THE PIT EXCAVATION. CUT AND REMOVE TWINE, BURLAP OR WIRE BASKETS FROM THE TOP TWO-THIRDS OF THE ROOT BALL.
2. PLANT MALUS SPECIES (DECIDUOUS APPLE TREES OR SHRUBS) DEEP ENOUGH IN PIT TO COVER THE GRAFT TO PREVENT SPROUTING FROM THE ROOT STOCK.

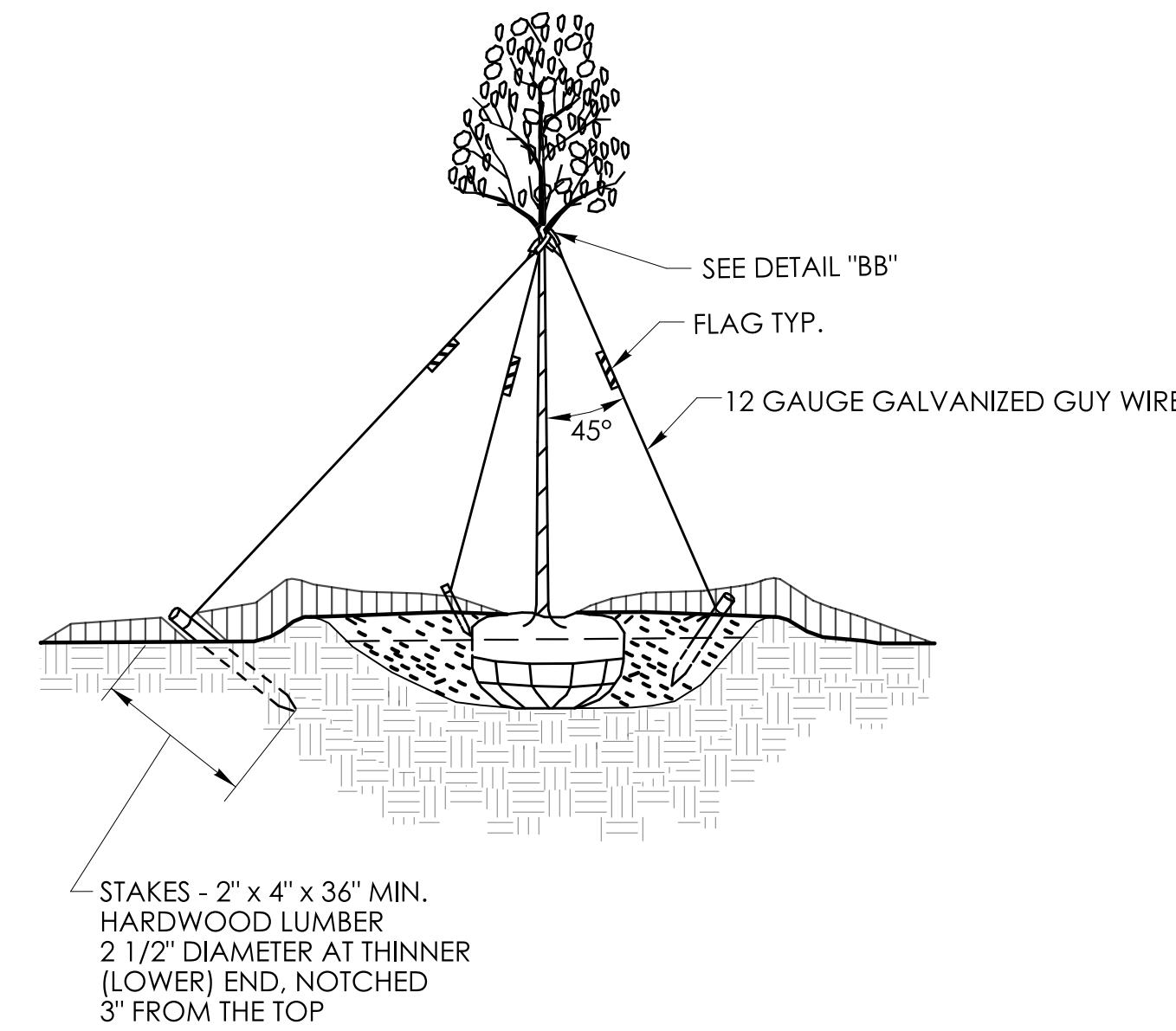
	NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____		STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: <b>LANDSCAPE PLANTING</b>	STANDARD SHEET NO.: <b>HW- 949_01a</b>
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## GENERAL NOTES:

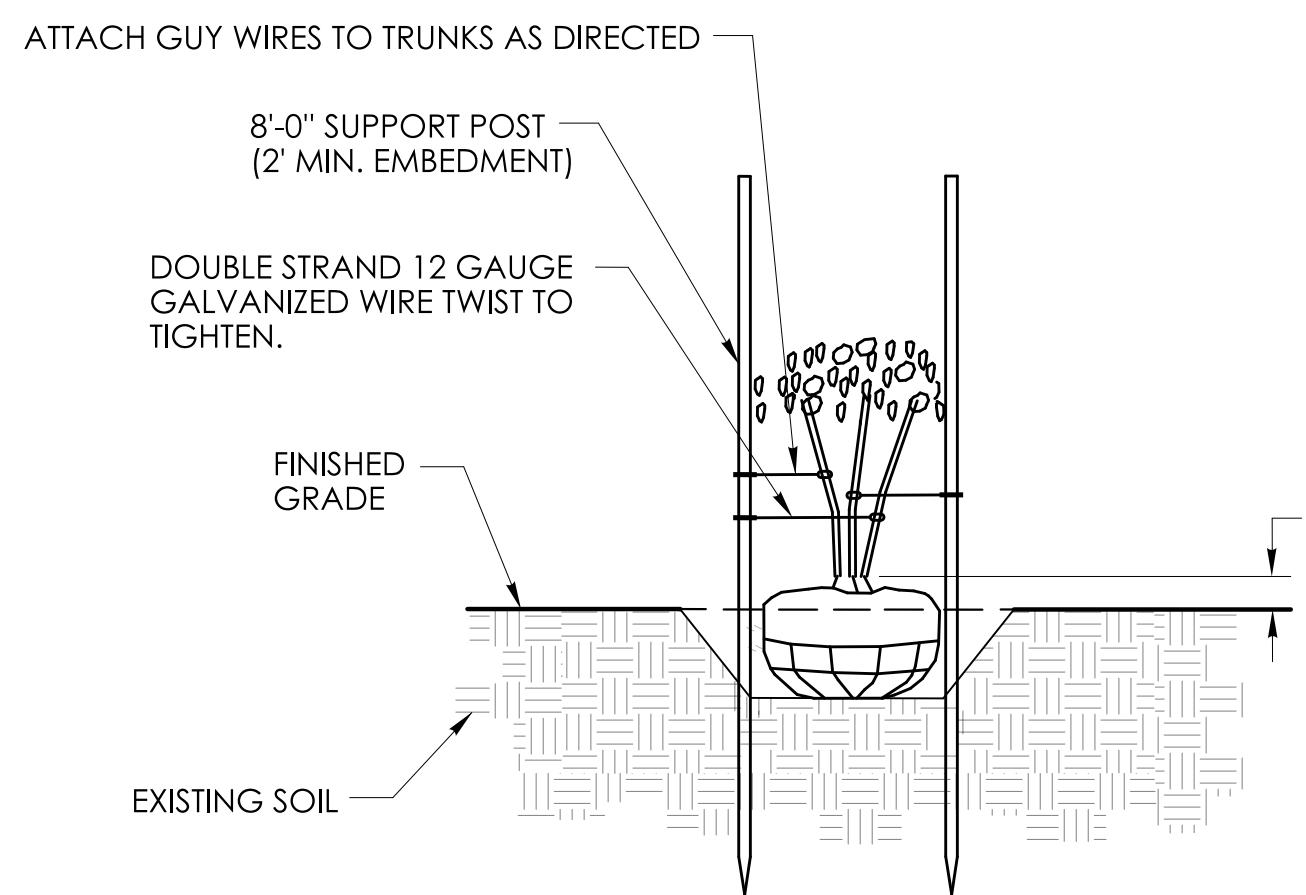
1. THE CONTRACTOR SHALL SUBMIT A STAKING PLAN FOR APPROVAL.
2. THE CONTRACTOR SHALL SUBMIT THE USE OF ANY OTHER MATERIALS FOR APPROVAL.
3. USE 3 POSTS FOR STAKING TREES 3" CALIPER OR GREATER AND EVERGREEN TREES 8' HIGH OR GREATER
4. USE DOUBLE STRAND 12 GAUGE GALVANIZED GUY WIRE FOR DECIDUOUS TREES GREATER THAN OR EQUAL TO 3" CALIPER AND USE DOUBLE STRAND 10 GAUGE GALVANIZED GUY WIRE FOR EVERGREEN TREES GREATER THAN OR EQUAL TO 8" CALIPER



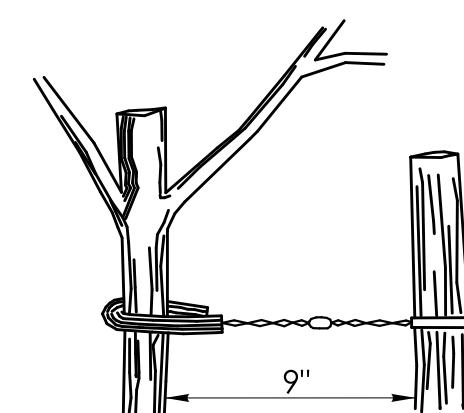
TWO STAKES



THREE GUYS AND STAKES



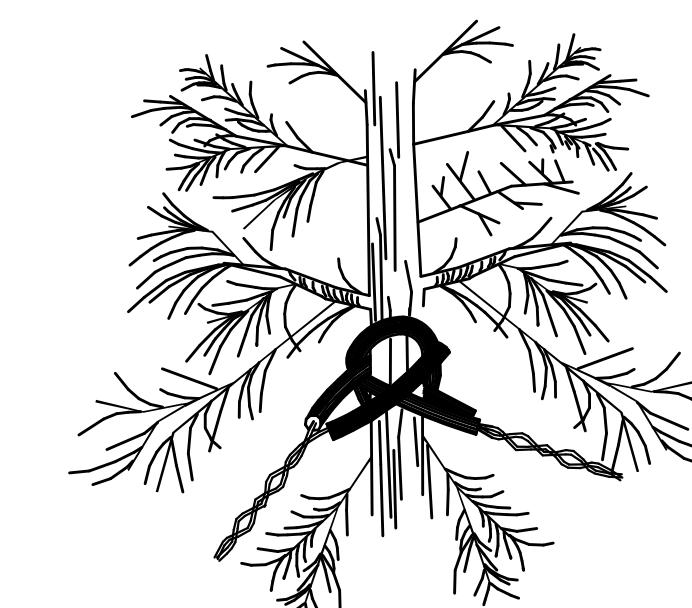
STAKING FOR MULTI-STEMMED TREES



DETAIL "AA"  
POST AND GUY WIRE

ANCHOR TREE TO POST(S) USING GALVANIZED GUY WIRE AND 3/8" MIN. INSIDE DIAMETER RUBBER HOSE

GUY WIRES SHOULD BE PLACED AT LEAST HALF WAY UP THE TRUNK



DETAIL "BB"  
GUY WIRES AROUND TRUNK

ANCHOR TREE TO STAKES USING GALVANIZED GUY WIRES AND 3/8" MIN. INSIDE DIAMETER RUBBER HOSE

GUY WIRES SHOULD BE PLACED AT LEAST HALF WAY UP THE TRUNK

	NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: <b>TREE STAKING</b>	STANDARD SHEET NO.: <b>HW- 949_01b</b>
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**ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #**

**\*\*REVISED  
OR ADDED**

**STANDARD SHEETS SHALL BE USED WITH STANDARD SPECIFICATIONS**

NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION 	STANDARD SHEET TITLE: <b>CTDOT STANDARD SHEET INDEX</b>	STANDARD SHEET NO.: <b>TR-STD_INDEX</b>
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DOCUMENT ALL LOOP DETECTOR VALUES BOTH CALCULATED AND MEASURED.

DEFINITIONS:

LOOP: #14 AWG WIRE IN SAWCUT, TERMINATED IN HANHOLE, IMSA SPEC 51-7.  
LEAD-IN: 14/2 SHIELDED TWISTED PAIR CABLE FROM HANHOLE TO CONTROLLER, IMSA SPEC 50-2.  
LOOP CIRCUIT: LOOP SAWCUT WIRE SPLICED TO 14/2 LEAD-IN CABLE.  
AMPLIFIER: ELECTRONIC DEVICE CONNECTED TO LOOP CIRCUIT. SENSES CHANGE IN RESONANT FREQUENCY AND CREATES AN OUTPUT TO THE CONTROLLER.  
MEGOMHMMETER: INSTRUMENT SPECIFICALLY DESIGNED TO TEST THE INSULATION RESISTANCE OF A CIRCUIT. COMMON MANUFACTURERS: AMEC®, AMPROBE®, FLUKE®, MEGGER®.

1: RESISTANCE:

1a: INSULATION RESISTANCE: PERFORM A 600 VOLT (MINIMUM) MEGOHMMETER TEST ON LOOP CIRCUIT. THE LOOP AMPLIFIER MUST BE DISCONNECTED FROM THE LOOP CIRCUIT OR THE LOOP AMPLIFIER WILL BE DAMAGED. THE RESISTANCE OF THE LOOP WIRE TO GROUND MUST BE GREATER THAN 100 MEG OHMS.  
1b: WIRE RESISTANCE: MEASURE THE DC RESISTANCE OF THE LOOP CIRCUIT. THE LOOP CIRCUIT MUST BE DISCONNECTED FROM THE AMPLIFIER. USING AN OHMMETER CONNECTED ACROSS THE LOOP CIRCUIT, MEASURE THE DC RESISTANCE OF THE CONDUCTORS. THE RESISTANCE SHOULD BE LESS THAN 4 OHMS.

NOTE: ALL TESTS SHALL BE DONE AT THE CONTROLLER ASSEMBLY (CA), HOWEVER IT IS RECOMMENDED TO PERFORM A PRELIMINARY MEGOHMMETER TEST AT THE HANHOLE PRIOR TO SEALING THE SAWCUT AND SPLICING TO THE LEAD-IN. IF A DEFECTIVE LOOP WIRE IS FOUND, IT MAY BE EASILY REPLACED.

2: LOOP CIRCUIT INDUCTANCE:

2a: CALCULATE INDUCTANCE OF LOOP ( $L_{LOOP}$ ) AND LEAD-IN CABLE ( $L_{14/2}$ ).

<u>LOOP INDUCTANCE (ENGLISH)</u>	<u>LOOP INDUCTANCE (METRIC)</u>
$L_{LOOP} = (P/4) (N^2 + N)$	$L_{LOOP} = (3.28P/4) (N^2 + N)$
LEAD-IN INDUCTANCE	LEAD-IN INDUCTANCE
$L_{14/2} = (0.24 \mu\text{H}/\text{FT}) (D)$	$L_{14/2} = (0.78 \mu\text{H}/\text{m}) (D)$

WHERE:

$L_{LOOP}$  = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS IN MICROHENRIES ( $\mu\text{H}$ ).  
 $L_{14/2}$  = INDUCTANCE OF LEAD-IN CABLE.  
 $P$  = PERIMETER OF INDIVIDUAL LOOP SEGMENT, IN FEET OR METERS.  
 $N$  = NUMBER OF TURNS.  
 $D$  = LENGTH OF LEAD-IN CABLE FROM SPLICING IN HANHOLE TO CONTROLLER, IN FEET OR METERS.  
 $L_T = L_1 + L_2 + L_3$  etc.,  
(TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN SERIES.)  
 $L_T = 1 / [(1 / L_1) + (1 / L_2) + (1 / L_3) +$  etc.],  
(TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN PARALLEL).

WHERE:

$L_T$  = TOTAL INDUCTANCE OF THE SEGMENTED ARRANGEMENT.  
 $L_1, L_2, L_3$  = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS.

EXAMPLE: (IN ENGLISH)

6' x 6', 4 TURNS, APPROXIMATELY 300' FROM THE CONTROLLER

$L_{LOOP} = (24/4) (4^2 + 4)$	$L_{14/2} = (0.24 \mu\text{H}/\text{FT}) (300)$
$L_{LOOP} = (6) (20)$	$L_{14/2} = (0.24) (300)$
$L_{LOOP} = 120 \mu\text{H}$	$L_{14/2} = 72 \mu\text{H}$

2b: MEASURE INDUCTANCE OF LOOP AND LEAD-IN AT CONTROLLER. USE INSTRUMENT DESIGNED TO MEASURE LOOP CIRCUIT INDUCTANCE.

3: POWER INTERRUPTION:

AFTER THE AMPLIFIER HAS TUNED AND IS OPERATING, DISCONNECT POWER BY REMOVING FUSE OR HARNESS CONNECTOR. RETURN POWER TO THE AMPLIFIER AND CONFIRM IT RE-TUNES AUTOMATICALLY WITHOUT ANY MANUAL ADJUSTMENTS.

**INDUCTIVE LOOP TEST PROCEDURE**

PIN	COLOR	FUNCTION
A	WHITE	110 VAC Neutral
B	BROWN	Output Relay Common (moving contact)
C	BLACK	110 VAC (Fused)
D	RED	Loop
E	ORANGE	Loop
F	YELLOW	Output Relay Contact (Closes with moving contact when detecting vehicle)
G	BLUE	Output Relay Contact (Opens with moving contact when detecting vehicle)
H	GREEN	Chassis Ground
J	GREY	110 VAC Delay/Extend Override
Shell		Ground (shall be connected to pin H in the connector)

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

INDUCTIVE LOOP DETECTOR

— SAW CUT

— RIGID METAL CONDUIT

— HANHOLE

**DETECTOR AMPLIFIER PIN DESIGNATION**

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATION BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED:  
- OVER 1" TO NEAREST 5 mm  
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE



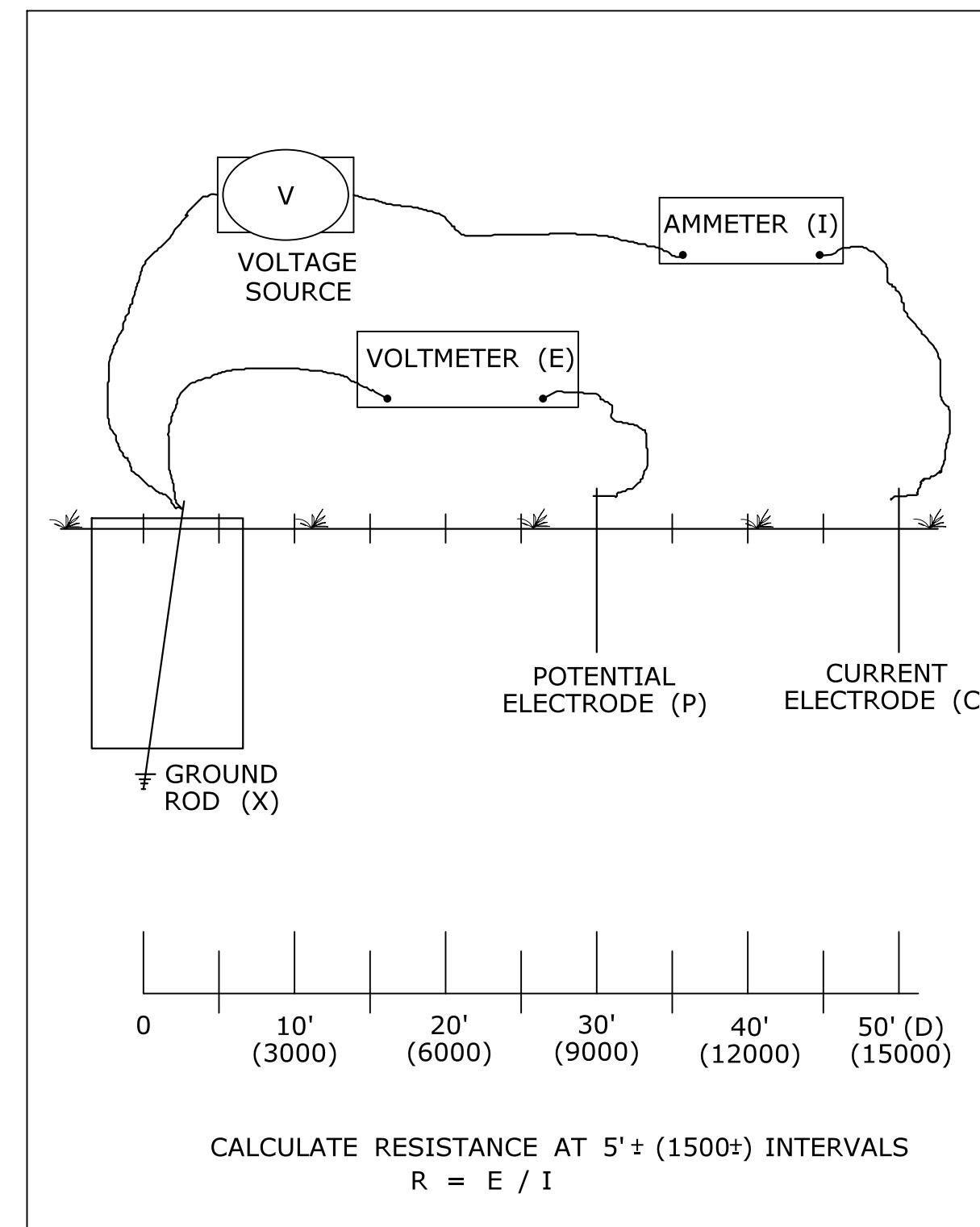
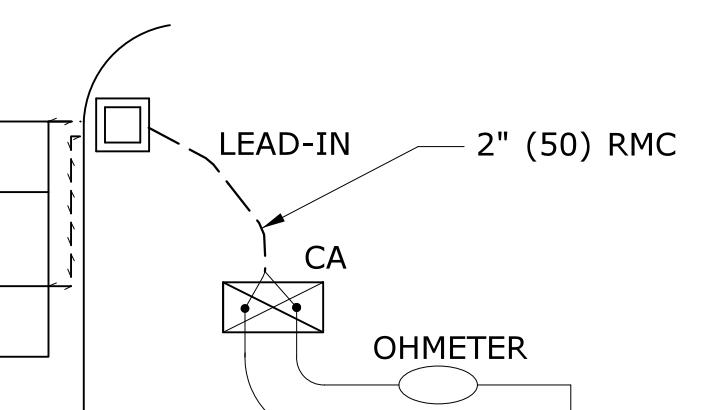
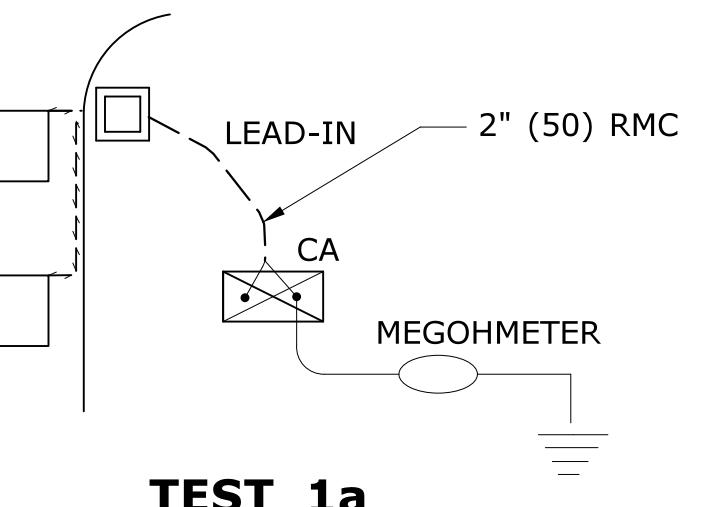
Filename: CTDOT\_TRAFFIC-STD.DGN Model: TR-1000-01

**TEST PROCEDURE:**

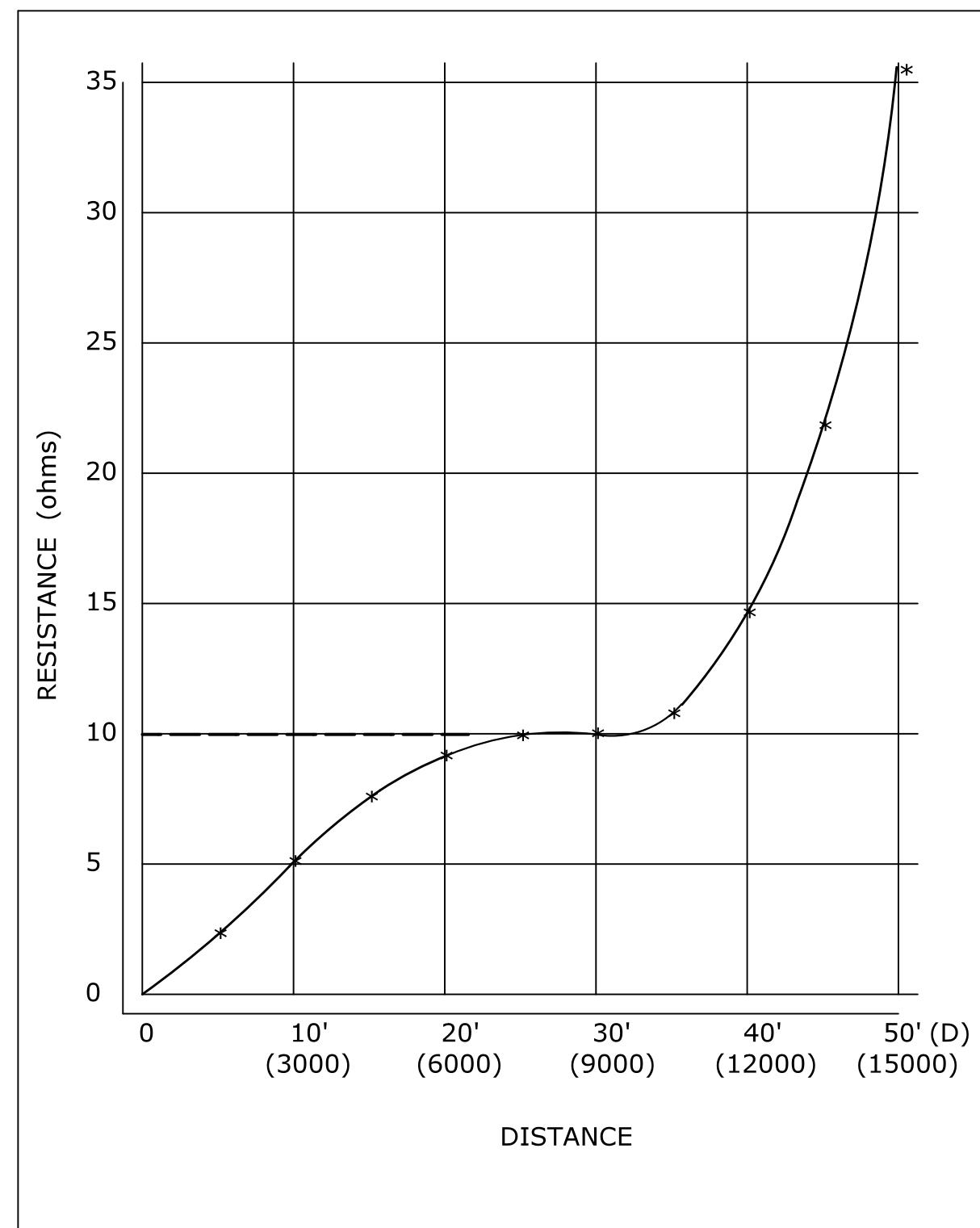
- INSERT ELECTRODE (C) A DISTANCE (D) FROM THE FOUNDATION. RECOMMEND A MINIMUM 50'.
- CONNECT A VOLTAGE SOURCE AND AMMETER BETWEEN THE FOUNDATION GROUND ROD (X) AND C.
- MEASURE THE CURRENT FLOW (I) BETWEEN X AND C.
- INSERT POTENTIAL ELECTRODE (P) AT 5'(1500) INTERVALS IN A STRAIGHT LINE TO ELECTRODE C.
- MEASURE VOLTAGE (E) AT EACH LOCATION OF P.
- CALCULATE RESISTANCE (R) AT EACH LOCATION OF P USING THE FORMULA  $R = E/I$ .
- THE ACTUAL GROUND RESISTANCE IS WHERE THE PLOTTED CURVE IS RELATIVELY FLAT, USUALLY AT 62% OF D. SEE EXAMPLE CHART: CURVE FLATTERS OUT AT 10 OHMS, APPROXIMATELY 30' (9000) FROM FOUNDATION.
- IF GROUND RESISTANCE IS GREATER THAN 10 OHMS, PERFORM CORRECTIVE ACTION AND RE-TEST.

**SUGGESTED CORRECTIVE ACTION:**

- INSTALL ADDITIONAL 10' (3000) GROUND ROD(S). REFER TO NESC SECTION 09, RULE 94.B.2. DRIVE ADDITIONAL GROUND RODS NO CLOSER TO FOUNDATION THAN 6'(1800). IF MORE THAN ONE IS NEEDED, SPACE MINIMUM 6'(1800) APART. BONDS TO ADDITIONAL GROUND ROD(S) SHALL BE MADE BY A CLAMP DESIGN FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE. TOP OF ADDITIONAL GROUND ROD(S) SHALL BE 6" (150) BELOW GRADE.
- IN AREAS OF SHALLOW BEDROCK, INSTALL A GROUND GRID OR ARRAY CONSISTING OF BURIED WIRE, RODS, STRIPS OR PLATES. REFER TO NESC SECTION 09, RULE 94.B.3. REFER TO NEC SECTION 250. MINIMUM DEPTH OF 18" (450). GRID CONNECTIONS AND BONDS ON GROUND GRID SHALL BE MADE BY CLAMPS DESIGNED FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE.



**3 POINT GROUND RESISTANCE TEST CIRCUIT**



**GROUND RESISTANCE CHART (EXAMPLE)**

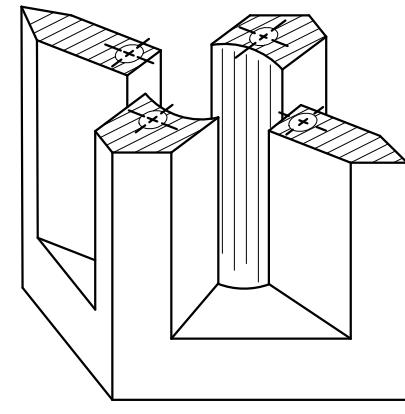
**NOTES:**

- WHEN REQUESTED BY THE ENGINEER, MEASURE RESISTANCE-TO-GROUND OF GROUND ROD AT TRAFFIC CONTROL FOUNDATIONS. SEE FALL-OF-POTENTIAL METHOD. IF LESS THAN 10 ohms, INSTALL SUPPLEMENTAL ELECTRODES AS REQUIRED. NEC ARTICLE 250.
- DURING THE TEST, THE GROUND ROD SHOULD NOT BE BONDED TO ANY RMC IN THE FOUNDATION.
- THE VOLTAGE SOURCE, VOLTMETER, AMMETER, ELECTRODES P AND C, AND CONNECTING CABLES ARE AVAILABLE AS A SPECIALIZED TEST INSTRUMENT.
- REFER TO NATIONAL ELECTRICAL SAFETY CODE (NEC) SECTION 09, GROUNDING METHODS FOR ELECTRIC SUPPLY AND COMMUNICATIONS FACILITIES.
- REFER TO NATIONAL ELECTRICAL CODE (NEC) CHAPTER 2, ARTICLE 250, GROUNDING.

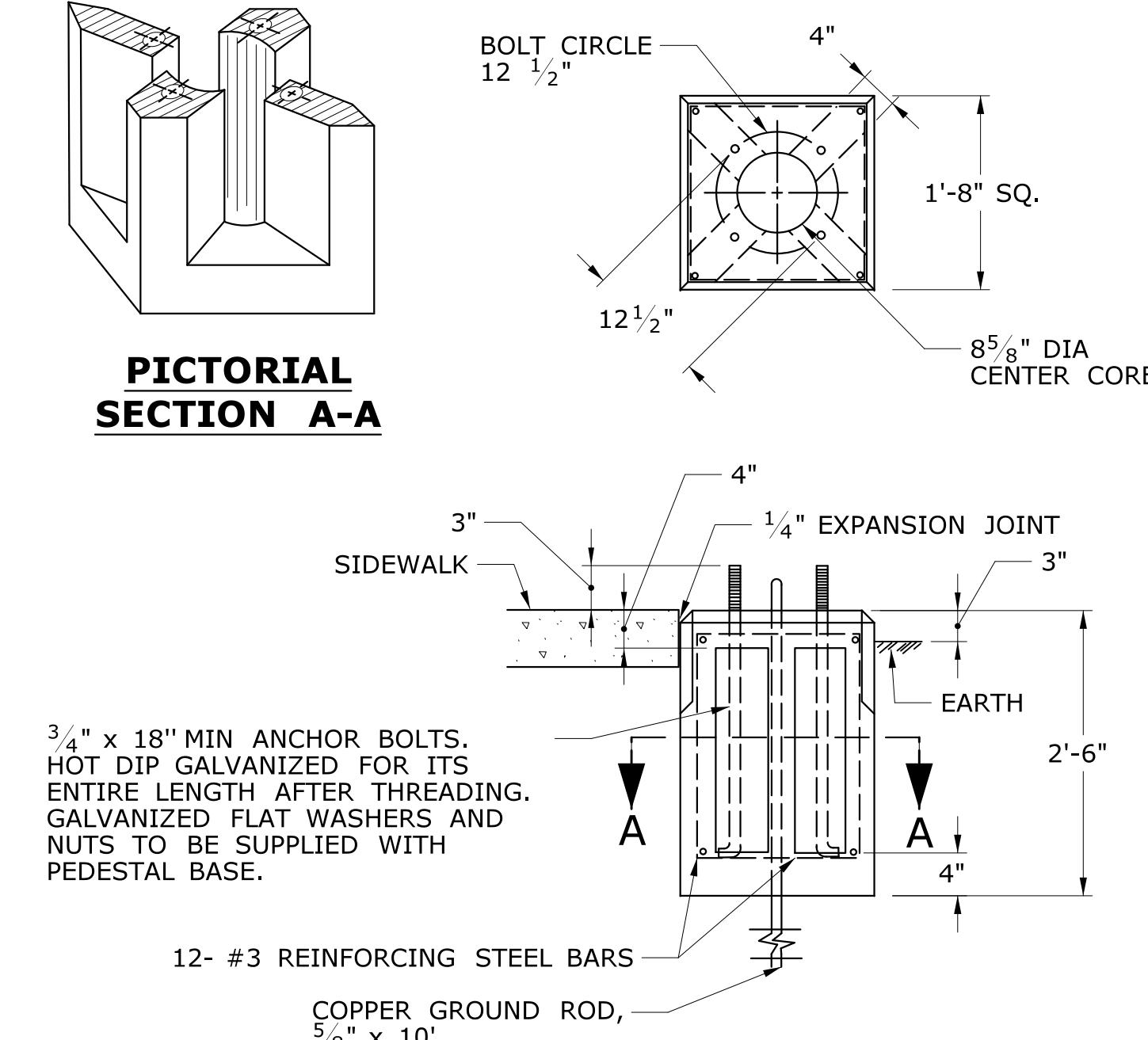
**3 POINT FALL-OF-POTENTIAL GROUND RESISTANCE TEST**

REV. DATE	REVISION DESCRIPTION	Plotted Date: 1/7/2014	SUBMITTED BY: NAME/DATE/TIME:	APPROVED BY: NAME/DATE/TIME:	STANDARD SHEET TITLE: GENERAL CLAUSES (TEST PROCEDURES)	STANDARD SHEET NO.: TR-1000_01
2 1-2014	REVISED GROUND RESISTANCE NOTES.					
1 4-2012	MINOR REVISIONS.					





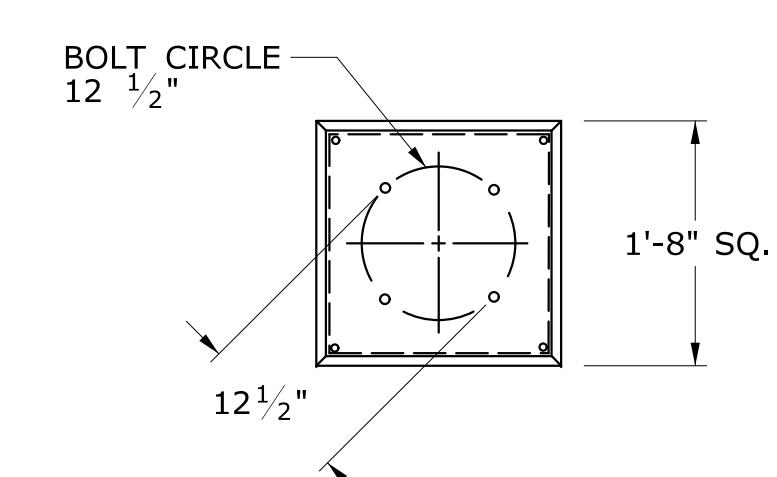
**PICTORIAL  
SECTION A-A**



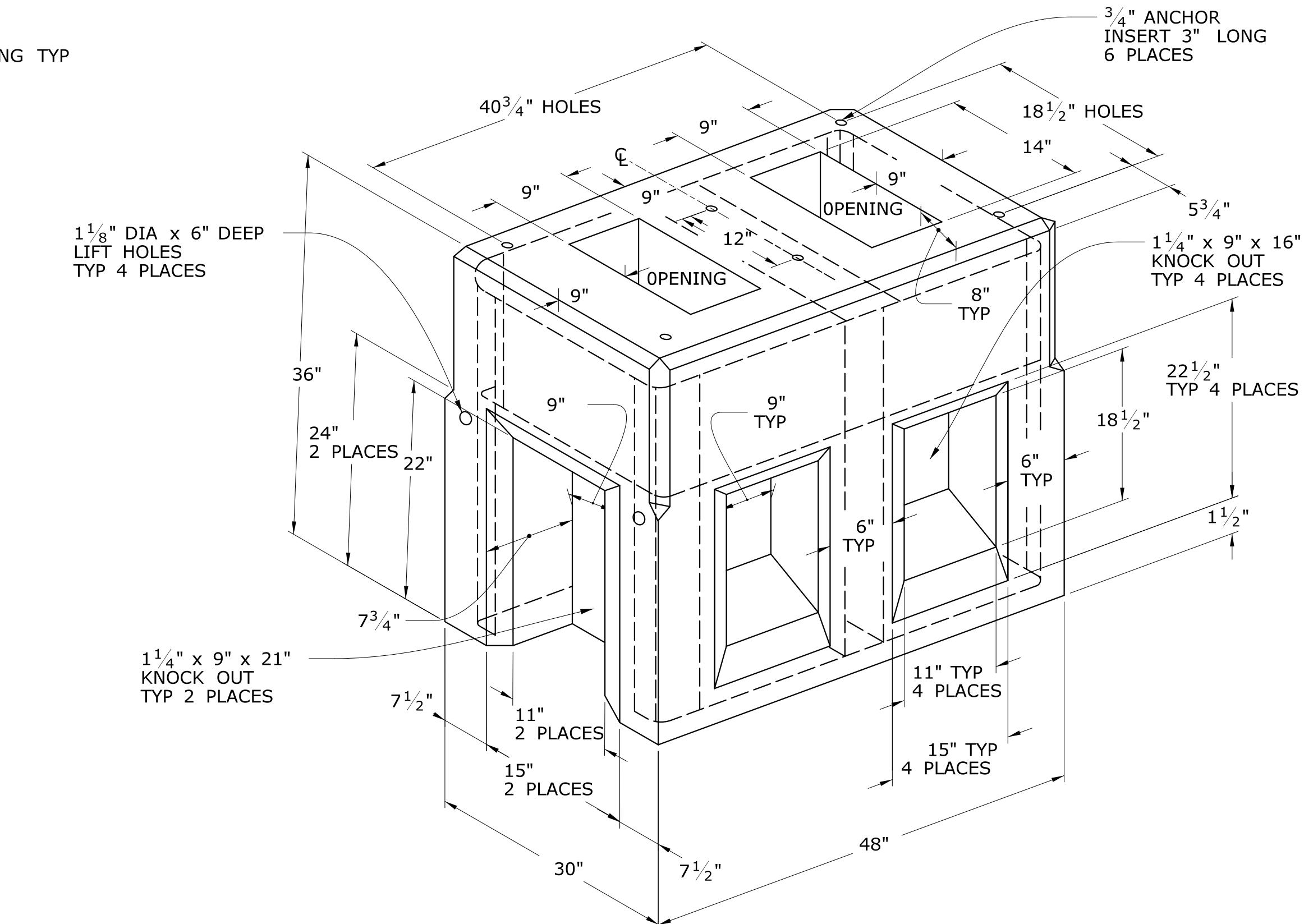
**TRAFFIC CONTROL FOUNDATION  
PEDESTAL - TYPE I - PRECAST**

TRAFFIC CONTROL FOUNDATION PEDESTAL TYPE I NOTES:

INSTALL FOUNDATION ON 12" OF COMPAKED GRANULAR FILL IN ACCORDANCE WITH SECTION 2.13  
LEVEL FOUNDATION WITH A PROJECTION OF 3" ABOVE FINISHED GRADE.  
INSTALL COPPER GROUND ROD: 5/8" X 10'  
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND  
GROUND ROD HAVE BEEN INSTALLED.  
CONDUITS SHALL NOT PROJECT MORE THAN 2" ABOVE FOUNDATION.  
CONCRETE: (CAST-IN PLACE) PCC04460 IN ACCORDANCE WITH SECTION M.03  
CONCRETE: (PRECAST) PRC04060 IN ACCORDANCE WITH ARTICLE M.14.01



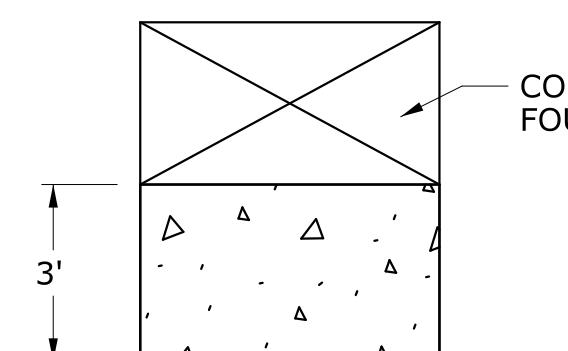
**TRAFFIC CONTROL FOUNDATION  
PEDESTAL - TYPE I - CAST IN PLACE**



**TRAFFIC CONTROL FOUNDATION  
CONTROLLER - TYPE IV - PRECAST**

TRAFFIC CONTROL FOUNDATION CONTROLLER TYPE IV NOTES:

INSTALL FOUNDATION ON 12" OF COMPAKED GRANULAR FILL IN ACCORDANCE WITH SECTION 2.13  
LEVEL FOUNDATION WITH A PROJECTION OF 4" ABOVE FINISHED GRADE.  
INSTALL COPPER GROUND ROD: 5/8" X 10'  
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND  
GROUND ROD HAVE BEEN INSTALLED. THE OPENINGS SHALL BE CAPPED WITH A 2"  
GROUT LEVEL WITH THE TOP OF THE FOUNDATION AND NEATLY FINISHED. THE GROUT  
SHALL CONFORM WITH THE REQUIREMENTS OF ARTICLE M.03.05.  
#4 REBAR 2" MIN COVER AROUND ALL OPENINGS, 3-#4 REBARS IN ALL CORNERS.  
CONDUITS SHALL NOT PROJECT MORE THAN 2" ABOVE FOUNDATION.  
CONCRETE: (PRECAST) PRC04060 IN ACCORDANCE WITH ARTICLE M.14.01



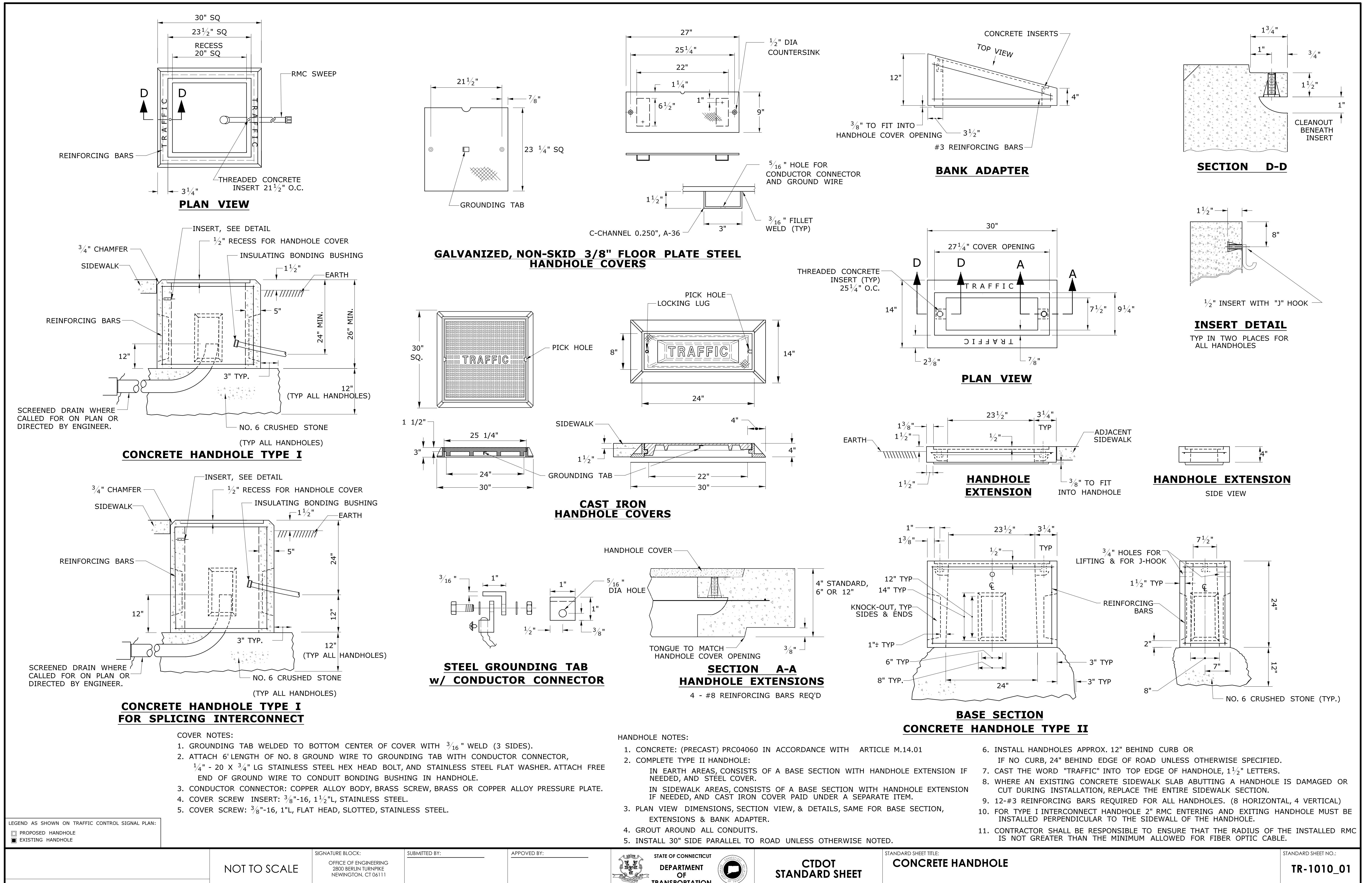
**TYPICAL CONCRETE SIDEWALK  
AT CONTROLLER FOUNDATION**

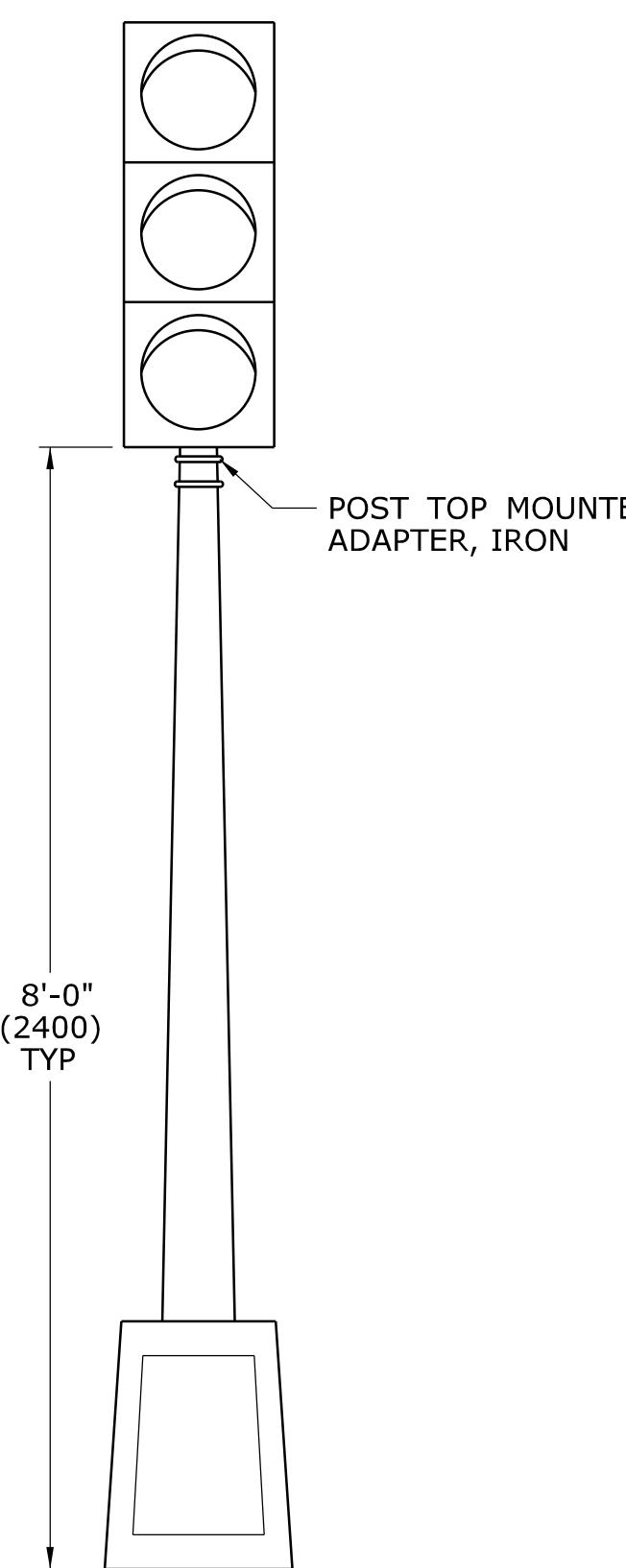
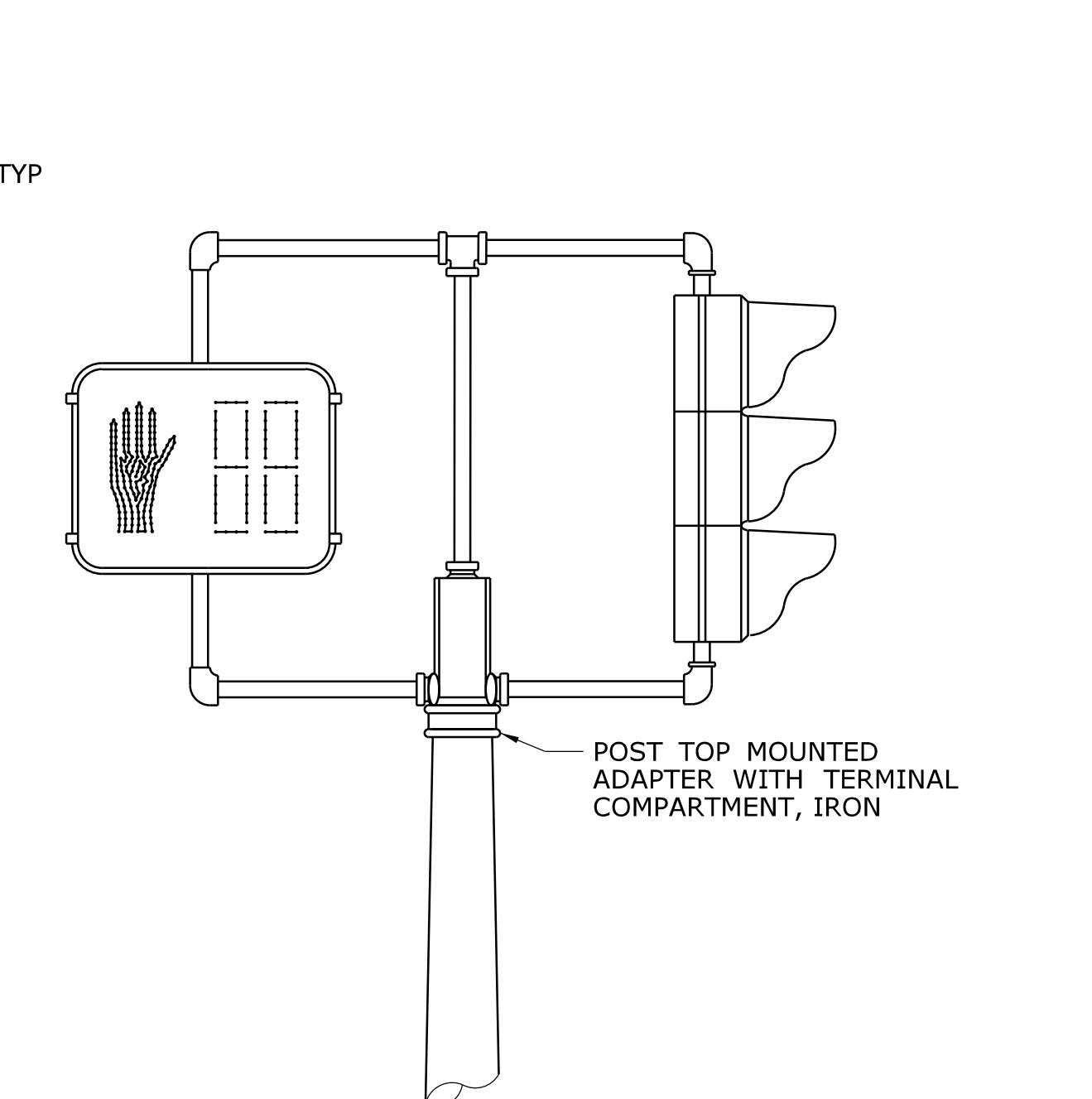
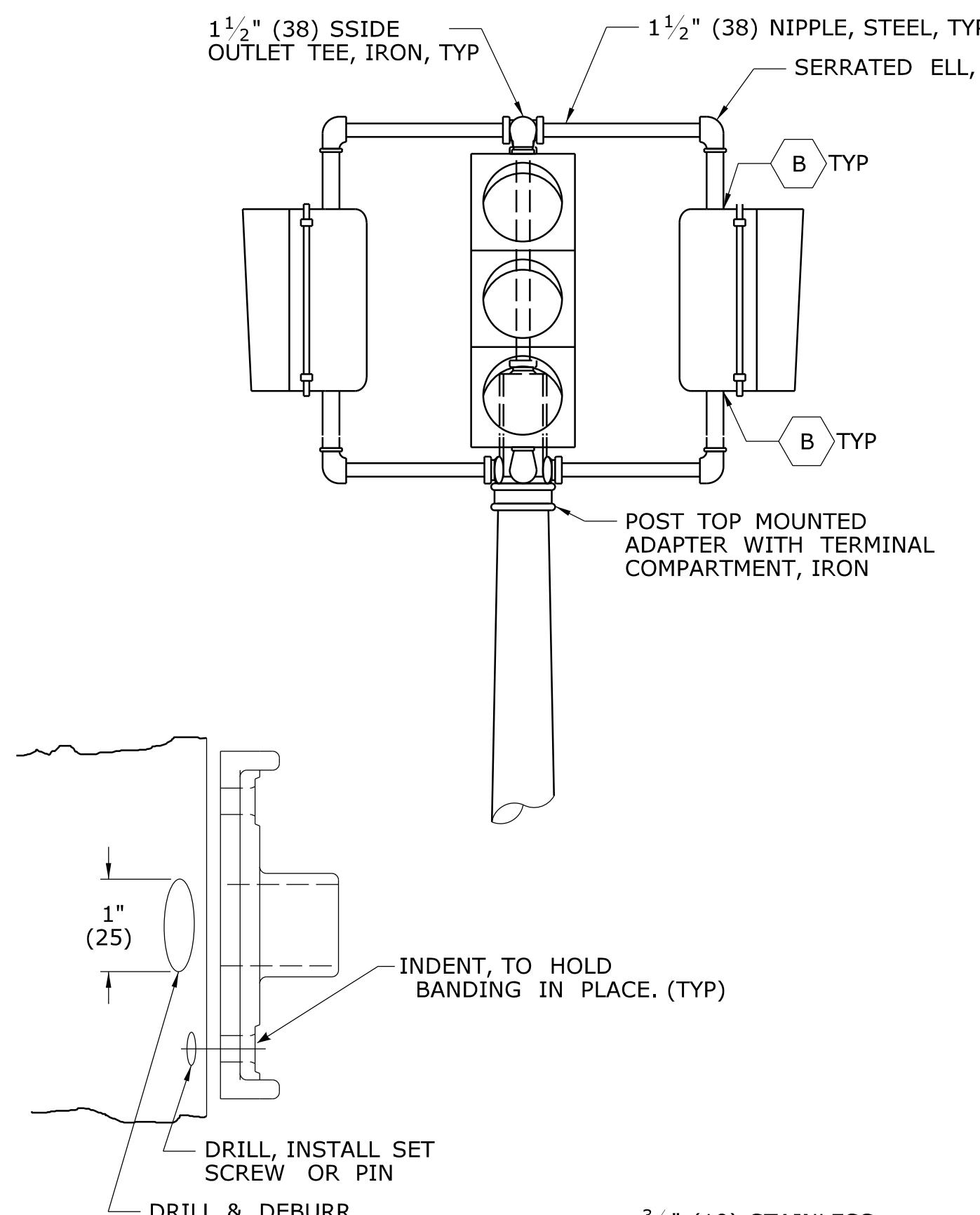
NOT TO SCALE

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

- PROPOSED CONTROLLER
- EXISTING CONTROLLER
- PEDESTAL MOUNTING

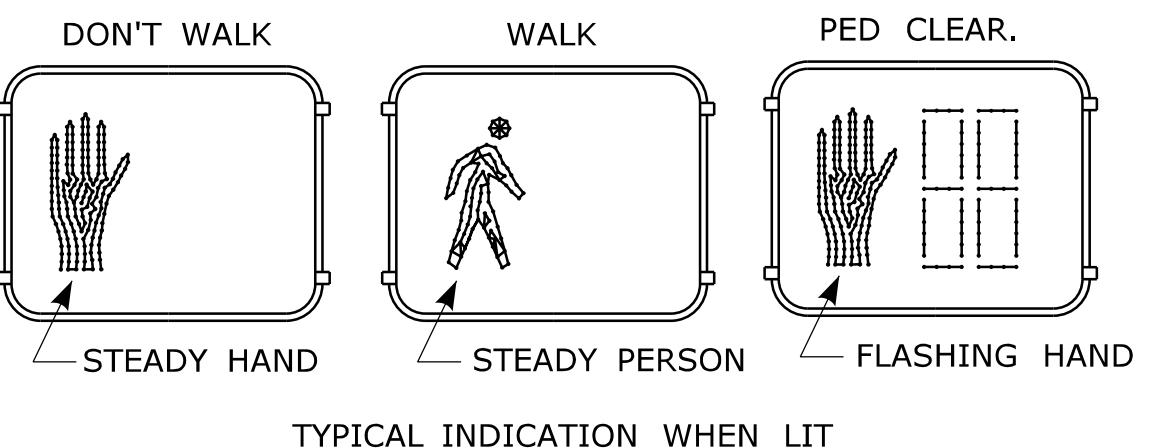
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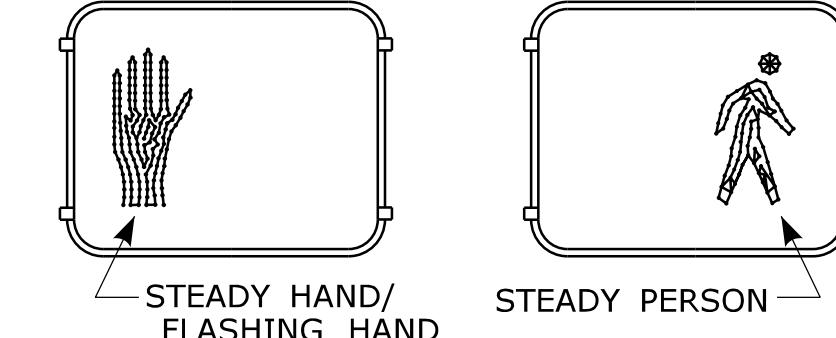


**ONE WAY TRAFFIC SIGNAL  
PEDESTAL MOUNTED**

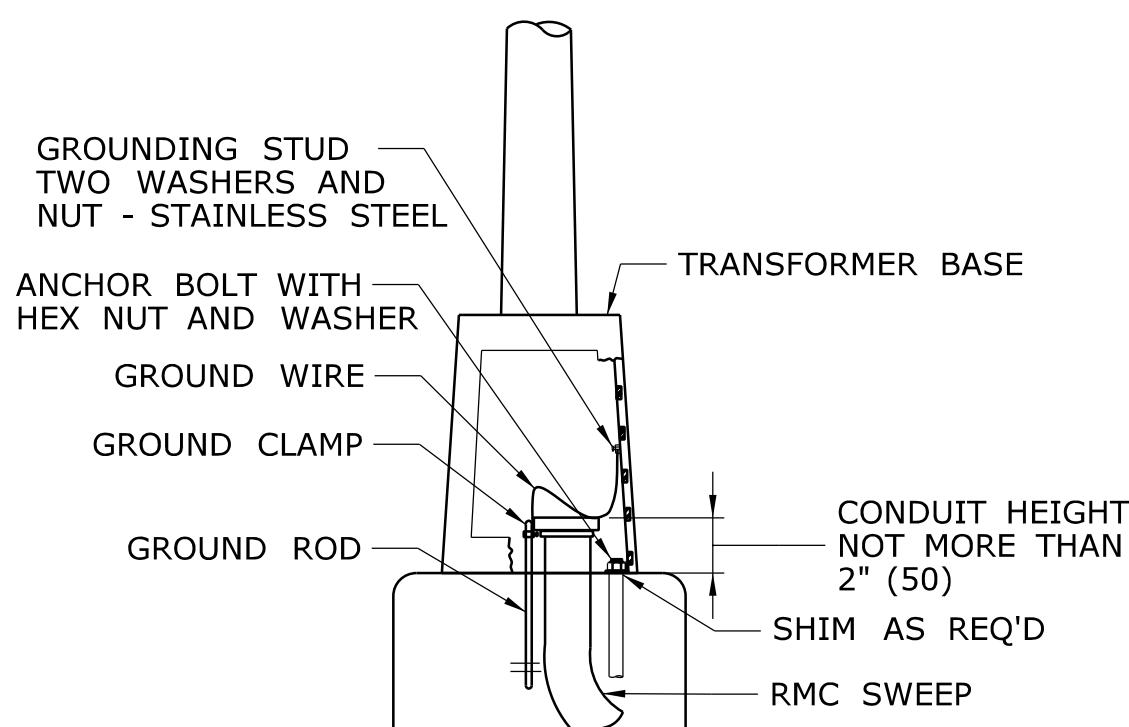
**ONE WAY WALK SIGNAL  
PEDESTAL MOUNTED**



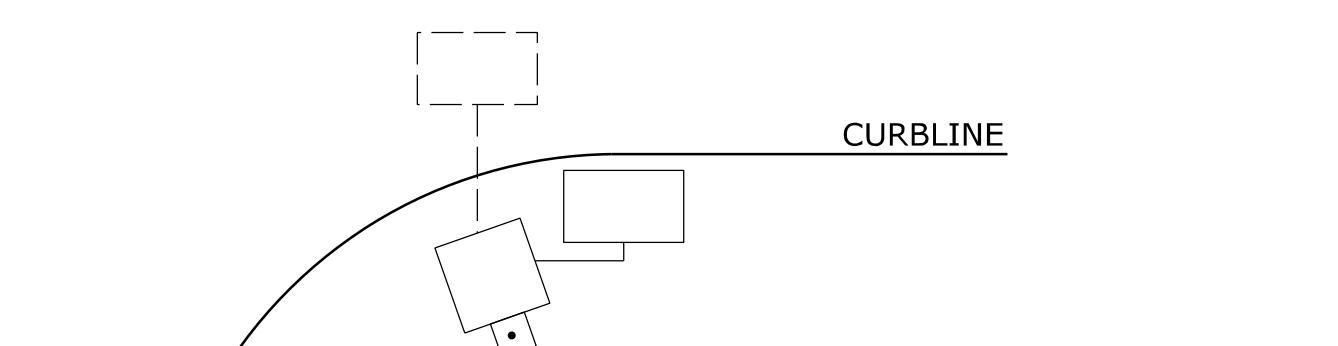
NON-COUNTDOWN DISPLAY, ONLY WHEN SHOWN ON PLAN.  
DON'T WALK/PED CLEAR.



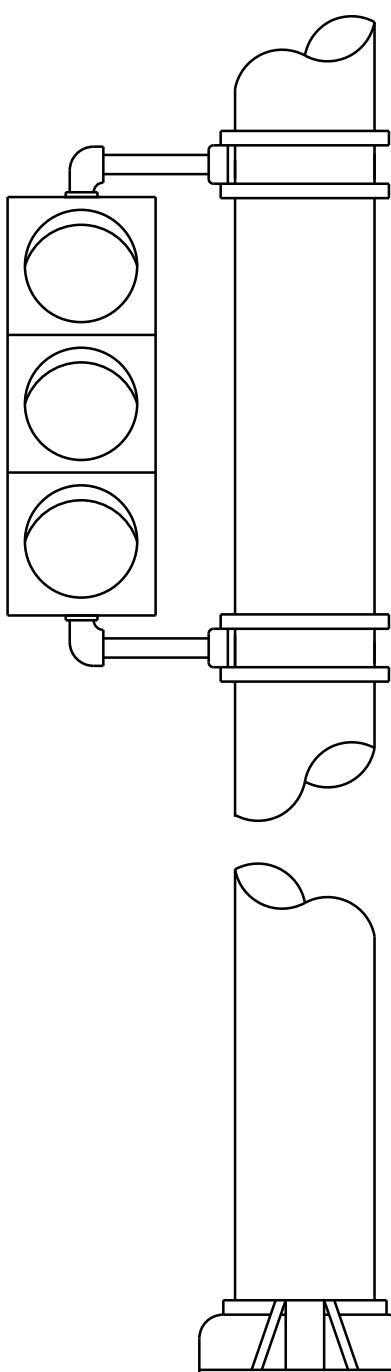
STEADY PERSON



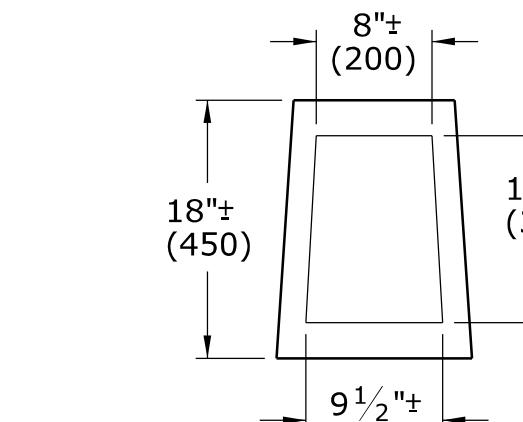
**ALUMINUM PEDESTAL  
INSTALLATION DETAIL**



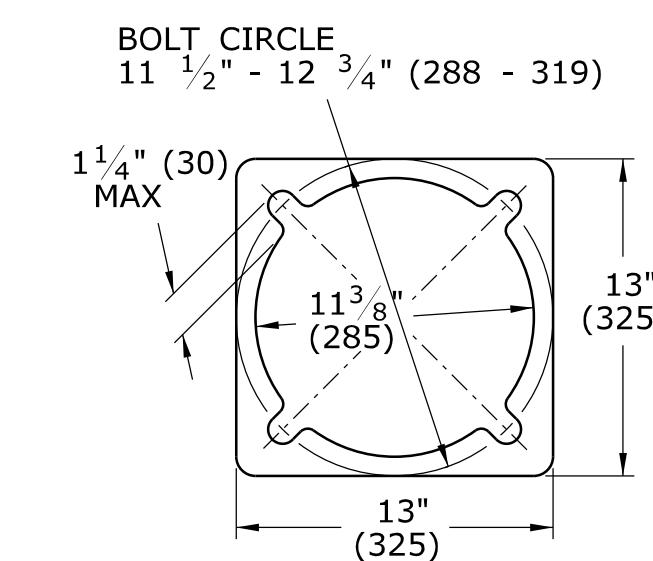
WHEN PEDESTALS OR SPAN POLES ARE INSTALLED CLOSE TO THE CURB,  
SIDE MOUNT PEDESTRIAN OR TRAFFIC SIGNALS TO AVOID VISOR DAMAGE  
FROM TURNING VEHICLES.



**ONE WAY TRAFFIC SIGNAL  
POLE MOUNTED**



**ALUMINUM PEDESTAL  
DOOR OPENING DETAIL**



NOTES:

- (A) SECURE LOWER HUB PLATE WITH STAINLESS STEEL SET SCREW OR PIN PRIOR TO BANDING TO PREVENT MOVEMENT. INSTALL CABLE THROUGH BOTTOM OF HUB PLATE.
- (B) REFER TO CTDOT TRAFFIC STANDARD SHEET, TR-1105-01, TRAFFIC SIGNALS & CABLE ASSIGNMENTS.
- (C) IF THREADED, MIN 1" (25) THREADED INTO BASE, SECURED WITH STAINLESS STEEL SET SCREWS.
- (D) BASE DESIGNED AS BREAK-AWAY.

INCANDESCENT WALK SIGNAL LAMPS ARE 67 WATTS, RATED AT 8000 HOURS LAMP LIFE.  
LED WALK SIGNAL LAMPS ARE MAXIMUM 15 WATTS, WARRANTED AT 5 YEAR LIFE.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:	
<input type="checkbox"/> STEEL SPAN POLE, MAST ARM ASSEMBLY SHAFT	<input type="checkbox"/> PEDESTRIAN SIGNAL
<input type="checkbox"/> ALUMINUM PEDESTAL	<input type="checkbox"/> PEDESTAL MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS
<input type="checkbox"/> TRAFFIC SIGNAL	<input type="checkbox"/> POLE MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS

2 4-2012	MINOR REVISIONS.
1 1-2010	INCLUDED COUNTDOWN PEDESTRIAN SIGNALS.
REV. DATE	REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS AND IS STATED AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE

Plotted Date: 4/14/2012

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm).  
METRIC DIMENSIONS ARE ROUNDED:  
- OVER 1" TO NEAREST 5 mm  
- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION

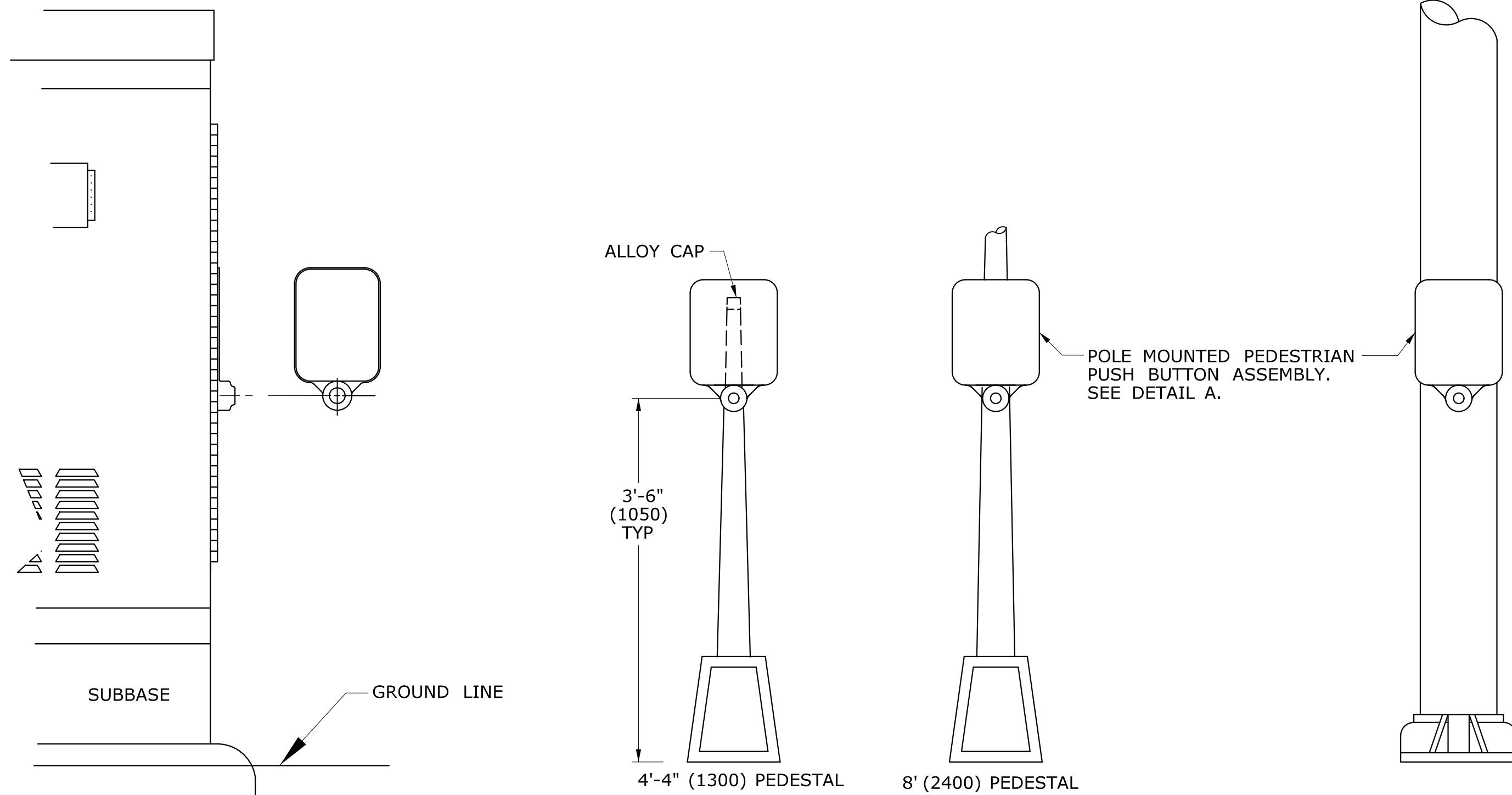
OFFICE OF ENGINEERING

CTDOT  
STANDARD SHEET

File Name: CTDOT-TRAFFIC-STD.dgn  
Model: TR-1102-01

SUBMITTED BY: NAME/DATE/TIME:  
APPROVED BY: NAME/DATE/TIME:

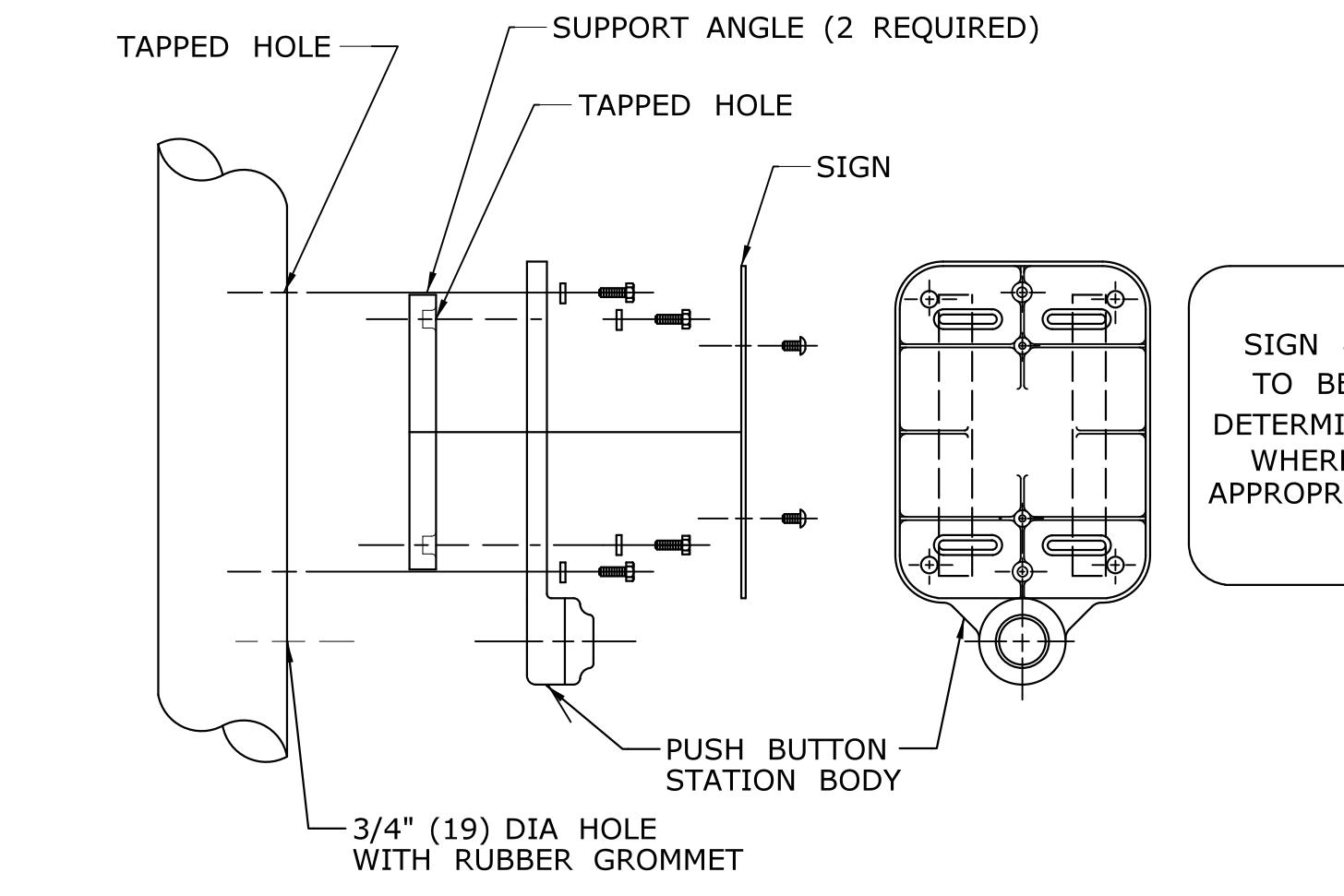
STANDARD SHEET TITLE:  
**PEDESTALS, PEDESTRIAN SIGNALS**  
STANDARD SHEET NO.: **TR-1102\_01**



**SURFACE MOUNTED**

**PEDESTAL MOUNTED**

**SPAN POLE/MAST ARM  
MOUNTED**



SIGN # 31-0833  
USE APPROPRIATE LEFT OR RIGHT ARROW

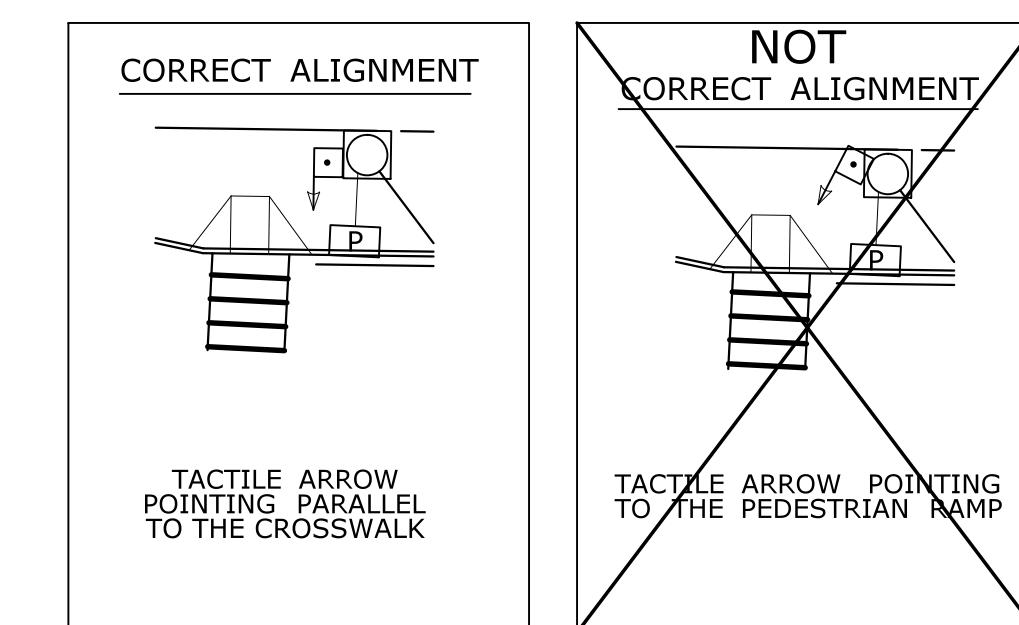
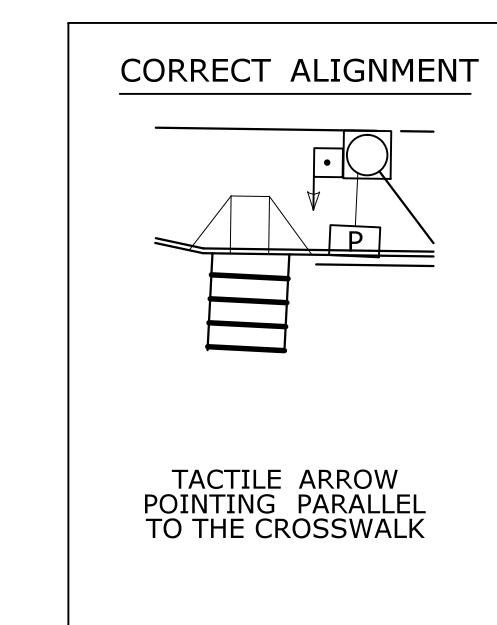
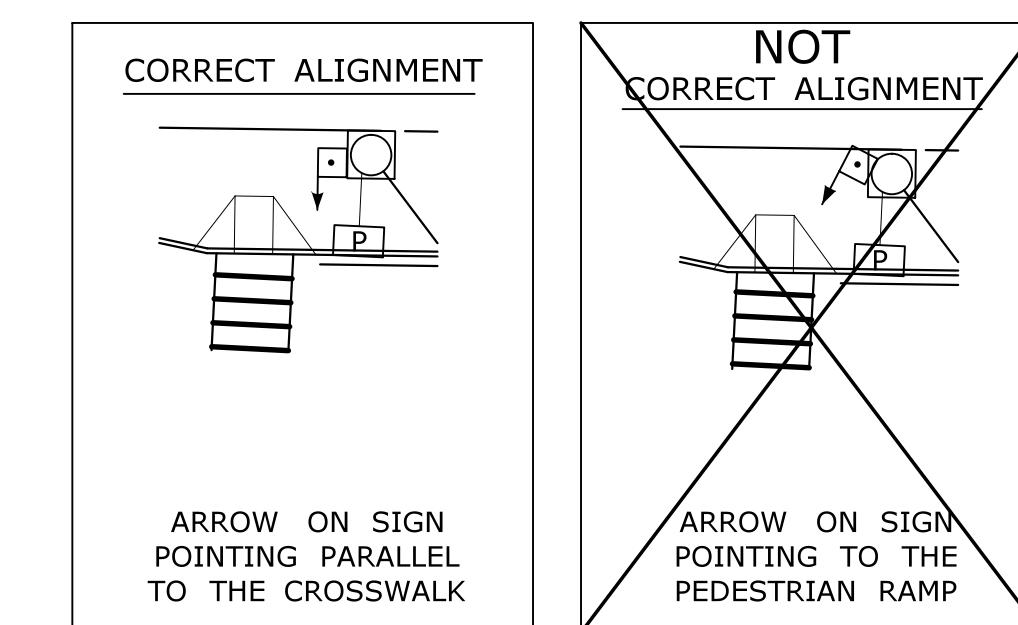
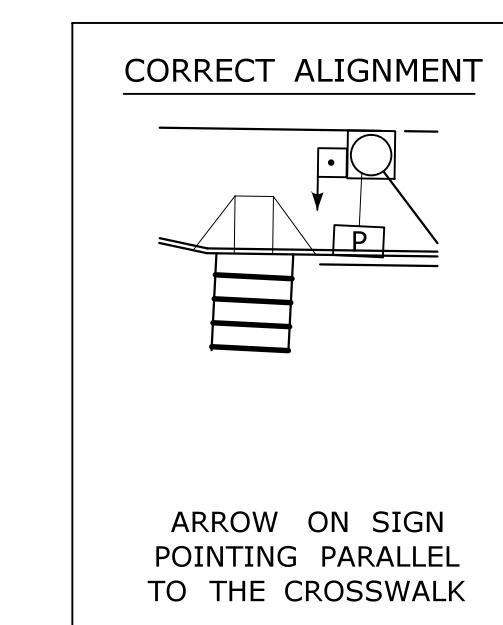


SIGN # 31-0835

**FOR CROSSING  
WITH SIDE STREET GREEN**

GENERAL NOTES:

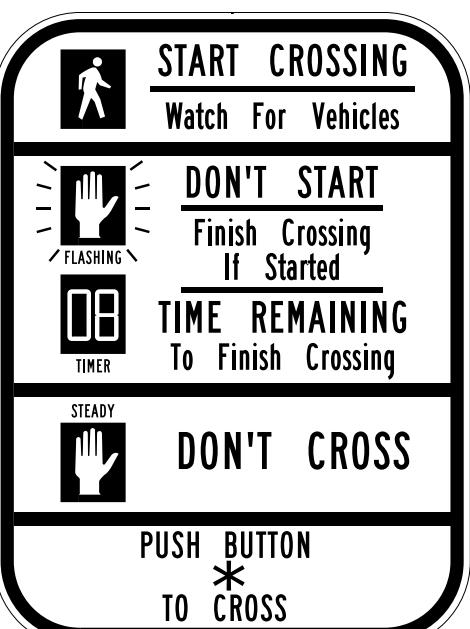
3'-6" (1050) FROM FINISHED GRADE SUCH AS SIDEWALK TO CENTER OF PUSH BUTTON.  
PUSH BUTTON INSTALLATIONS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS  
WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, CURRENT EDITION GOVERNS.  
4'-4" (1300) PEDESTAL TO INCLUDE ALLOY CAP SECURED WITH STAINLESS STEEL SET SCREW.



**PEDESTRIAN PUSH BUTTON ALIGNMENT**

**ACCESSIBLE PEDESTRIAN SIGNAL AND DETECTOR**

**EXAMPLE ALIGNMENTS  
FOR EXCLUSIVE PEDESTRIAN PHASE**



\* USE APPROPRIATE ARROW UNLESS  
OTHERWISE NOTED ON PLAN.

FOR NEW PUSHBUTTON HOUSING,  
USE 9" x 15" SIGN NO. 31-0856.

FOR EXISTING PUSHBUTTON HOUSING,  
WITH 9" x 12" SIZE, USE SIGN NO. 31-0845.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:  
 PEDESTRIAN PUSH BUTTON  
 PEDESTRIAN PUSH BUTTON, PEDESTAL MOUNTED  
 PEDESTRIAN PUSH BUTTON, POLE MOUNTED

3 8-2018	UPDATED PEDESTRIAN SIGN LEGENDS AND NOTES.
2 4-2014	ADDED PEDESTRIAN EXAMPLE ALIGNMENTS
1 4-2012	MINOR REVISIONS & UPDATED SIGN #31-0845.
REV. DATE	REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS AND IS STATED AND IS UNDERTAKEN TO GUARANTEE THAT IT INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DIMENSIONS ARE IN ENGLISH ("") & METRIC UNITS (mm).  
METRIC DIMENSIONS ARE ROUNDED:  
- OVER 1" TO NEAREST 5 mm  
- UNDER 1" TO NEAREST 1 mm.  
NOT TO SCALE

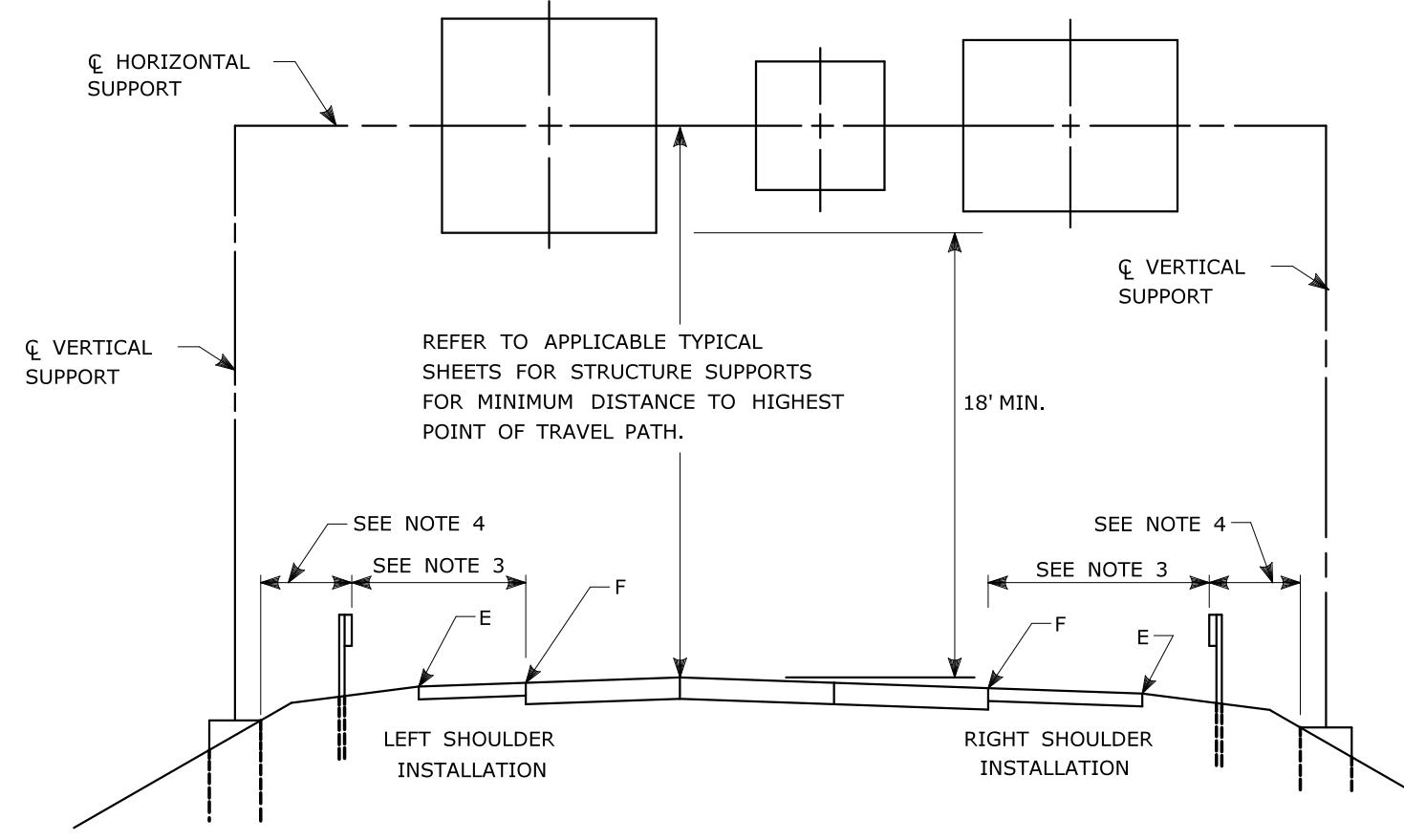
STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION  
NOT TO SCALE  
Filename: CTDOT\_TRAFFIC\_STD\_2018-01-25.dgn Model: TR-1107\_01

SUBMITTED BY: NAME/DATE/TIME:  
APPROVED BY: NAME/DATE/TIME:

CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:  
PEDESTRIAN PUSH BUTTONS

STANDARD SHEET NO.:  
TR-1107\_01



#### TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS

NOTES:

- 1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
- 2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANES.
- 3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
- 4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
- 5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.

FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.

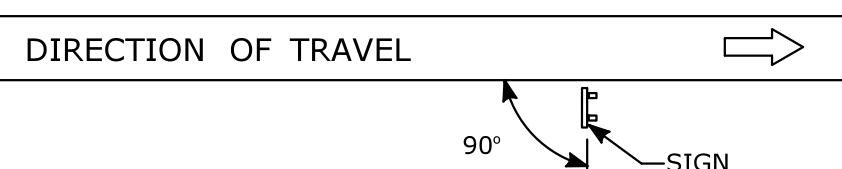


DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

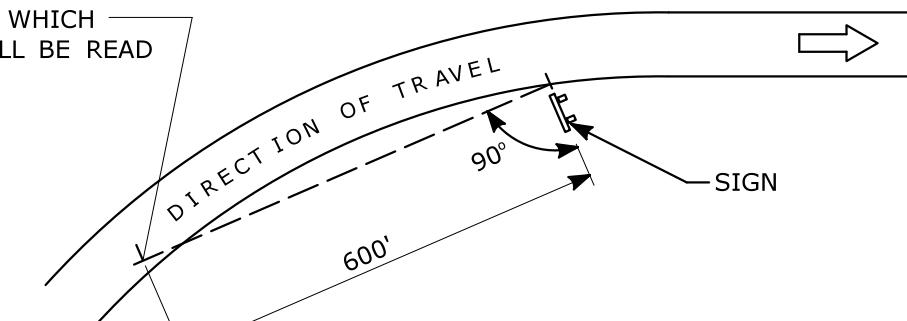
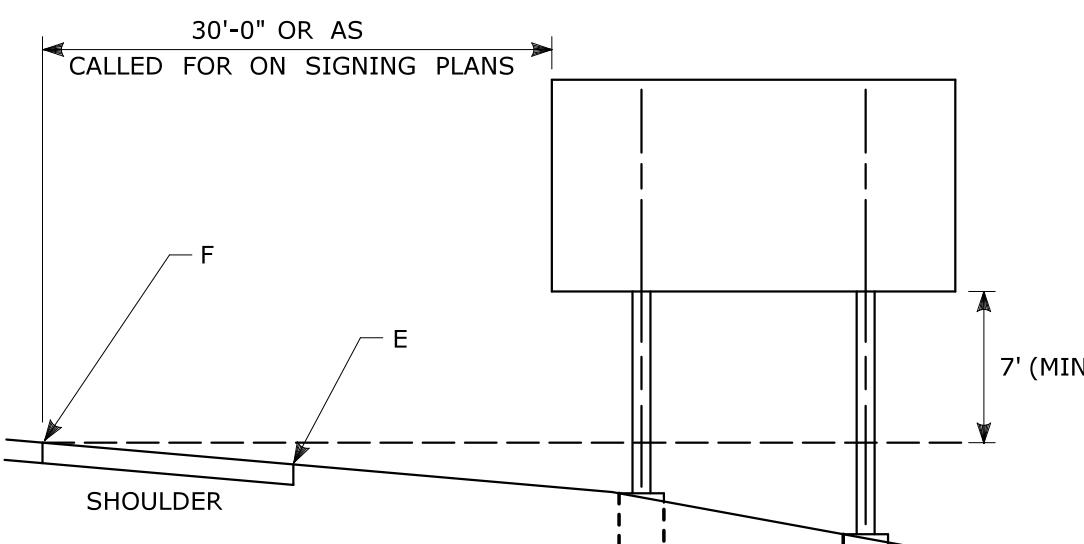


DIAGRAM "B"

#### SIGN ORIENTATION DETAILS FOR SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

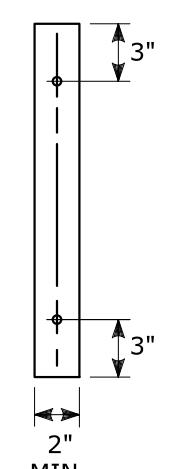


#### TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

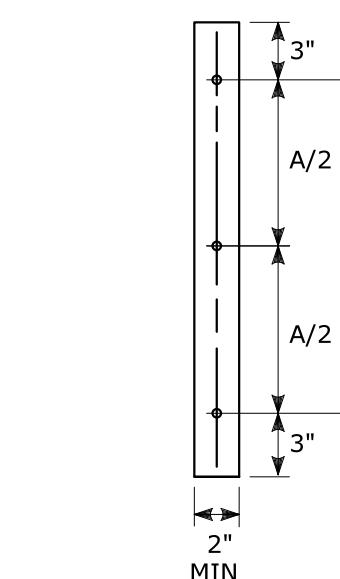
NOTES:

- 1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
- 2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
- 3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
- 4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

RETROREFLECTIVE STRIPS  
48" LONG OR LESS:



RETROREFLECTIVE STRIPS  
OVER 48" LONG:

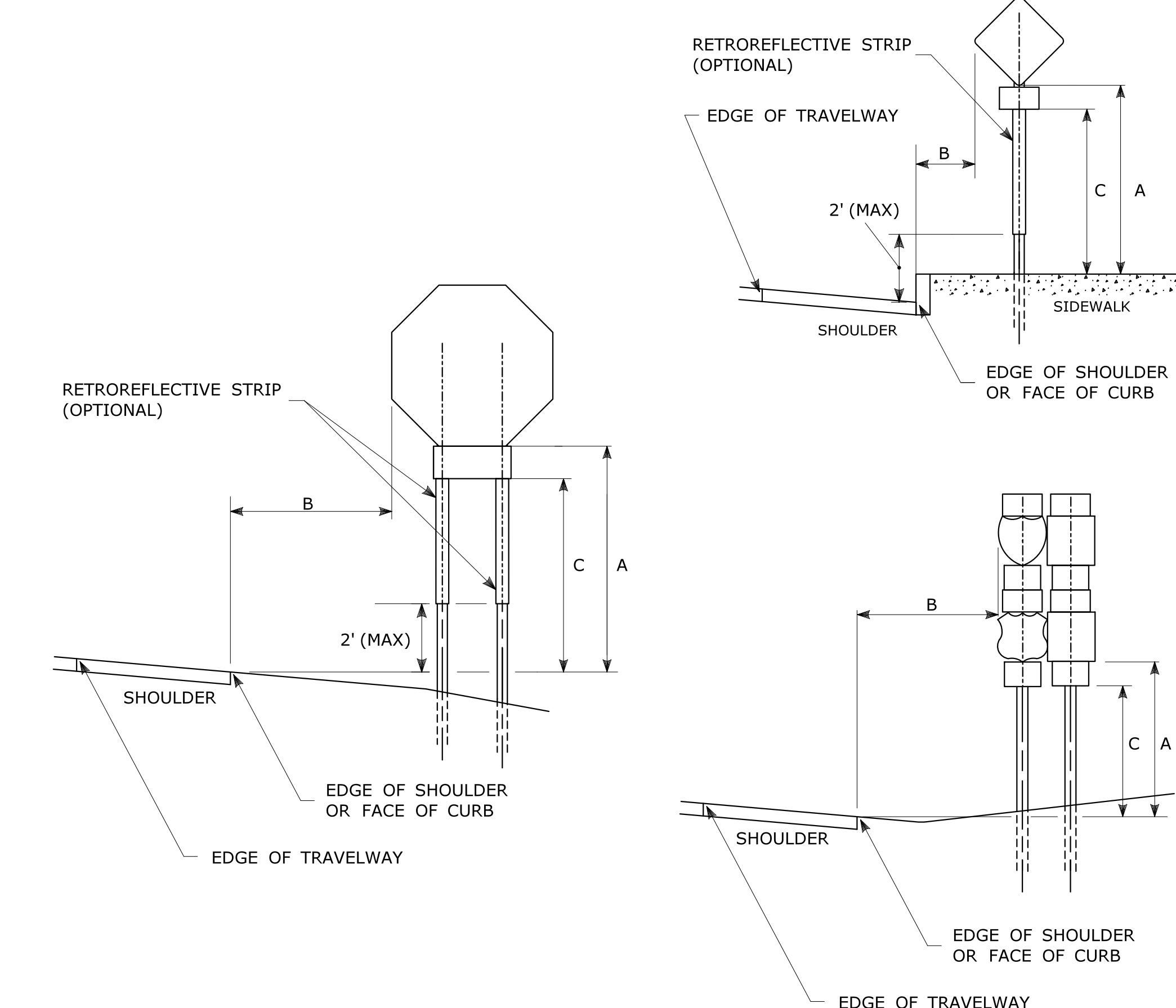


#### RETROREFLECTIVE STRIP DETAIL

NOTES:

RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE. REFER TO STANDARD SHEET No. TR-1208-02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS.

RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.



#### TYPICAL SIGN PLACEMENT DETAIL

NOTES:

ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY. REFER TO STANDARD SHEET No. TR-1208-02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING.

IF A RETROREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY. PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET ①	DIM."C" MIN PLAQUE HEIGHT ①	ASSEMBLY LOCATION
7' ②	6' 12' ③	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMPS • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS
4'	6' 12' ③	N/A	INCIDENT MANAGEMENT SIGNS AND MILE POST MARKER ASSEMBLIES LOCATED ON FREEWAYS AND EXPRESSWAYS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' ④	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' ④	7'	SIDEWALKS ⑤

① OR AS DIRECTED BY THE ENGINEER

② 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.

③ 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE  
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.

④ A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.

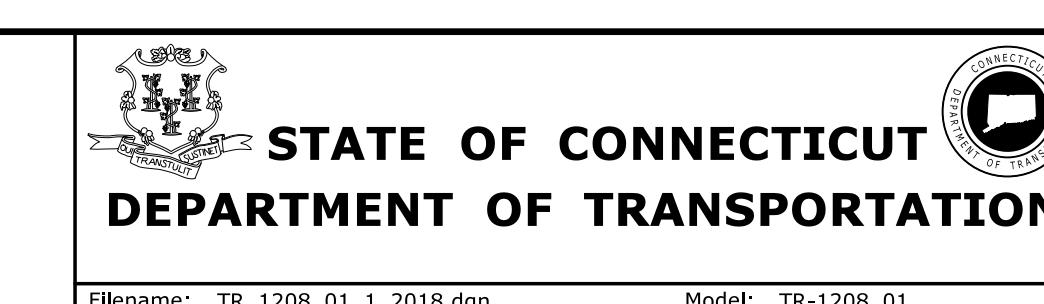
⑤ A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.

3	8-2018	INCLUDED INCIDENT MANAGEMENT AND MILE MARKER SIGNS.
2	4-2017	MINOR REVISIONS.
1	2-2011	MINOR REVISIONS.
REV. DATE		REVISION DESCRIPTION

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS AND IS STATED AND IS NOT GUARANTEED TO STATE AND IS THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 8/10/2018

NOT TO SCALE



Filenumber: TR-1208-01-1-2018.dgn

Model: TR-1208-01

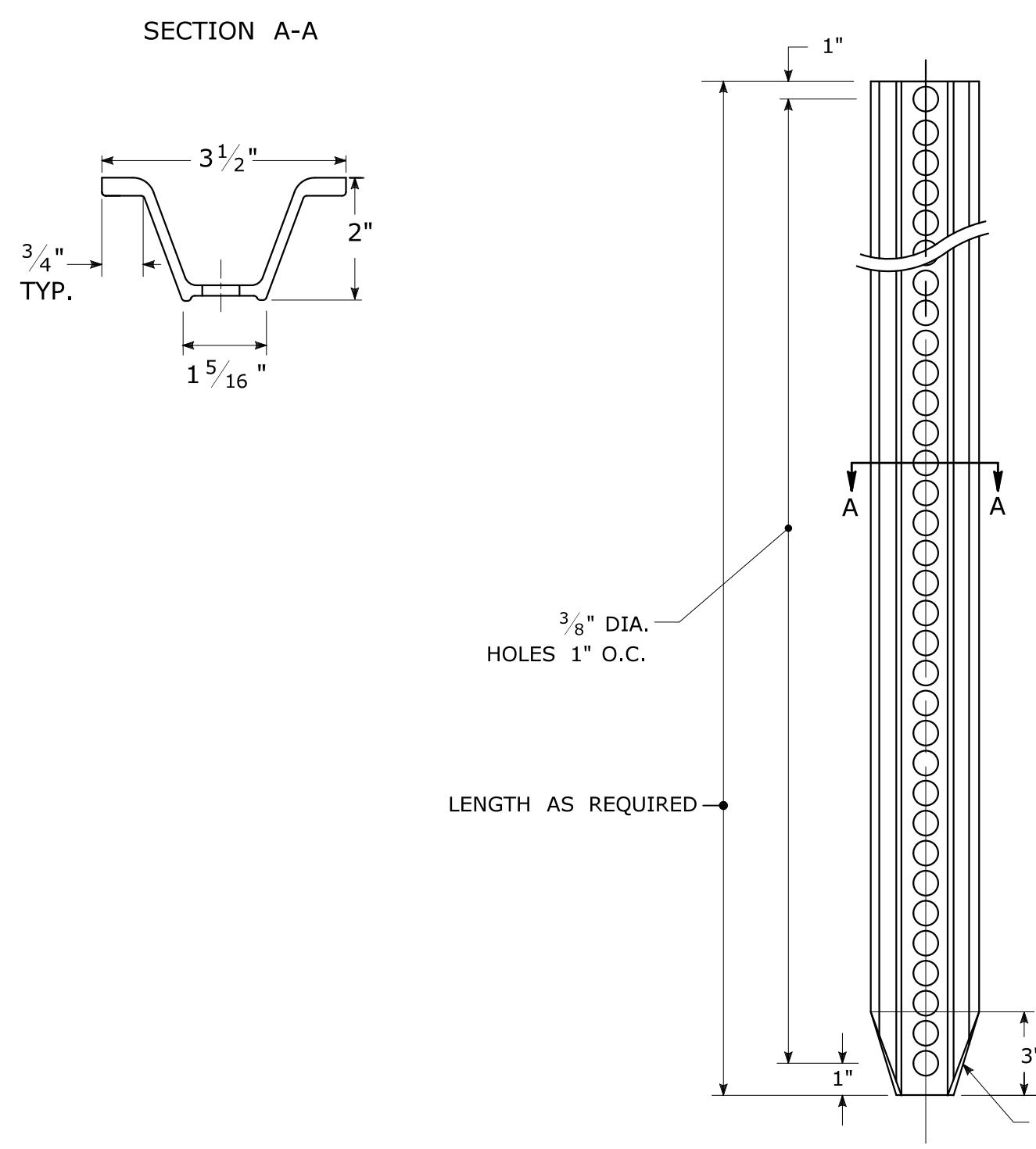
SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

CTDOT  
STANDARD SHEET  
OFFICE OF ENGINEERING

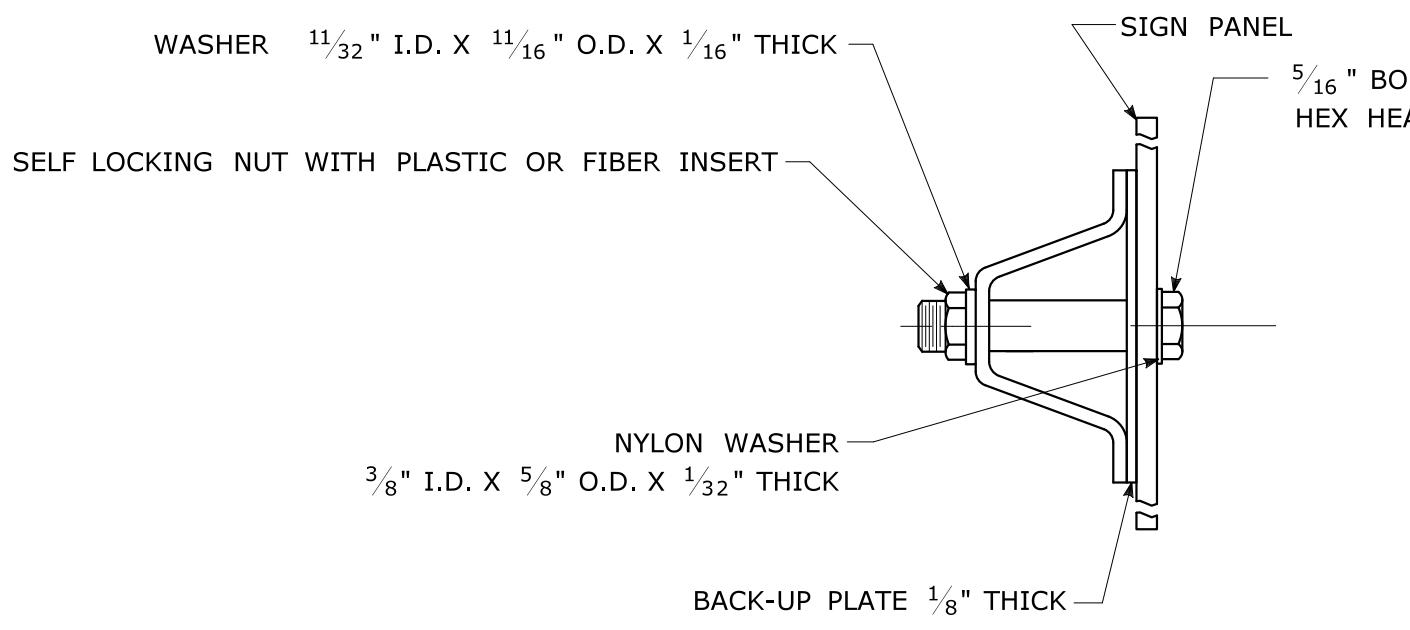
STANDARD SHEET TITLE:  
**SIGN PLACEMENT AND  
RETROREFLECTIVE STRIP DETAILS**

STANDARD SHEET NO.:  
**TR-1208\_01**

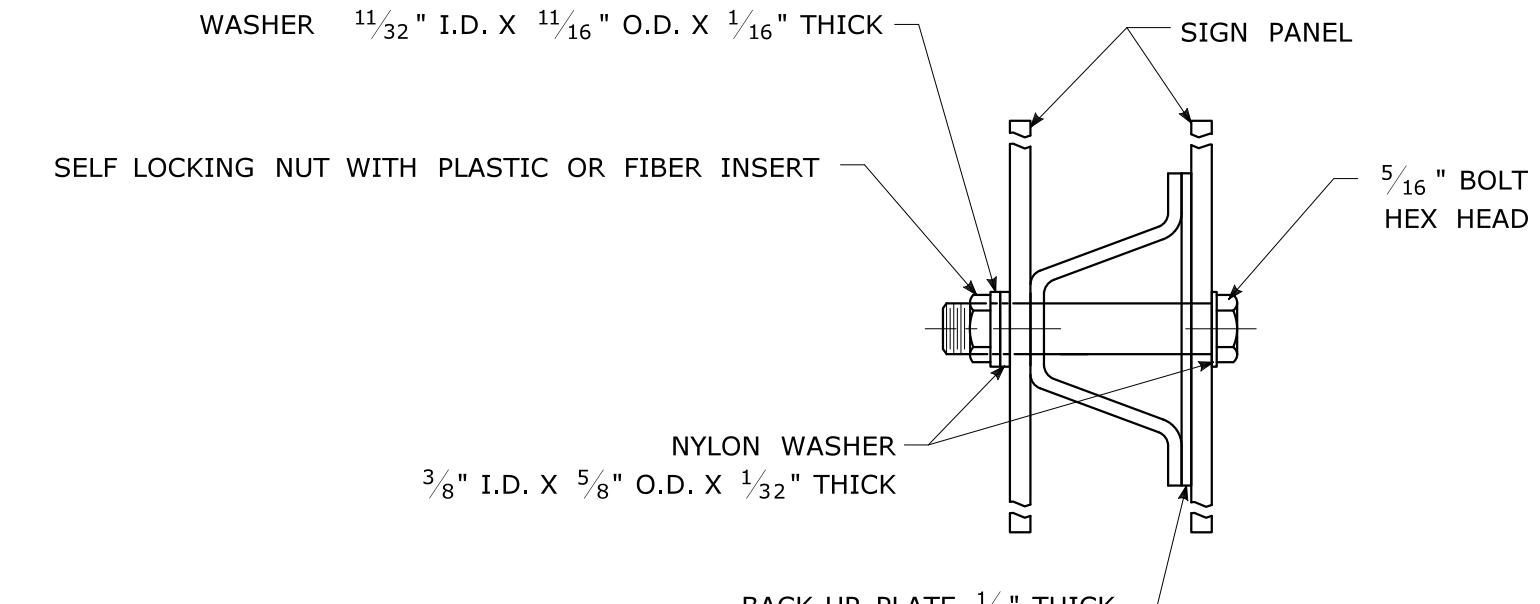
### TYPICAL METAL SIGN POSTS



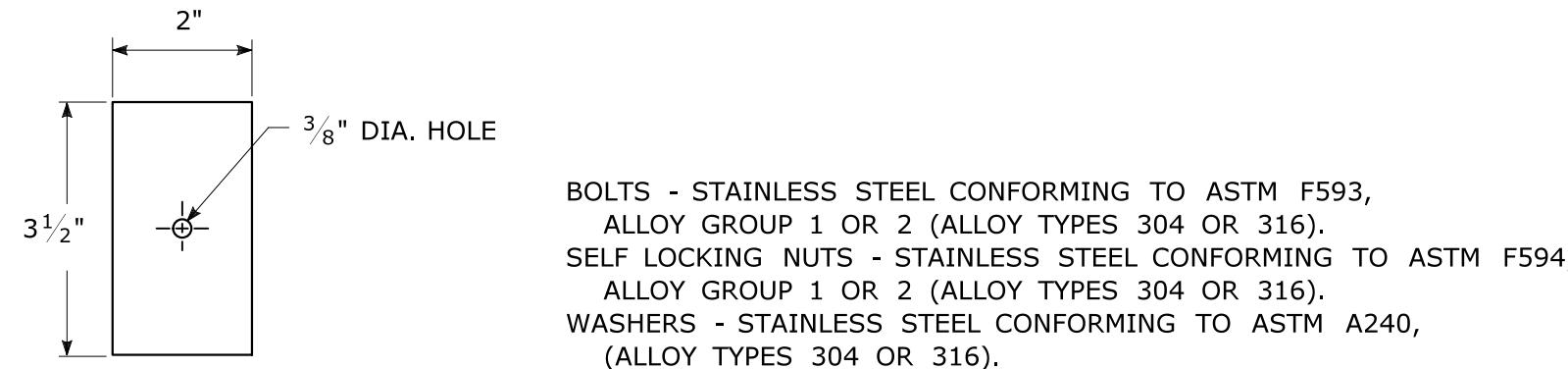
### TYPICAL SIGN PANEL ATTACHMENT



### TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT

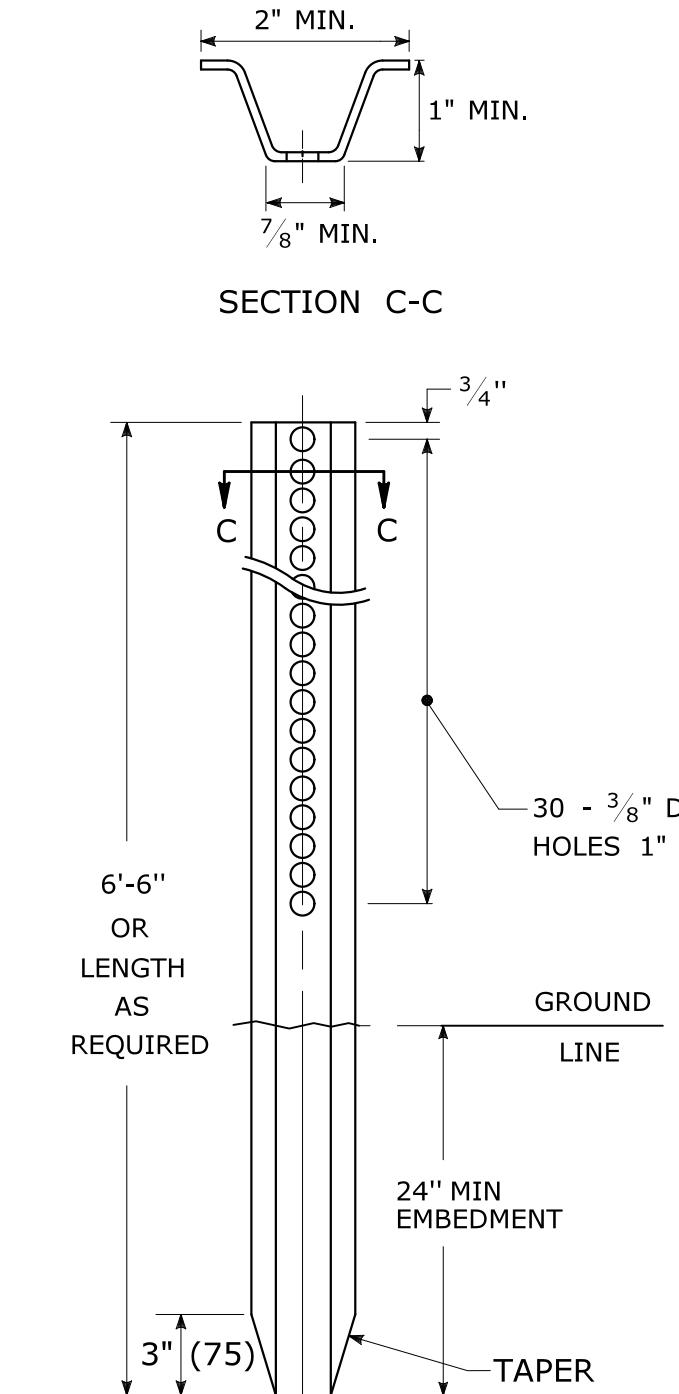


### TYPICAL BACK-UP PLATE



### METAL DELINEATOR POST

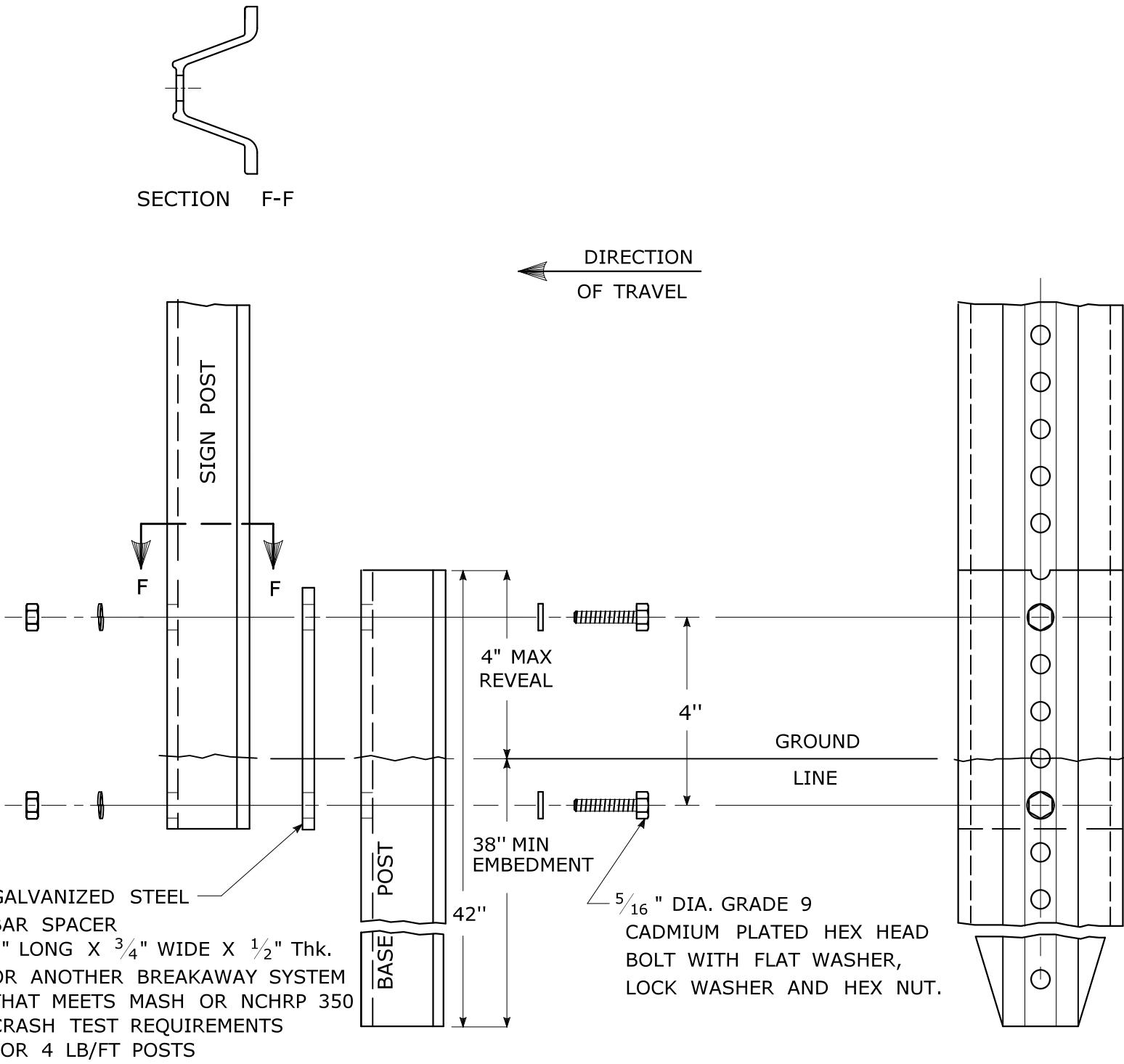
WT./FT. = 1.12 LBS./FT. MIN.



### GENERAL NOTES:

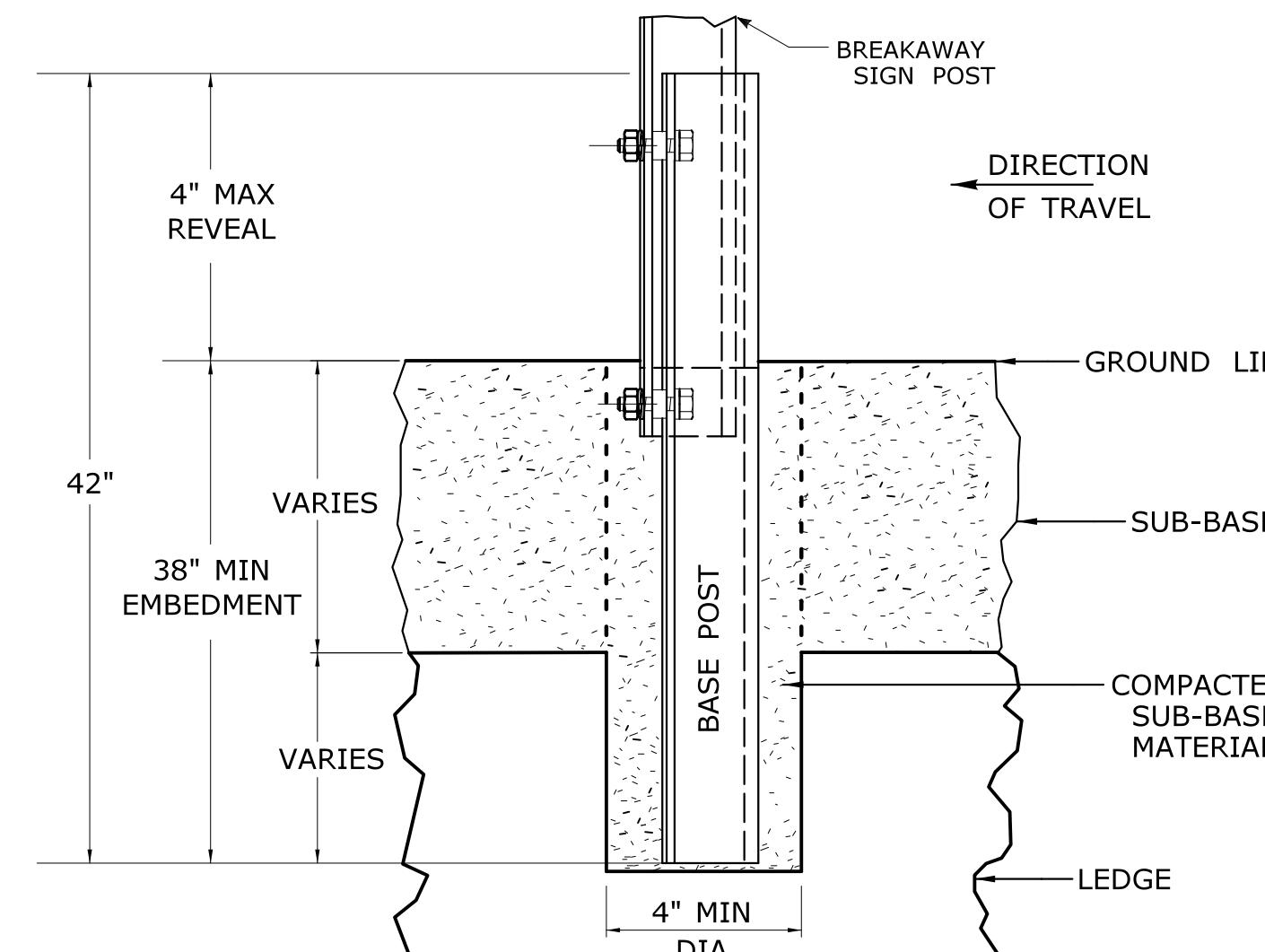
1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

### BREAKAWAY INSTALLATION FOR 4 LBS./FT. POSTS

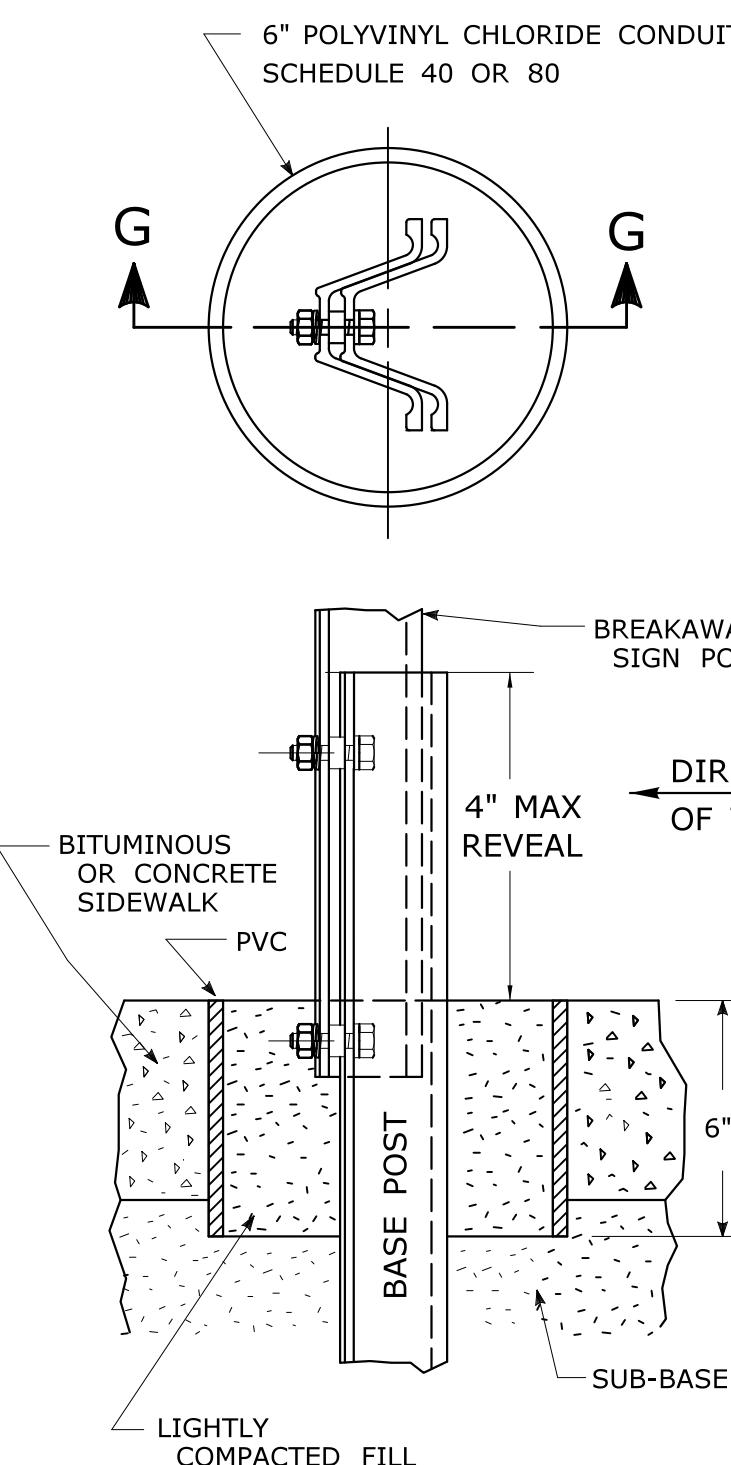


### TYPICAL SIGN POST INSTALLATION IN LEDGE

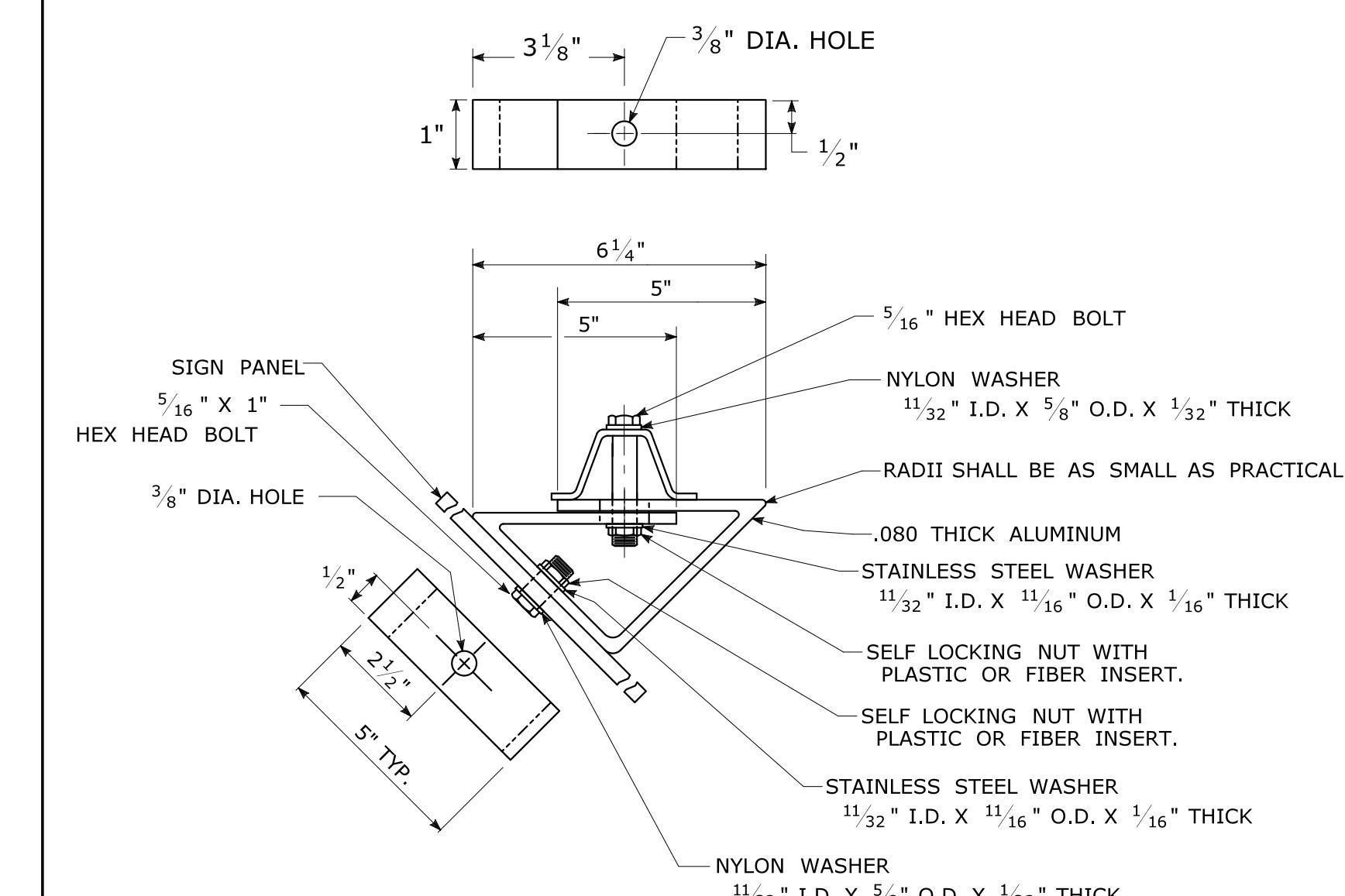
LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38". HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



### TYPICAL SLEEVE FOR PAVED AREAS



### 45° MOUNTING BRACKET FOR INSTALLATION OF PARKING SIGNS

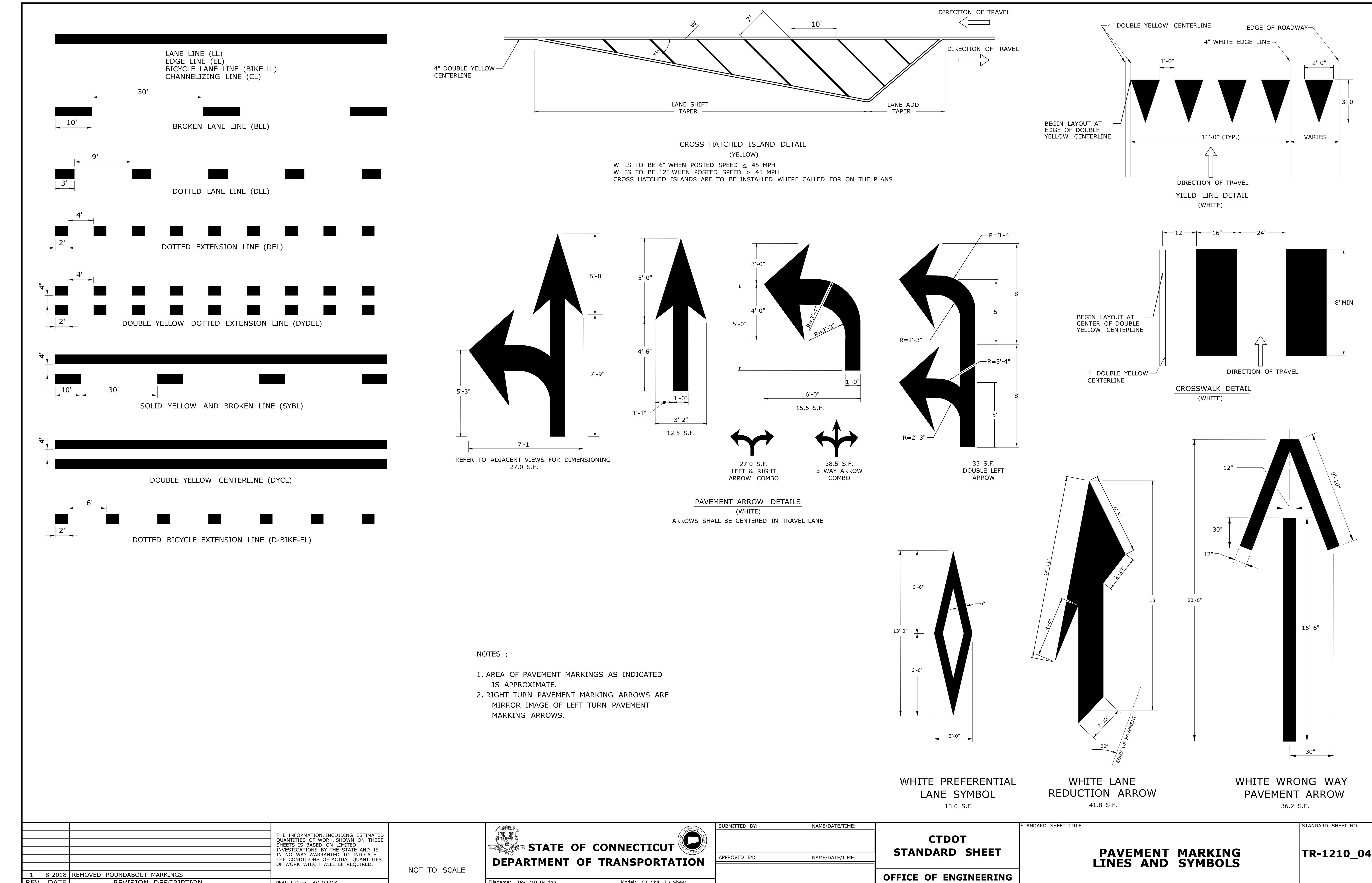


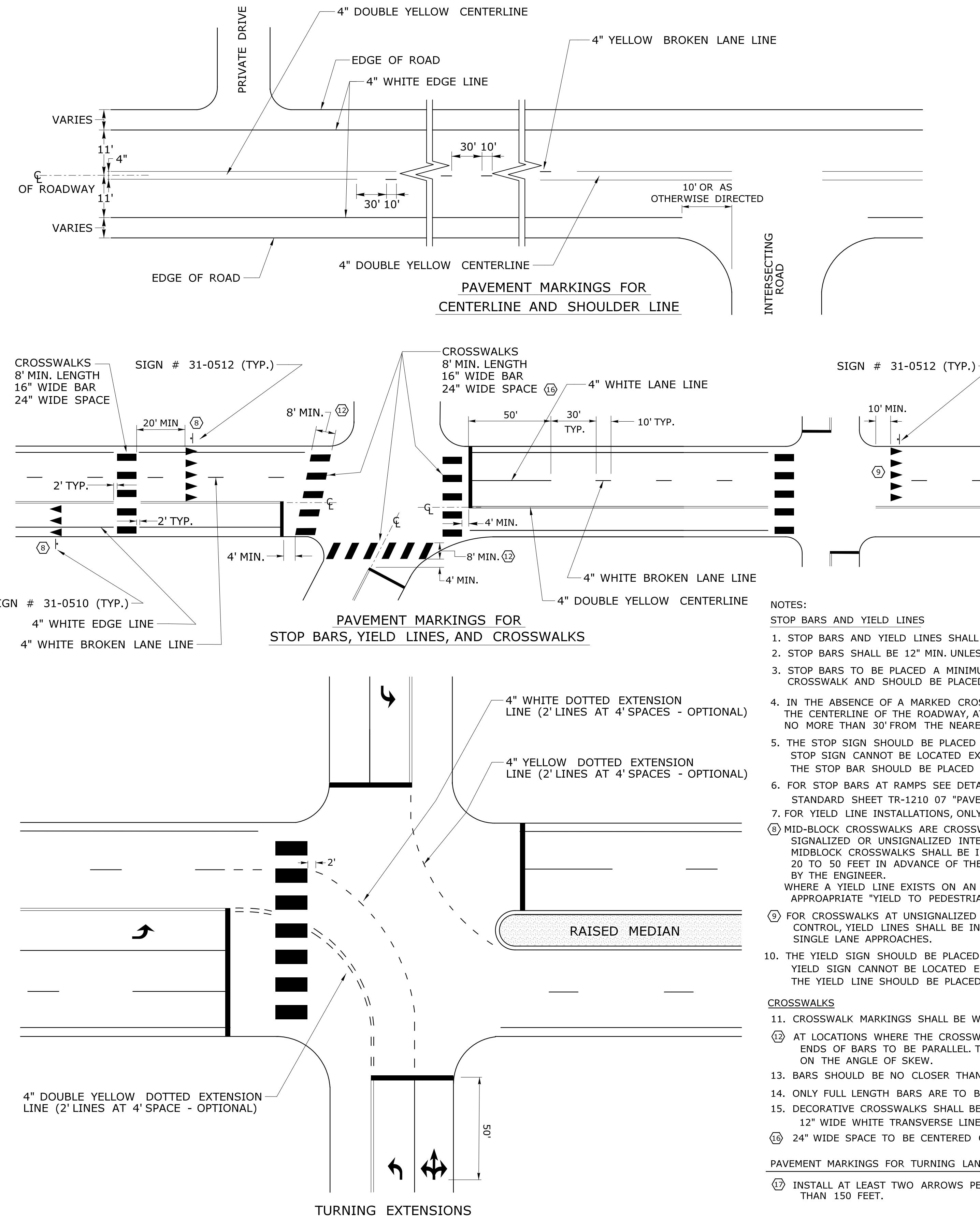
NOT TO SCALE  
REV. DATE  
2 6-2017  
1 2-2011  
REV. DATE  
REVISION DESCRIPTION

STATE OF CONNECTICUT  
DEPARTMENT OF TRANSPORTATION  
Plotted Date: 6/6/2017

SUBMITTED BY:  
NAME/DATE/TIME:  
APPROVED BY:  
NAME/DATE/TIME:  
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:  
METAL SIGN POSTS  
AND SIGN MOUNTING DETAILS  
GUIDE SHEET NO.:  
TR-1208\_02





NOTES:

STOP BARS AND YIELD LINES

1. STOP BARS AND YIELD LINES SHALL BE WHITE.
2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
3. STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
5. THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
6. FOR STOP BARS AT RAMPS SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMPS".
7. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.

⑧ MID-BLOCK CROSSWALKS ARE CROSSWALKS LOCATED MORE THAN 50 FEET FROM A SIGNALIZED OR UNSIGNALIZED INTERSECTION. YIELD LINES ASSOCIATED WITH MIDBLOCK CROSSWALKS SHALL BE INSTALLED AND SHOULD BE LOCATED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER.

WHERE A YIELD LINE EXISTS ON AN APPROACH TO A CROSSWALK, THE APPROPRIATE "YIELD TO PEDESTRIANS" SIGN IS REQUIRED.

⑨ FOR CROSSWALKS AT UNSIGNALIZED INTERSECTIONS WITH MINOR STREET STOP CONTROL, YIELD LINES SHALL BE INSTALLED ON MULTI-LANE APPROACHES, BUT NOT SINGLE LANE APPROACHES.

10. THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

CROSSWALKS

11. CROSSWALK MARKINGS SHALL BE WHITE.

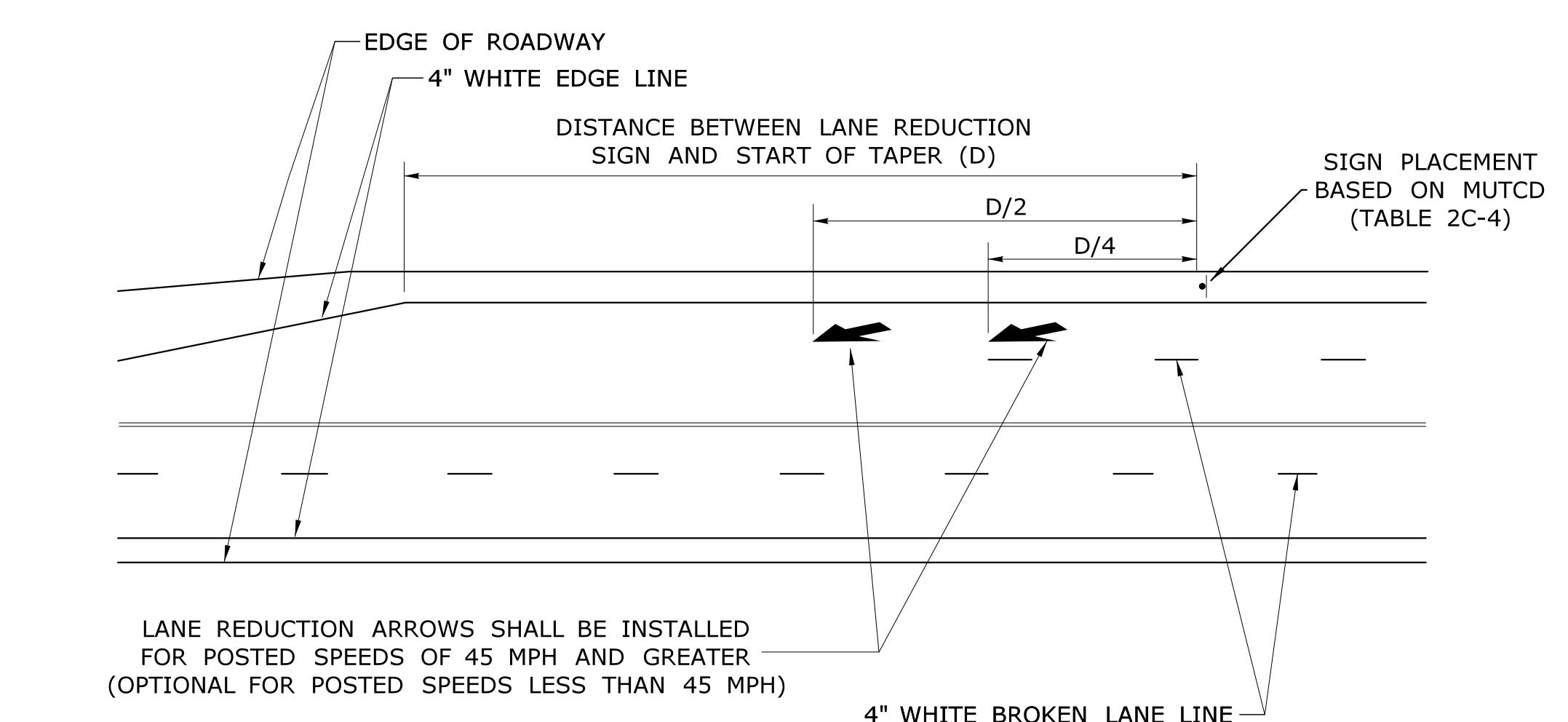
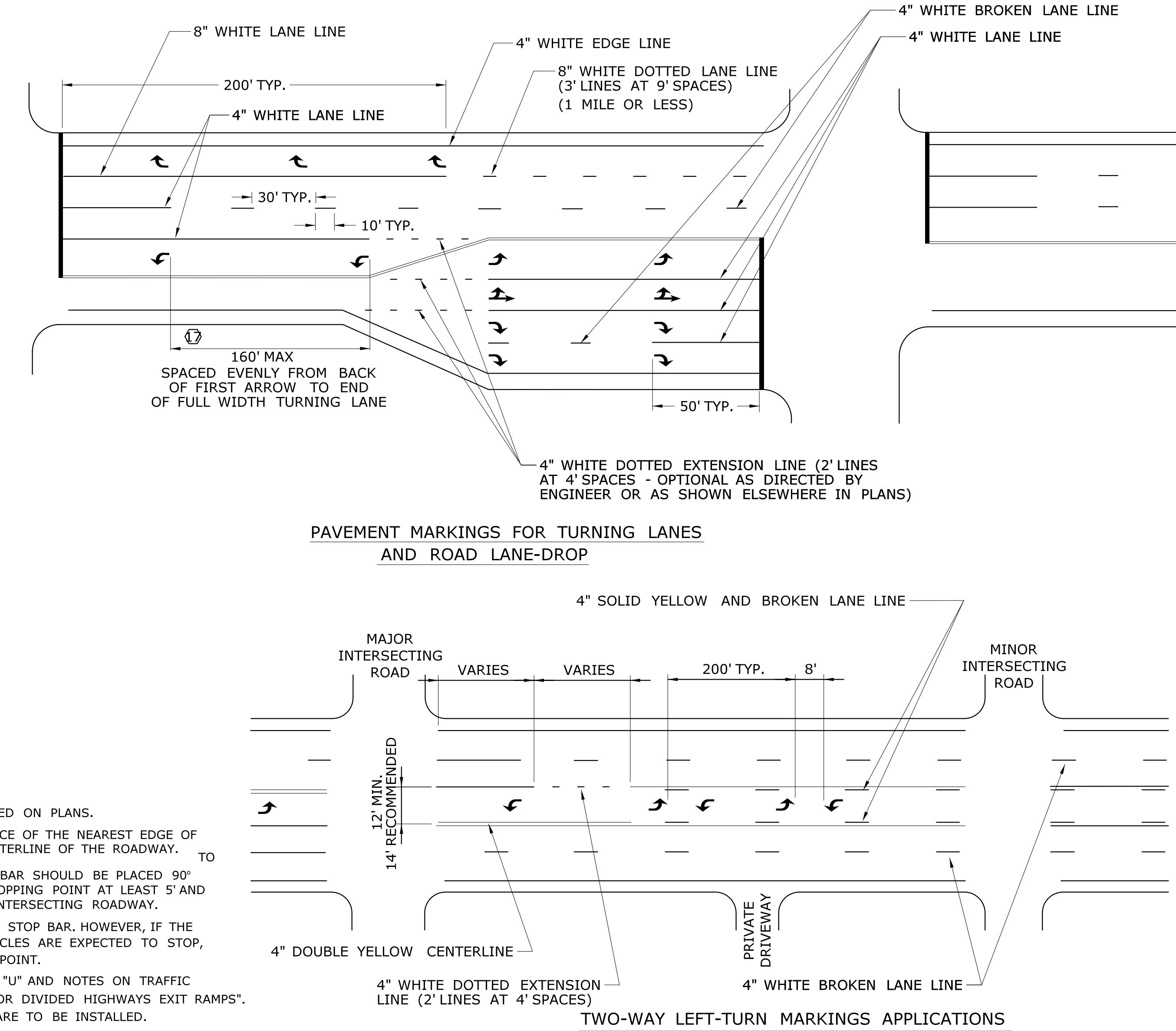
⑫ AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO THE ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.

13. BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
14. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
15. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.

⑯ 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.

PAVEMENT MARKINGS FOR TURNING LANES

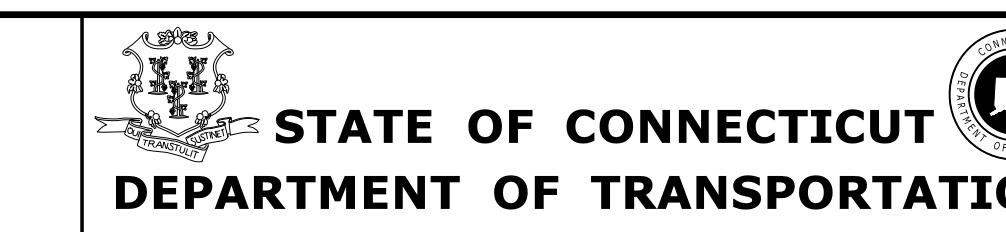
⑰ INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.



1	8-2018	REVISED YIELD LINE SIGNAGE AND NOTES.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITY OF WORK WHICH WILL BE REQUIRED.

NOT TO SCALE





# STATE OF CONNECTICUT



## DEPARTMENT OF TRANSPORTATION

1. **What is the primary purpose of the study?**

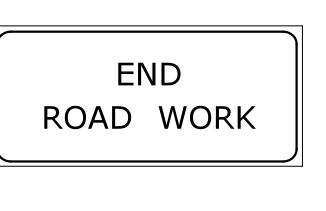
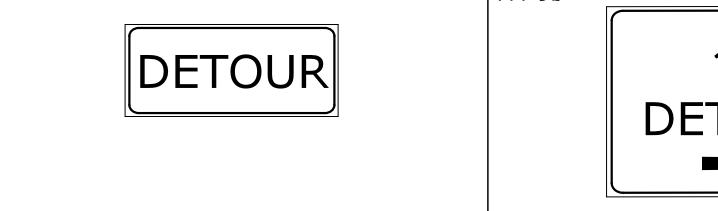
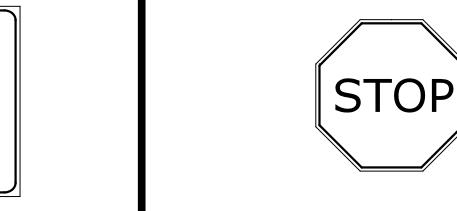
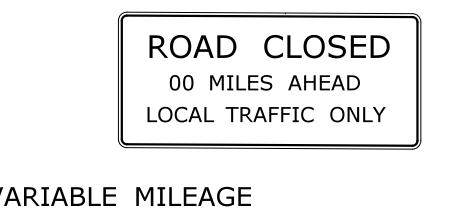
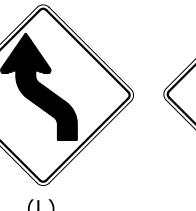
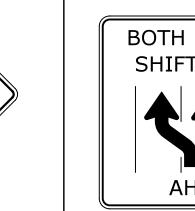
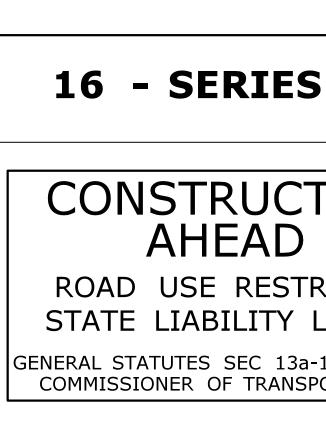
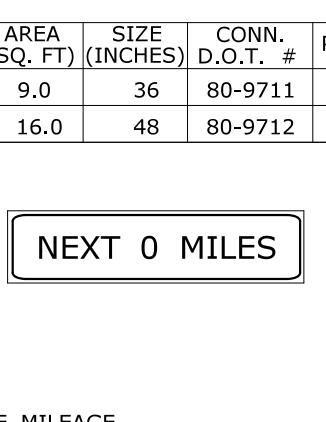
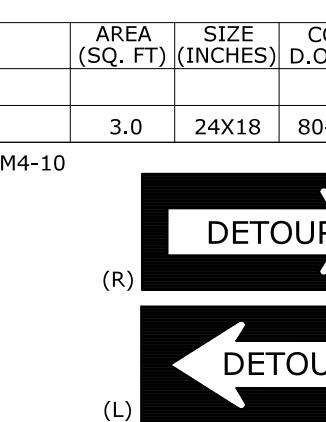
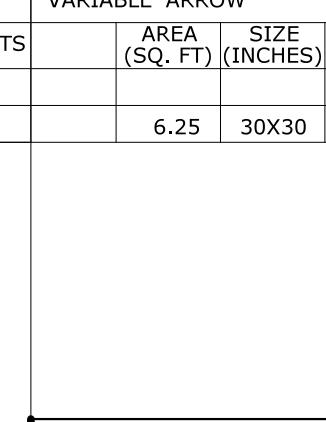
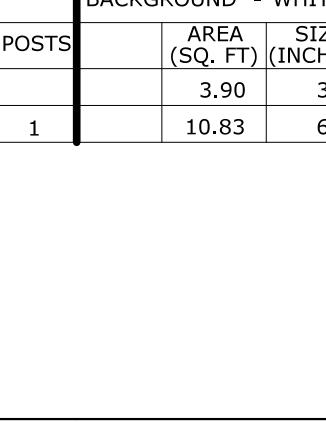
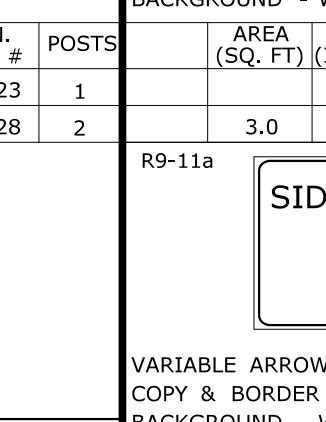
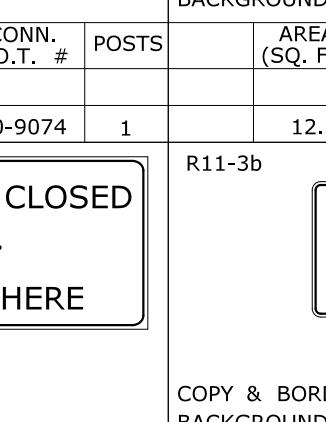
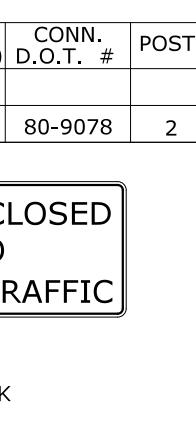
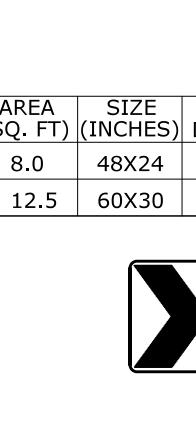
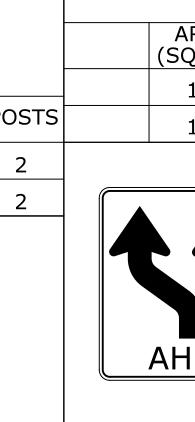
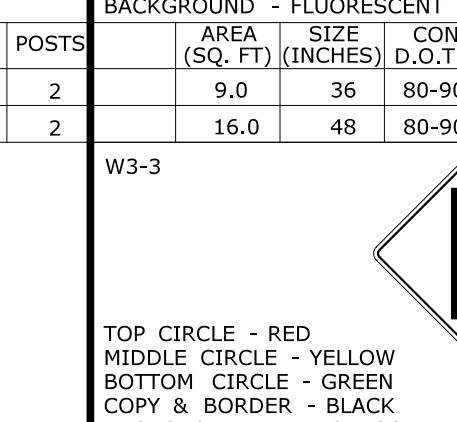
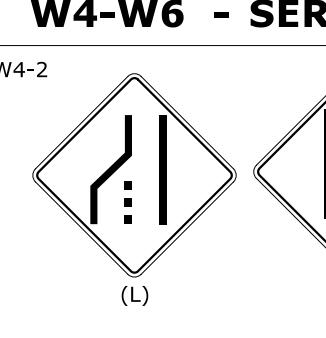
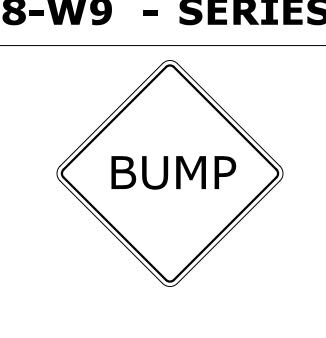
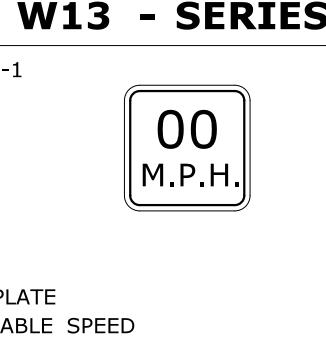
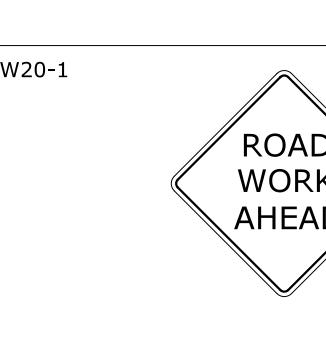
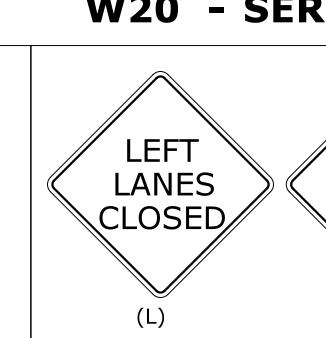
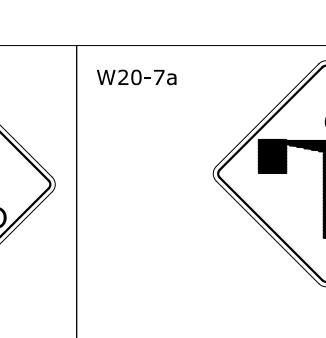
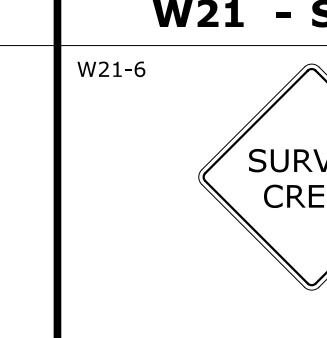
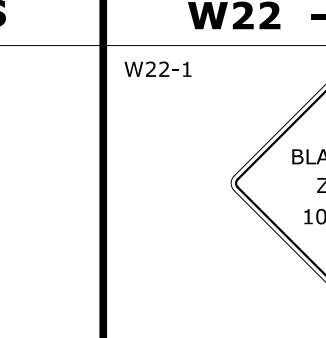
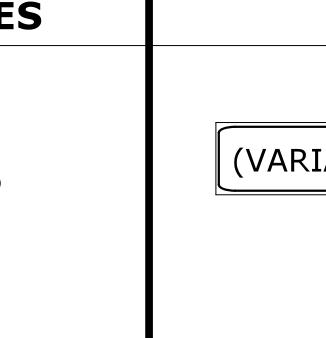
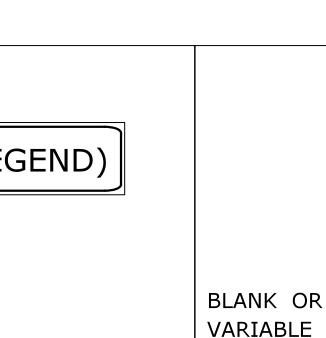
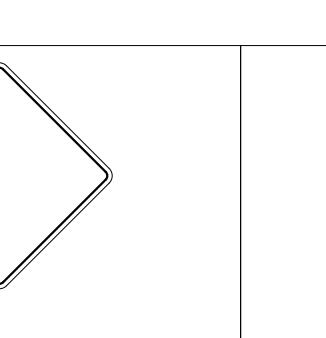
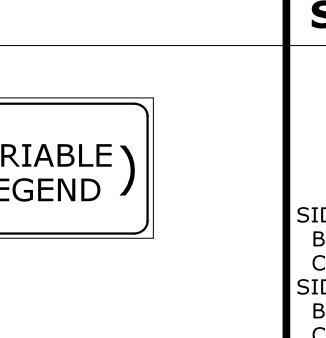
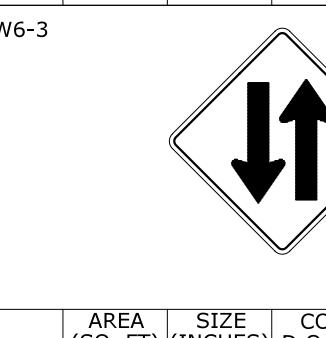
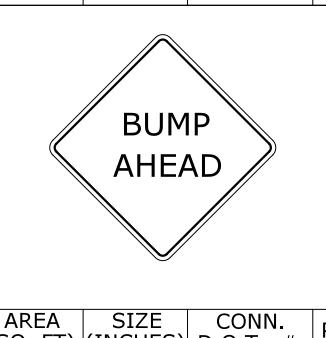
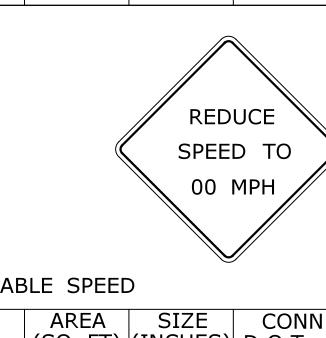
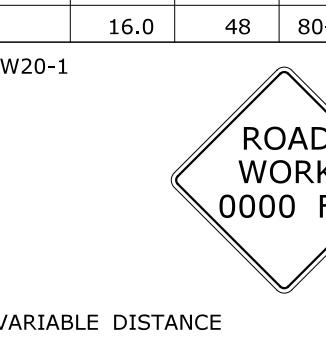
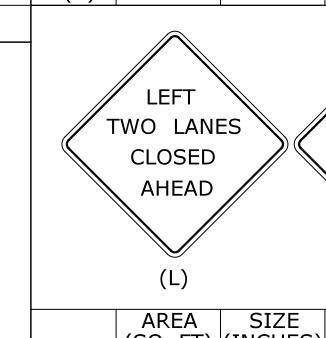
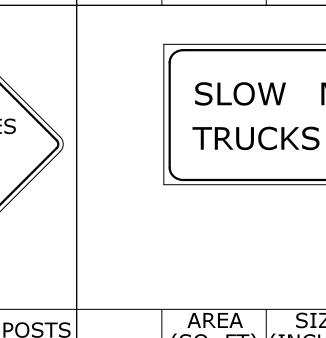
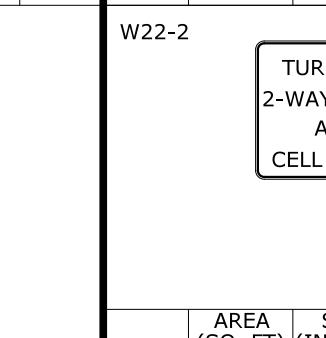
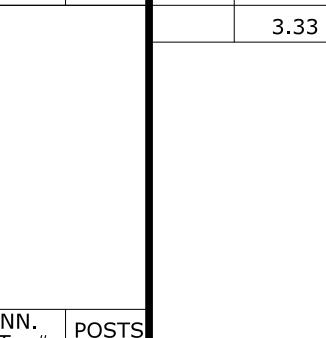
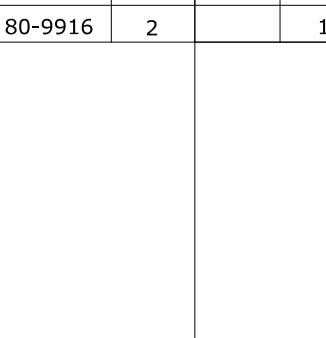
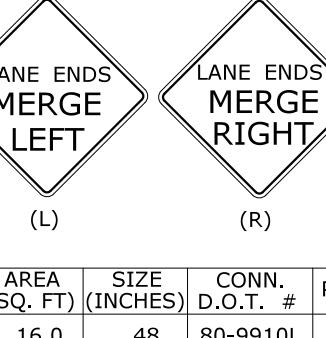
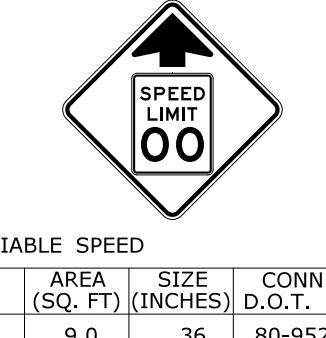
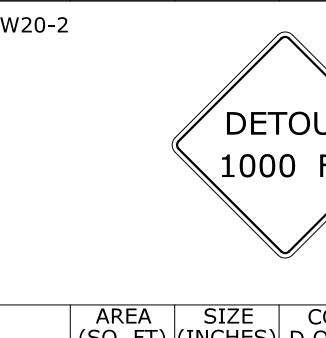
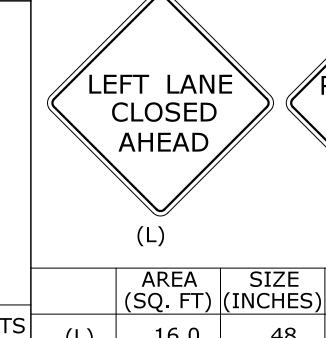
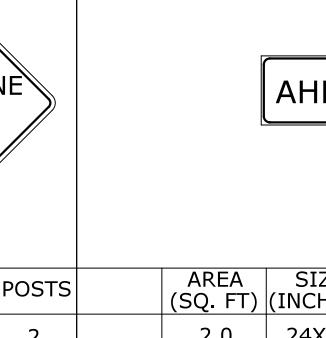
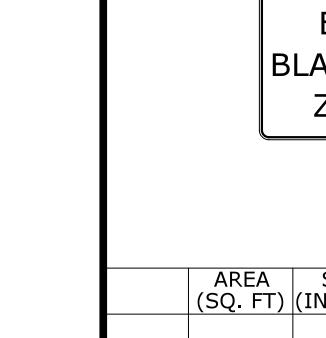
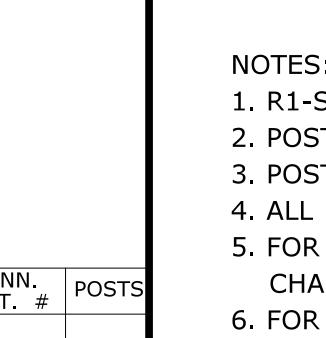
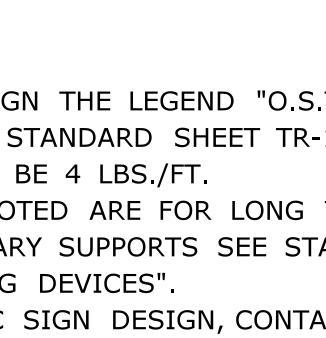
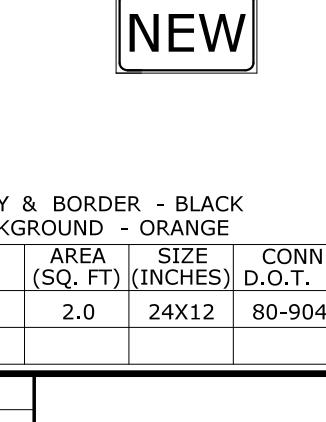
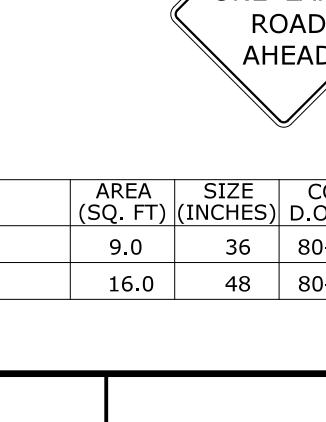
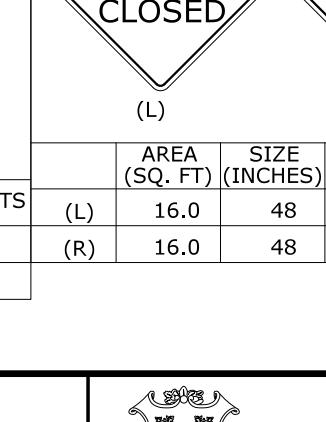
 <b>CONNECTICUT</b> <b>TRANSPORTATION</b>	<b>SUBMITTED BY:</b>	<b>NAME/DATE/TIME:</b>
	<hr/>	
<b>APPROVED BY:</b>	<b>NAME/DATE/TIME:</b>	
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# **CTDOT**

# **STANDARD SHEET**

ARD SHEET TITLE:	STANDARD SHEET NO.:
<b>PAVEMENT MARKINGS FOR NON FREEWAYS</b>	<b>TR-1210_08</b>



E5 - SERIES		G20 - SERIES		M4 - SERIES		R1 - SERIES		R9 & R11 - SERIES		W1 - SERIES		W3 - SERIES																																																																																																							
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16.0	48	80-9835	2																																																																																																																
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