

MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PROJECT NO. L138-0003

STRATFORD, CONNECTICUT

GENERAL NOTES:

1. CONSTRUCTION SPECIFICATIONS:
State of Connecticut Department of Transportation, Standard Specifications for Roads, Bridges, Facilities, and Incidental Construction, Form 819, dated 2024, Supplemental Specifications, and Special Provisions
2. 400 FOOT GRID BASED ON CONNECTICUT COORDINATE SYSTEM N.A.D. 1983
3. VERTICAL DATUM BASED ON NAVD 1988

DISCLAIMER

IT IS THE RESPONSIBILITY OF EACH BIDDER AND ALL OTHER INTERESTED PARTIES TO OBTAIN ALL BIDDING RELATED INFORMATION AND DOCUMENTS FROM THE TOWN OR THE TOWN'S REPRESENTATIVE.

PERSONS AND/OR ENTITIES WHICH REPRODUCE AND/OR MAKE SUCH INFORMATION AVAILABLE BY ANY MEANS ARE NOT AUTHORIZED BY THE TOWN TO DO SO AND MAY BE LIABLE FOR CLAIMS RESULTING FROM THE DISSEMINATION OF UNOFFICIAL, INCOMPLETE AND/OR INACCURATE INFORMATION.

DESIGN CRITERIA

DESIGN STANDARD: CTDOT HIGHWAY DESIGN MANUAL, 2003 EDITION, INCLUDING REVISIONS TO FEBRUARY 2013

MAIN STREET (STATE ROUTE 113)
FUNCTIONAL CLASSIFICATION: URBAN PRINCIPAL ARTERIAL - OTHER
POSTED SPEED: 30 MPH

SURVEY PERFORMED BY
BSC GROUP, INC.
DATE: FEBRUARY 2023

LIST OF DRAWINGS			
SHEET NO.	TITLE	SHEET NO.	STANDARD DRAWINGS
T-01	TITLE SHEET	HW-INX 1 - 2	HIGHWAY STANDARD SHEET INDEX
KEY-01	KEY PLAN	HW-286_01	DRAINAGE TRENCH EXCAVATION
XCP-01 - XCP-03	EXISTING CONDITIONS PLAN	HW-586_01	CATCH BASIN AND DROP INLETS TYPES "C" AND "C-L" STRUCTURES
TYP-01	TYPICAL SECTIONS	HW-586_04	PRECAST CATCH BASIN AND ROUND STRUCTURE
GEN-01 - GEN-04	GENERAL PLAN	HW-586_07A	CATCH BASIN TYPE "C" AND "C-L" TOPS
PMSP-01 - PMSP-04	PAVEMENT MARKING & SIGNAGE PLAN	HW-586_07C	CATCH BASIN TYPE "C" AND "C-L" DOUBLE GRATE TYPE II TOPS
GRAD-01	GRADING PLAN	HW-586_08	CATCH BASIN FRAMES AND GRATES
DET-01 - DET-02	MISCELLANEOUS DETAILS	HW-813_02	STONE CURBING
GS-01 - GS-16	STATE GUIDE SHEET DETAILS	HW-815_01	BITUMINOUS CONCRETE CURBING
1 - 4	SIGNAL PLANS	HW-921_01	CONCRETE SIDEWALKS
		HW-922_01	BITUMINOUS CONCRETE SIDEWALK AND BITUMINOUS CONCRETE DRIVEWAY
		HW-949_01A	LANDSCAPE PLANTING
		HW-949_01B	TREE STAKING
		TR-STD_INDEX	TRAFFIC STANDARD SHEET INDEX
		TR-1000_01	GENERAL CLAUSES (TEST PROCEDURES)
		TR-1001_01	TRENCHING & BACKFILLING, ELECTRICAL CONDUIT
		TR-1002_01	TRAFFIC CONTROL FOUNDATIONS
		TR-1010_01	CONCRETE HANDHOLE
		TR-1102_01	PEDESTALS, PEDESTRIAN SIGNALS
		TR-1107_01	PEDESTRIAN PUSH BUTTON
		TR-1208_01	SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS
		TR-1208_02	METAL SIGN POSTS AND SIGN MOUNTING DETAILS
		TR-1210_04	PAVEMENT MARKING LINES AND SYMBOLS
		TR-1210_08	PAVEMENT MARKINGS FOR NON FREEWAYS
		TR-1210_09	PAVEMENT MARKINGS FOR BICYCLE LANES, PARKING STALLS, AND RAILROAD CROSSINGS
		TR-1220_01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
		TR-1220_02	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES

SEMI-FINAL DESIGN SUBMISSION FOR STREETSCAPE IMPROVEMENTS IN THE TOWN OF STRATFORD, CT

FROM STA. 10+00 TO STA. 54+68 ON MAIN STREET

TO BE MAINTAINED BY THE TOWN OF STRATFORD

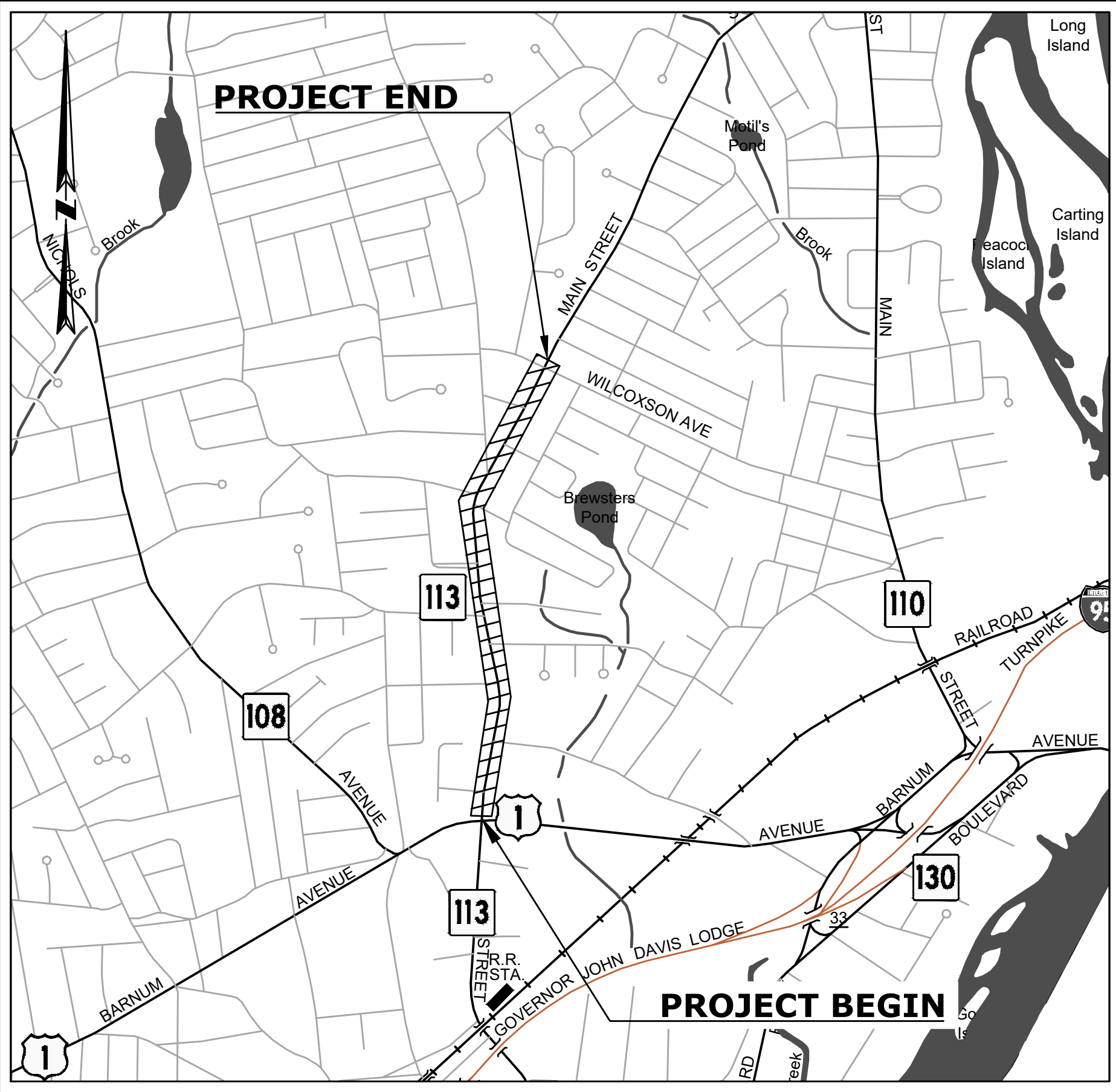
PLANS FOR:
MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2
STRATFORD, CONNECTICUT 06615

THE DESIGN APPEARS TO CONFORM TO APPLICABLE CRITERIA. APPROVAL IS NOT TO BE CONSTRUED TO MEAN THAT ALL ASPECTS OF THE DESIGN HAVE BEEN PERSONALLY CHECKED BY THE UNDERSIGNED.

SUBMITTED BY: ROBERT S. NEWTON, PE

APPROVAL BY:

APPROVAL BY:



PROJECT LOCATION PLAN

500 0 1000 2000 4000 feet
SCALE: 1" = 1000'±

PLANS PREPARED FOR:

METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

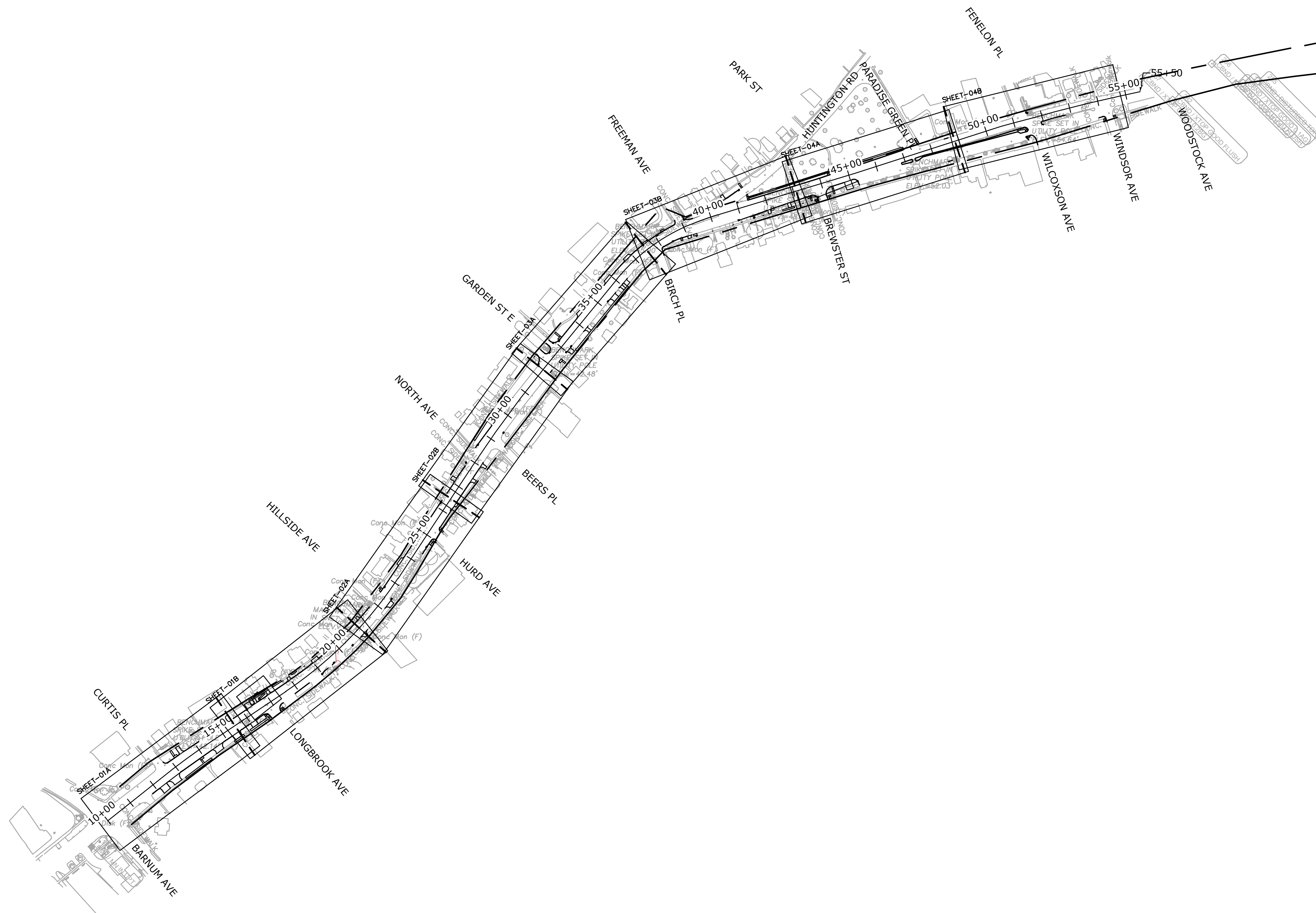
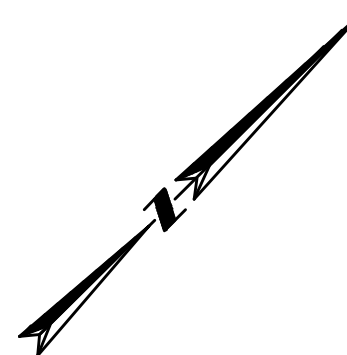
PLANS PREPARED BY:

BSC GROUP
BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033
860 652 8227

PROGRESS PRINT - 60% DESIGN - OCTOBER 2024

REV.	DATE	REVISION DESCRIPTION

T-01



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MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

KEY PLAN

OCTOBER 2024

REVISIONS:

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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Glastonbury, Connecticut
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860 652 8227

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SCALE: 1" = 200'

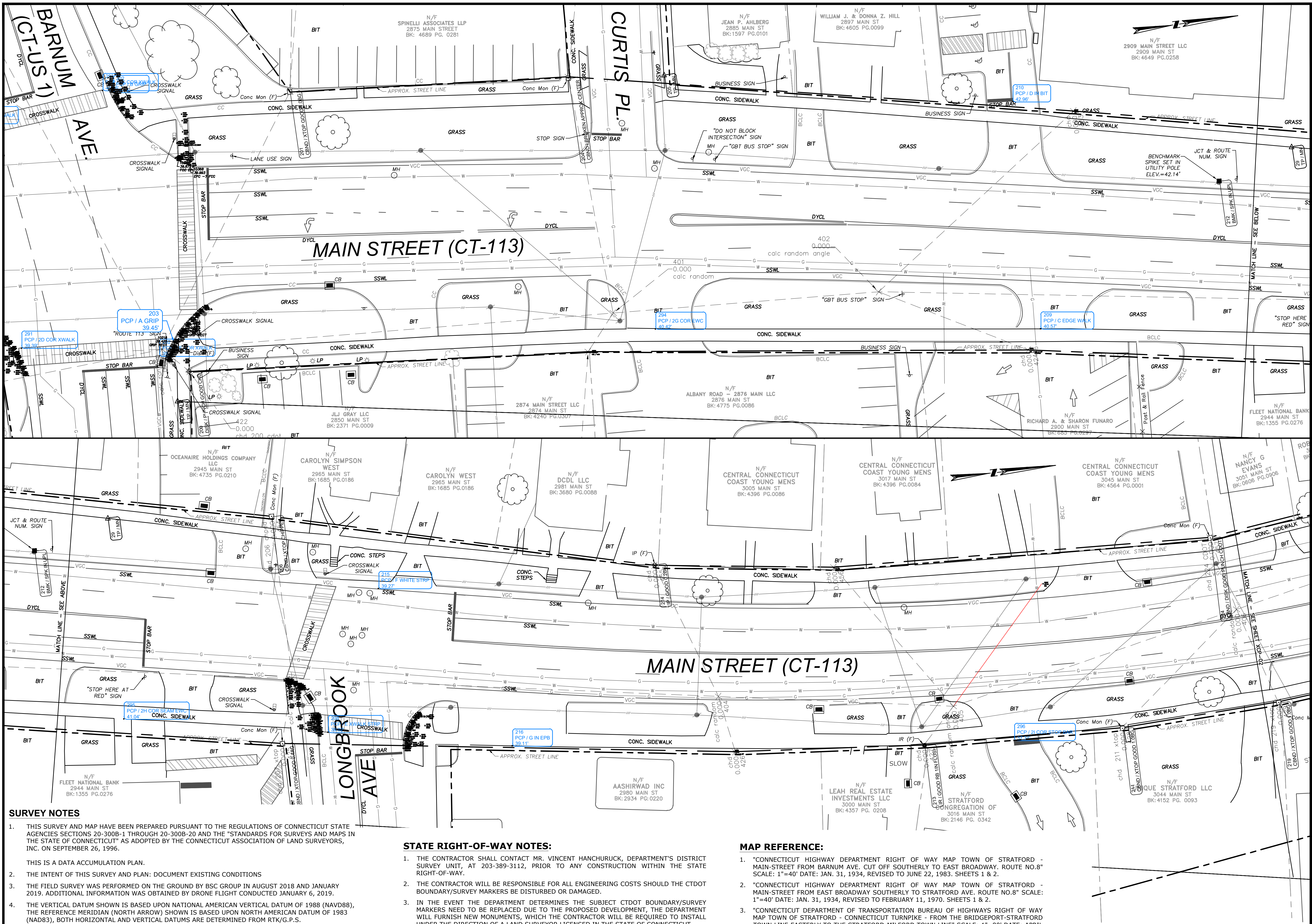


FILE: 8387300\CIVIL\DRAWINGS\

DWG. NO:

JOB. NO: 83873.00

KEY-01



SURVEY NOTES

- THIS SURVEY AND MAP HAVE BEEN PREPARED PURSUANT TO THE REGULATIONS OF CONNECTICUT STATE AGENCIES SECTIONS 20-300B-1 THROUGH 20-300B-20 AND THE "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT" AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. ON SEPTEMBER 26, 1996.
- THIS IS A DATA ACCUMULATION PLAN.
- THE INTENT OF THIS SURVEY AND PLAN: DOCUMENT EXISTING CONDITIONS
- THE FIELD SURVEY WAS PERFORMED ON THE GROUND BY BSC GROUP IN AUGUST 2018 AND JANUARY 2019. ADDITIONAL INFORMATION WAS OBTAINED BY DRONE FLIGHT CONDUCTED JANUARY 6, 2019.
- THE VERTICAL DATUM SHOWN IS BASED UPON NATIONAL AMERICAN VERTICAL DATUM OF 1988 (NAVD88), THE REFERENCE MERIDIAN (NORTH ARROW) SHOWN IS BASED UPON NORTH AMERICAN DATUM OF 1983 (NAD83), BOTH HORIZONTAL AND VERTICAL DATUMS ARE DETERMINED FROM RTK/G.P.S.
- EXISTING UTILITIES, WHERE SHOWN HEREON, ARE APPROXIMATE. NO GUARANTEE IS IMPLIED OR INTENDED AS TO THE ACCURACY, LOCATION OR THAT ALL UTILITIES AND/OR SUBSURFACE STRUCTURES ARE SHOWN. CONSULT WITH THE APPROPRIATE UTILITY COMPANY OR AGENCY PRIOR TO DESIGNING IMPROVEMENTS, COMMENCING DEMOLITION OR CONSTRUCTION. "CALL BEFORE YOU DIG" 1-800-922-4455.

STATE RIGHT-OF-WAY NOTES:

- THE CONTRACTOR SHALL CONTACT MR. VINCENT HANCHURUCK, DEPARTMENT'S DISTRICT SURVEY UNIT, AT 203-389-3112, PRIOR TO ANY CONSTRUCTION WITHIN THE STATE RIGHT-OF-WAY.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL ENGINEERING COSTS SHOULD THE CTDOT BOUNDARY/SURVEY MARKERS BE DISTURBED OR DAMAGED.
- IN THE EVENT THE DEPARTMENT DETERMINES THE SUBJECT CTDOT BOUNDARY/SURVEY MARKERS NEED TO BE REPLACED DUE TO THE PROPOSED DEVELOPMENT, THE DEPARTMENT WILL FURNISH NEW MONUMENTS, WHICH THE CONTRACTOR WILL BE REQUIRED TO INSTALL UNDER THE DIRECTION OF A LAND SURVEYOR LICENSED IN THE STATE OF CONNECTICUT.
- THE CTDOT BOUNDARY/SURVEY MARKERS SHALL BE VERIFIED AND ACCEPTED BY THE DISTRICT 3 SURVEY UNIT PRIOR TO RELEASING THE ENCROACHMENT PERMIT BOND.

MAP REFERENCE:

- "CONNECTICUT HIGHWAY DEPARTMENT RIGHT OF WAY MAP TOWN OF STRATFORD - MAIN-STREET FROM BARNUM AVE. CUT OFF SOUTHERLY TO EAST BROADWAY. ROUTE NO.8" SCALE: 1"=40' DATE: JAN. 31, 1934, REVISED TO JUNE 22, 1983. SHEETS 1 & 2.
- "CONNECTICUT HIGHWAY DEPARTMENT RIGHT OF WAY MAP TOWN OF STRATFORD - MAIN-STREET FROM EAST BROADWAY SOUTHERLY TO STRATFORD AVE. ROUTE NO.8" SCALE: 1"=40' DATE: JAN. 31, 1934, REVISED TO FEBRUARY 11, 1970. SHEETS 1 & 2.
- "CONNECTICUT DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS RIGHT OF WAY MAP TOWN OF STRATFORD - CONNECTICUT TURNPIKE - FROM THE BRIDGEPORT-STRATFORD TOWN LINE EASTERLY TO THE STRATFORD-MILFORD TOWN LINE" SCALE: 1"=80' DATE: APRIL 10, 1972, REVISED TO NOVEMBER 21, 1975. SHEET 5 OF 8.

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MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2
STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)
IN
STRATFORD
CONNECTICUT
EXISTING CONDITIONS
PLAN

OCTOBER 2024

REVISIONS:

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP
BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033
860 652 8227

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SCALE: 1" = 20'

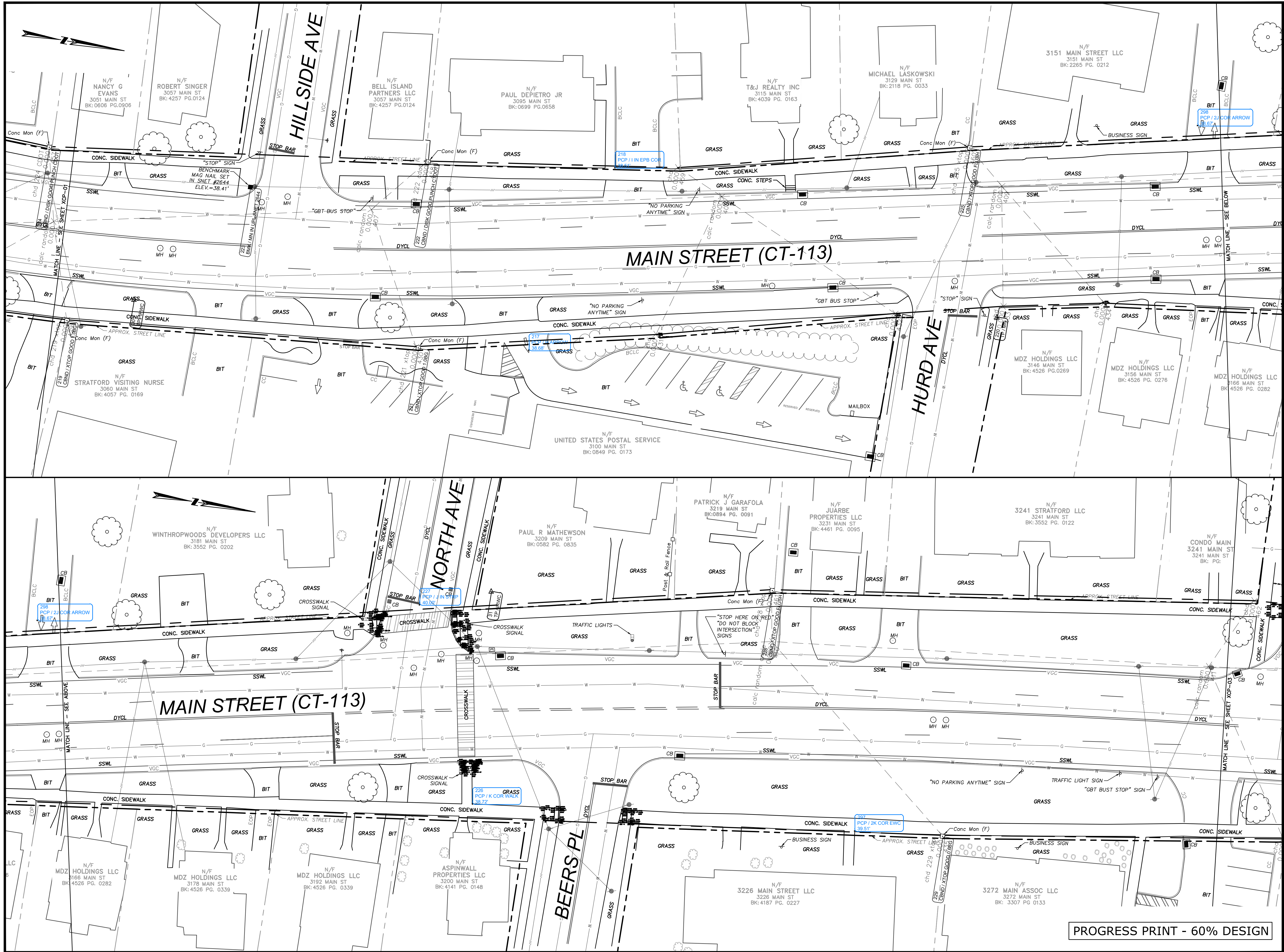


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DWG. NO:

JOB. NO: 83873.00

XCP-01



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

EXISTING CONDITIONS
PLAN

OCTOBER 2024

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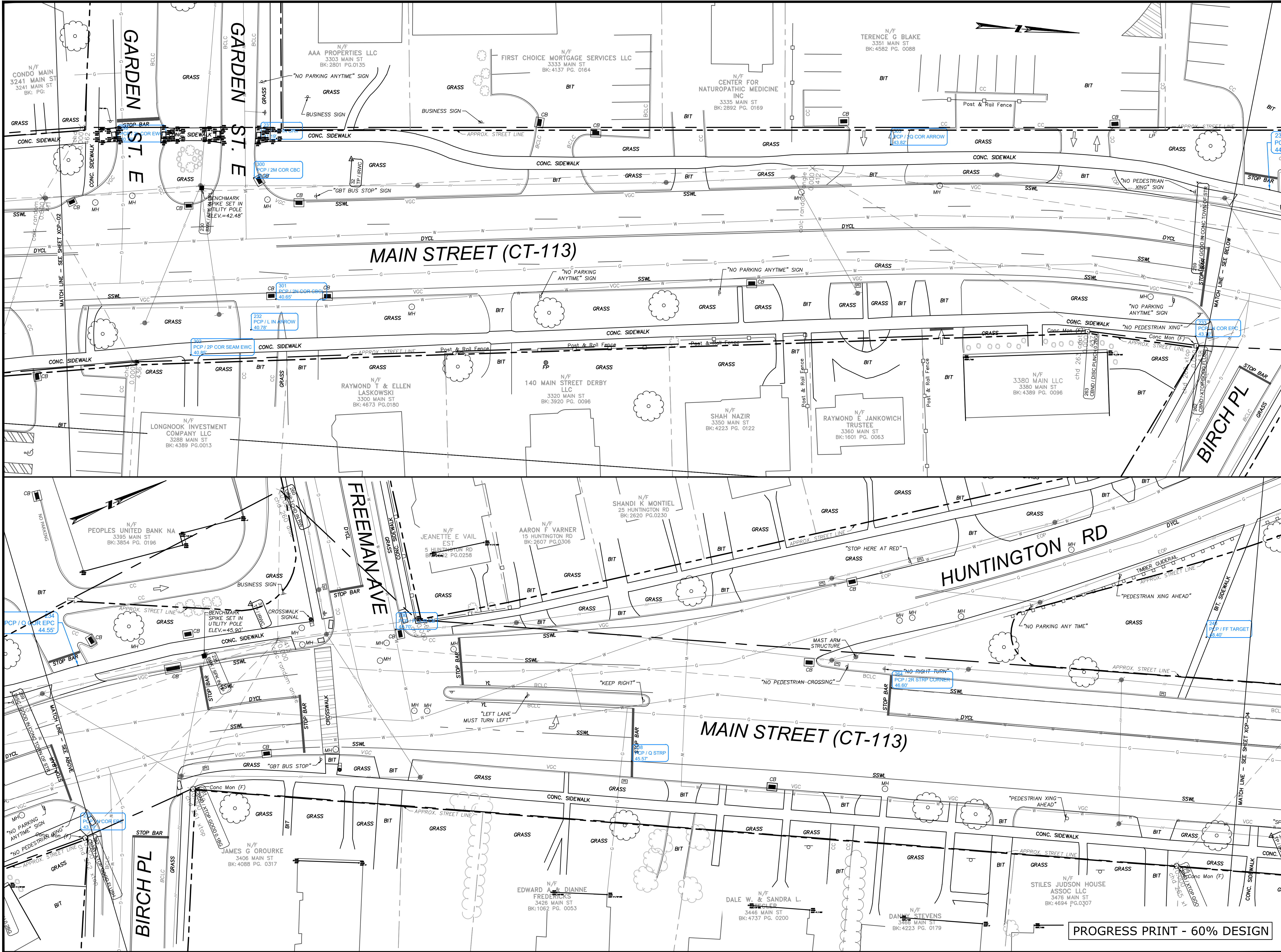
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DWG. NO:

JOB. NO: 83873.00

XCP-02



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

EXISTING CONDITIONS
PLAN

OCTOBER 2024

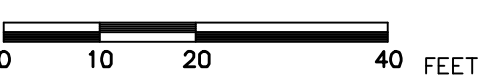
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SCALE: 1" = 20'



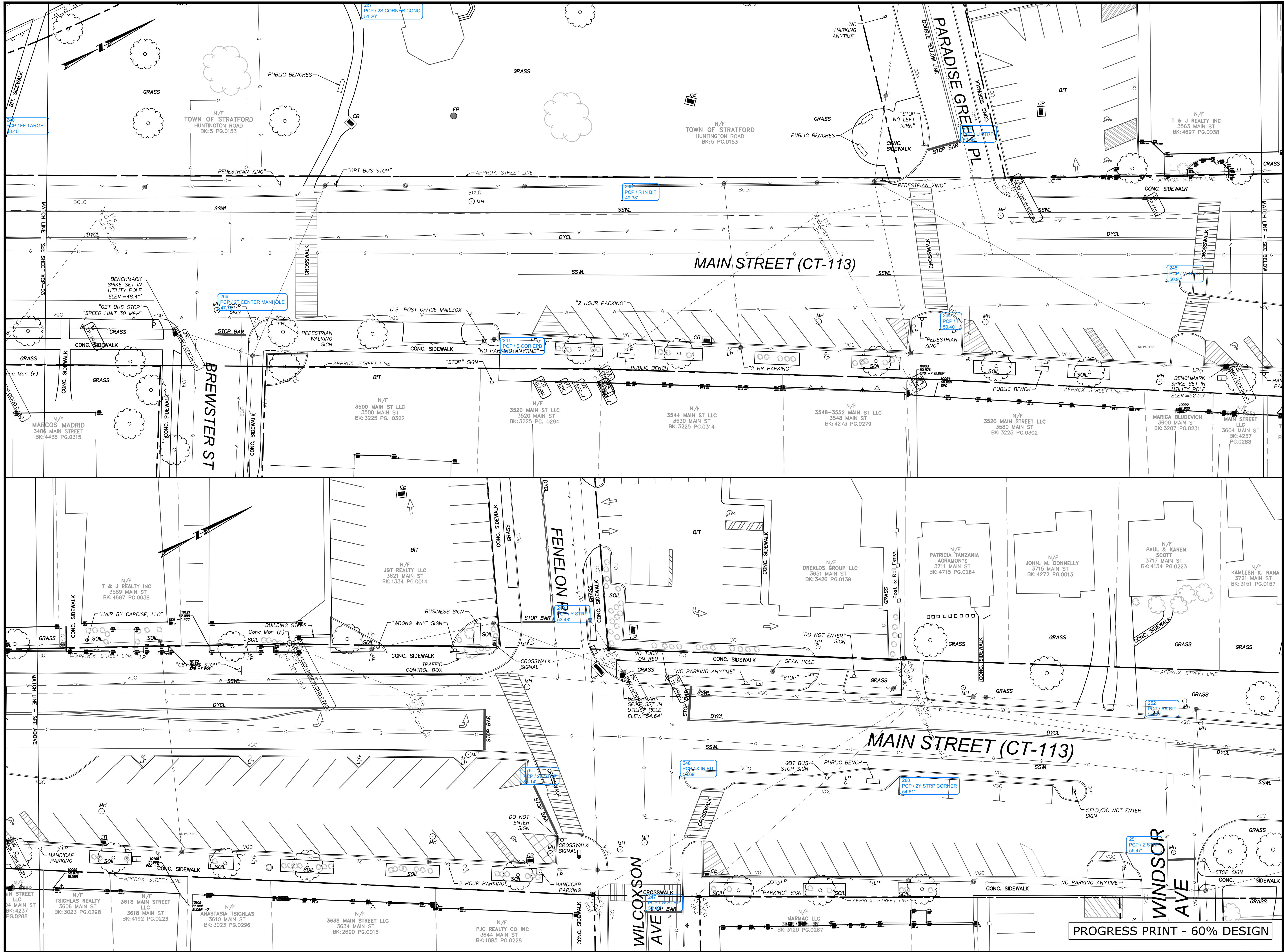
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XCP-03

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MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

EXISTING CONDITIONS
PLAN

OCTOBER 2024

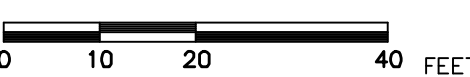
REVISIONS:

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SCALE: 1" = 20'



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DWG. NO:

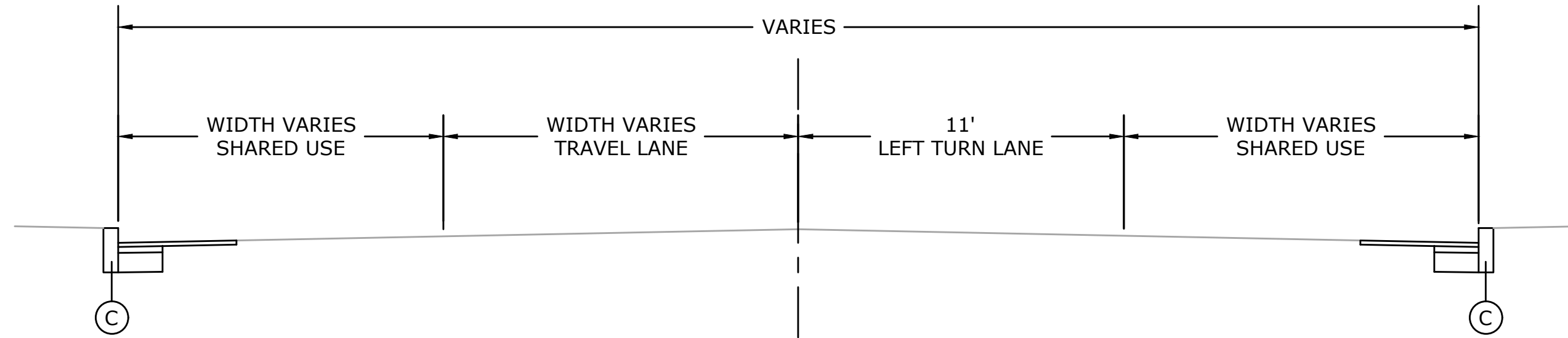
JOB. NO: 83873.00

XCP-04

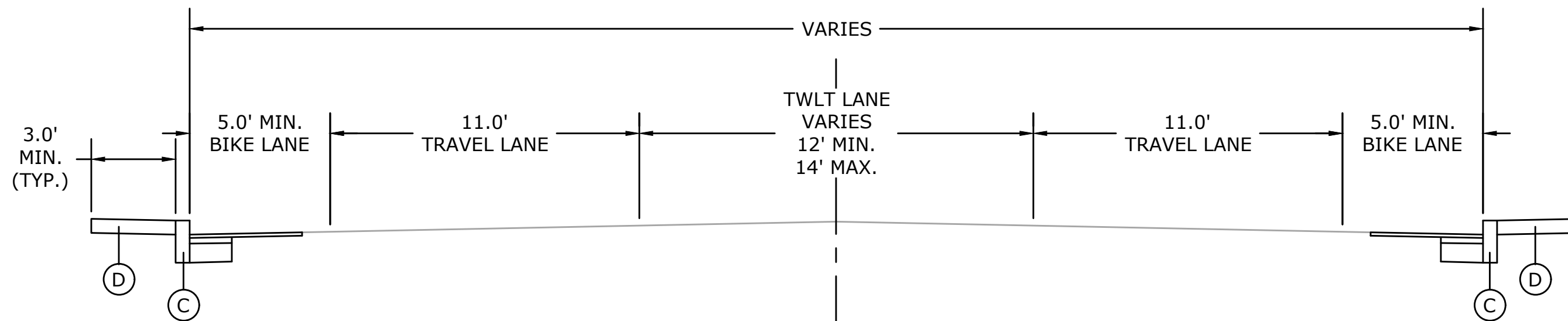
PROGRESS PRINT - 60% DESIGN

LEGEND

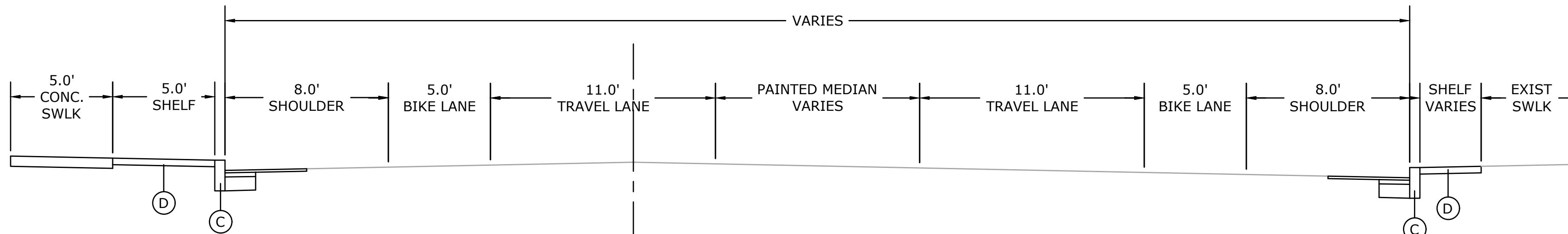
- (A) - 2.0" S0.5 LEVEL 2 HMA
(B) - GRANITE STONE CURB (SEE PLANS)
(C) - REMOVE/RESET EXISTING CURB (SEE PLANS)
(D) - 4" TOPSOIL AND SEED
(E) - 7" S1.0 LEVEL 2 HMA
(F) - 10" PROCESSED AGGREGATE BASE
(G) - CONCRETE UNIT PAVERS



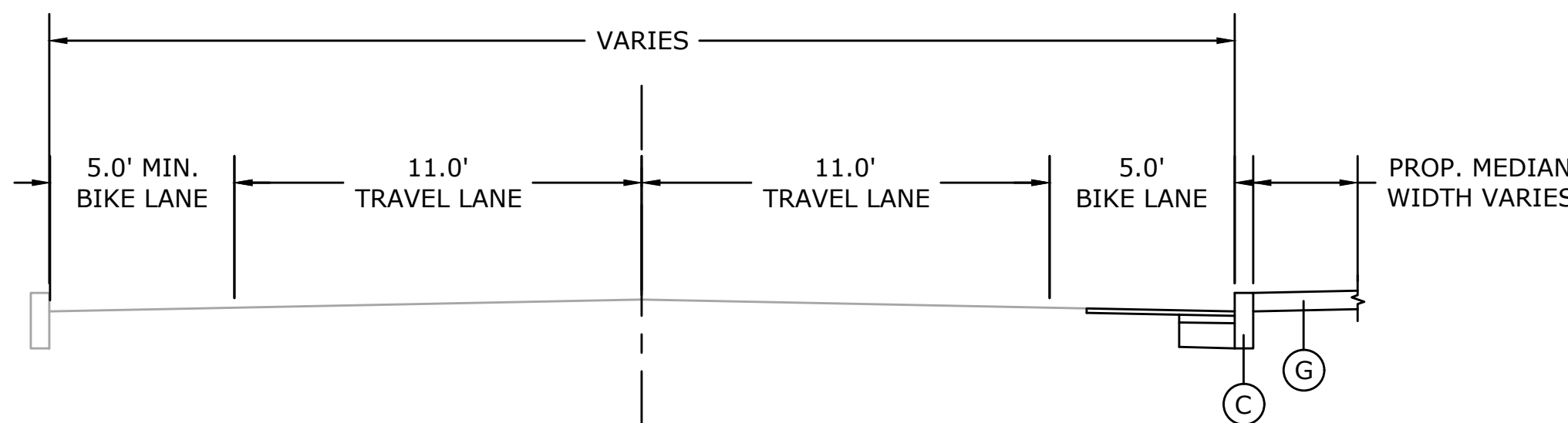
TYPICAL SECTION
STA. 10+00 TO STA. 16+50
SCALE: 1" = 4'



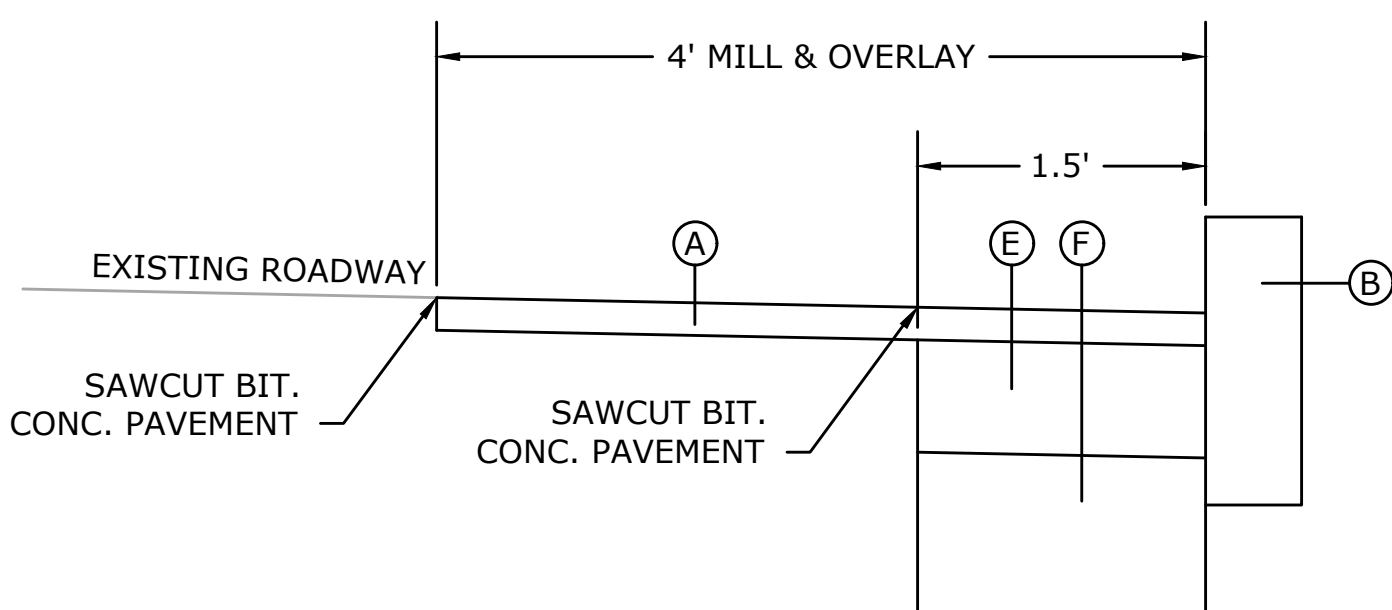
TYPICAL SECTION
STA. 17+25 TO STA. 37+50
SCALE: 1" = 4'



TYPICAL SECTION
STA. 41+50 TO STA. 47+10
SCALE: 1" = 4'



TYPICAL SECTION
STA. 47+75 TO STA. 51+00
SCALE: 1" = 4'



CURB REPLACEMENT ENLARGEMENT (SEE PLANS)
SCALE: 1" = 1'

PROGRESS PRINT - 60% DESIGN

MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

TYPICAL SECTIONS

OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG
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SCALE: 1" = 4'

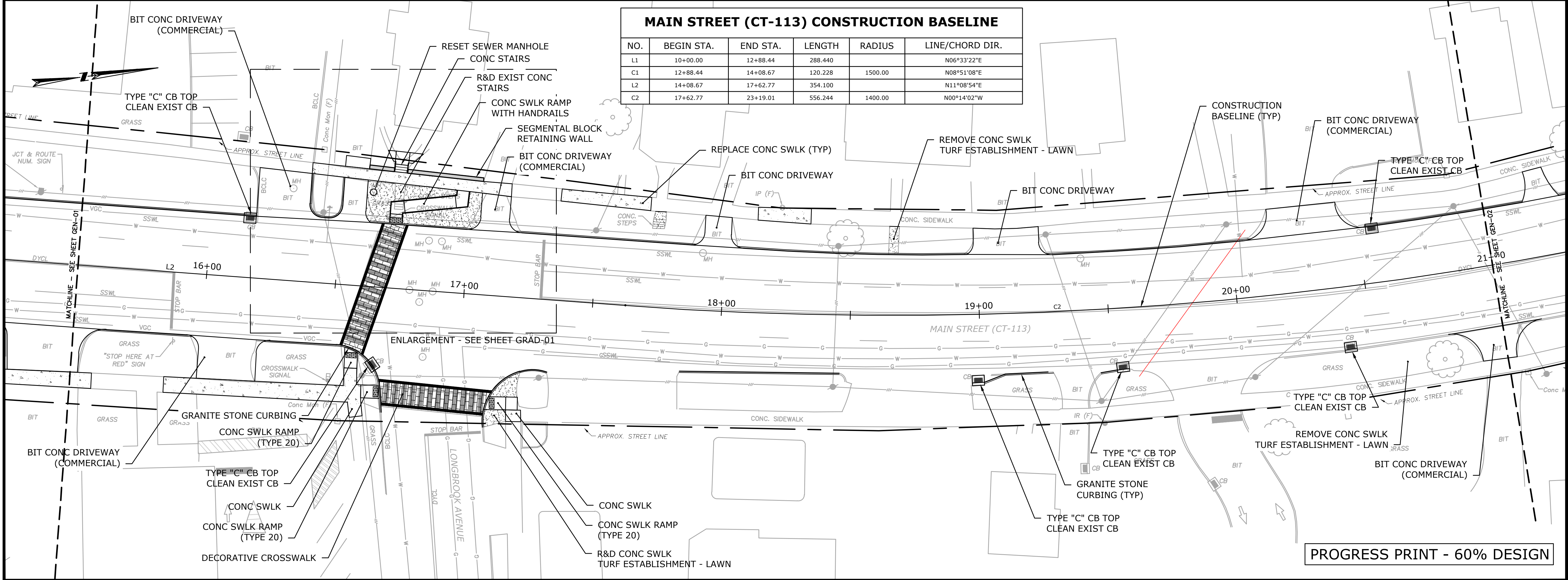
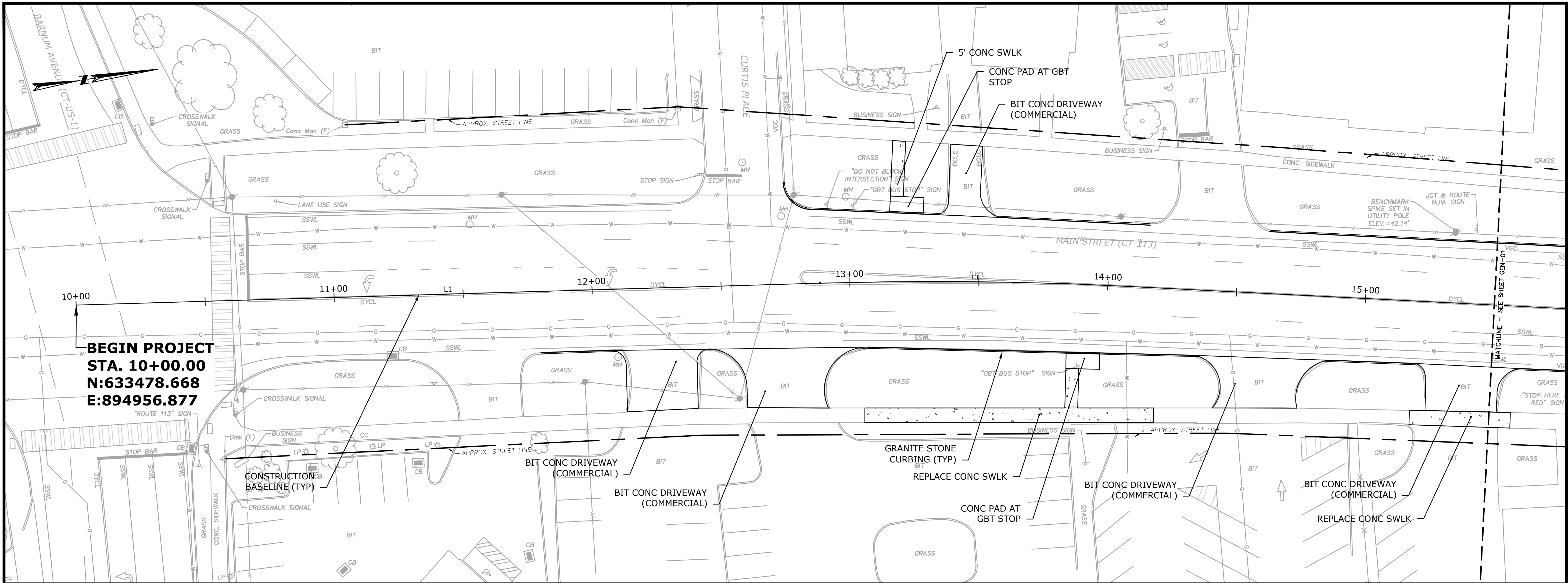
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FILE: 8387300\CIVIL\DRAWINGS\

DWG. NO:

JOB. NO: 83873.00

TYP-01



MAIN STREET (CT-113) CONSTRUCTION BASELINE					
NO.	BEGIN STA.	END STA.	LENGTH	RADIUS	LINE/CHORD DIR.
L1	10+00.00	12+88.44	288.440		N06°33'22"E
C1	12+88.44	14+08.67	120.228	1500.00	N08°51'08"E
L2	14+08.67	17+62.77	354.100		N11°08'54"E
C2	17+62.77	23+19.01	556.244	1400.00	N00°14'02"W

MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

GENERAL PLAN

OCTOBER 2024

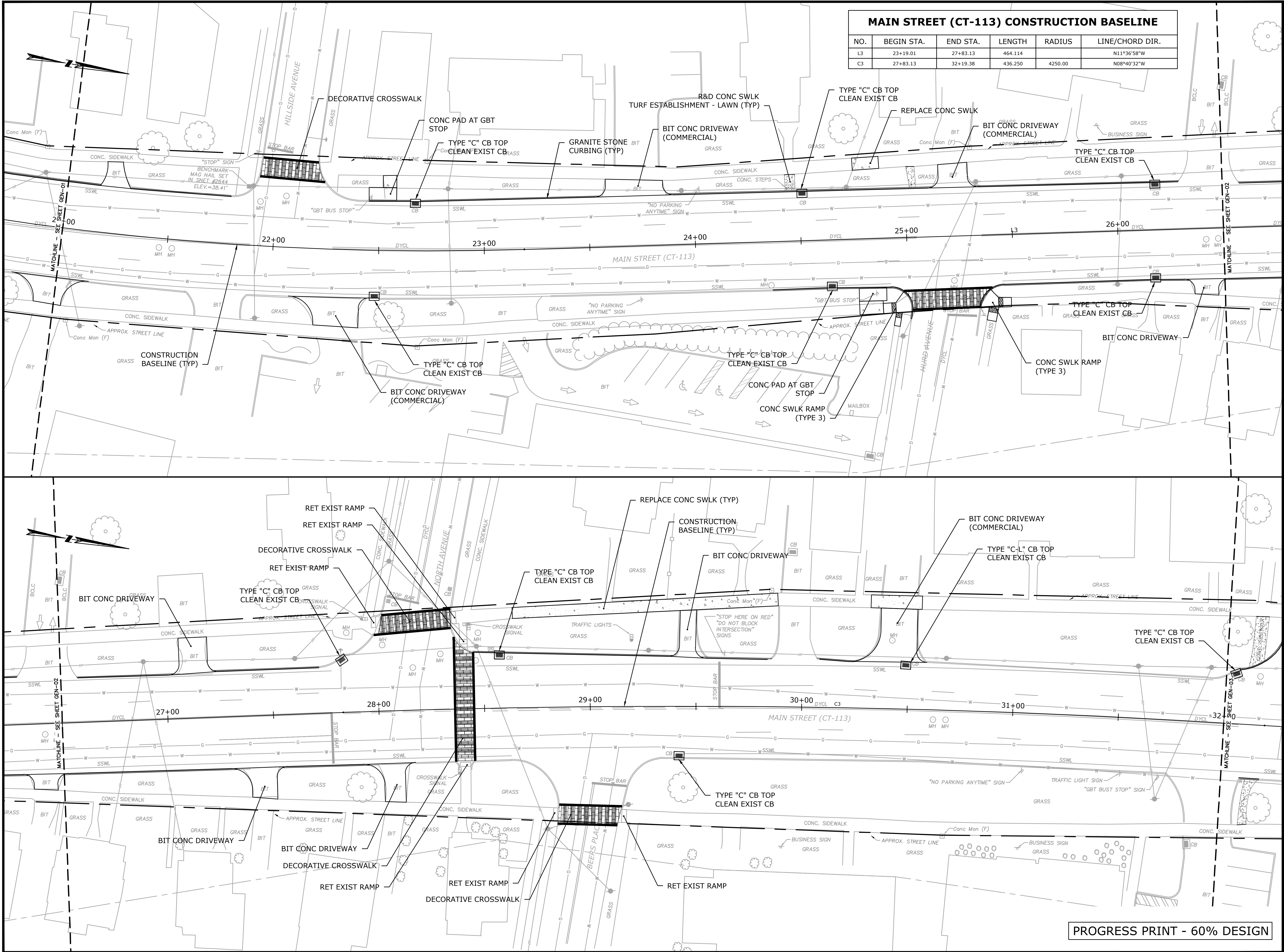
REVISIONS:	

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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SCALE: 1" = 20'
0 10 20 40 FEET
FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO.:
JOB. NO: 83873.00
GEN-01

PROGRESS PRINT - 60% DESIGN



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

GENERAL PLAN

OCTOBER 2024

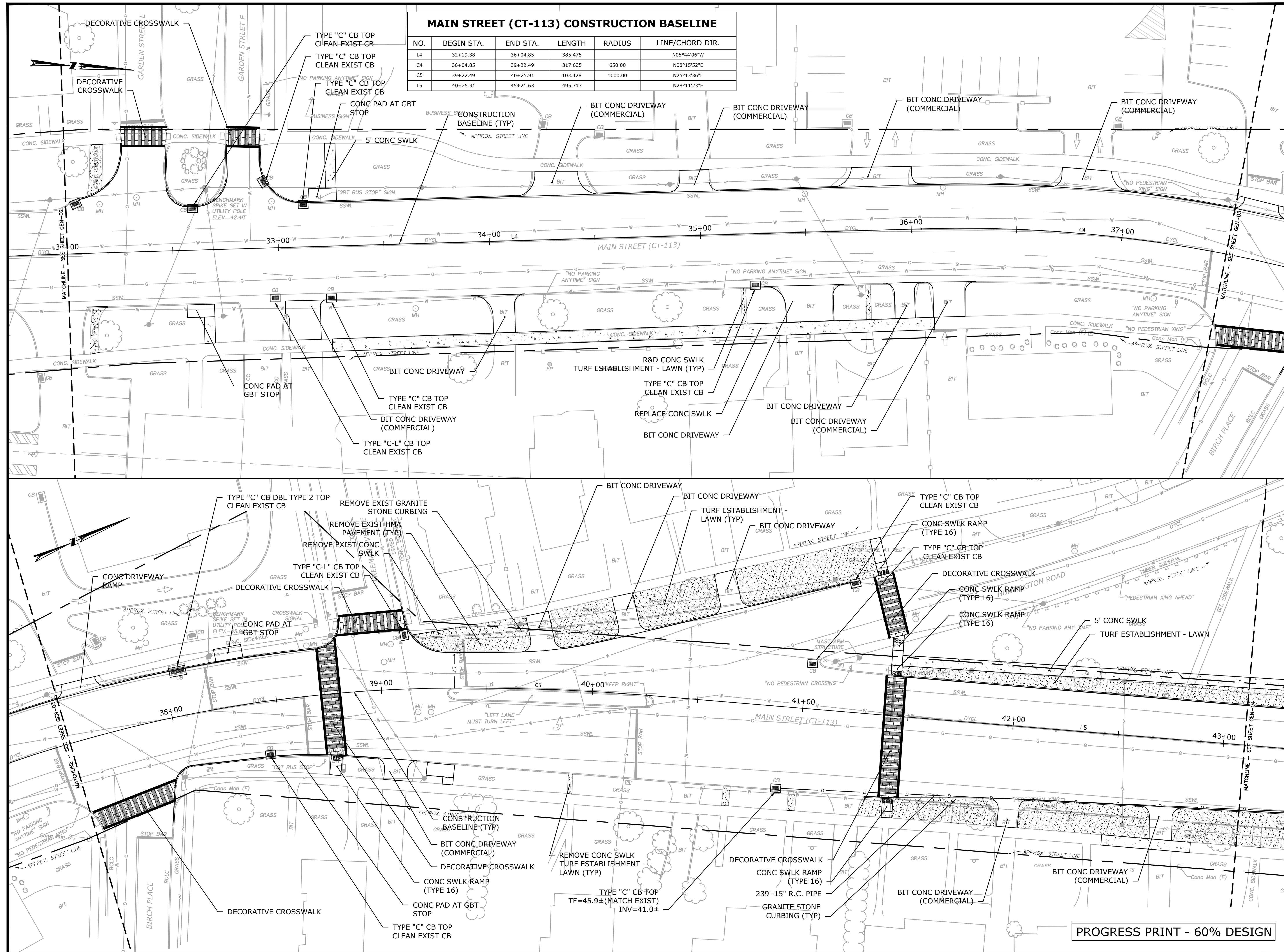
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FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO: GEN-02
JOB. NO: 83873.00



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

GENERAL PLAN

OCTOBER 2024

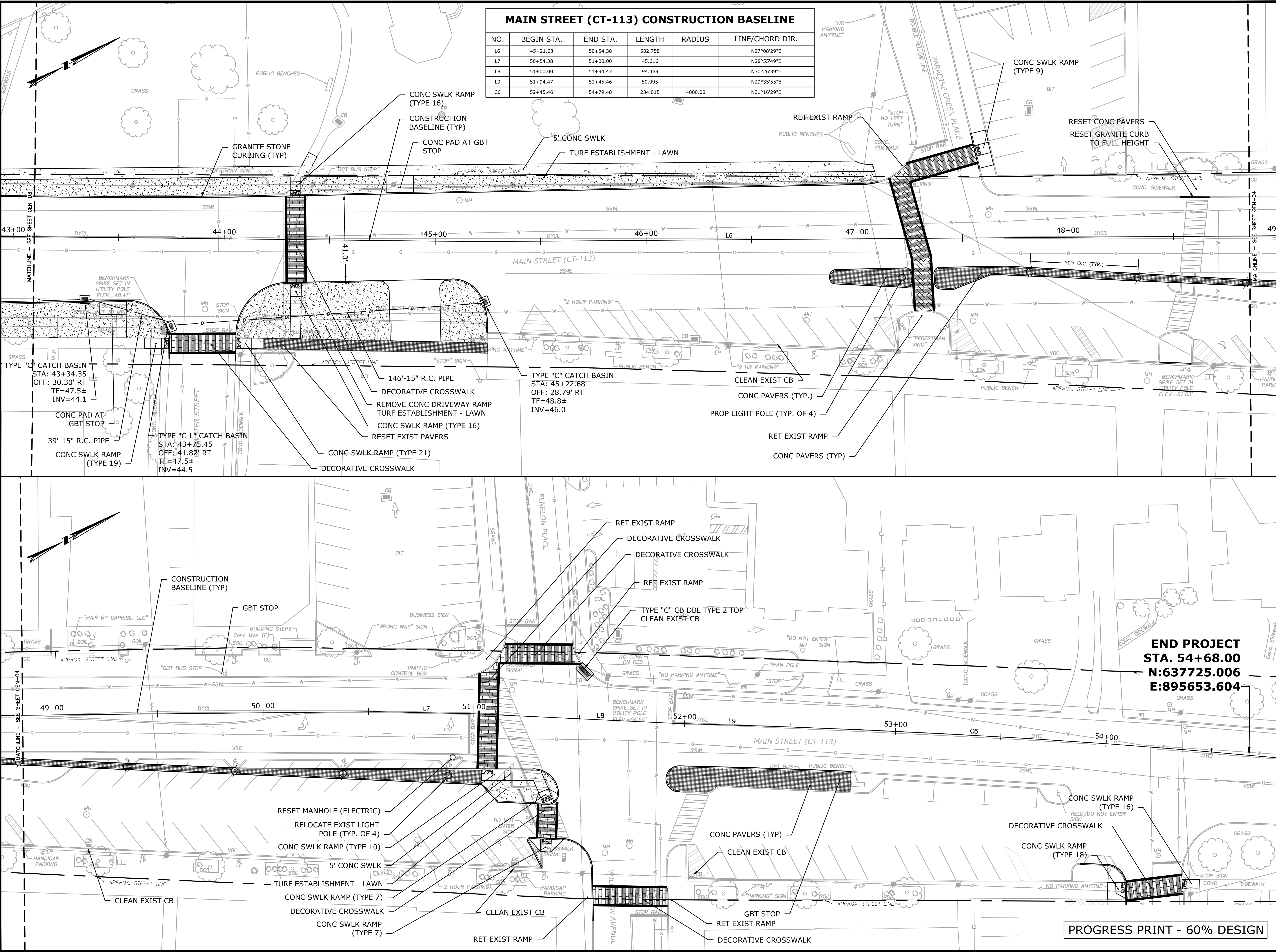
REVISIONS:

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DWG. NO: GEN-03
JOB. NO: 83873.00

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MAIN STREET (CT-113) CONSTRUCTION BASELINE					
NO.	BEGIN STA.	END STA.	LENGTH	RADIUS	LINE/CHORD DIR.
L6	45+21.63	50+54.38	532.758		N27°08'29"E
L7	50+54.38	51+00.00	45.616		N28°55'49"E
L8	51+00.00	51+94.47	94.469		N30°26'39"E
L9	51+94.47	52+45.46	50.995		N29°35'55"E
C6	52+45.46	54+79.48	234.015	4000.00	N31°16'29"E

MAIN STREET
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MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

GENERAL PLAN

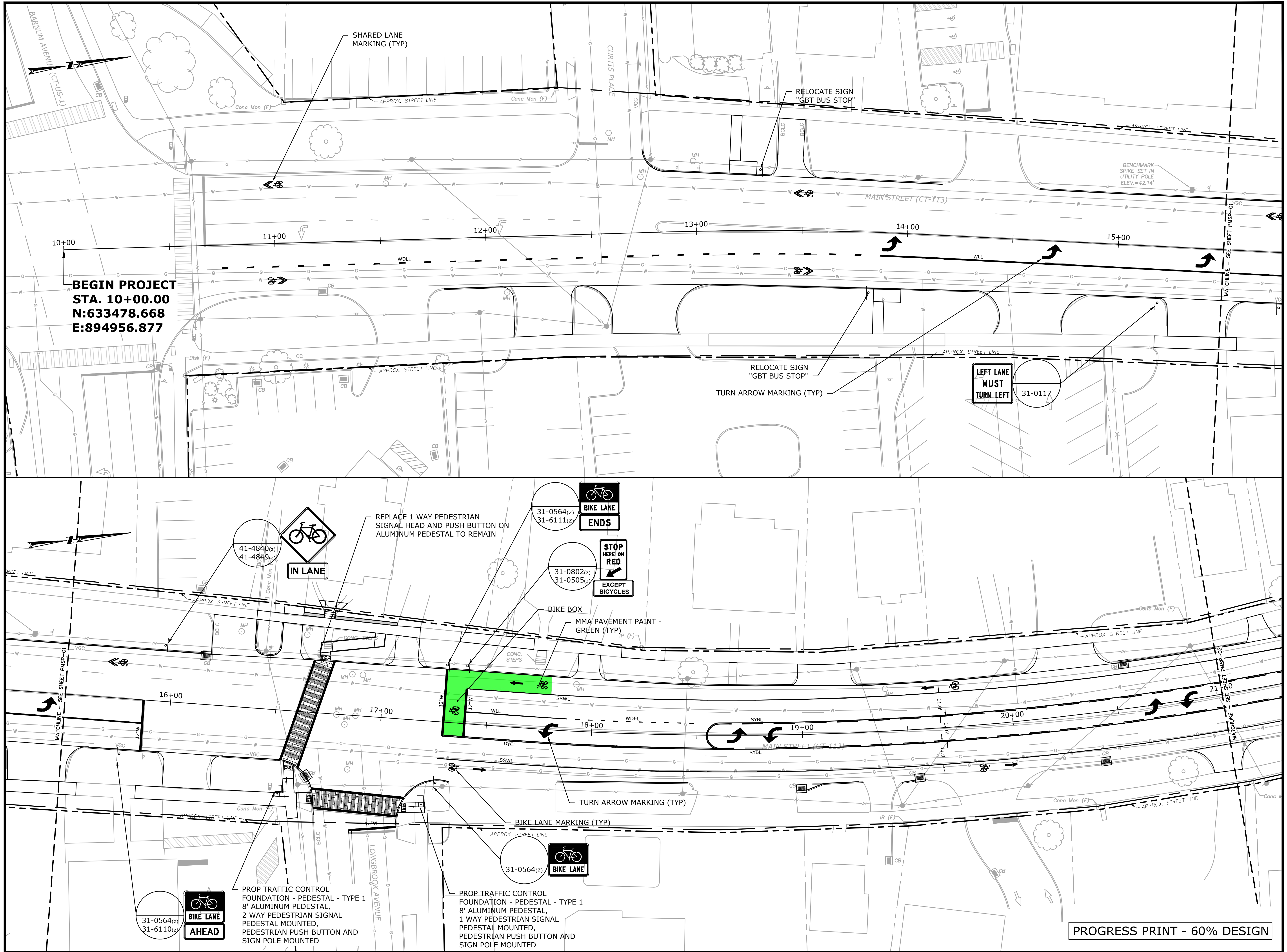
OCTOBER 2024

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DWG. NO:
JOB. NO: 83873.00
GEN-04



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD
CONNECTICUT

PAVEMENT MARKINGS &
SIGNAGE PLAN

OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG

1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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SCALE: 1" = 20'

0 10 20 40 FEET

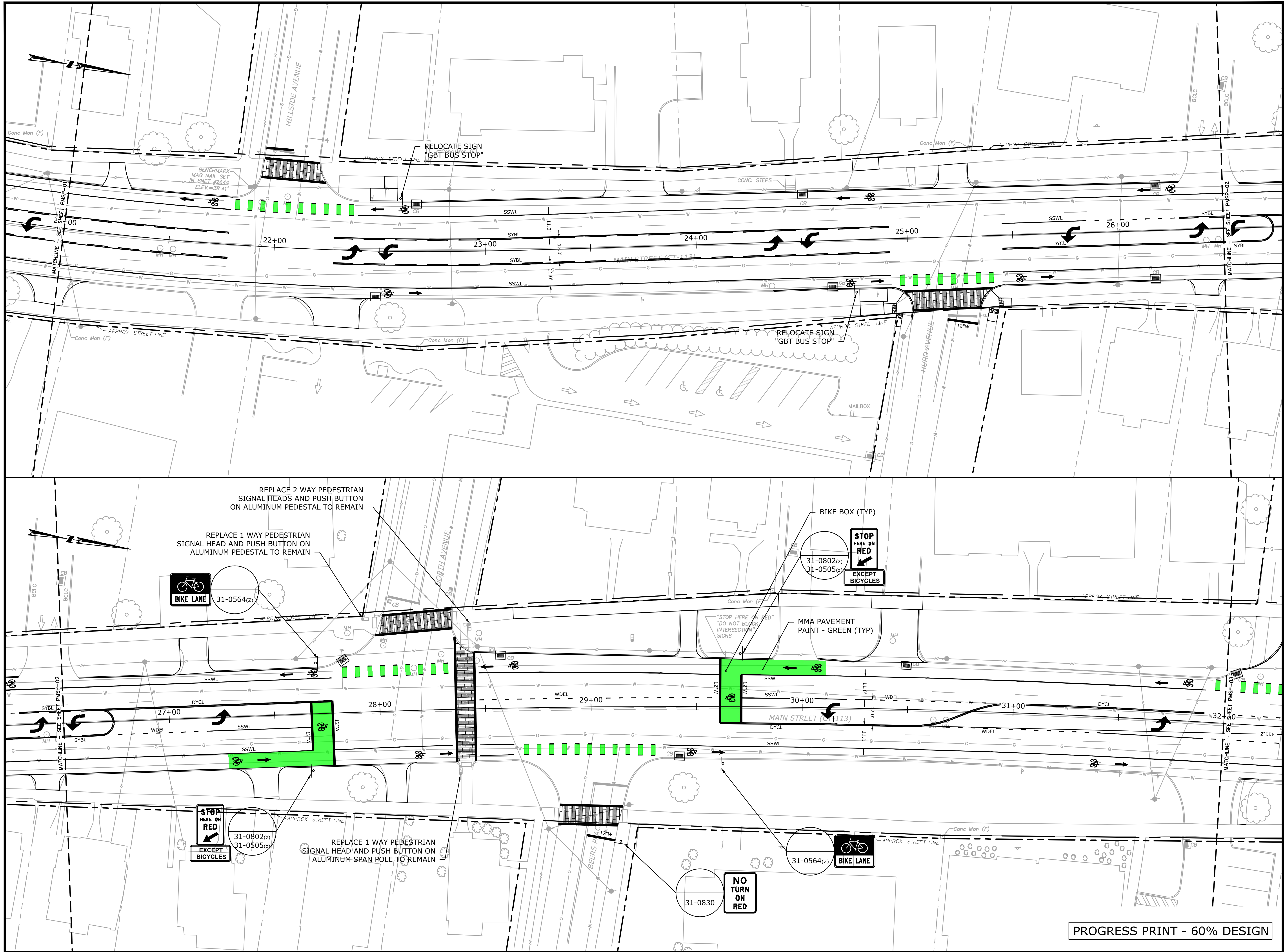
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DWG. NO:

JOB. NO: 83873.00

PMSP-01

PROGRESS PRINT - 60% DESIGN



MAIN STREET
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PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD
CONNECTICUT

PAVEMENT MARKINGS &
SIGNAGE PLAN

OCTOBER 2024

REVISIONS:

NO.	DESCRIPTION	DATE

PREPARED FOR:

METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

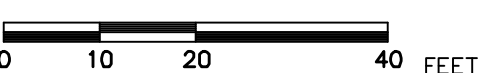
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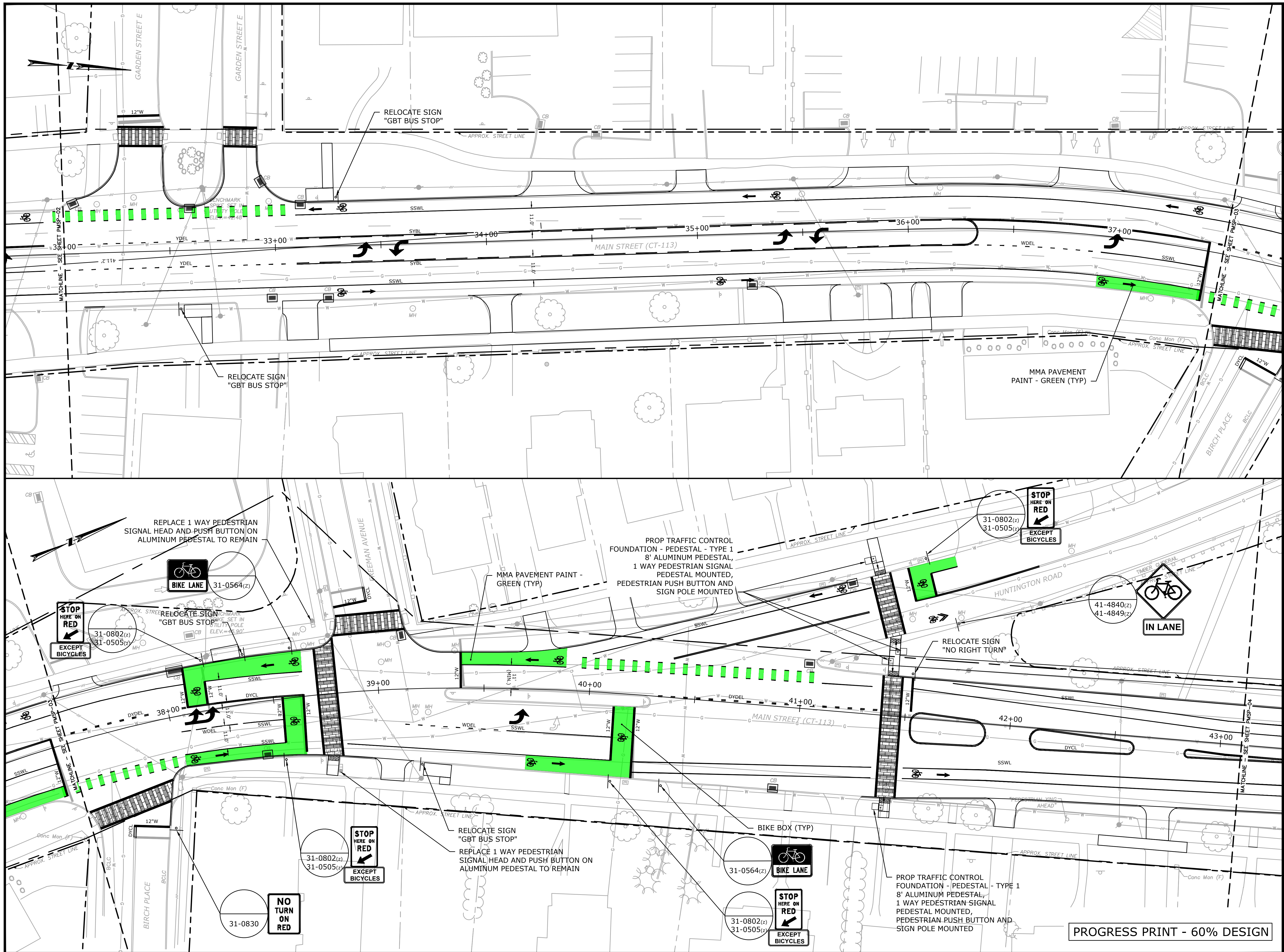
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DWG. NO:

JOB. NO: 83873.00

PMSP-02

PROGRESS PRINT - 60% DESIGN



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MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

PAVEMENT MARKINGS &
SIGNAGE PLAN

OCTOBER 2024

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BRIDGEPORT, CT 06604

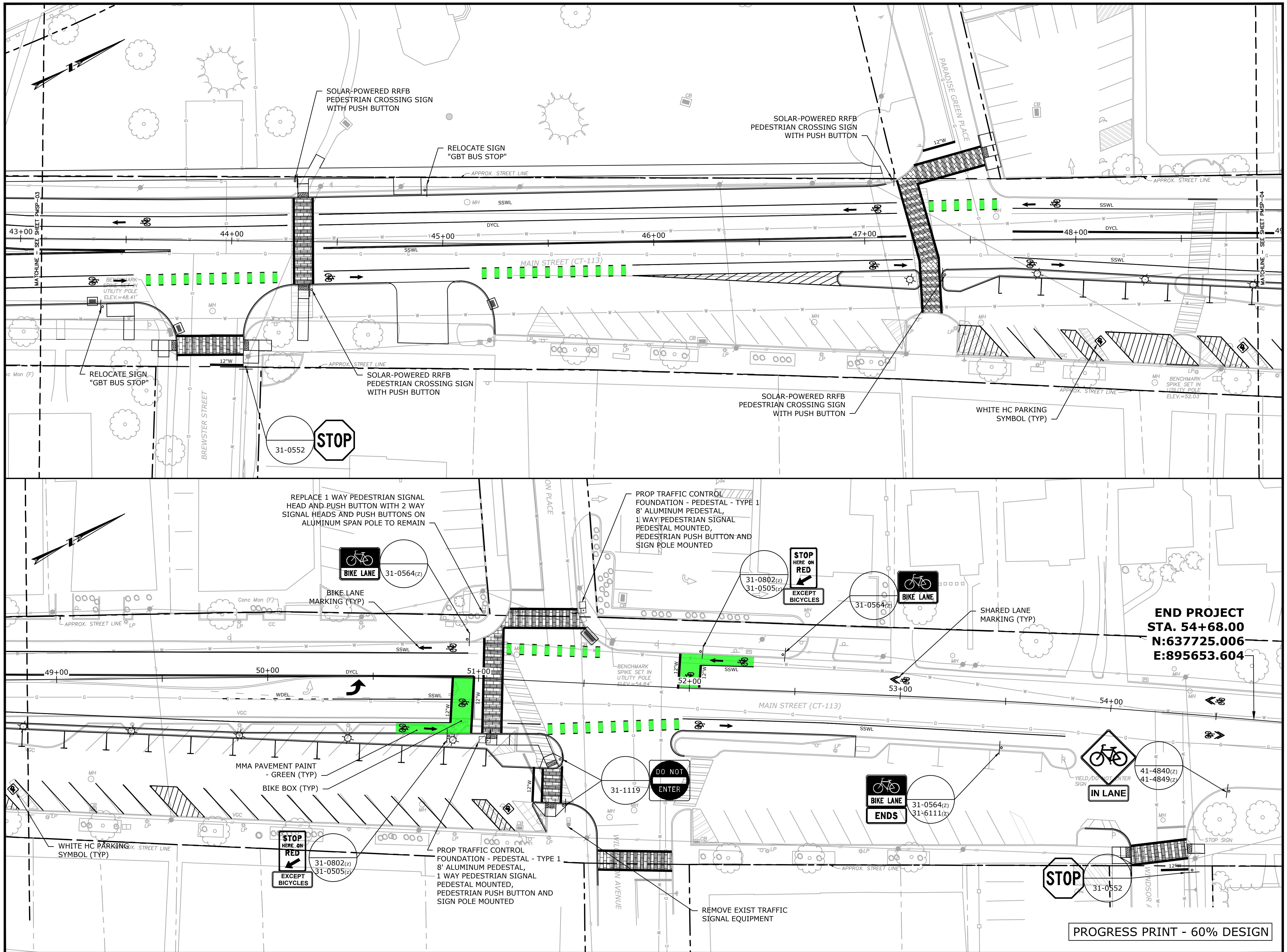
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SCALE: 1" = 20'
0 10 20 40 FEET

FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO:
JOB. NO: 83873.00

PMSP-03

PROGRESS PRINT - 60% DESIGN



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

PAVEMENT MARKINGS &
SIGNAGE PLAN

OCTOBER 2024

REVISIONS:

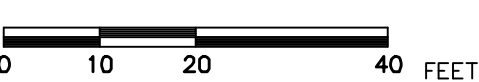
PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP
BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033

860 652 8227

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SCALE: 1" = 20'



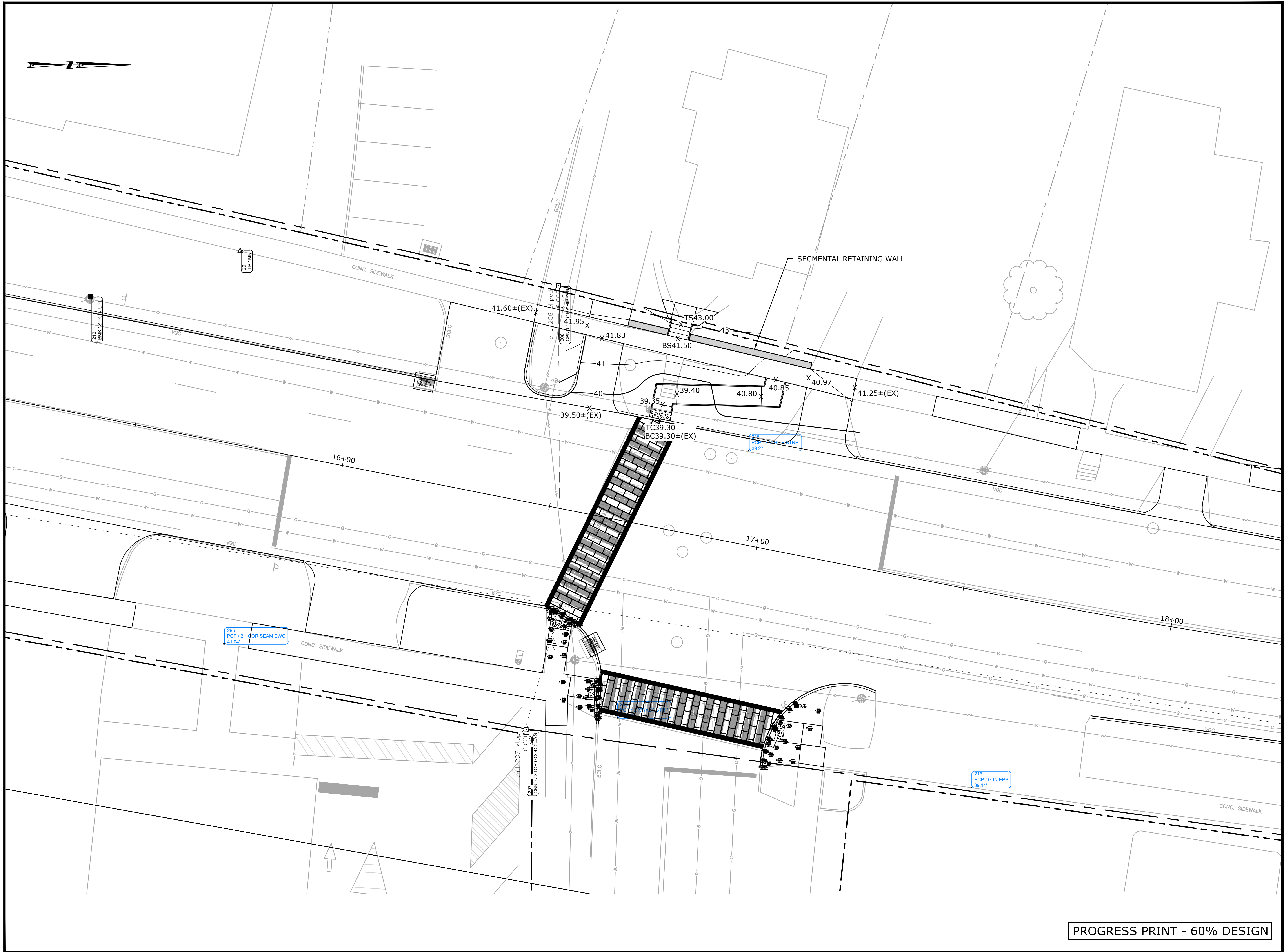
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DWG. NO:

JOB. NO: 83873.00

PMSP-04

PROGRESS PRINT - 60% DESIGN



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

GRADING PLAN

OCTOBER 2024

REVISIONS:

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METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP
BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033
860 652 8227

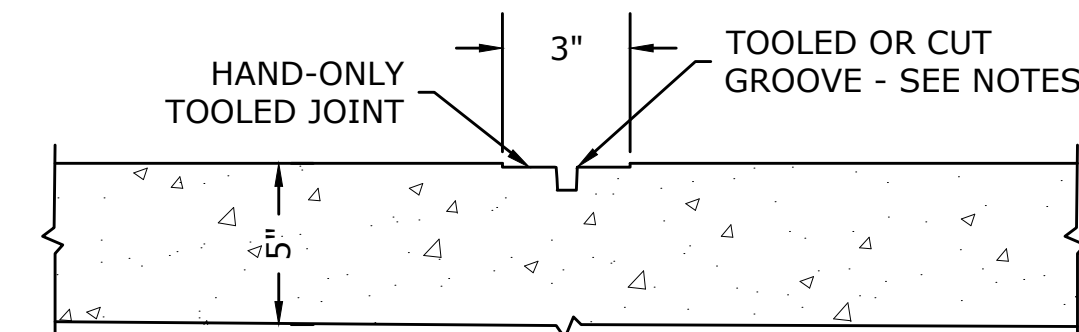
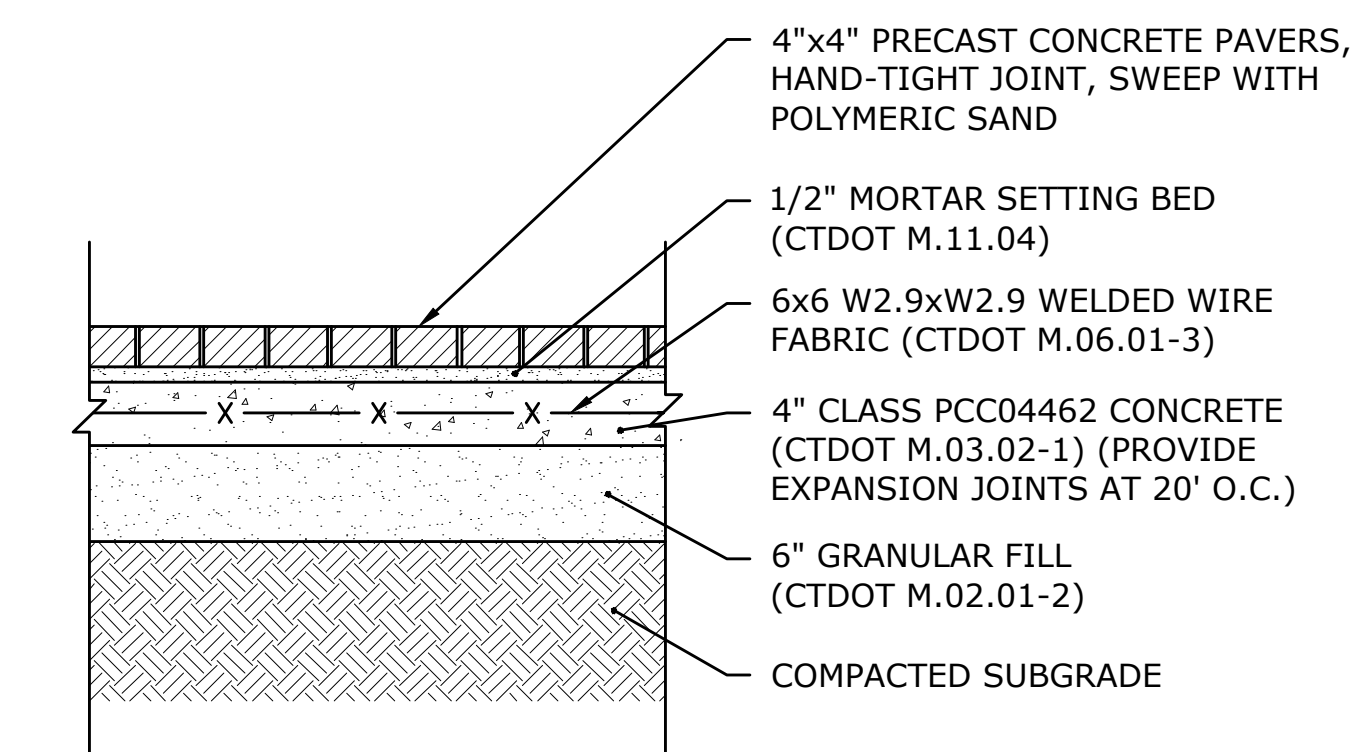
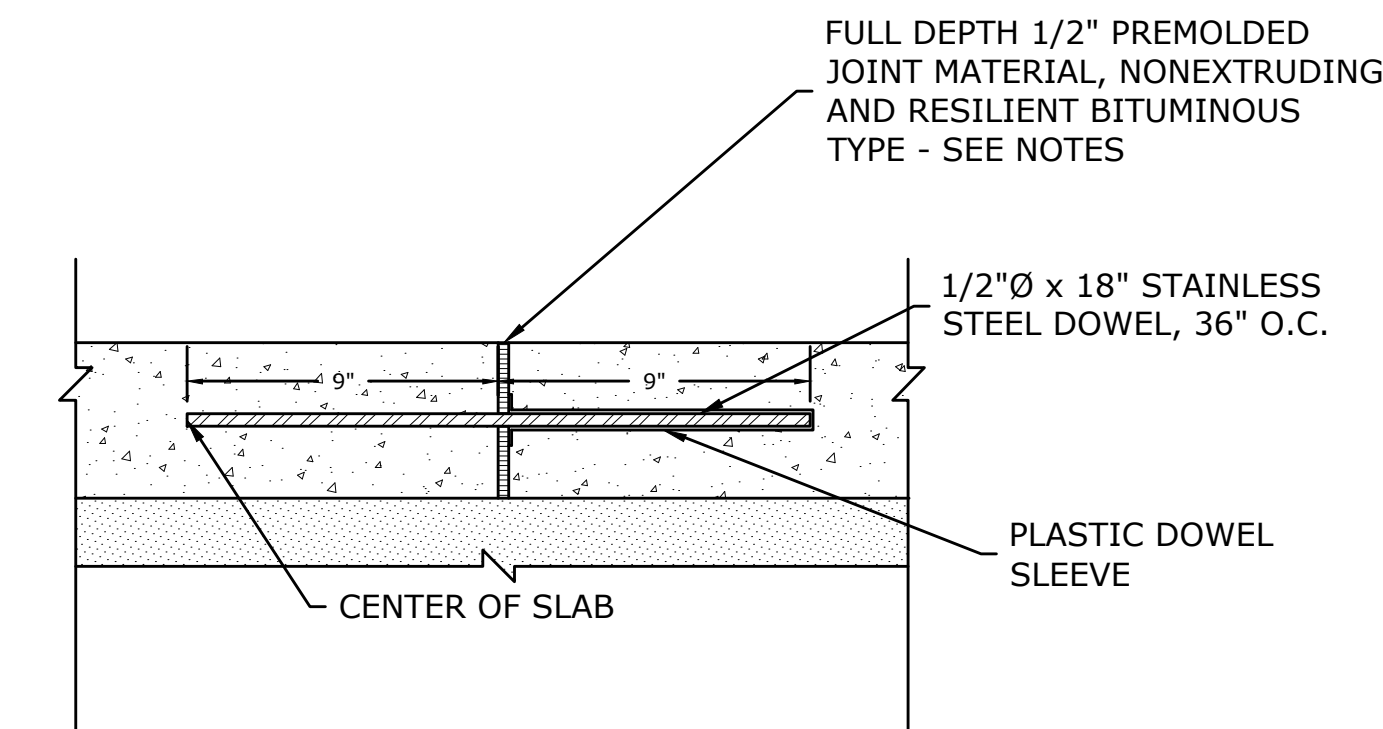
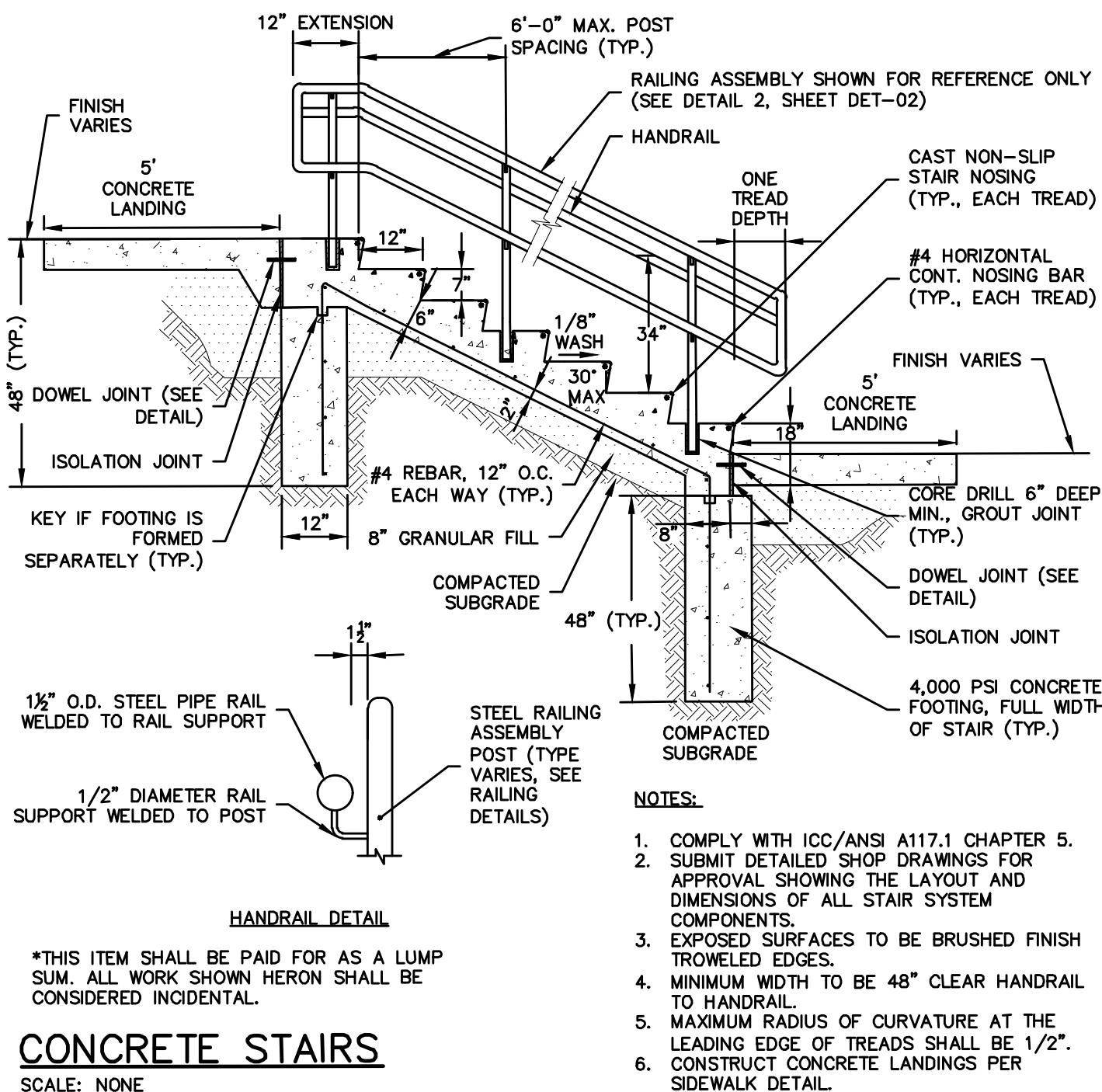
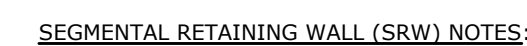
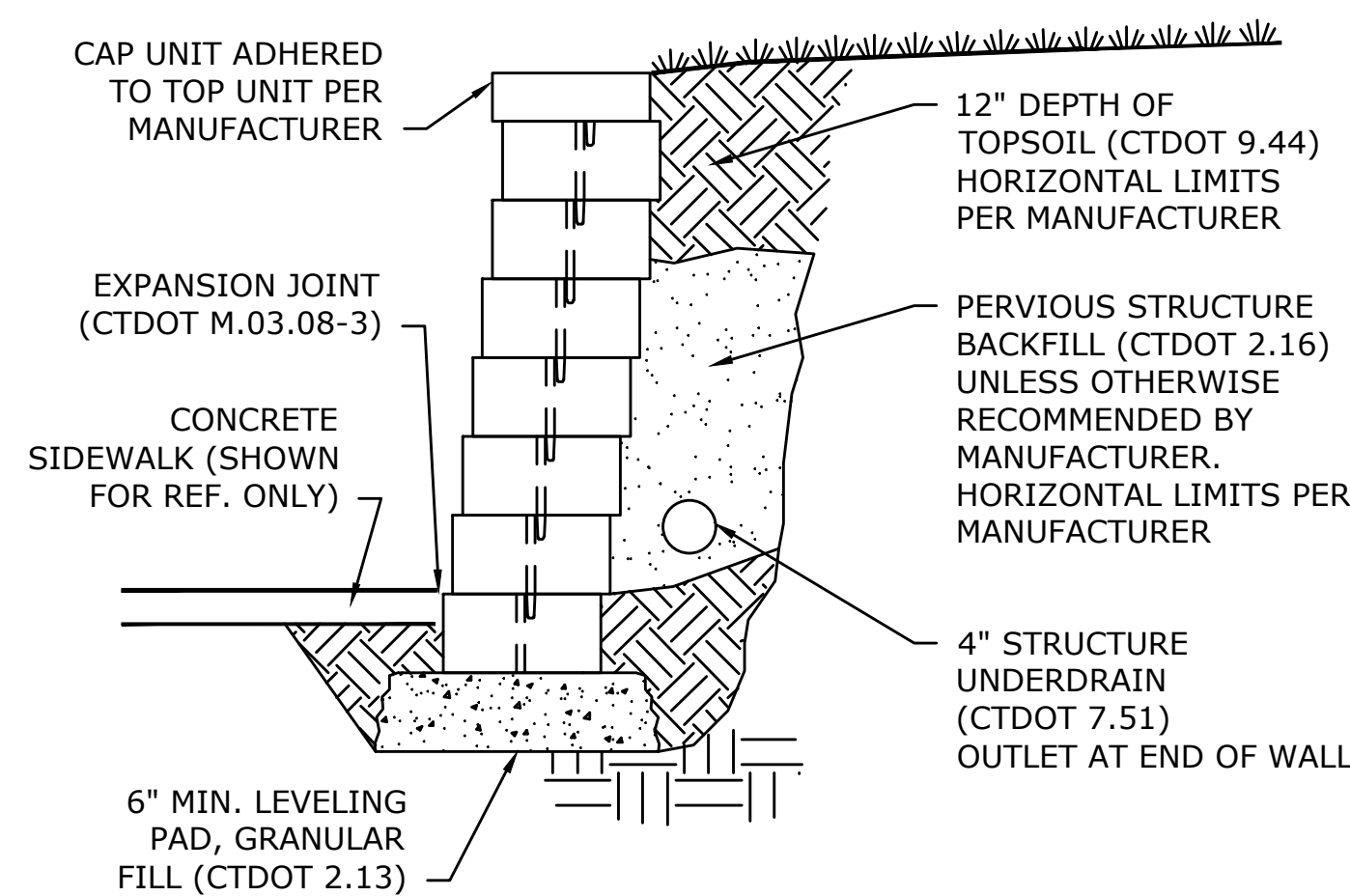
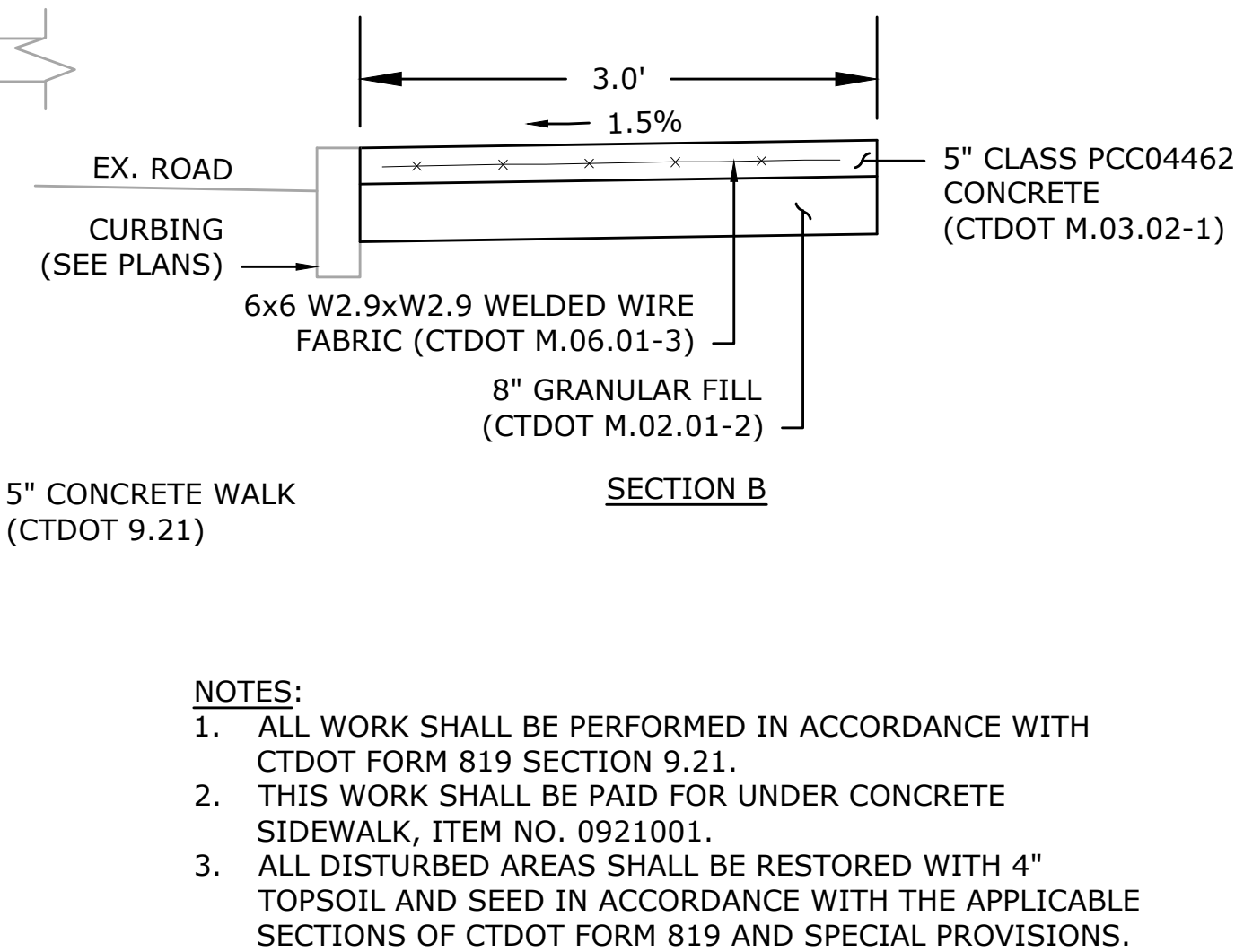
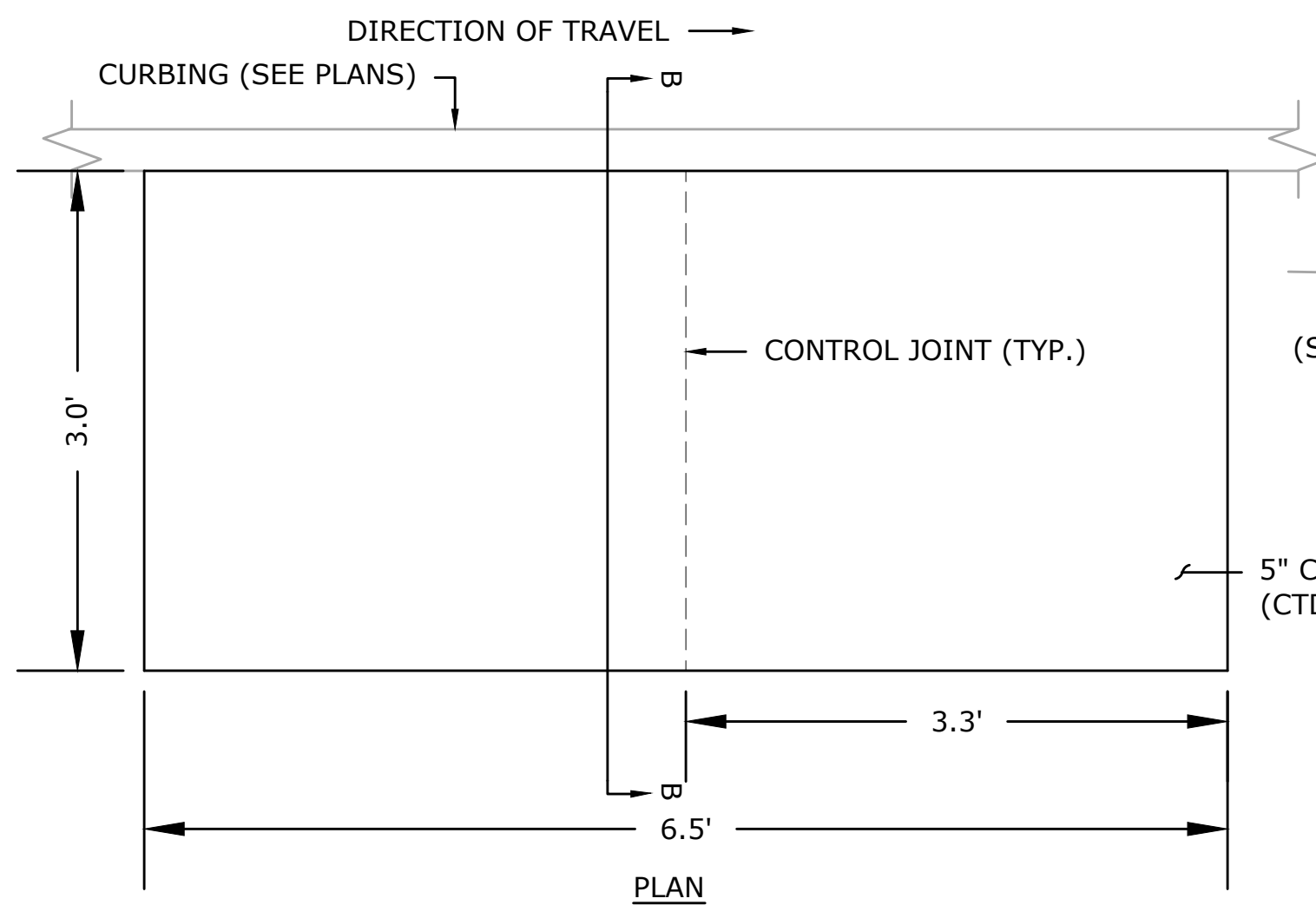
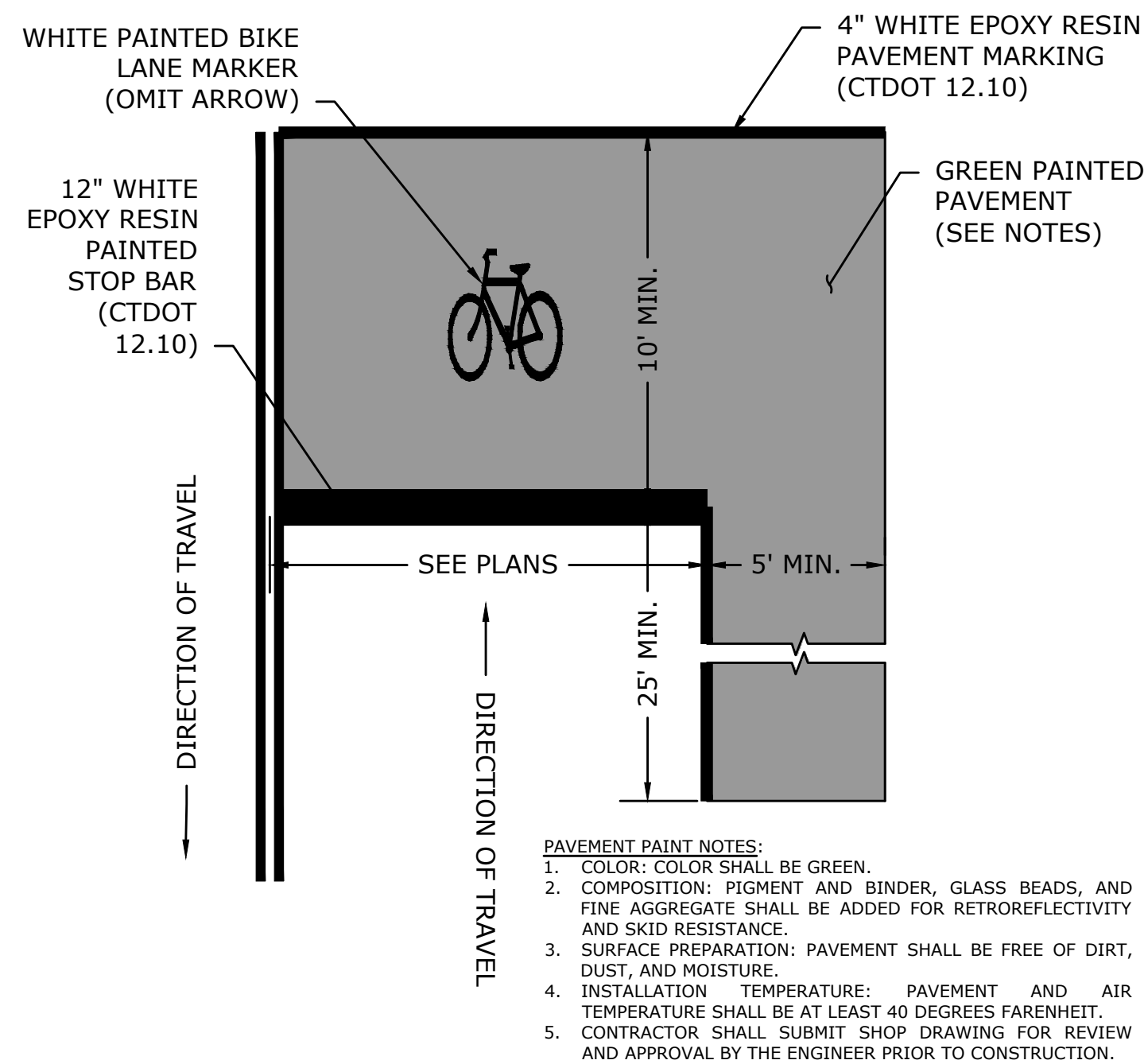
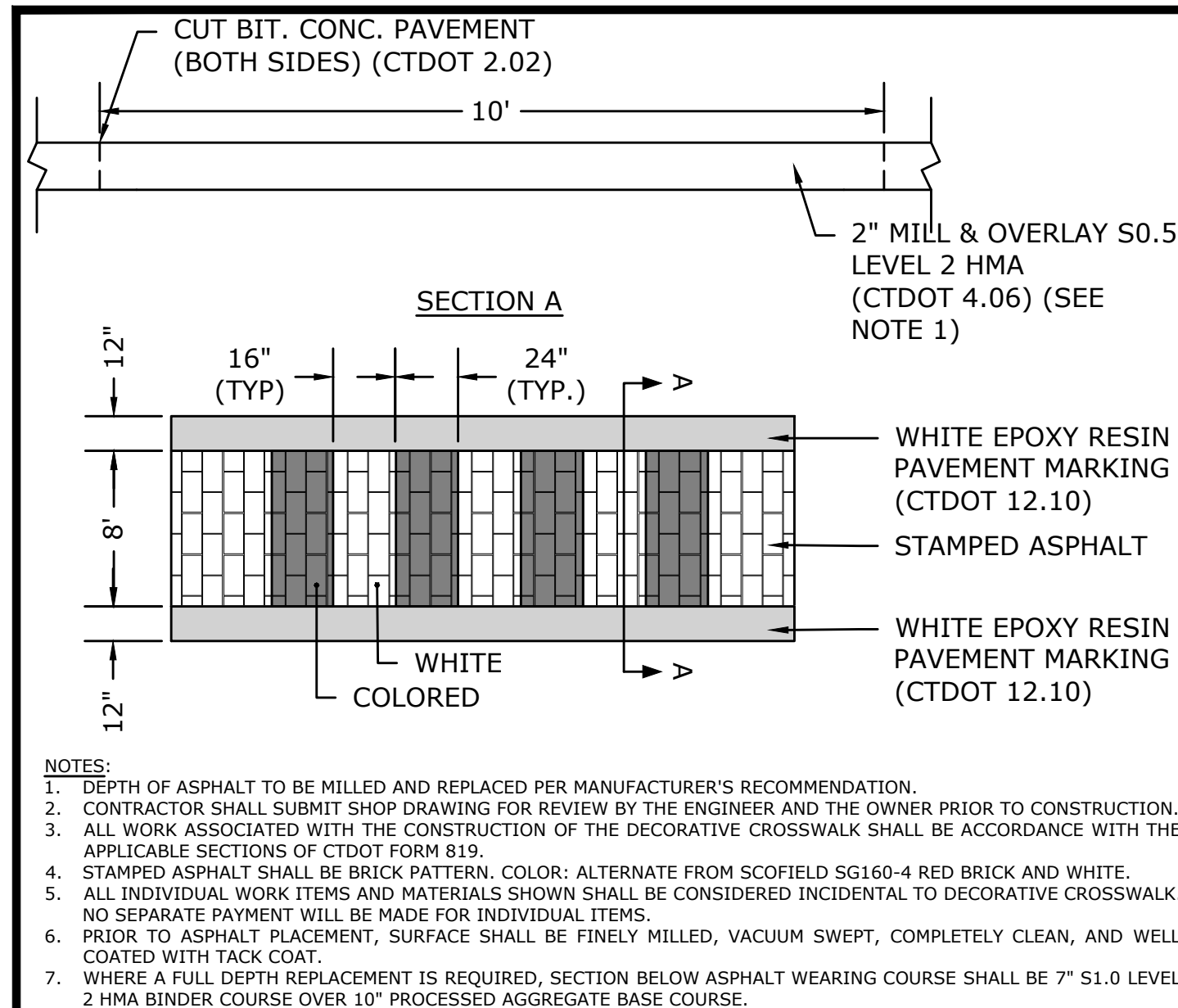
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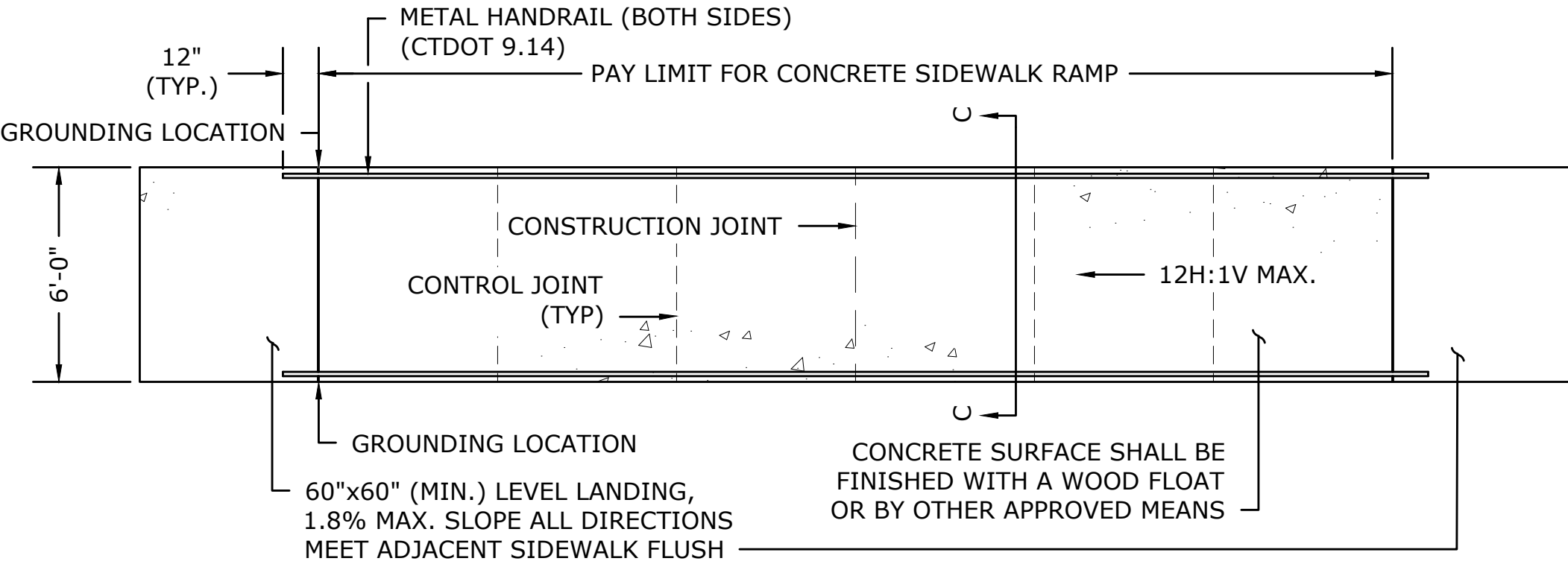
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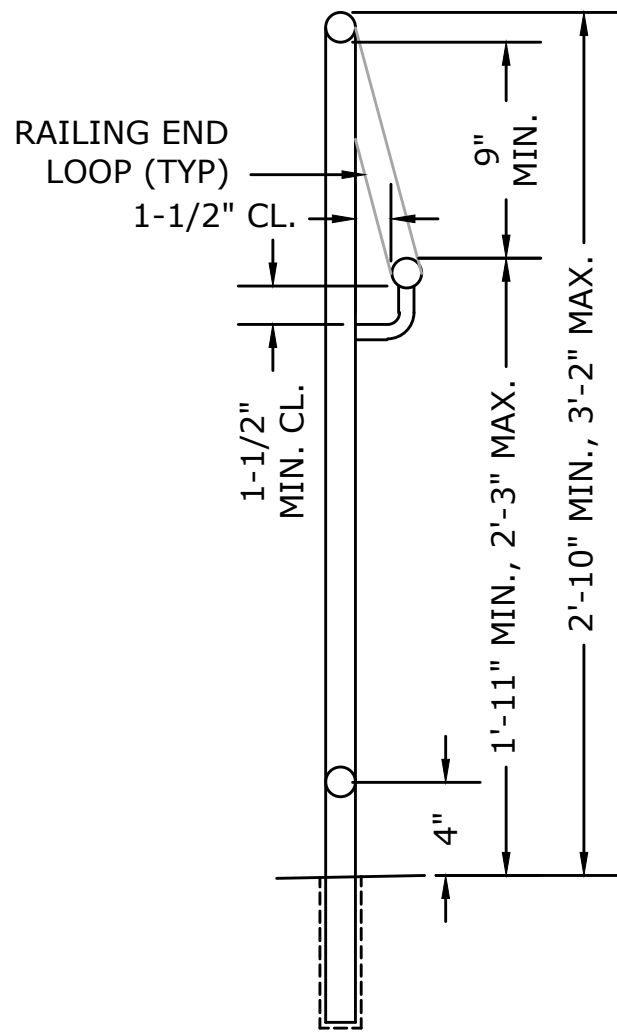
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JOB. NO: 83873.00
GRAD-01

PROGRESS PRINT - 60% DESIGN



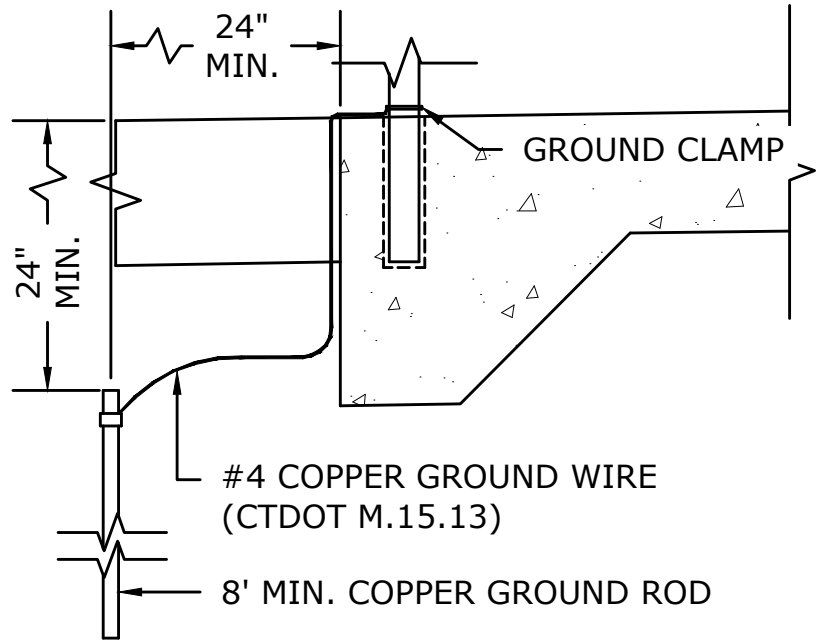


TYPICAL CONCRETE SIDEWALK RAMP
SCALE: NONE

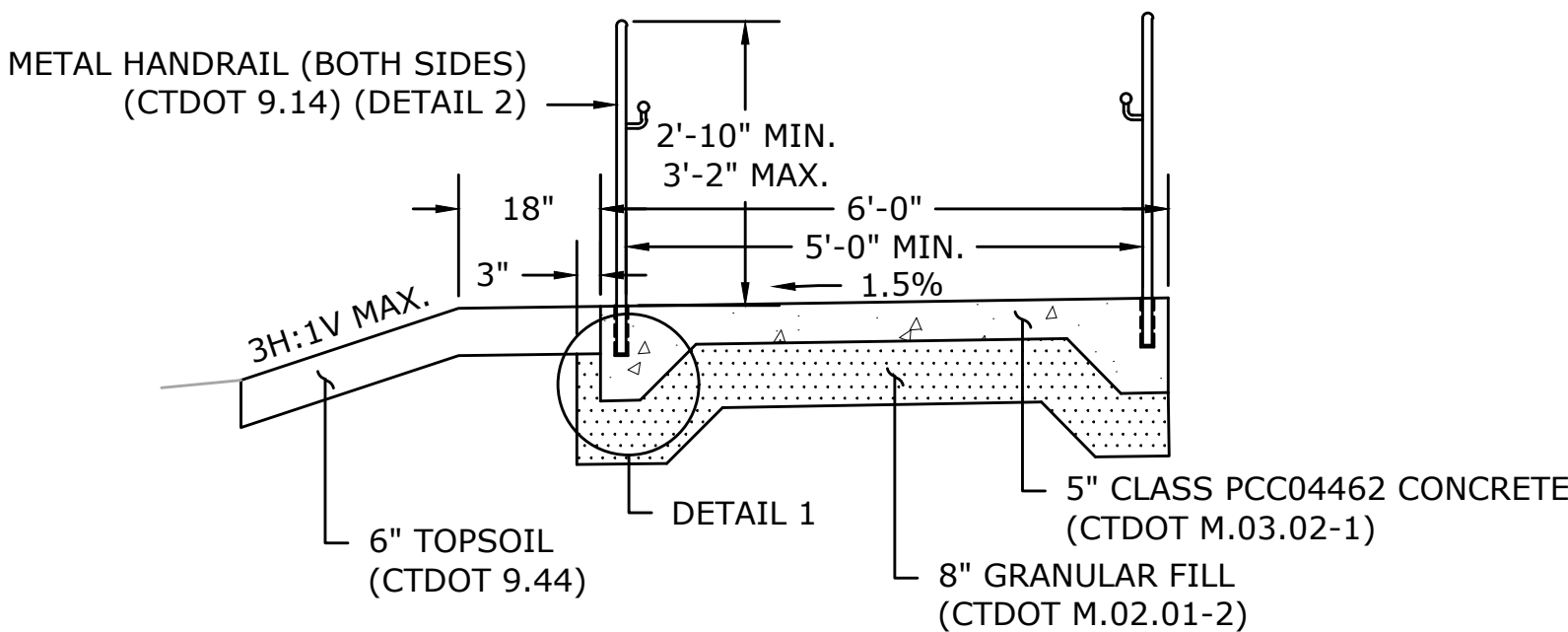


NOTE:
1. EACH HANDRAIL SHALL BE GROUNDED IN ACCORDANCE WITH SPECIAL PROVISION 0914001A. SEE DETAIL, THIS SHEET.

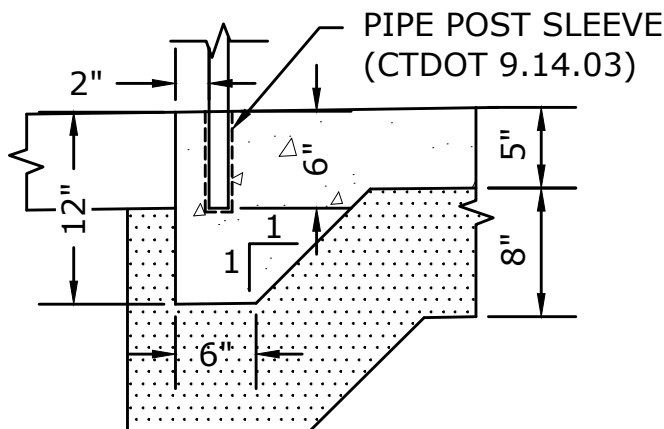
DETAIL 2
SCALE: NONE



GROUNDING DETAIL
SCALE: NONE



SECTION C
SCALE: NONE



DETAIL 1
SCALE: NONE

MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003
MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

MISCELLANEOUS DETAILS

OCTOBER 2024

REVISIONS:		

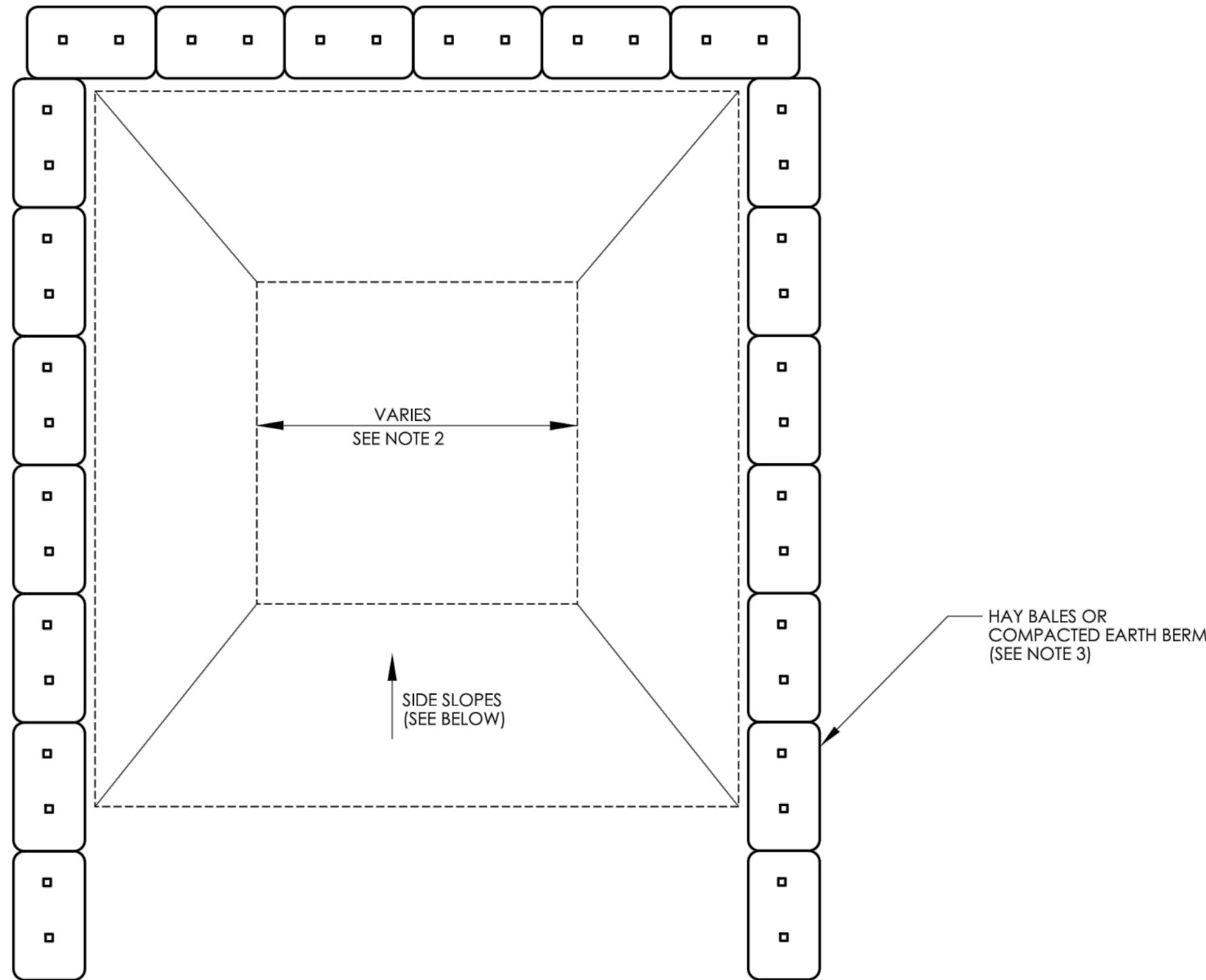
PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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Glastonbury, Connecticut
06033
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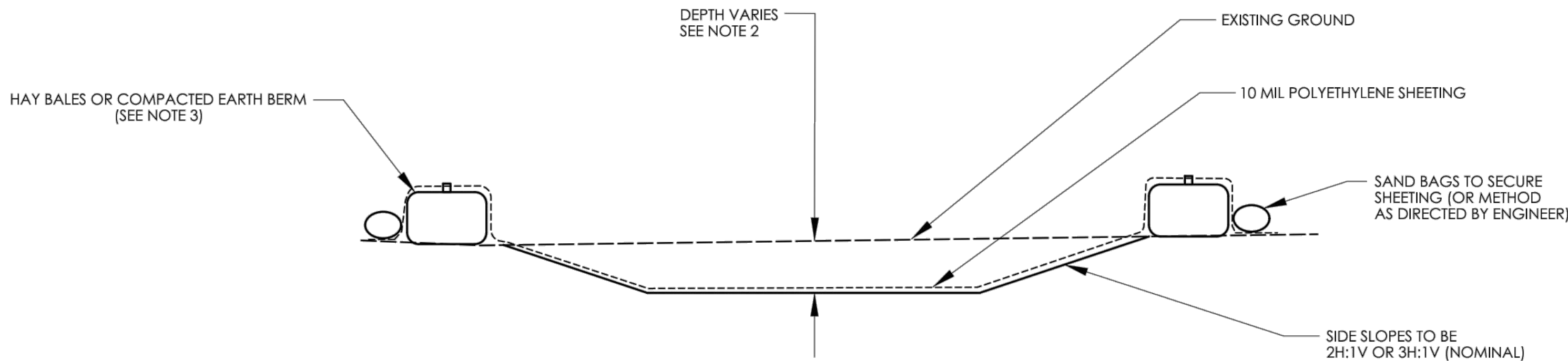
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FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO:
JOB. NO: 83873.00
DET-02

PROGRESS PRINT - 60% DESIGN



PLAN



CONCRETE WASHOUT AREA
NOT TO SCALE
(SEE NOTE 2)

GENERAL NOTES:

1. CONCRETE WASHOUT AREA(S) SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE. THE CONCRETE WASHOUT AREA SHALL BE ENTIRELY SELF-CONTAINED.
2. THE CONTRACTOR SHALL SUBMIT THE DESIGN, LOCATION AND SIZING OF THE CONCRETE WASHOUT AREA(S) WITH THE PROJECT'S EROSION AND SEDIMENTATION CONTROL PLAN AND SHALL BE APPROVED BY THE ENGINEER.

LOCATION: WASHOUT AREA(S) ARE TO BE LOCATED AT LEAST 50 FEET FROM ANY STREAM, WETLAND, STORM DRAINS, OR OTHER SENSITIVE RESOURCE. THE FLOOD CONTINGENCY PLAN MUST ADDRESS THE CONCRETE WASHOUT IF THE WASHOUT IS TO BE LOCATED WITHIN THE FLOODPLAIN.

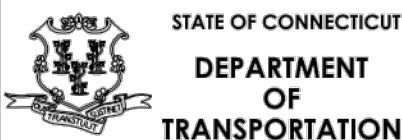
SIZE: THE WASHOUT MUST HAVE SUFFICIENT VOLUME TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS INCLUDING, BUT NOT LIMITED TO, OPERATIONS ASSOCIATED WITH GROUT AND MORTAR.
3. SURFACE DISCHARGE IS UNACCEPTABLE. THEREFORE, HAY BALES OR OTHER CONTROL MEASURES, AS APPROVED BY THE ENGINEER, SHOULD BE USED AROUND THE PERIMETER OF THE CONCRETE WASHOUT AREA FOR CONTAINMENT.
4. SIGNS SHOULD BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE CONCRETE AREA(S) AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CONCRETE WASHOUT TO OPERATORS OF CONCRETE TRUCKS AND PUMP RIGS. WASHOUT AREA(S) SHOULD BE FLAGGED WITH SAFETY FENCING OR OTHER APPROVED METHOD.
5. WASHOUT AREA(S) ARE TO BE INSPECTED AT LEAST ONCE A WEEK FOR STRUCTURAL INTEGRITY, ADEQUATE HOLDING CAPACITY AND CHECKED FOR LEAKS, TEARS, OR OVERFLOWS. (AS REQUIRED BY THE CONSTRUCTION SITE ENVIRONMENTAL INSPECTION REPORT) WASHOUT AREA(S) SHOULD BE CHECKED AFTER HEAVY RAINS.
6. HARDENED CONCRETE WASTE SHOULD BE REMOVED AND DISPOSED OF WHEN THE WASTE HAS ACCUMULATED TO HALF OF THE CONCRETE WASHOUT'S DEPTH. THE WASTE CAN BE STORED AT AN UPLAND LOCATION, AS APPROVED BY THE ENGINEER. ALL CONCRETE WASTE SHALL BE DISPOSED OF IN A MANNER CONSISTENT WITH ALL APPLICABLE LAWS, REGULATIONS, AND GUIDELINES.
7. PAYMENT FOR THIS ITEM IS TO BE INCLUDED UNDER THE GENERAL COST OF THE WORK FOR THE PROJECT, INCLUDING SITE RESTORATION.

REV.	DATE	REVISION DESCRIPTION

DESIGNER/DRAFTER: CHECKED BY:	SIGNATURE/ BLOCK:	APPROVED BY:	PROJECT NUMBER: 0000-0000 PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION TOWNS: SAMPLE TOWN(S) DRAWING TITLE: CONCRETE WASHOUT AREA	DRAWING NO. SHEET NO.
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PLOTTED DATE: 11/25/2022

OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111



MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

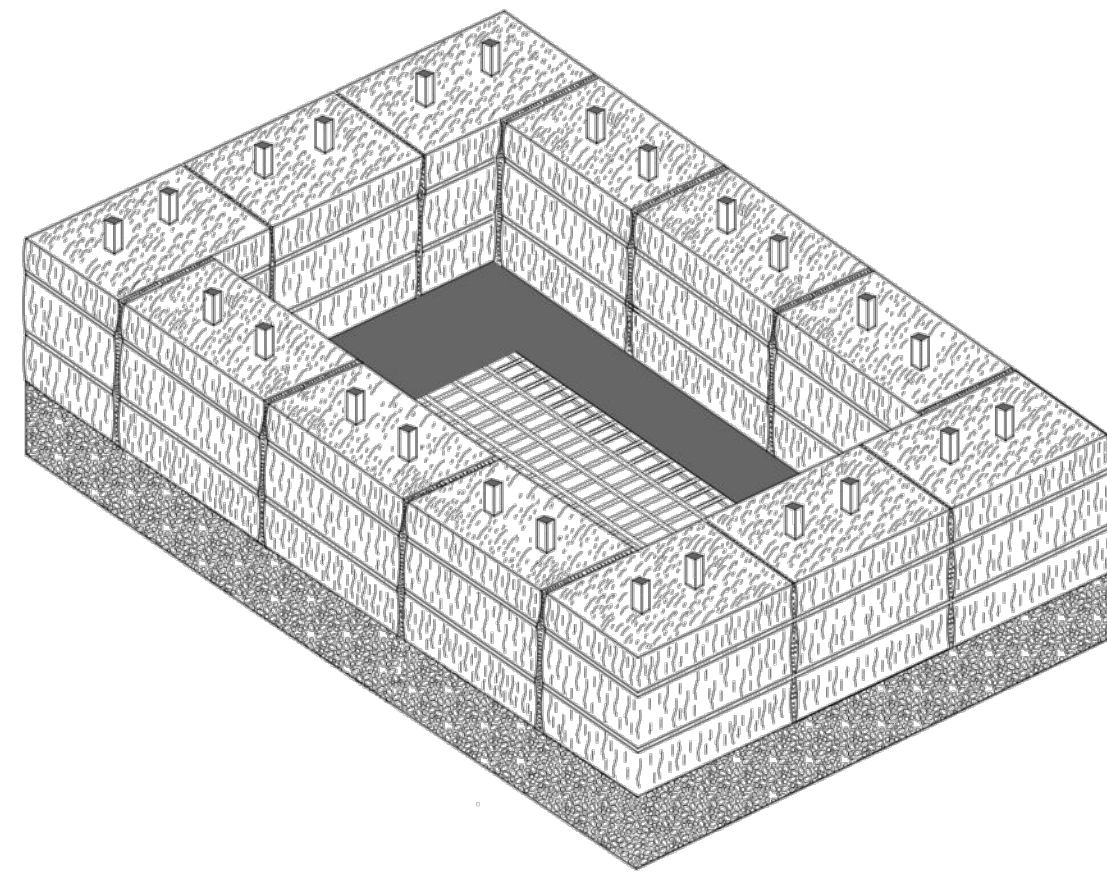
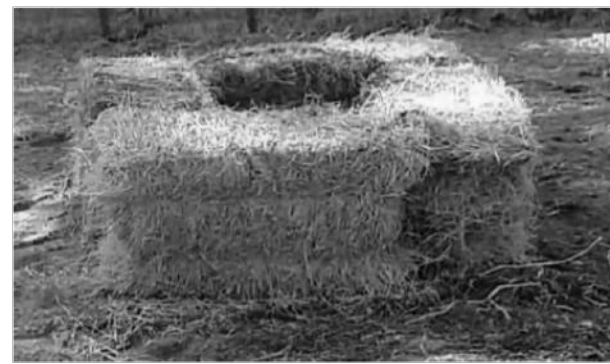
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SCALE: AS SHOWN

FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO:
JOB. NO: 83873.00

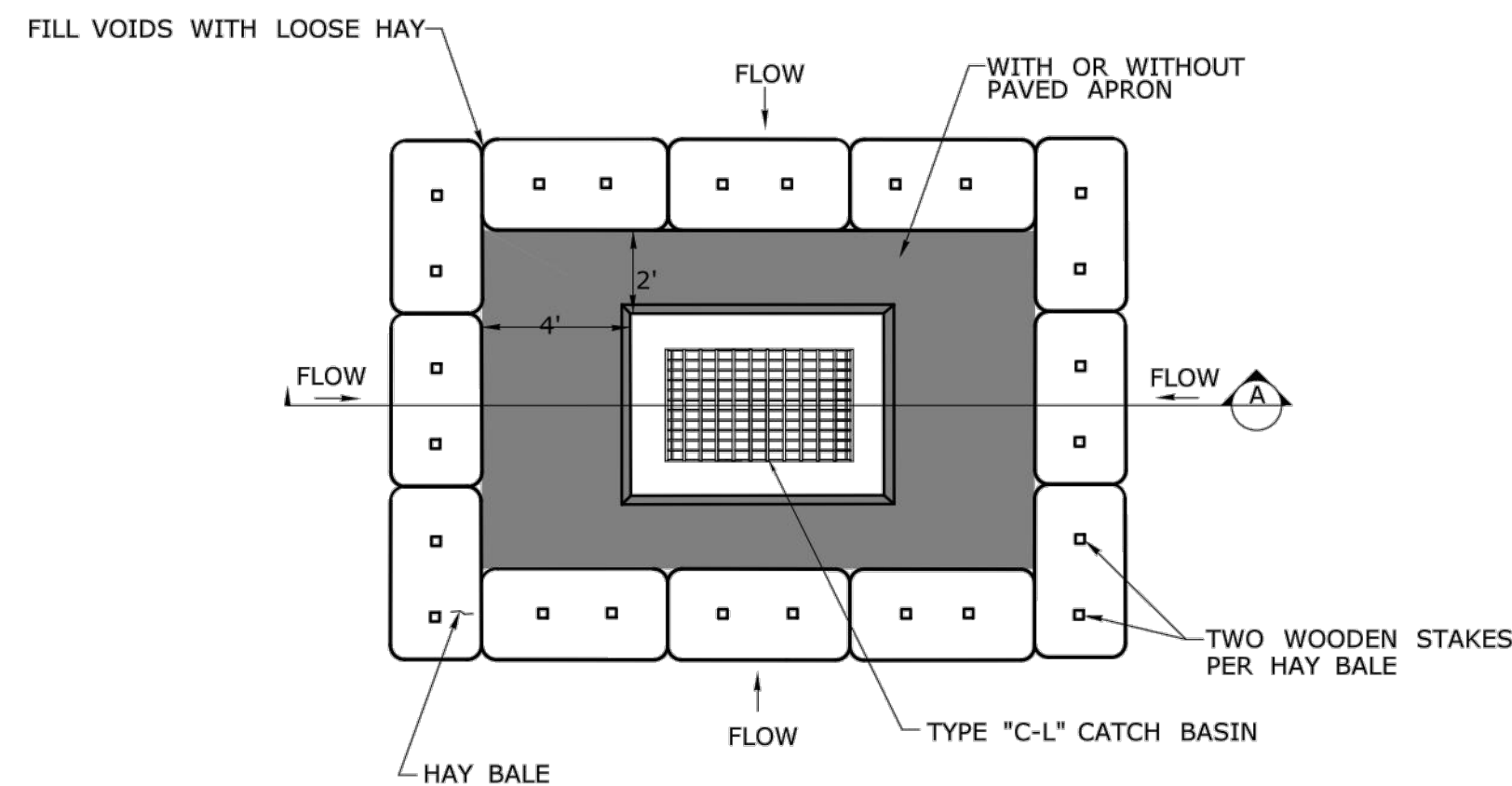
GS-01

PROGRESS PRINT - 60% DESIGN

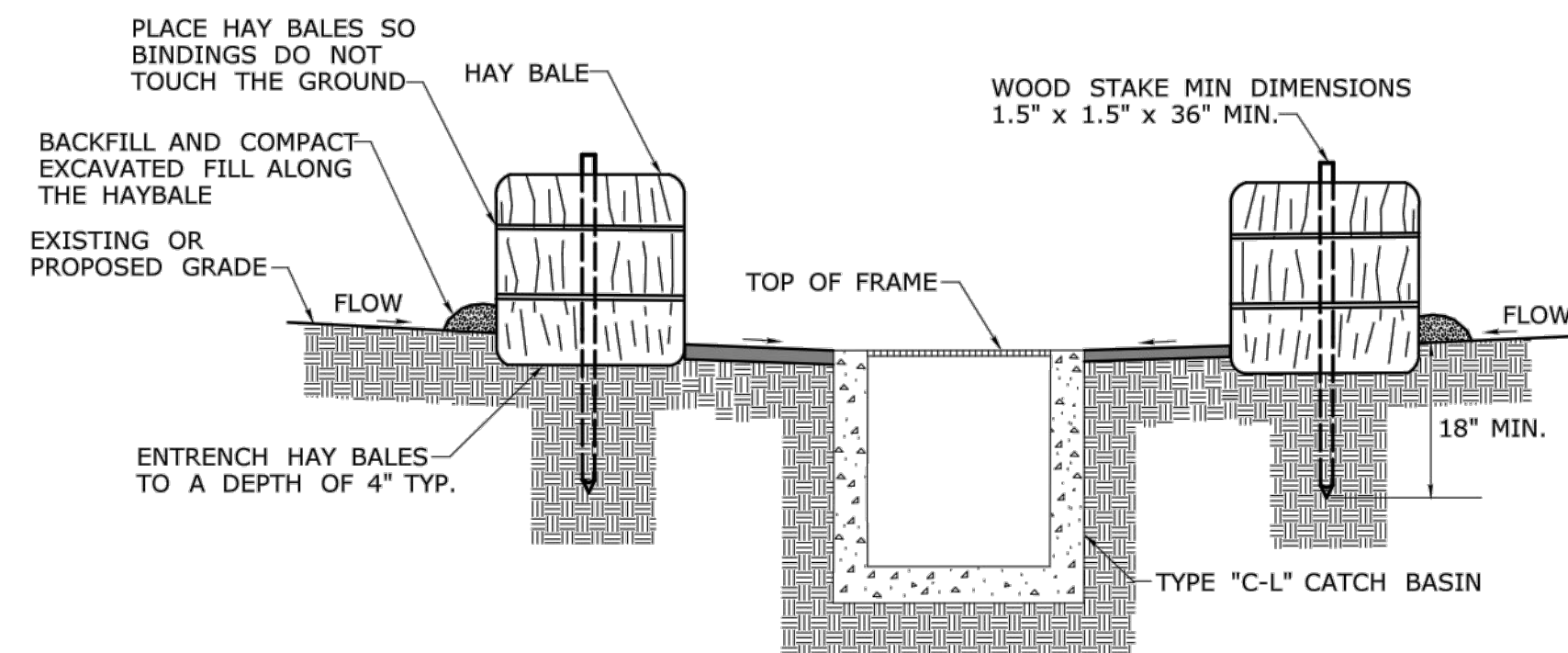


HAY BALES AT TYPE "C-L" CATCH BASIN

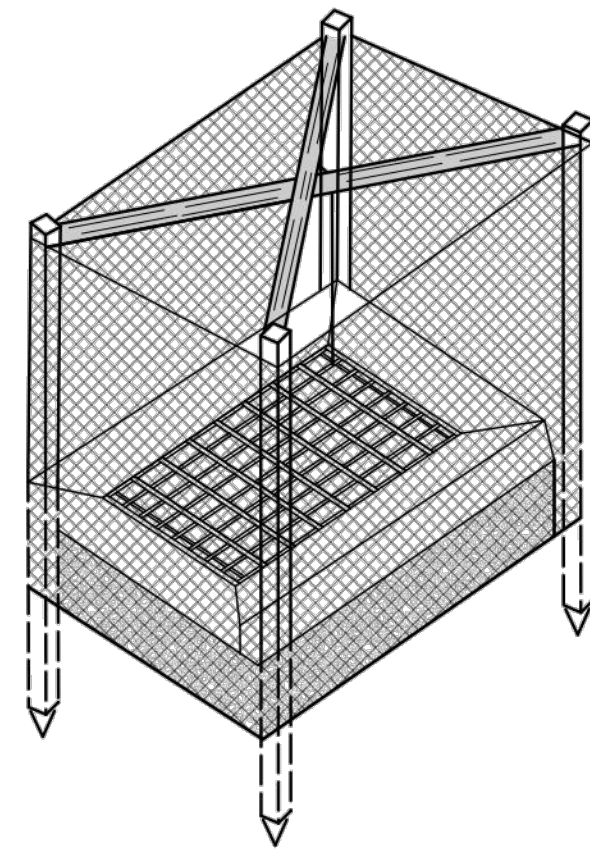
NOTE: THIS ITEM WILL BE MEASURED BY LINEAR FEET OF SEDIMENTATION CONTROL SYSTEM



PLAN THIS DETAIL APPLIES TO OFF ROAD LOCATIONS

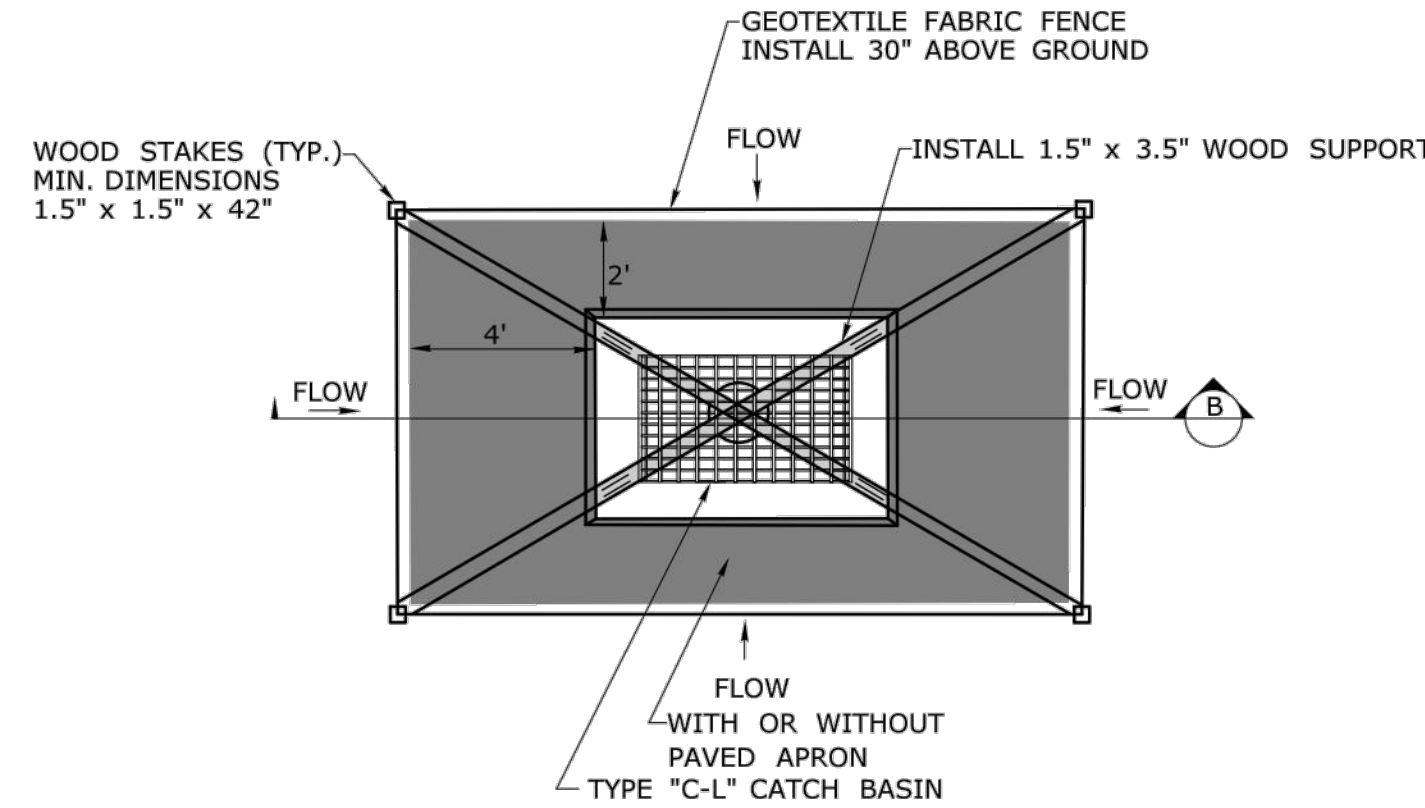


SECTION A

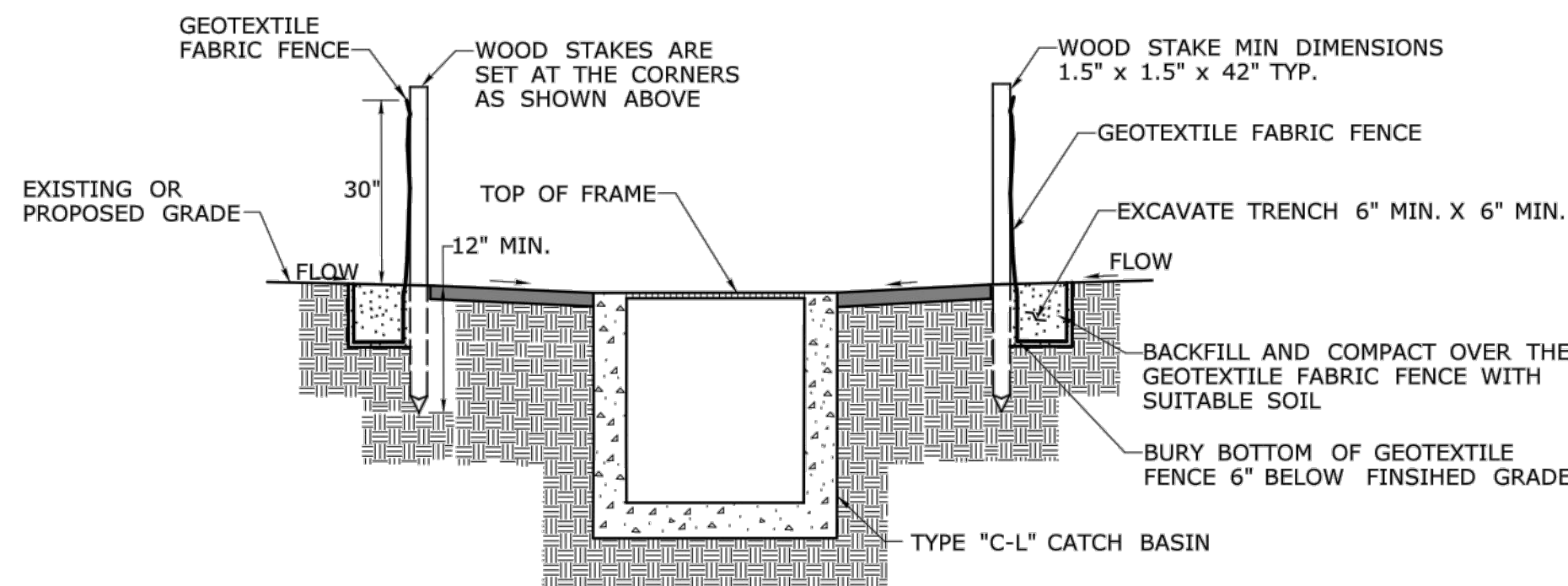


GEOTEXTILE FABRIC FENCE AT TYPE "C-L" CATCH BASIN

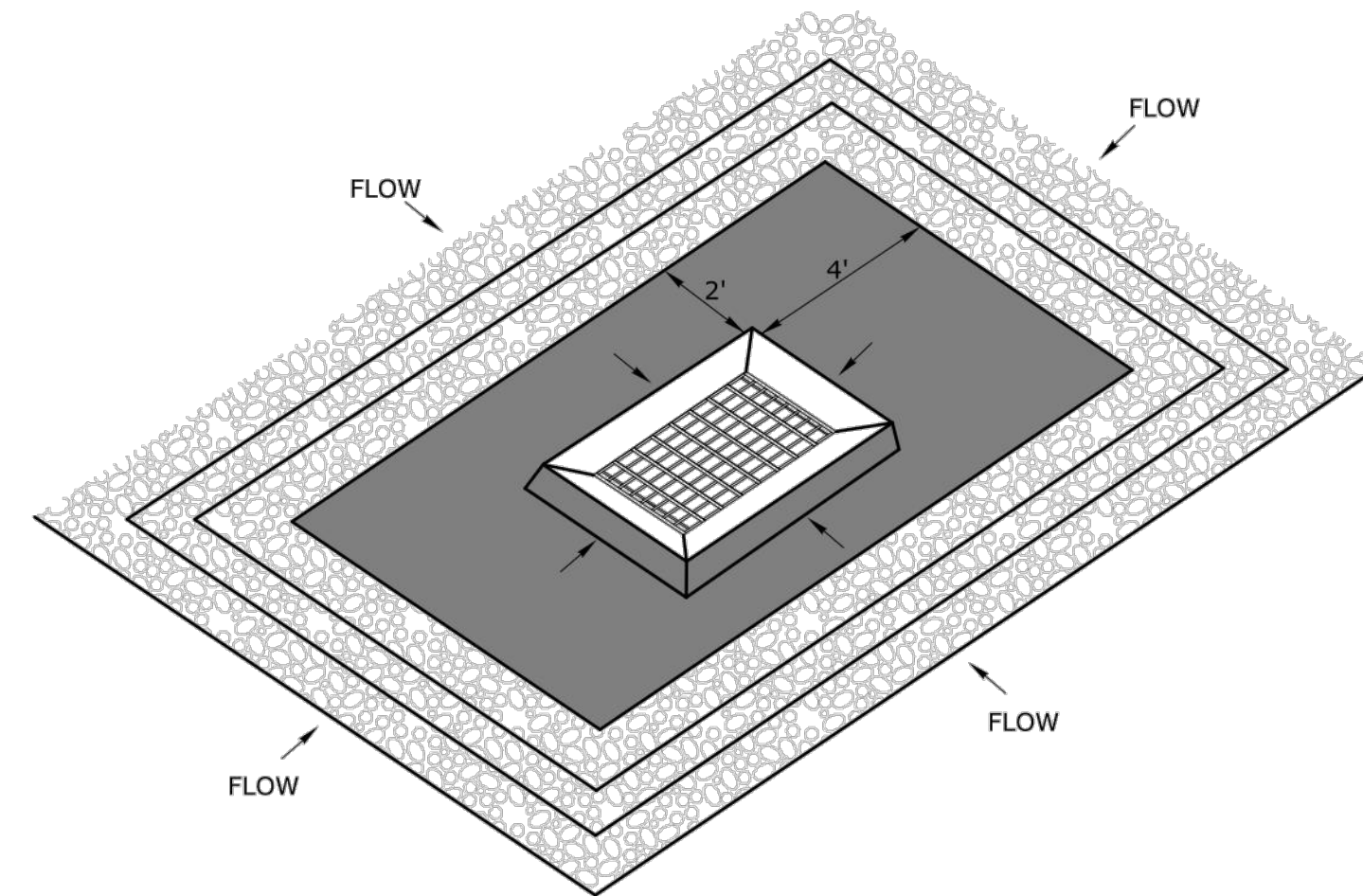
NOTE: THIS ITEM WILL BE MEASURED BY LINEAR FEET OF SEDIMENTATION CONTROL SYSTEM



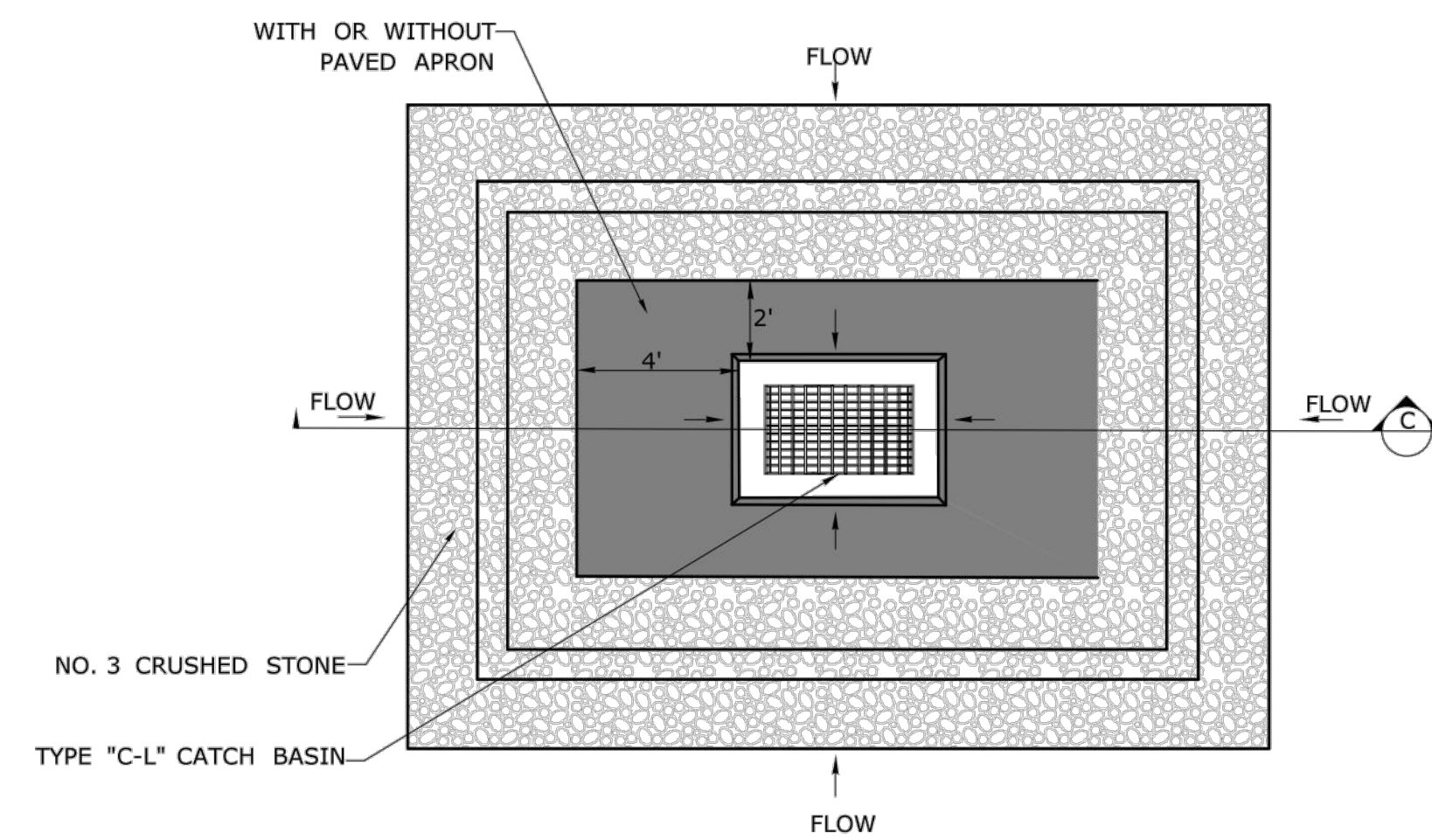
PLAN THIS DETAIL APPLIES TO OFF ROAD LOCATIONS



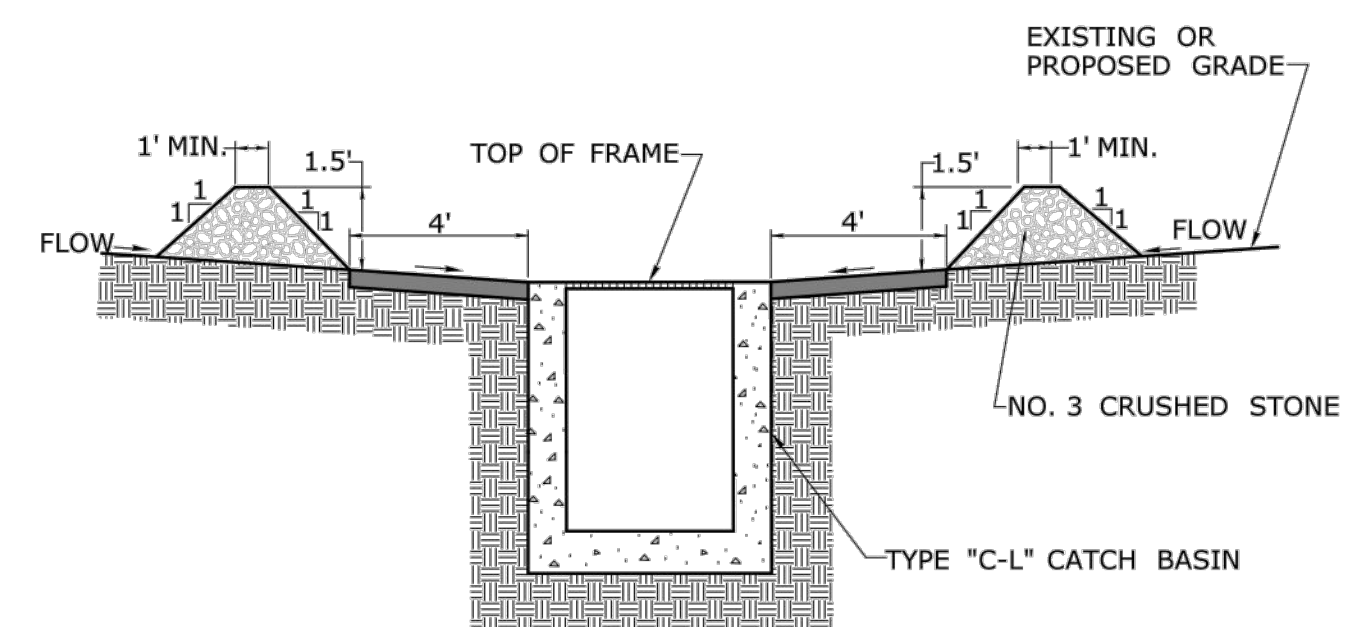
SECTION B



STONE CHECK DAM AT TYPE "C-L" CATCH BASIN



PLAN THIS DETAIL APPLIES TO OFF ROAD LOCATIONS



SECTION C

REV.	DATE	REVISION DESCRIPTION

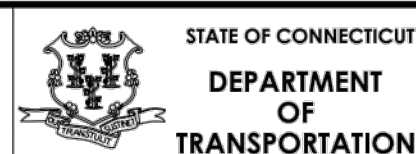
DESIGNER/DRAFTER: CHECKED BY:

LASTED SAVED BY: RichardBH FILE NAME: W:\CT_CONNECT_DDE\CT_Configuration\Organization\Cat\CTDOT_Borders_Contract.cel
PLOTTED DATE: 11/23/2022

SIGNATURE/
BLOCK:

OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

APPROVED BY:



PROJECT NUMBER: 0000-0000
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION
TOWNS: SAMPLE TOWN(S)
DRAWING TITLE: EROSION CONTROL MEASURES AT CATCH BASINS - HAY BALE, FABRIC FENCE AND STONE CHECK DAM

DRAWING NO.

SHEET NO.

MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

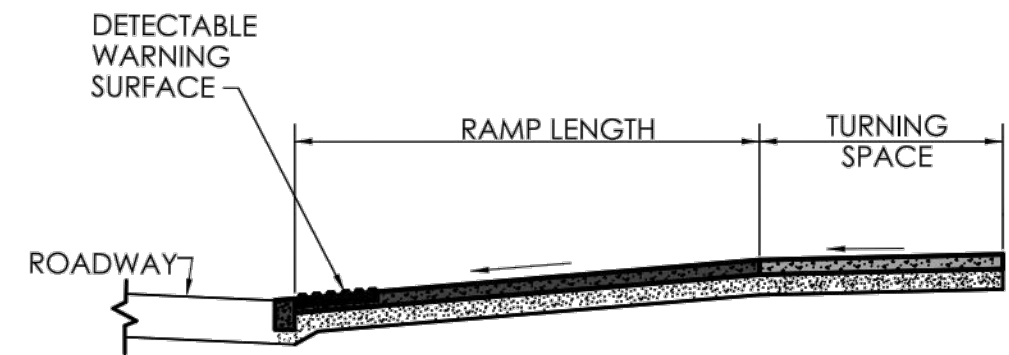
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Glastonbury, Connecticut
06033
860 652 8227

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FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO:
JOB. NO: 83873.00

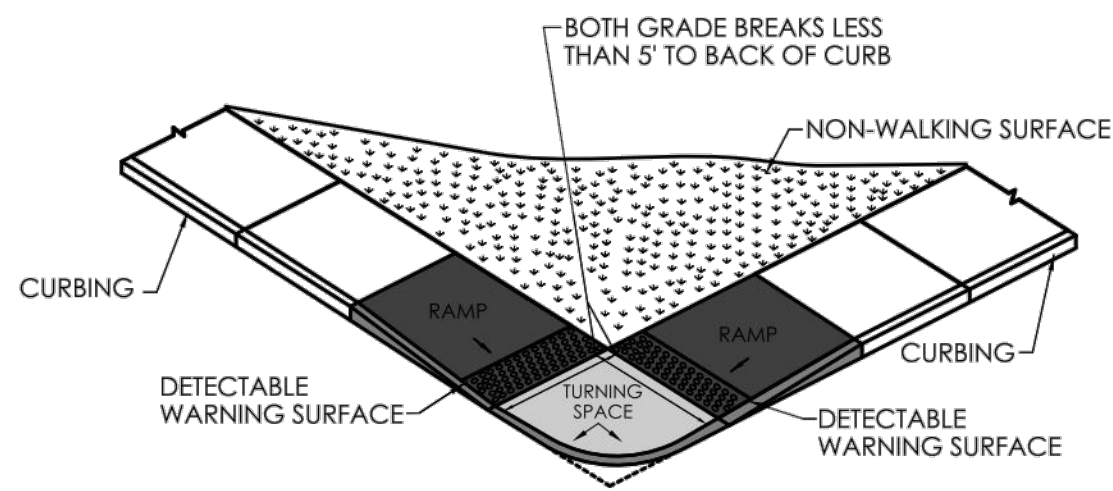
GS-02

PROGRESS PRINT - 60% DESIGN



BASIC RAMP ATTRIBUTES

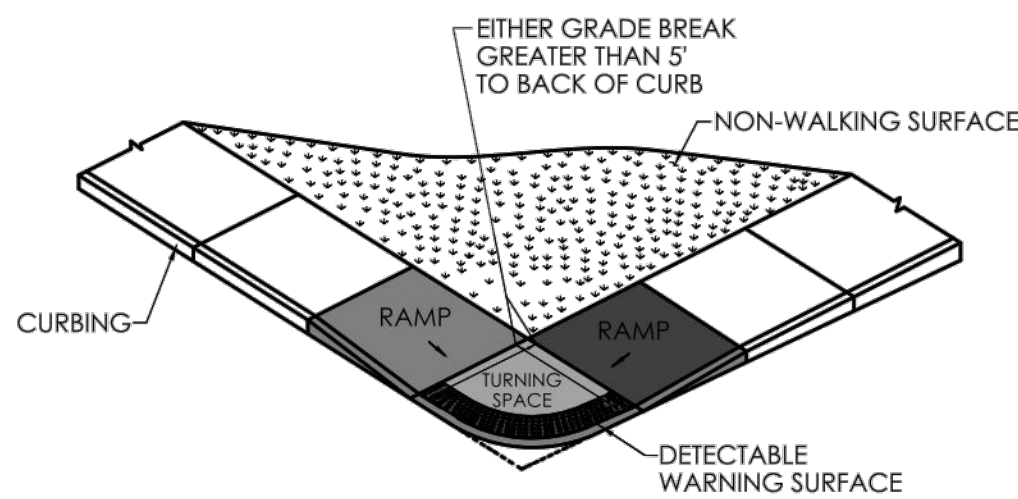
- PLAN VIEW OF RAMP COMPONENTS
- SECTION VIEW OF TYPICAL RAMP
- WHEELCHAIR CROSS-SLOPE CRITERIA
- RAMP WARPING DETAIL
- GUTTER COUNTER SLOPE DETAILS
- GRADE BREAK DETAILS



TWO DIRECTIONAL PERPENDICULAR RAMP(S)

WITH GRADE BREAK TO BACK OF CURB OF 5' OR LESS

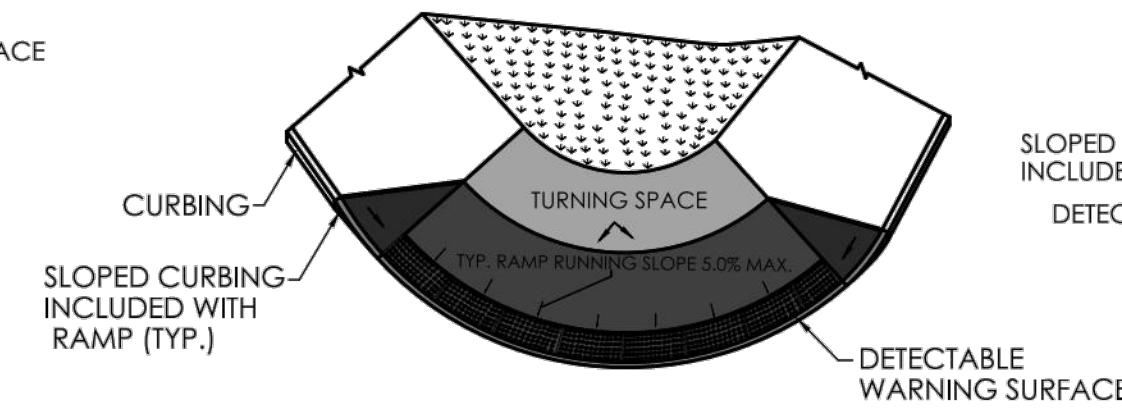
- TYPE 1 SIDEWALK ABUTS ROADWAY
- TYPE 3 SIDEWALK SEPARATED FROM ROADWAY WITH NONWALK AREA



TWO DIRECTIONAL PERPENDICULAR RAMP(S)

WITH GRADE BREAK TO BACK OF CURB GREATER THAN 5'

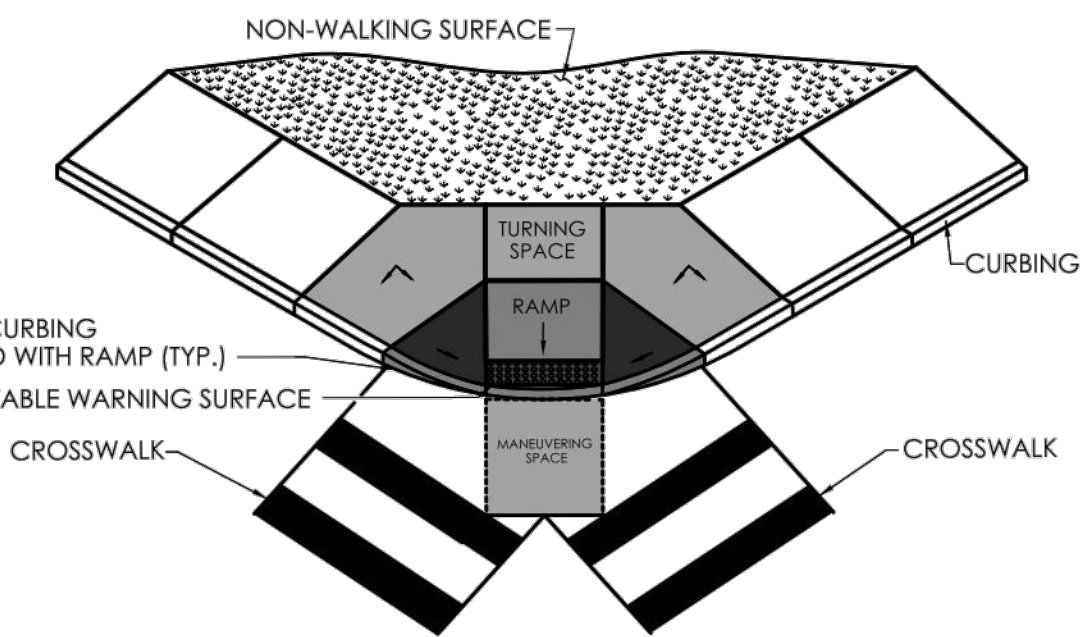
- TYPE 2 SIDEWALK ABUTS ROADWAY
- TYPE 4 SIDEWALK SEPARATED FROM ROADWAY WITH NONWALK AREA



BLENDED RAMP WITH

TURNING SPACE AT THE TOP

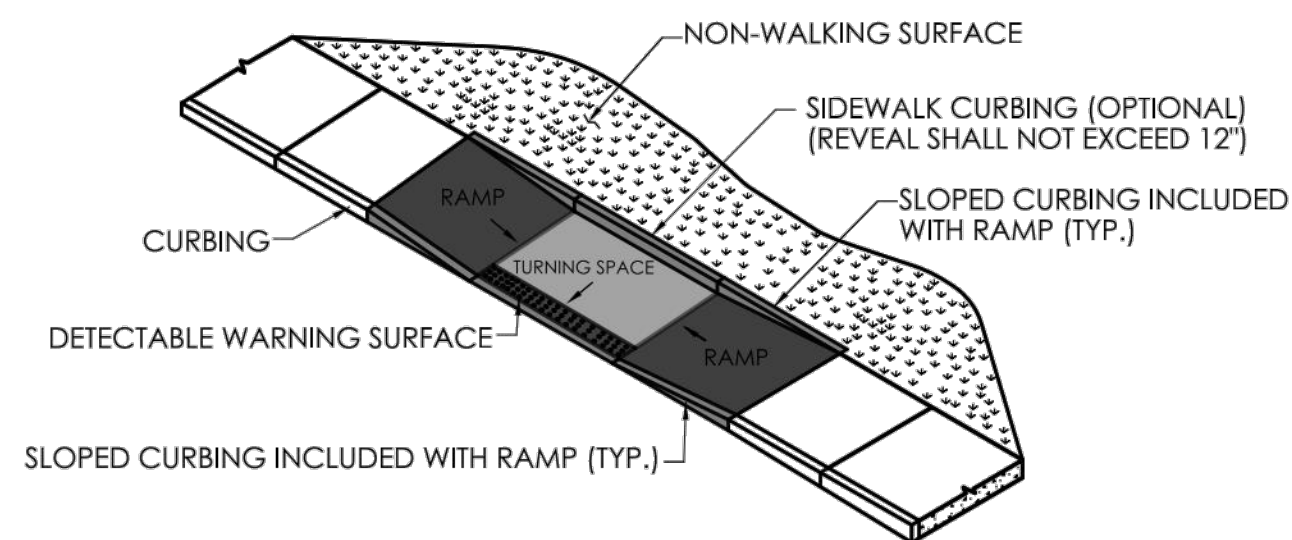
TYPE 5 SIDEWALK ABUTS ROADWAY



PERPENDICULAR RAMP(S) WITH

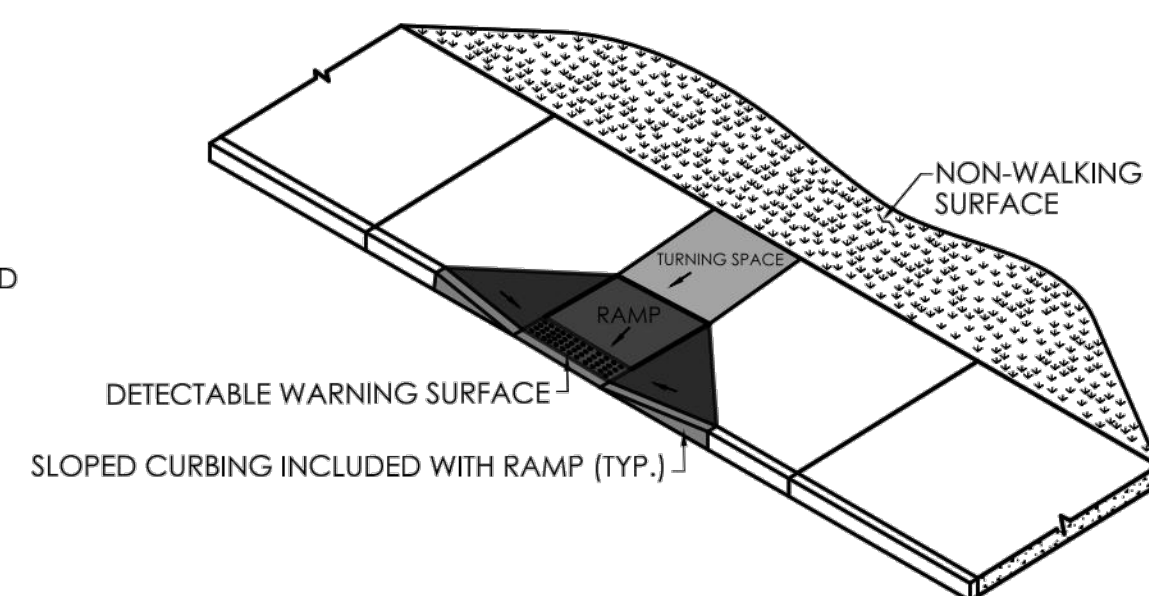
STREET MANEUVERING SPACE

- TYPE 6 TURNING SPACE OBSTRUCTION PRESENT
- TYPE 7 NO TURNING SPACE OBSTRUCTION



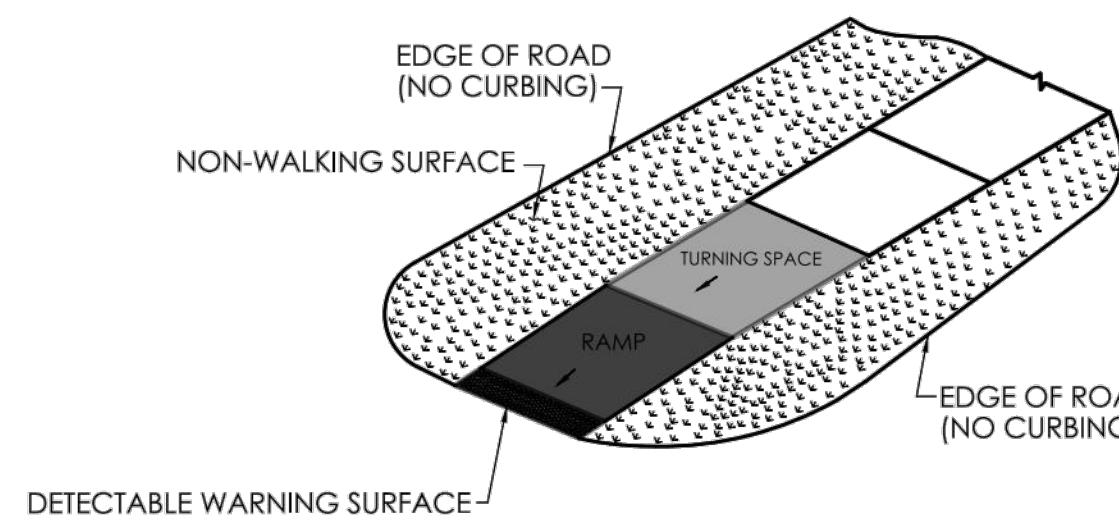
PARALLEL RAMP(S)

- TYPE 9 TWO RAMP(S) APPROACH TO TURNING SPACE
- TYPE 10a SINGLE RAMP WITH TURNING SPACE CONSTRAINED ON ONE SIDE
- TYPE 10b SINGLE RAMP CONSTRAINED ON TWO (2) OR MORE SIDES



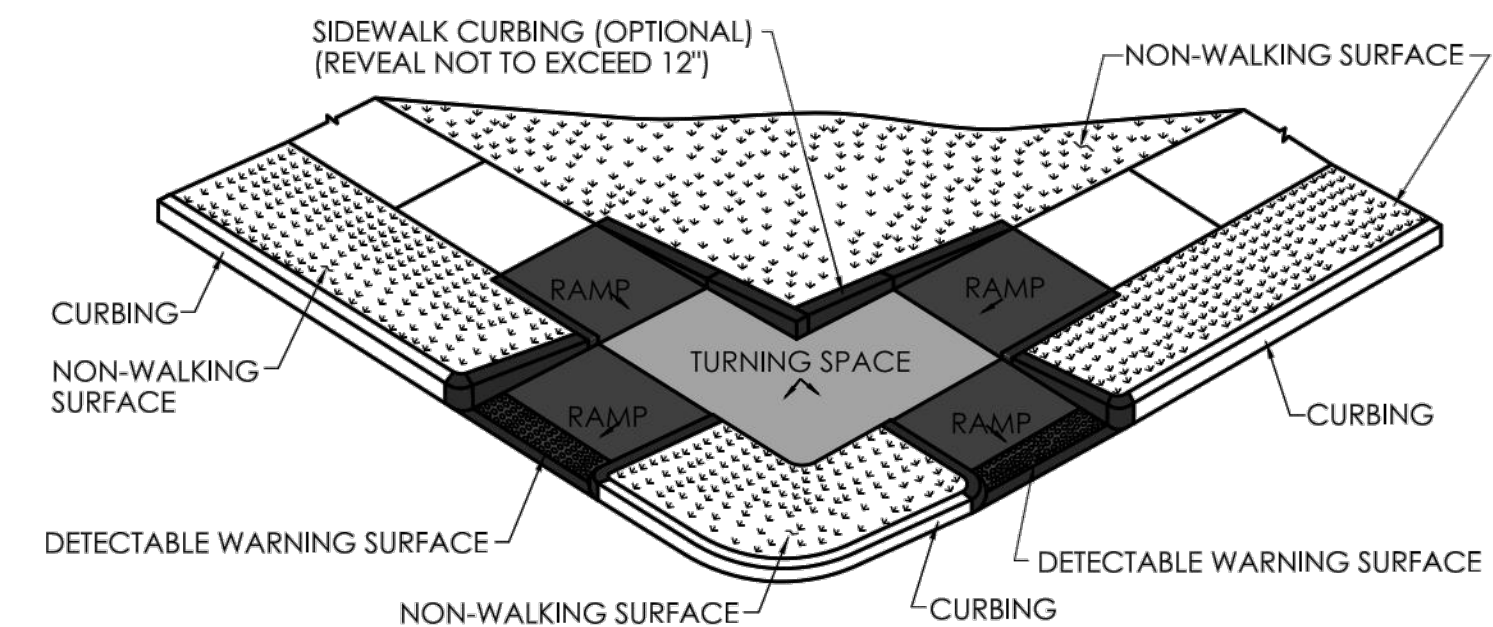
PERPENDICULAR RAMP(S)

- TYPE 8 SINGLE RAMP TO TURNING SPACE CONSTRAINED ON ONE SIDE
- TYPE 11 60" X 60" TURNING SPACE WITH NON-WALKABLE SURFACE
- TYPE 12 60" X 60" TURNING SPACE WITH NON-WALKABLE SURFACE
- TYPE 13 TURNING SPACE WITH NON-WALKABLE SURFACE



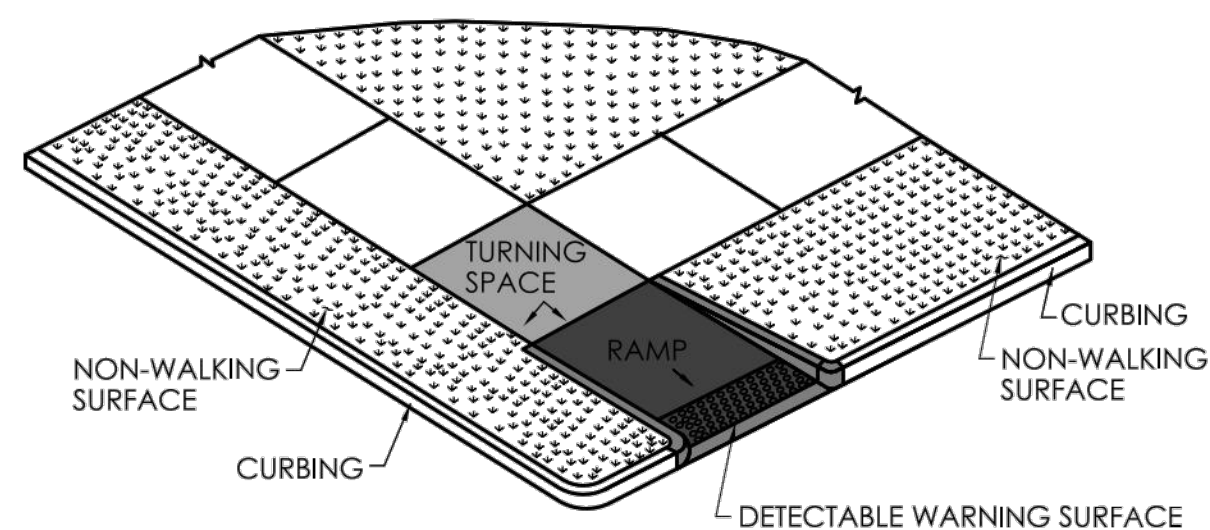
SINGLE DIRECTION PERPENDICULAR RAMP(S)

- TYPE 14 TURNING SPACE GRADE BREAK GREATER THAN 5'
- TYPE 15 TURNING SPACE GRADE BREAK 5 FT OR LESS
- TYPE 16 RAMP WITH RETURN CURBING
- TYPE 17 RAMP WITH NO RETURN CURBING



PERPENDICULAR RAMP(S)

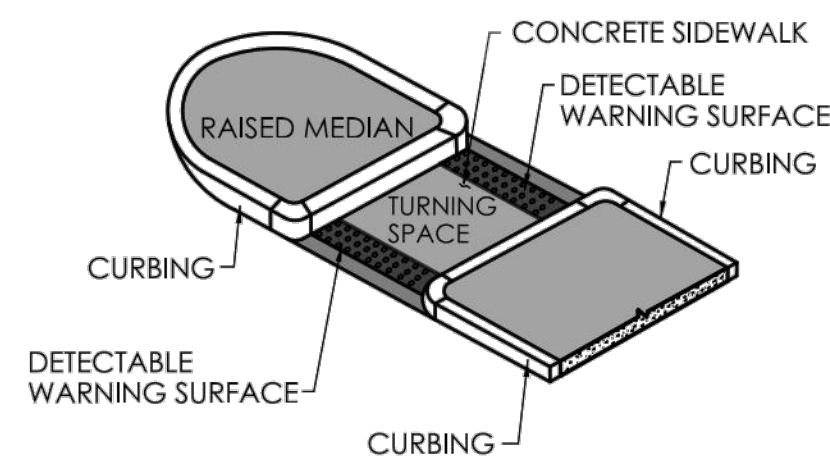
- TYPE 18 EXAMPLE OF RAMP FLARE/CURB APPLICATIONS
- TYPE 19 COMBINATION SIDEWALK RAMPS



RESTRICTED PEDESTRIAN CROSSING

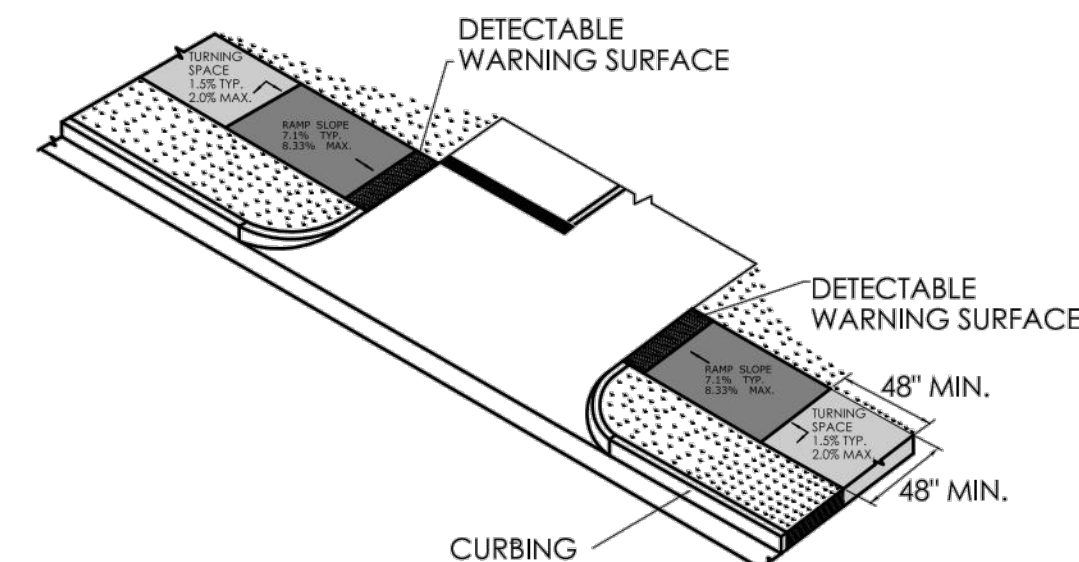
SIDEWALK RAMP(S)

- TYPE 20 SINGLE RAMP FROM TURNING SPACE
- TYPE 21 TWO RAMP(S) TO TURNING SPACE



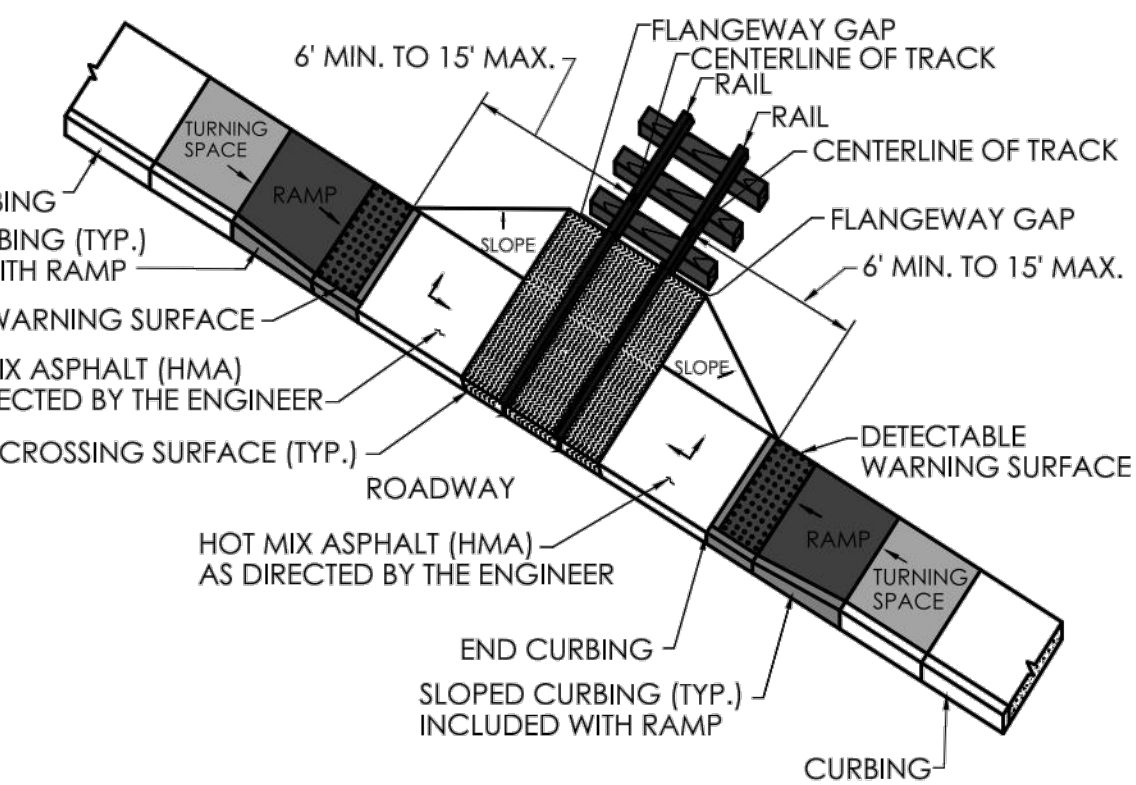
PEDESTRIAN REFUGE ISLAND(S)

- TYPE 22 ISLAND WIDTH 6 FT OR MORE
- TYPE 23 ISLAND LESS THAN 6 FT WIDE
- TYPE 24 REFUGE ISLAND WITH ELEVATED LANDING
- TYPE 25 RIGHT TURN SLIP-LANE REFUGE ISLAND
- TYPE 26 REFUGE ISLAND WITH OFFSET ACCESS



DRIVEWAY(S)

- TYPE 29 DRIVEWAY SIGNALIZED, STOP CONTROLLED OR YIELD WITH DETECTABLE WARNING SURFACE AND NON WALKING SURFACE
- TYPE 30 UNSIGNALIZED, NO STOP CONTROL OR YIELD DRIVEWAY CROSSING WITH NON WALKING SURFACE
- TYPE 31 DRIVEWAY CROSSING SIGNALIZED, STOP CONTROL OR YIELD WITH DETECTABLE WARNING SURFACE
- TYPE 32 UNSIGNALIZED, NO STOP CONTROL OR YIELD DRIVEWAY CROSSING



RAILROAD CROSSING RAMPS

- TYPE 27 RAILROAD CROSSING WITHOUT GATE
- TYPE 28 RAILROAD CROSSING WITH GATE

REV.	DATE	REVISION DESCRIPTION

DESIGNER/DRAFTER: CHECKED BY:

LASTED SAVED BY: RichardEH FILE NAME: W:\CT_CONNECT_DOE\CT_Configuration\Organization\Cell\CTDOT_Borders_Contract.cel PLOTTED DATE: 1/12/2024

SIGNATURE/ BLOCK:

OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

APPROVED BY:



STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION



PROJECT NUMBER: 0000-0000
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION
TOWNS: SAMPLE TOWN(S)
DRAWING TITLE: CONCRETE SIDEWALK RAMP(S) *** INDEX SHEET ***

DRAWING NO.

SHEET NO.

MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP

BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033

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FILE: 8387300\CIVIL\DRAWINGS\

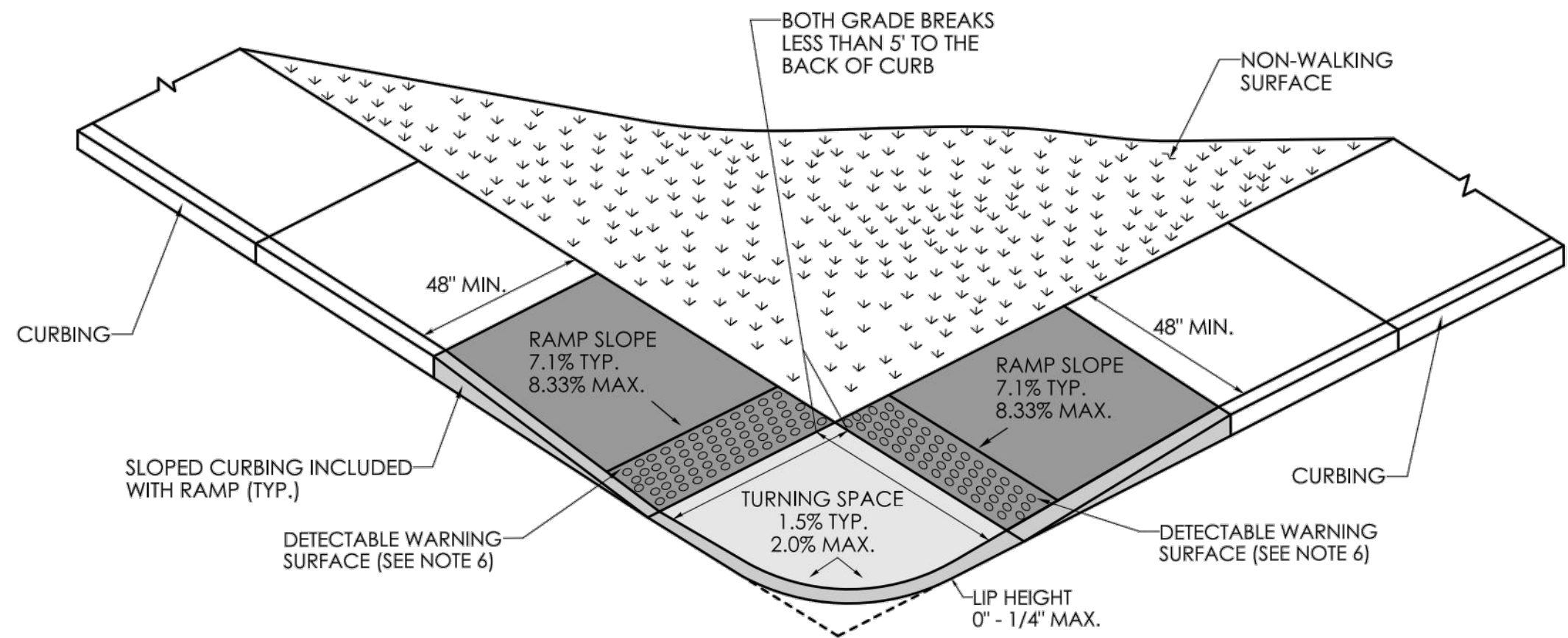
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JOB. NO: 83873.00

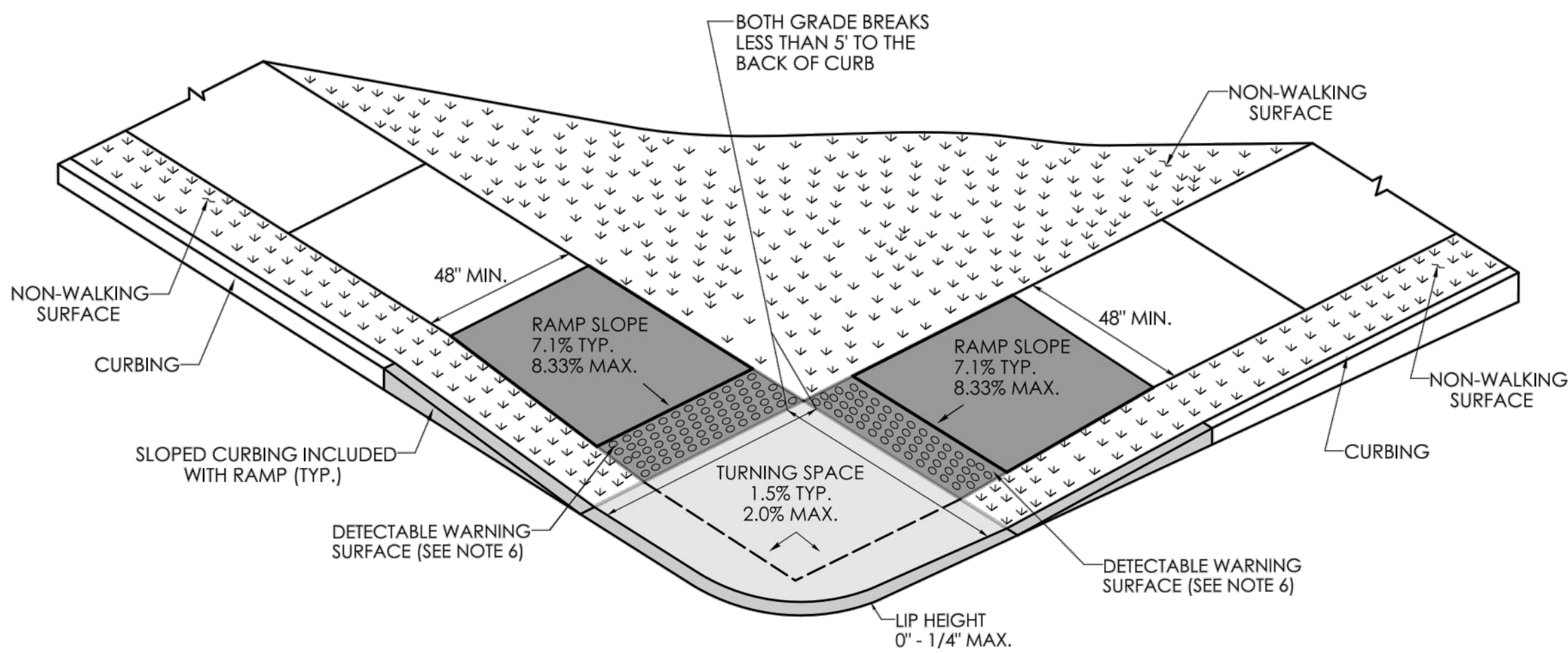
GS-03

PROGRESS PRINT - 60% DESIGN

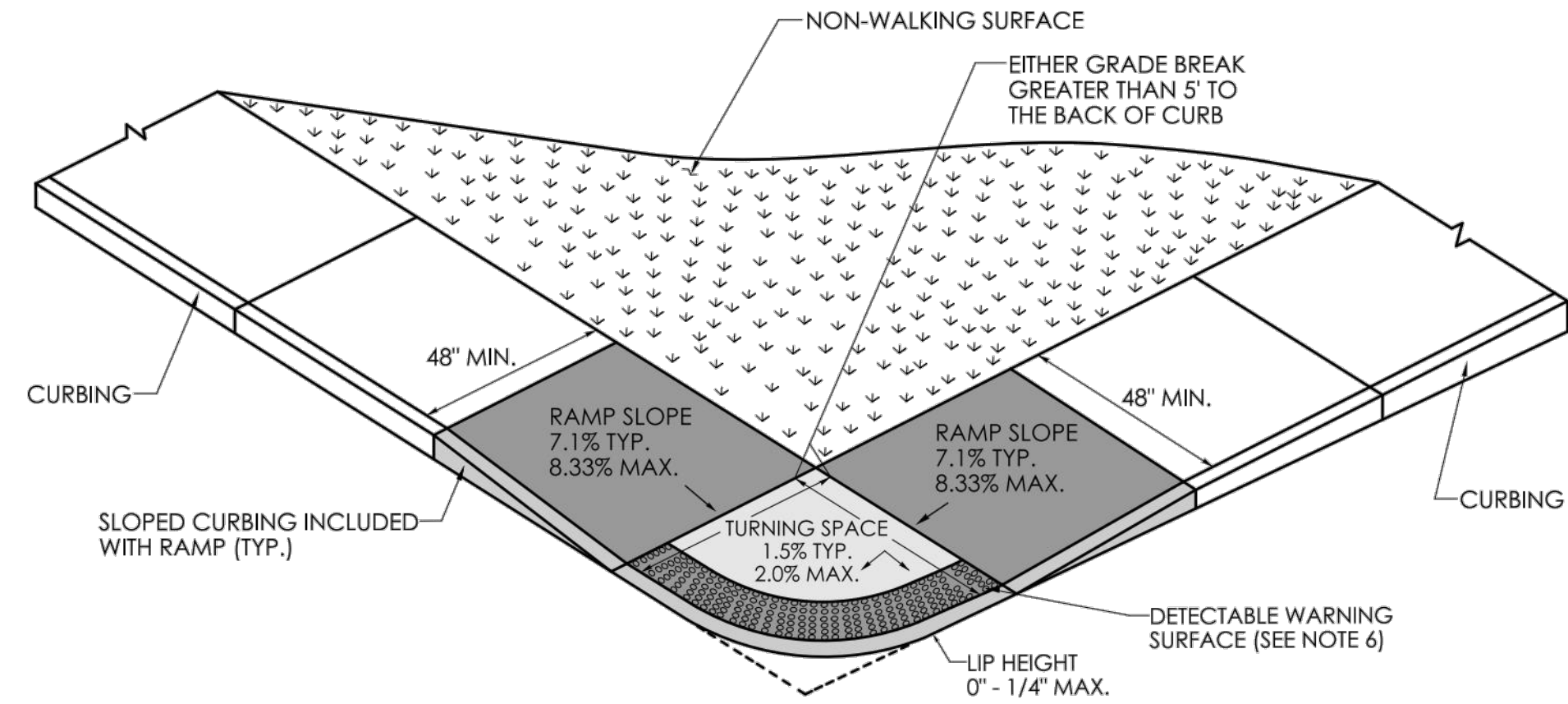
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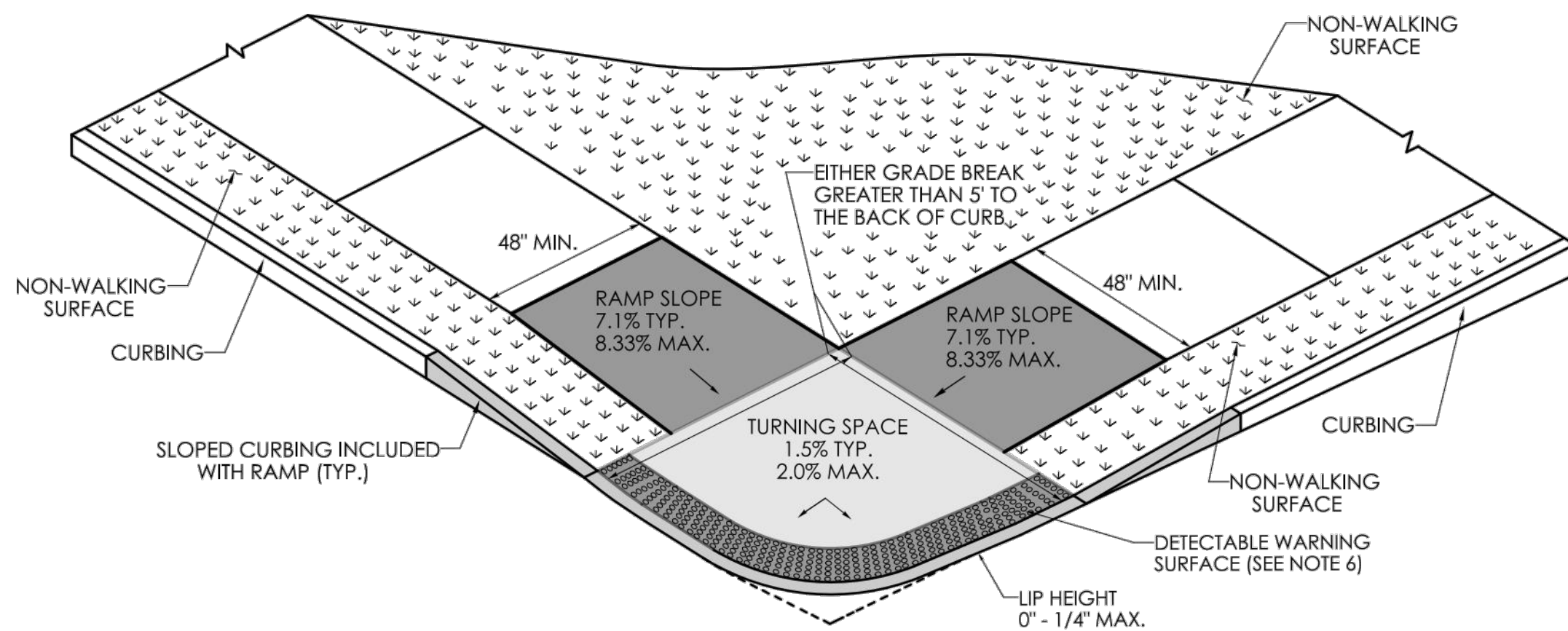
**TWO DIRECTIONAL PERPENDICULAR RAMP
WITH A GRADE BREAK TO BACK OF CURB OF 5' OR LESS
(TYPE 1)**



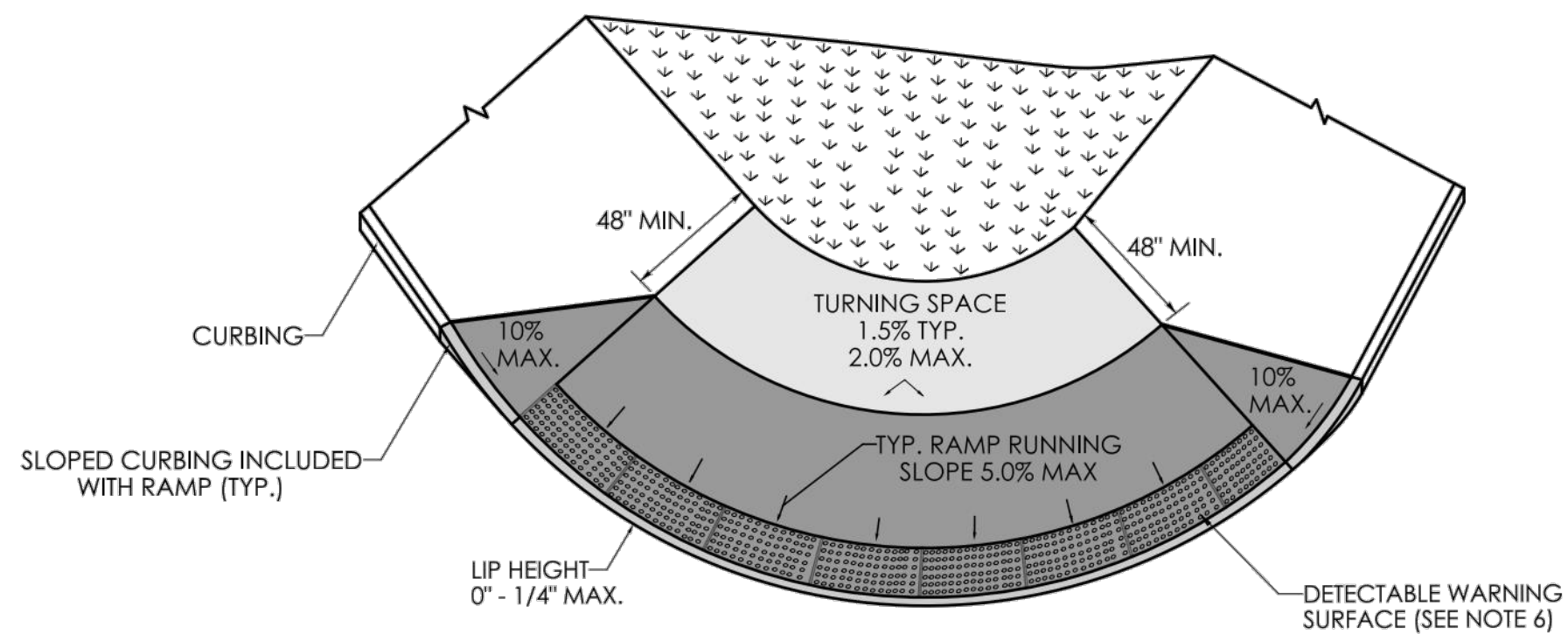
**TWO DIRECTIONAL PERPENDICULAR RAMP
WITH A GRADE BREAK
TO BACK OF CURB OF 5' OR LESS
WITH NON-WALKING SURFACE
(TYPE 3)**



**TWO DIRECTIONAL PERPENDICULAR RAMP
WITH GRADE BREAK TO BACK OF CURB GREATER THAN 5'
(TYPE 2)**



**TWO DIRECTIONAL PERPENDICULAR RAMP
WITH A GRADE BREAK
TO BACK OF CURB GREATER THAN 5'
WITH NON-WALKING SURFACE
(TYPE 4)**



**BLENDED TRANSITION
WITH TURNING SPACE AT TOP
(TYPE 5)**

- GENERAL NOTES:**
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
 2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
 3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
 4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
 5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
 6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
 7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
 8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
 9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

**MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2**

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG

1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP

BUILD | SUPPORT | CONNECT

665 Winding Brook Drive
Glastonbury, Connecticut
06033

860 652 8227

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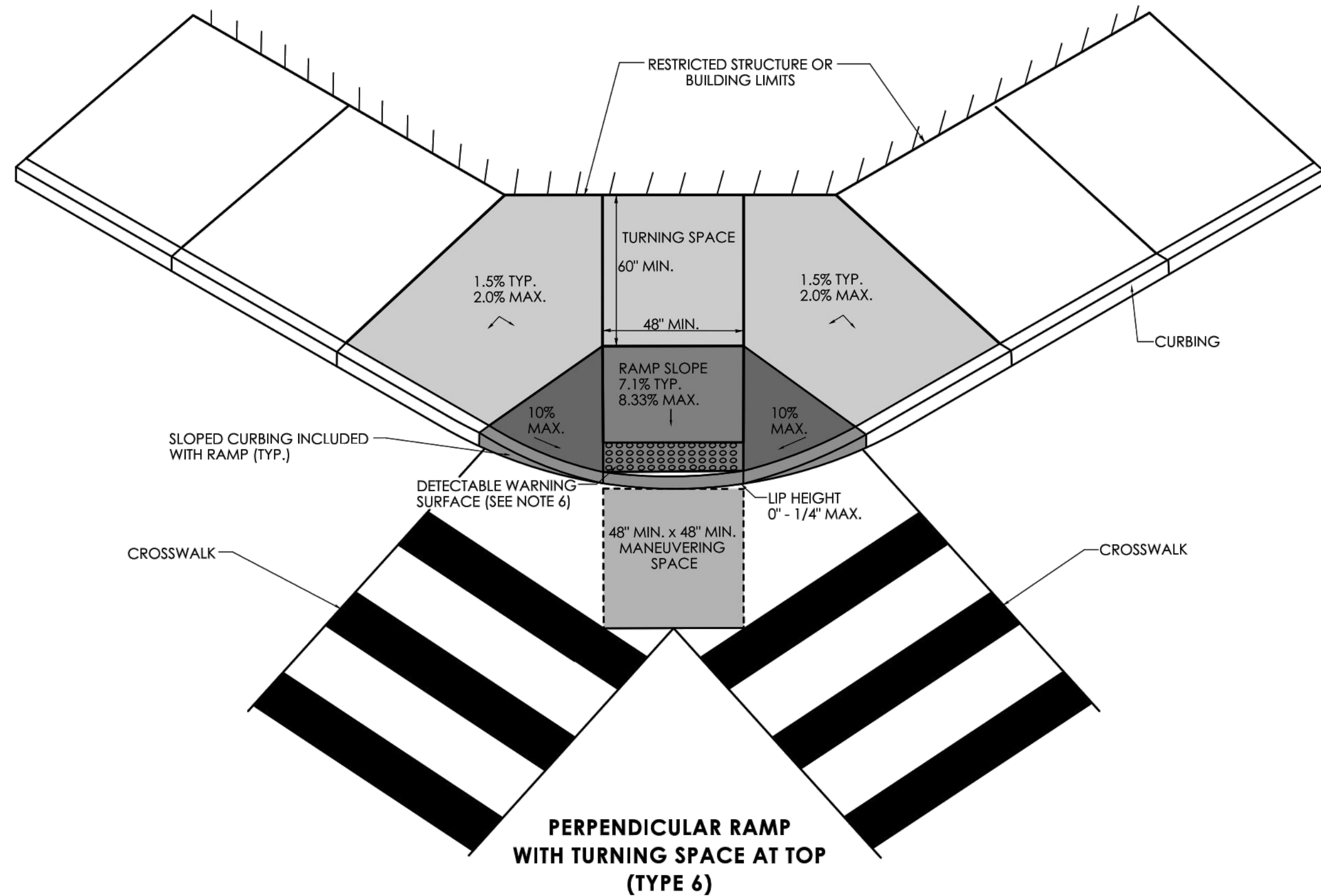
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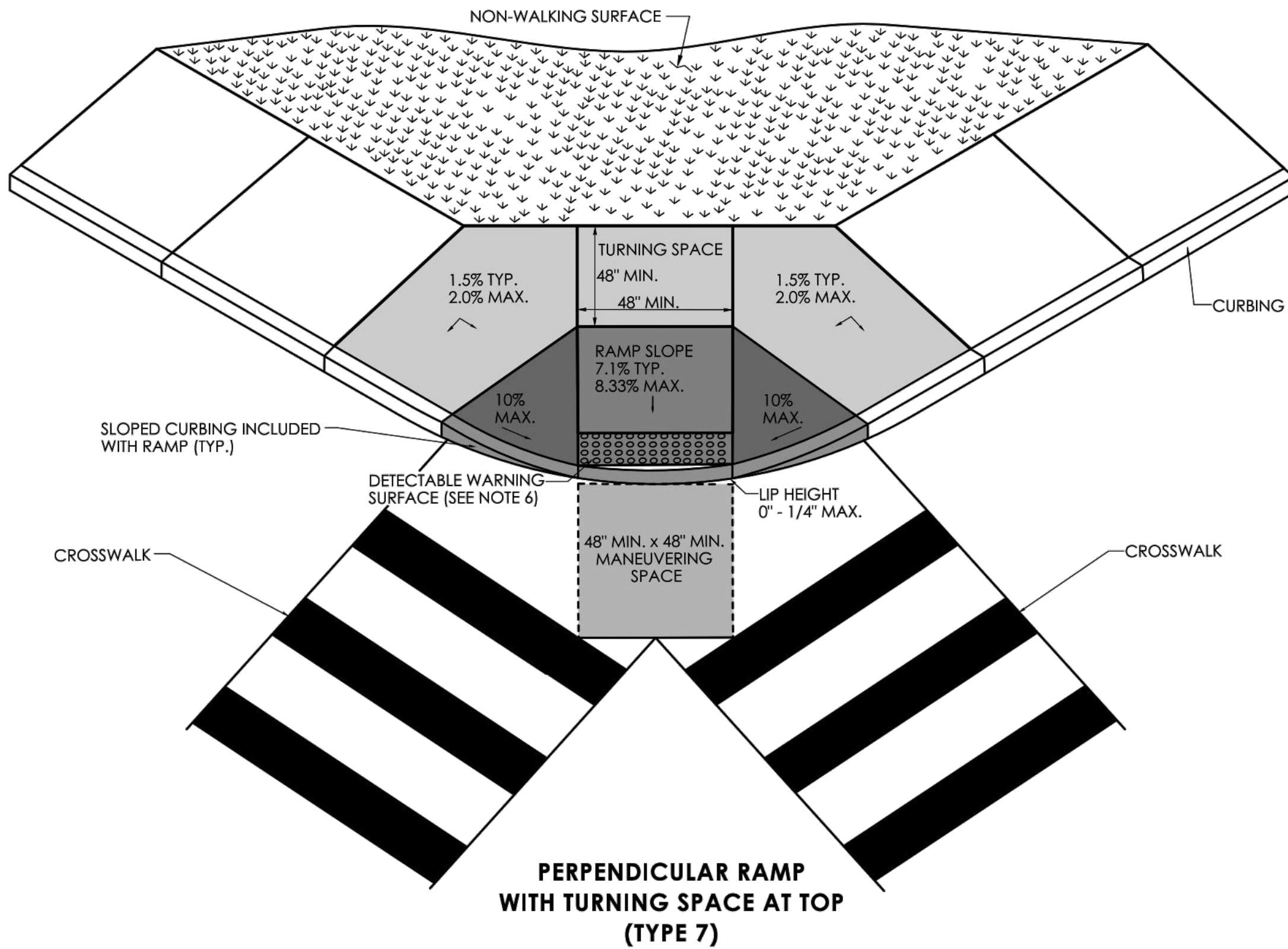
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- GENERAL NOTES:**
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
 2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED $\frac{1}{4}$ INCH.
 3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
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 7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
 8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
 9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



REV.	DATE	REVISION DESCRIPTION

DESIGNER/DRAFTER: _____	CHECKED BY: _____	SIGNATURE/BLOCK: _____	APPROVED BY: _____	PROJECT NUMBER: 0000-0000	DRAWING NO. _____
OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111		STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION		PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION	SHEET NO. _____
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PLOTTED DATE: 1/12/2024				DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 3 OF 11	

MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN

STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:	

PREPARED FOR:

METROCOG

1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP

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Glastonbury, Connecticut
06033

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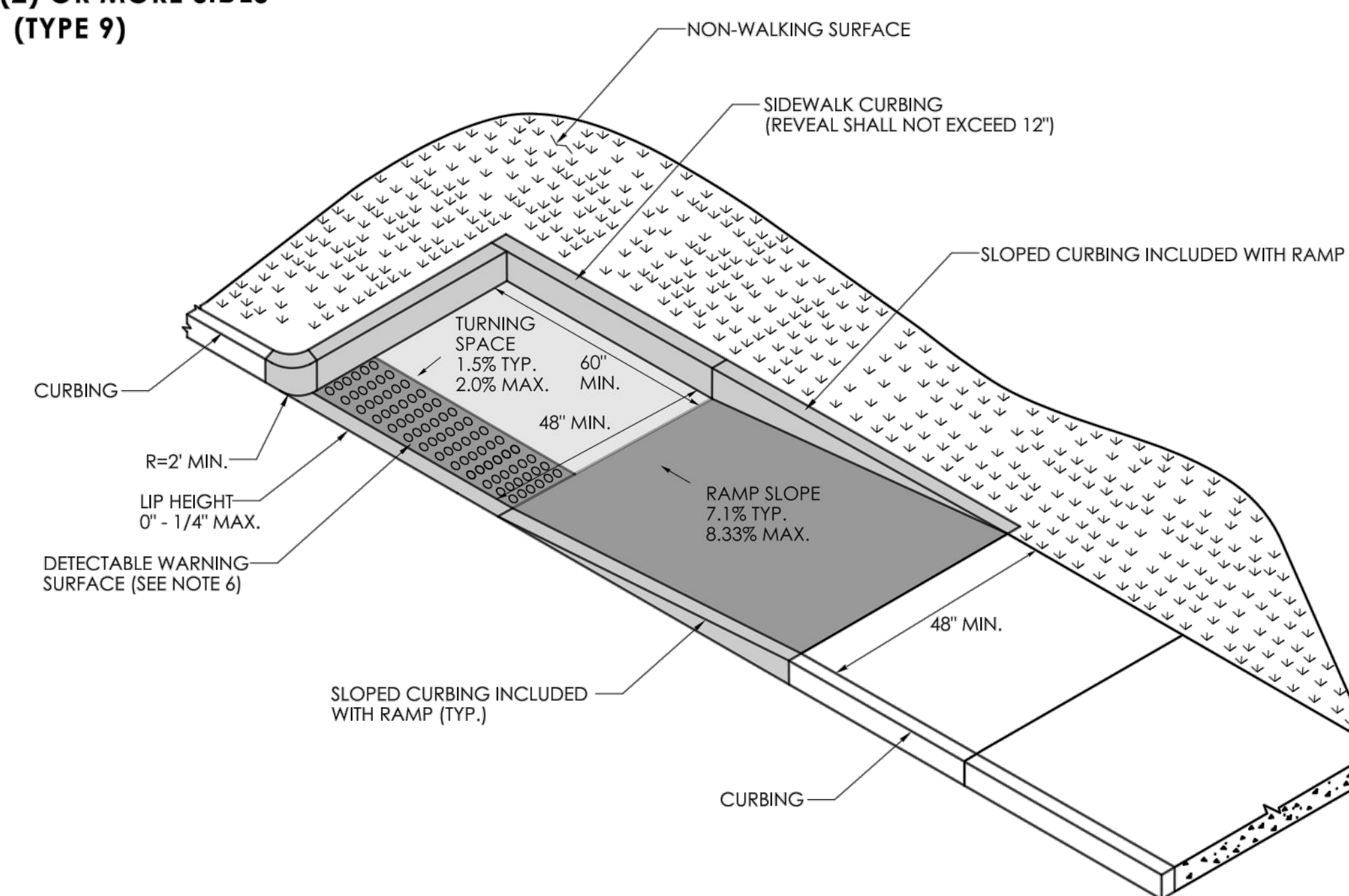
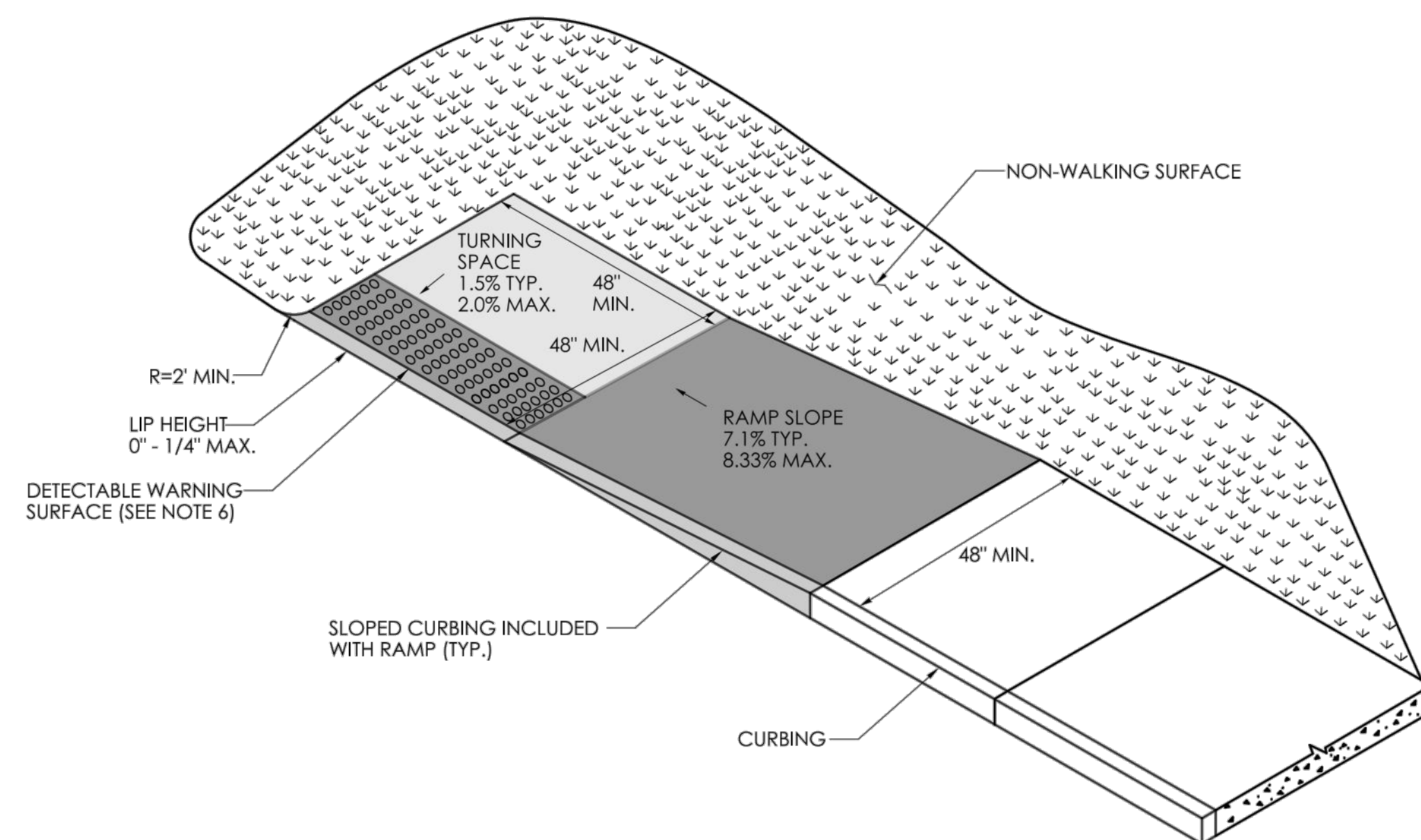
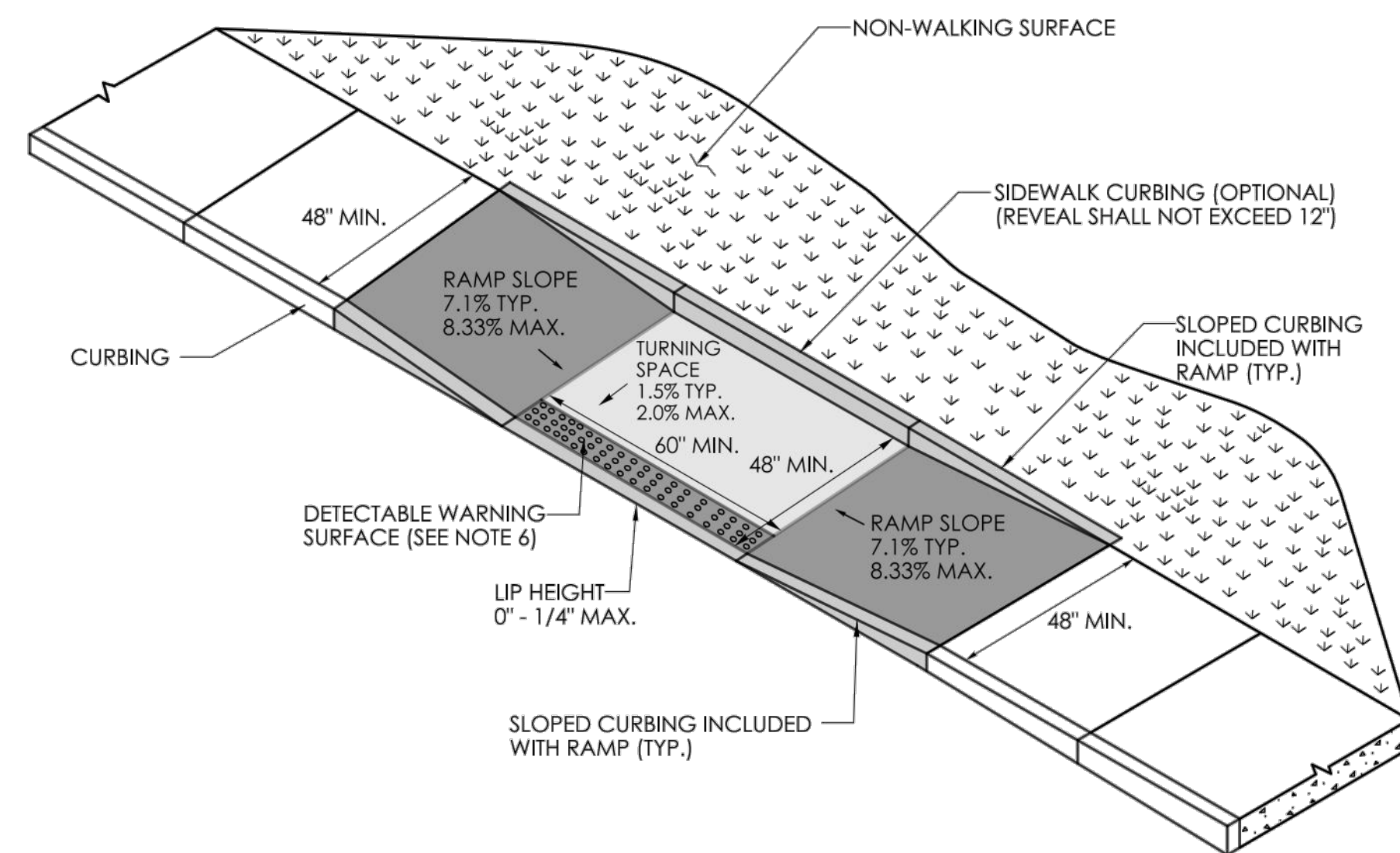
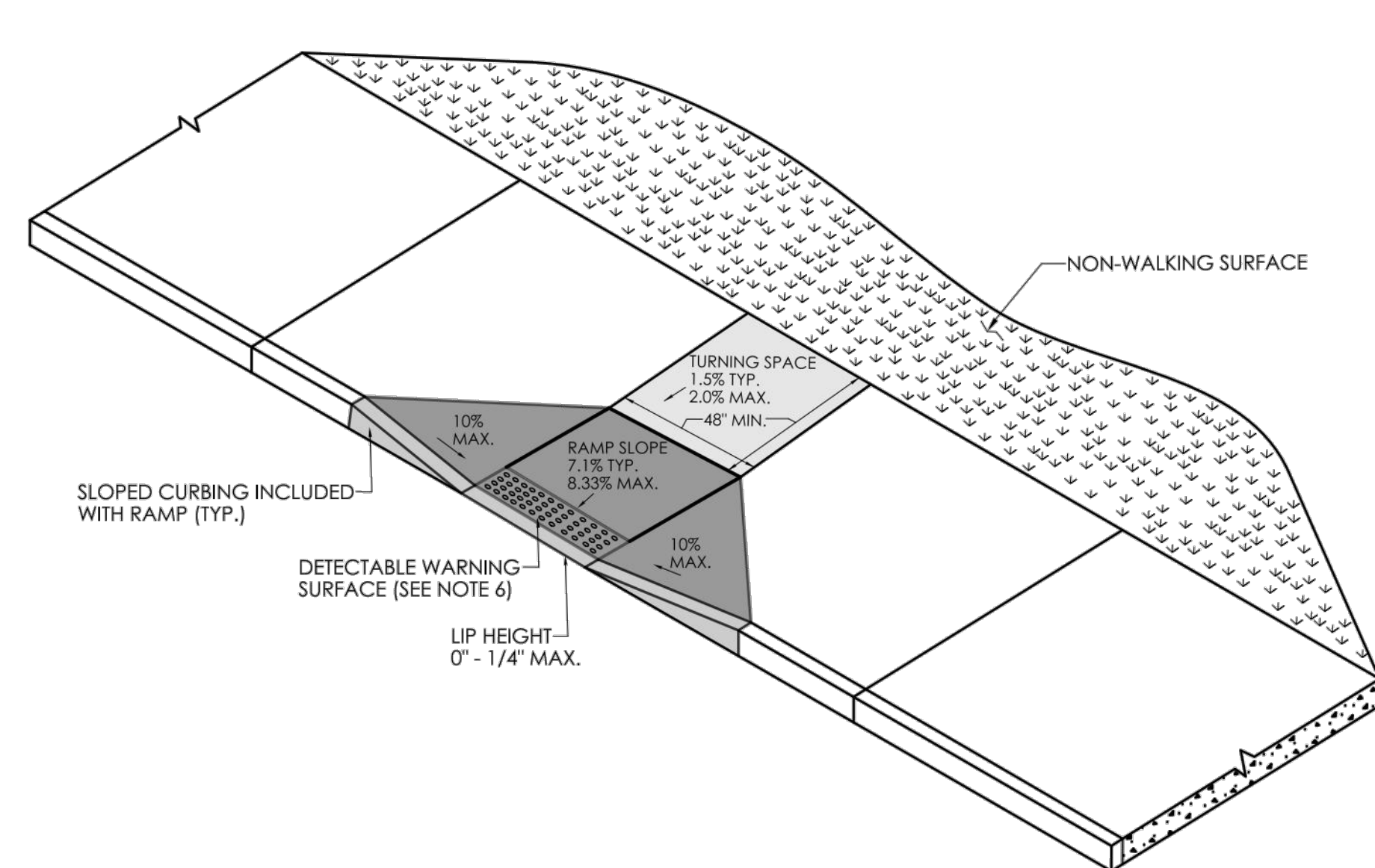
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- GENERAL NOTES:**

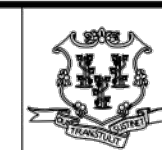
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED ¼ INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

REV	DATE	REVISION DESCRIPTION

REV	DESIGNER/DRAFTER:	CHECKED BY:	
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BLOCK:

APPROVED BY:



STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION

PROJECT NUMBER: 0000-0000
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION
TOWN(S): SAMPLE TOWN(S)
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 4 OF 11

DRAWING NO.

SHEET NO.

MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

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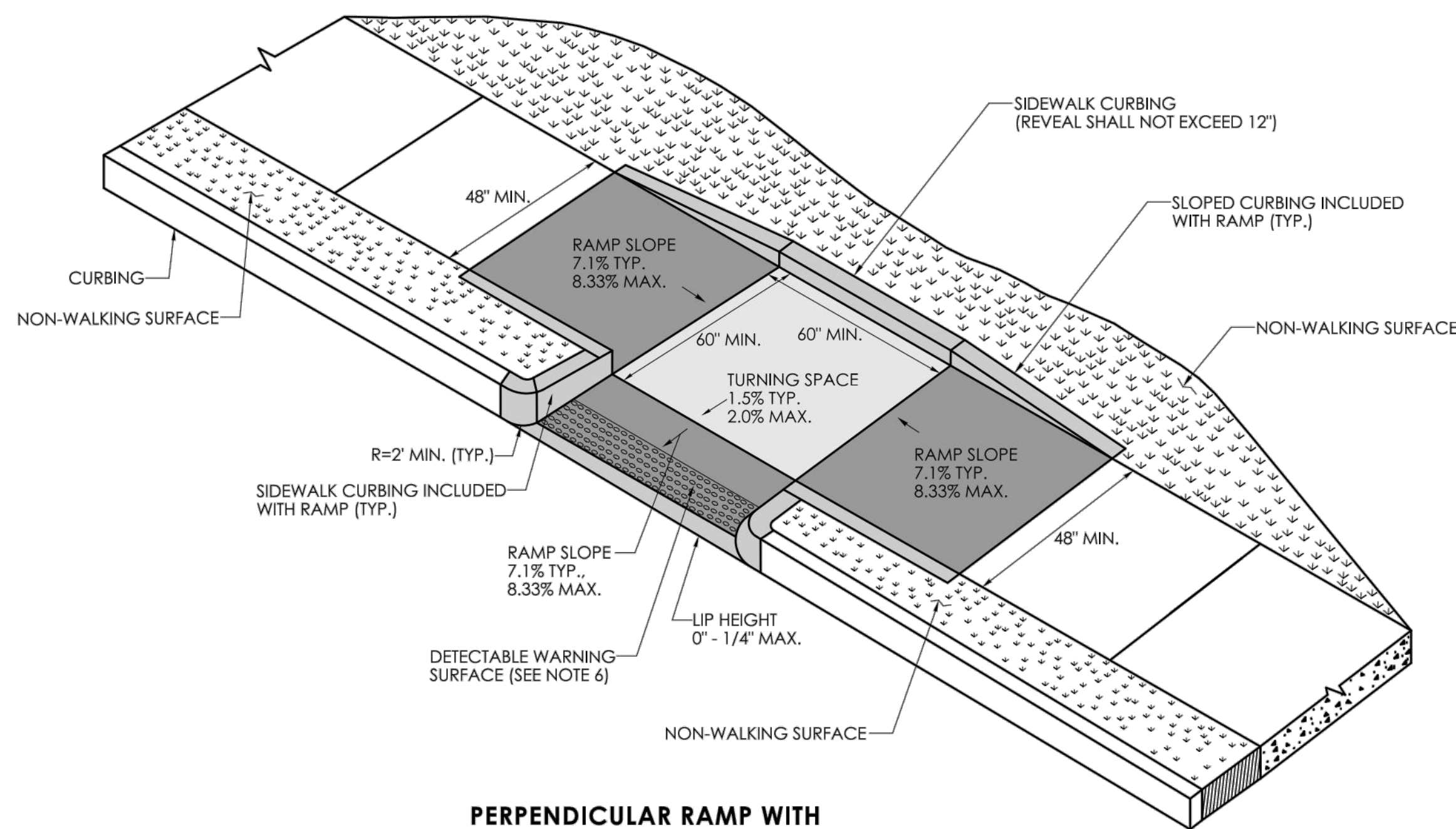
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ROGEPOR, CT 06604

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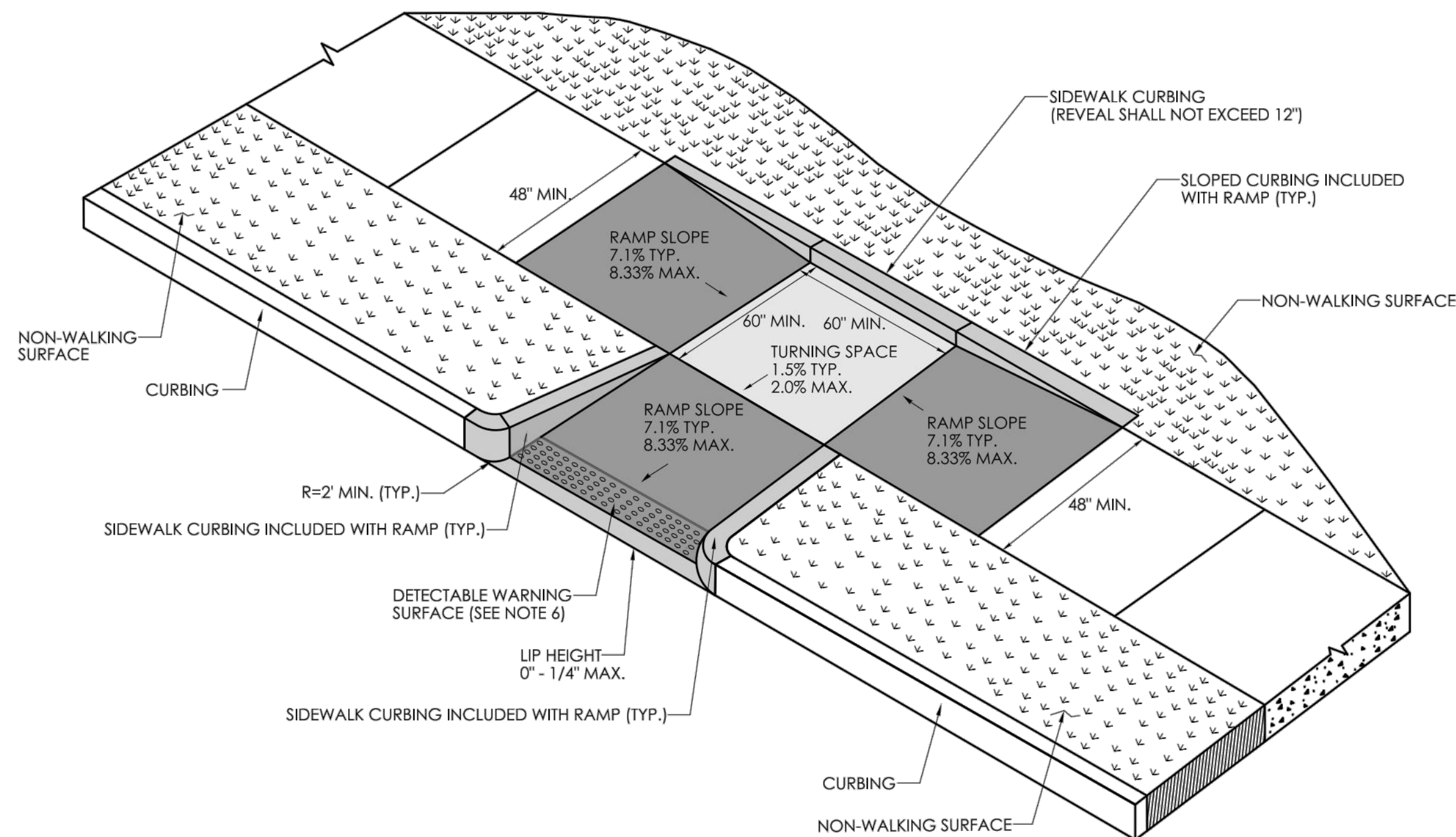
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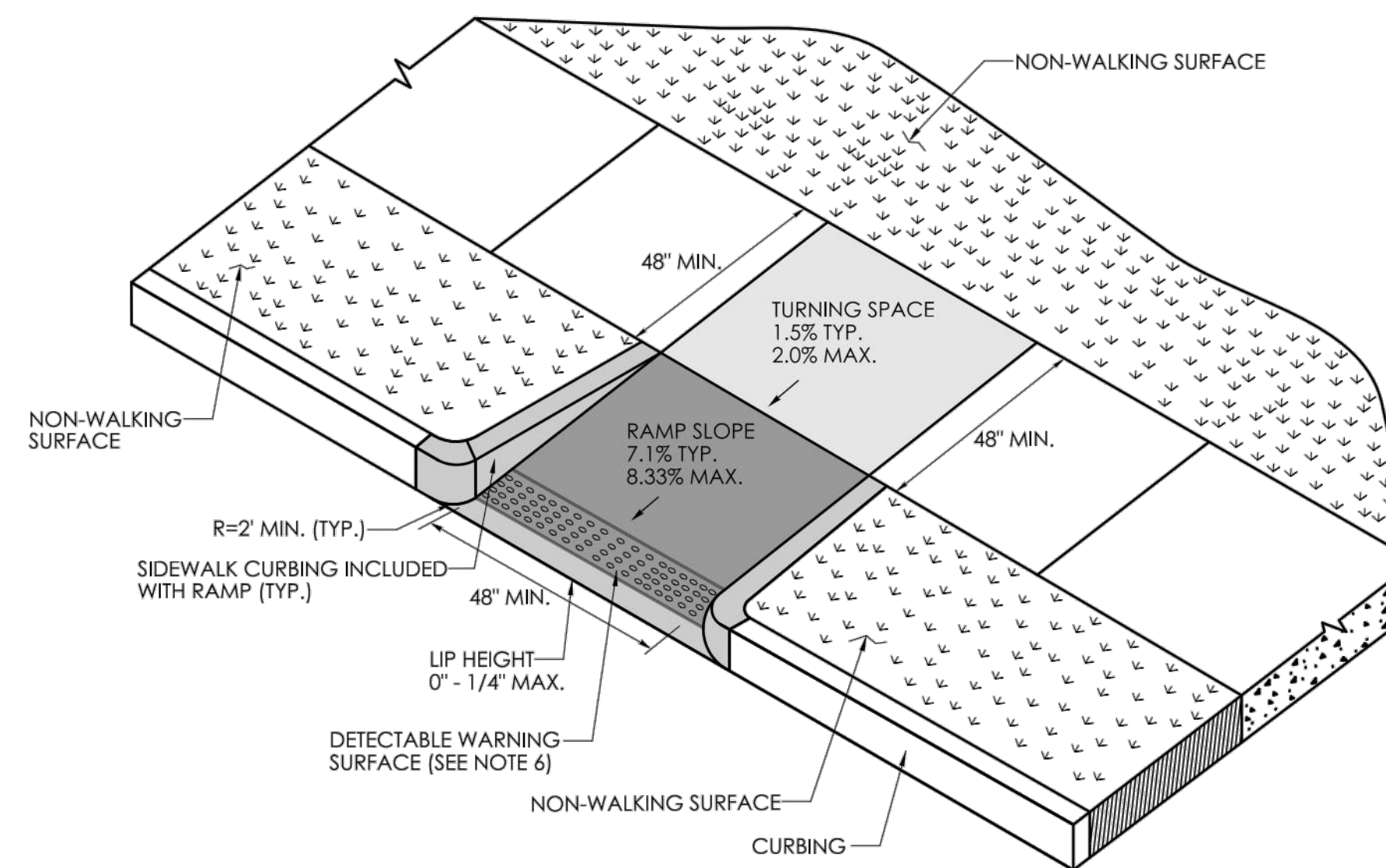
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PERPENDICULAR RAMP WITH
NON-WALKING SURFACE
(TYPE 11)



PERPENDICULAR RAMP WITH SIDEWALK CURB
AND NON-WALKING SURFACE
(TYPE 12)



PERPENDICULAR RAMP
WITH NON-WALKING SURFACE
(TYPE 13)

GENERAL NOTES:

1. SIDEWALK RAMPs SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPs AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPs SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

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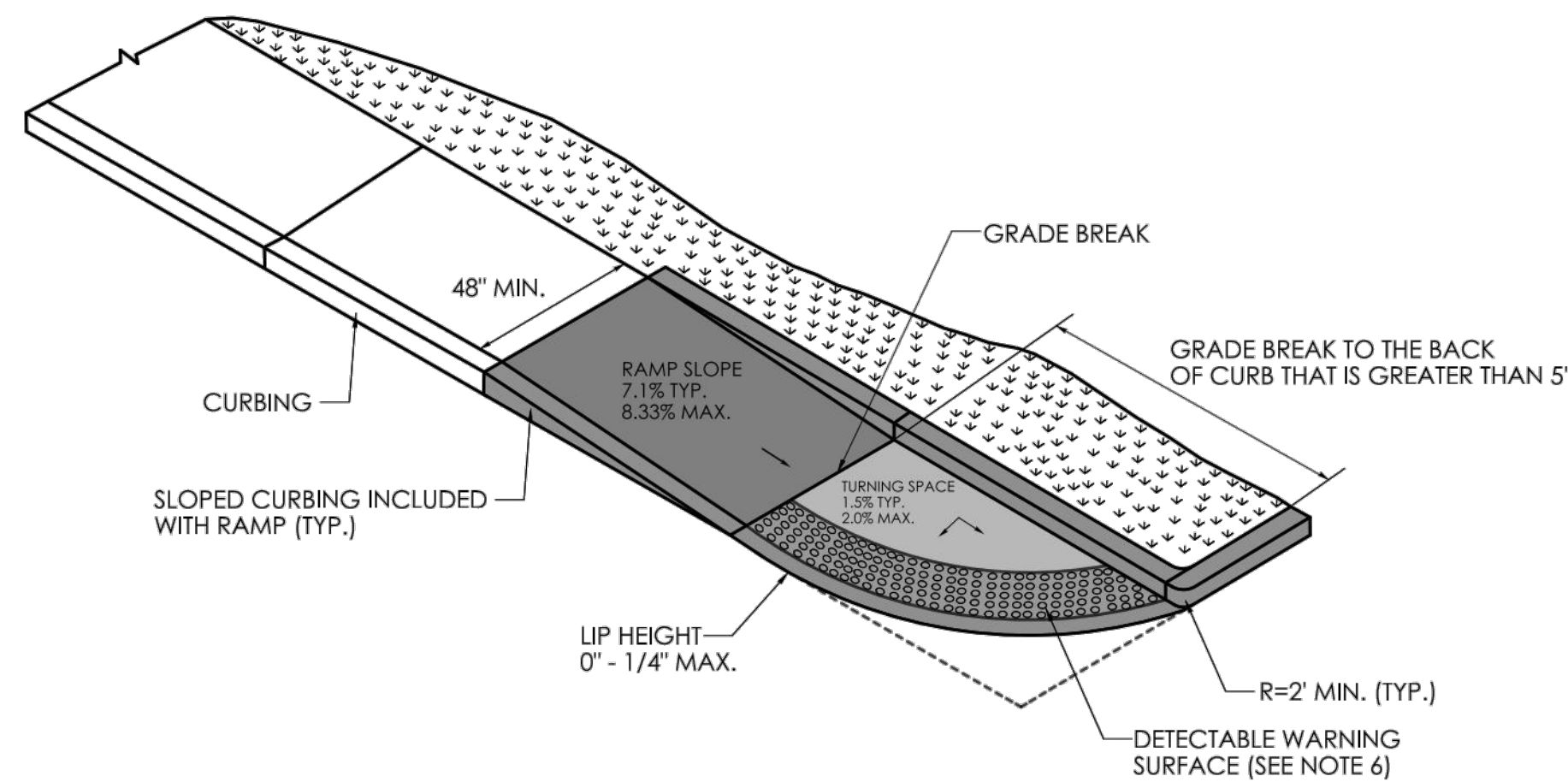
PREPARED FOR:
METROCOG
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BRIDGEPORT, CT 06604

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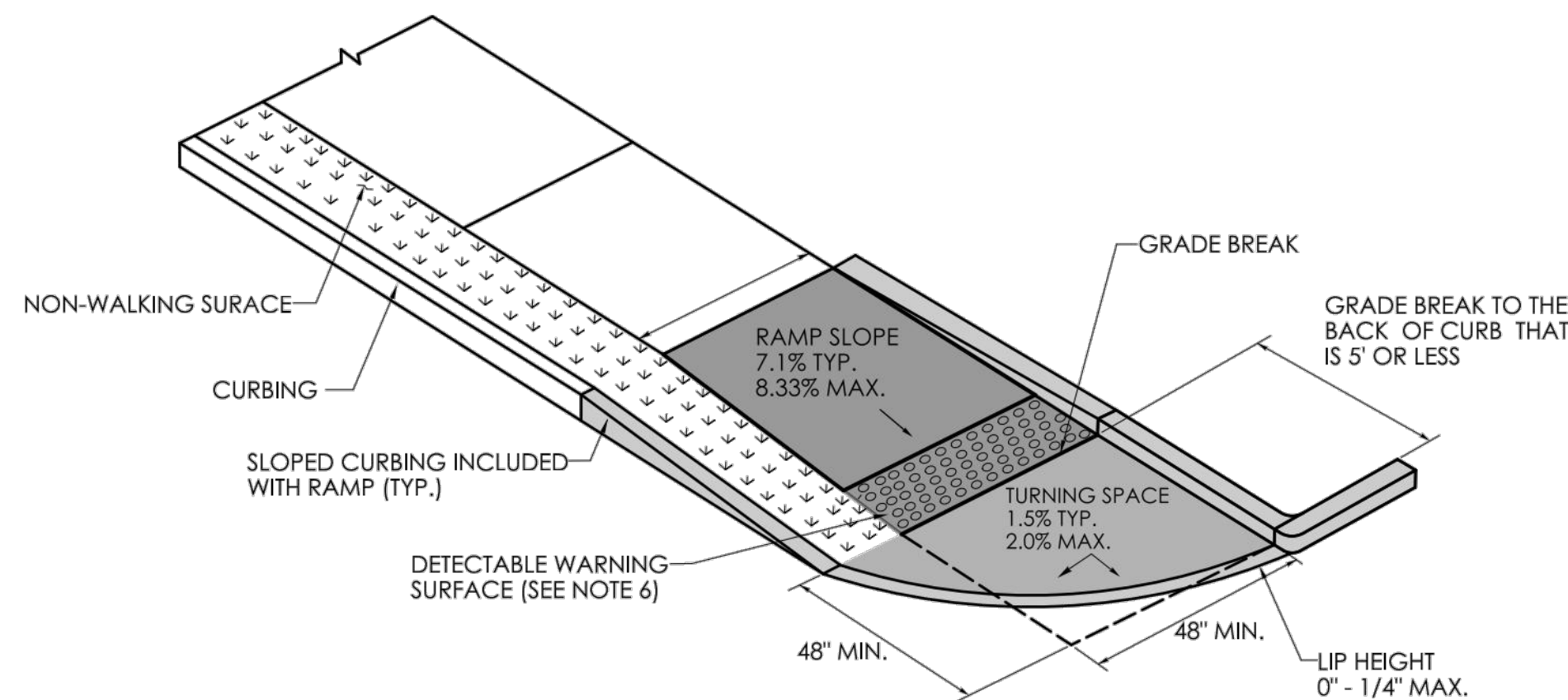
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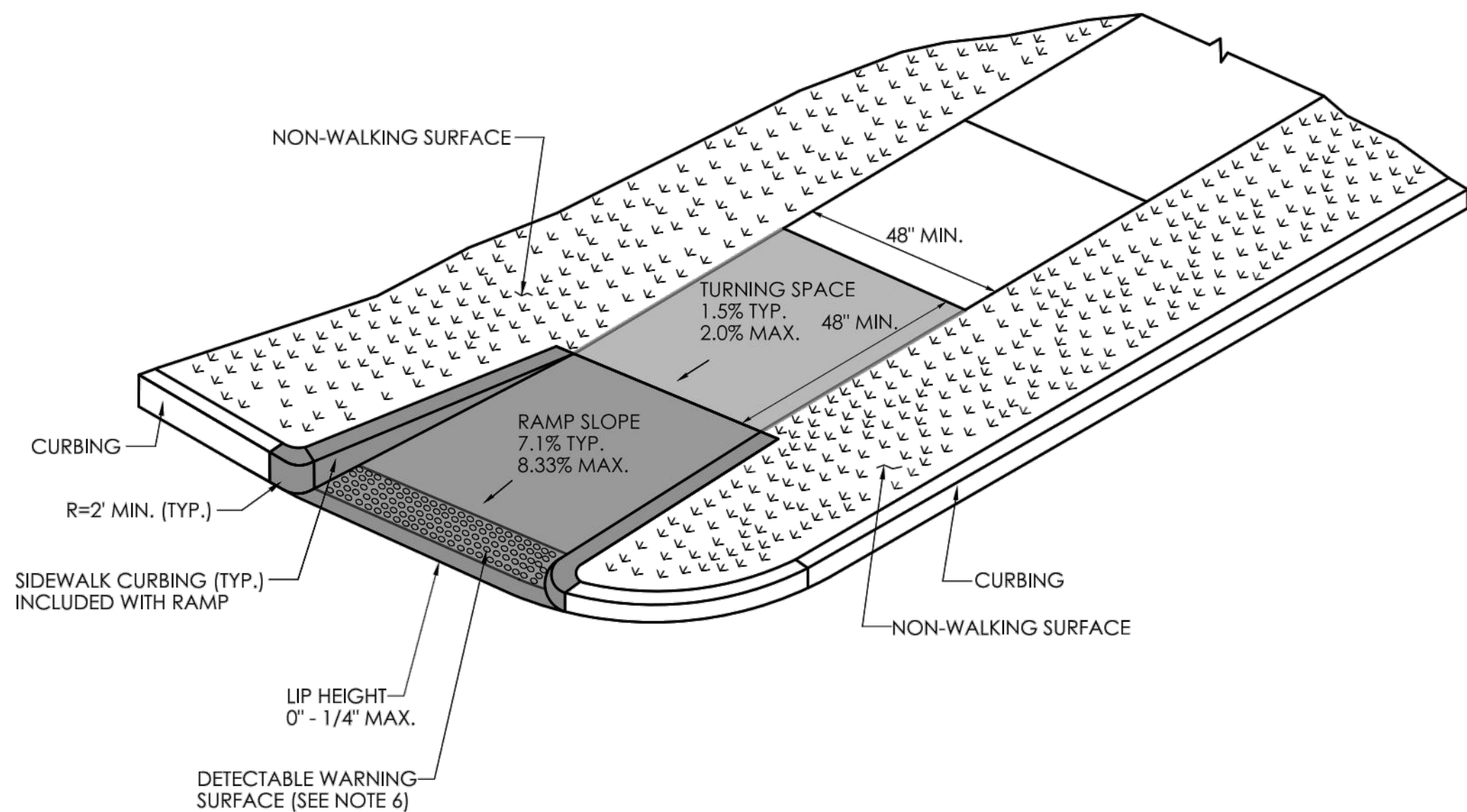
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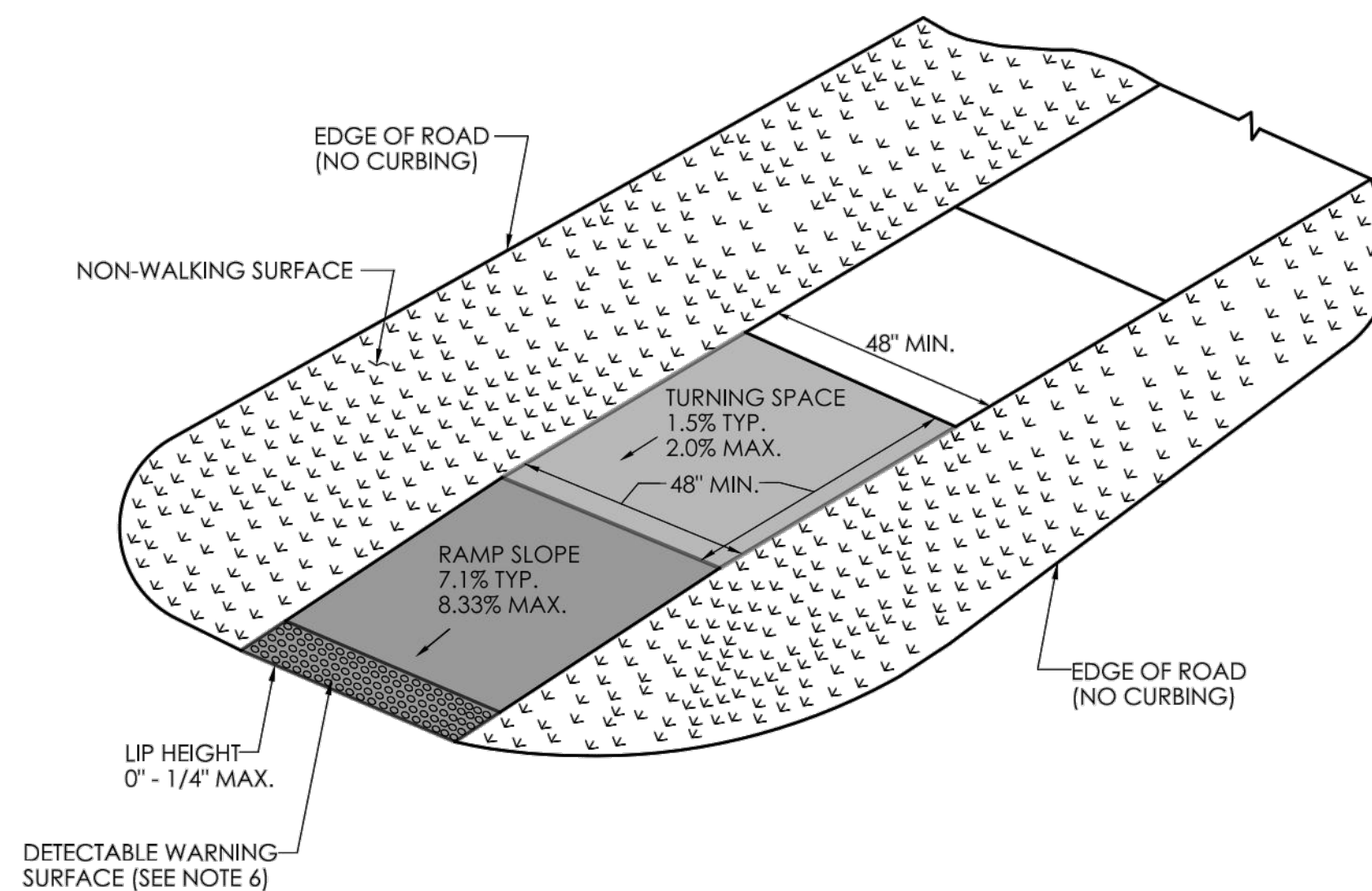
**SINGLE DIRECTION PERPENDICULAR RAMP
GRADE BREAK TO THE BACK OF CURB
GREATER THAN 5'
(TYPE 14)**



**SINGLE DIRECTION PERPENDICULAR RAMP
WITH GRADE BREAK TO THE BACK
OF CURB THAT IS 5' OR LESS
(TYPE 15)**



**SINGLE DIRECTION PERPENDICULAR RAMP
RETURN CURB WITH NON-WALKING SURFACE
(TYPE 16)**



**SINGLE DIRECTION PERPENDICULAR RAMP
NO CURB WITH NON-WALKING SURFACE
(TYPE 17)**

- GENERAL NOTES:**
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
 2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED $\frac{1}{4}$ INCH.
 3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
 4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
 5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
 6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
 7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELNES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
 8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-07.
 9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

**MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2**

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

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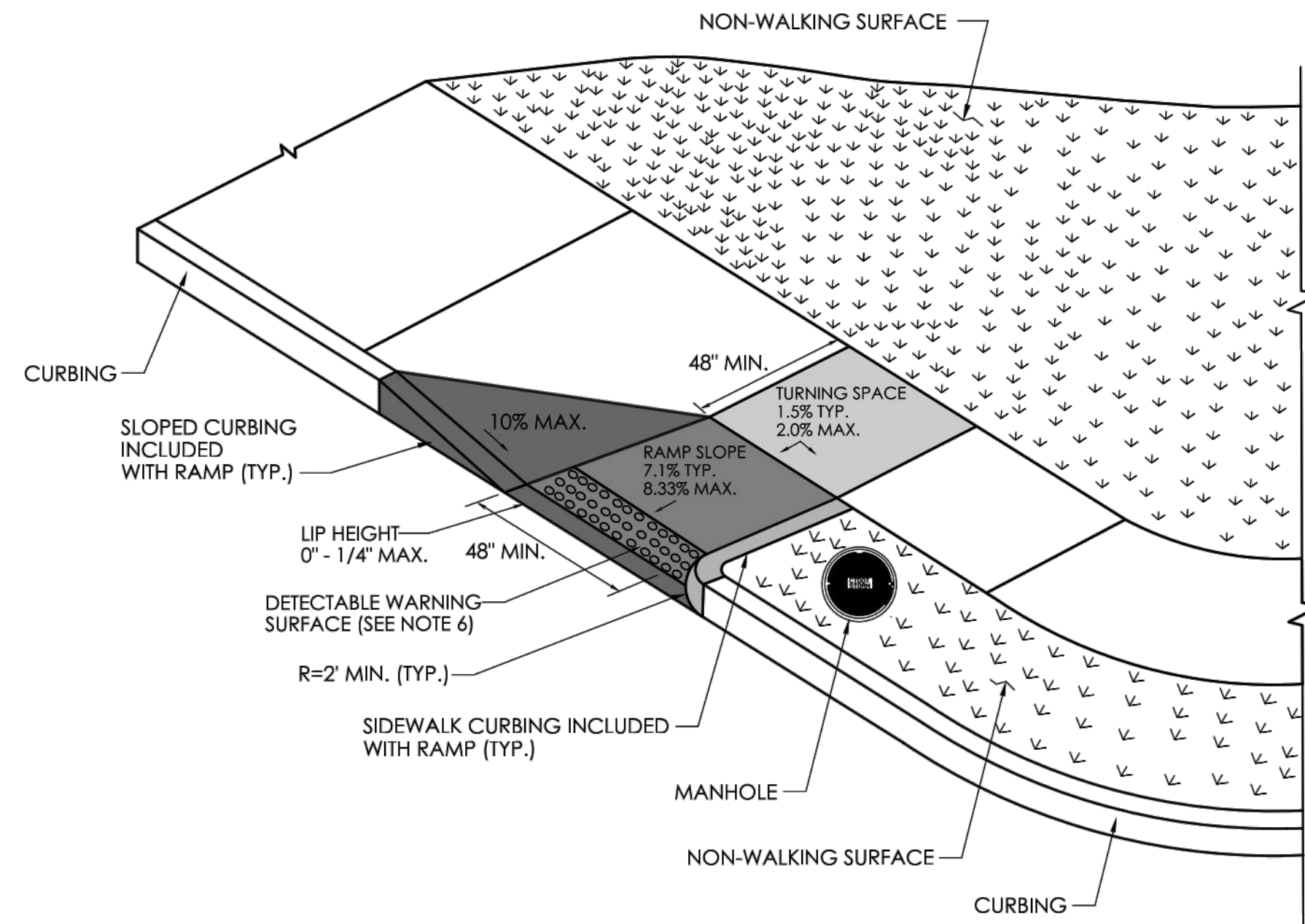
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1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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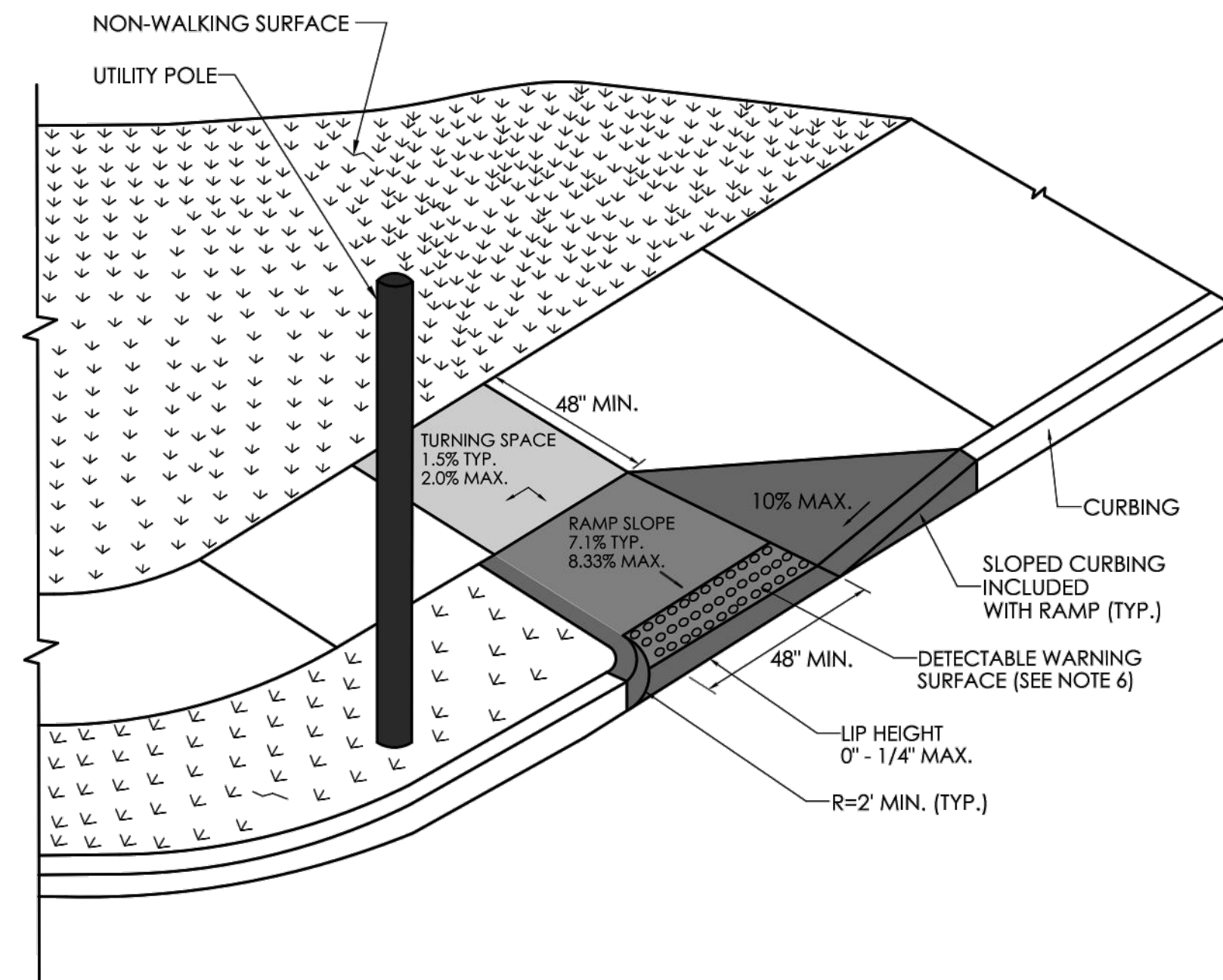
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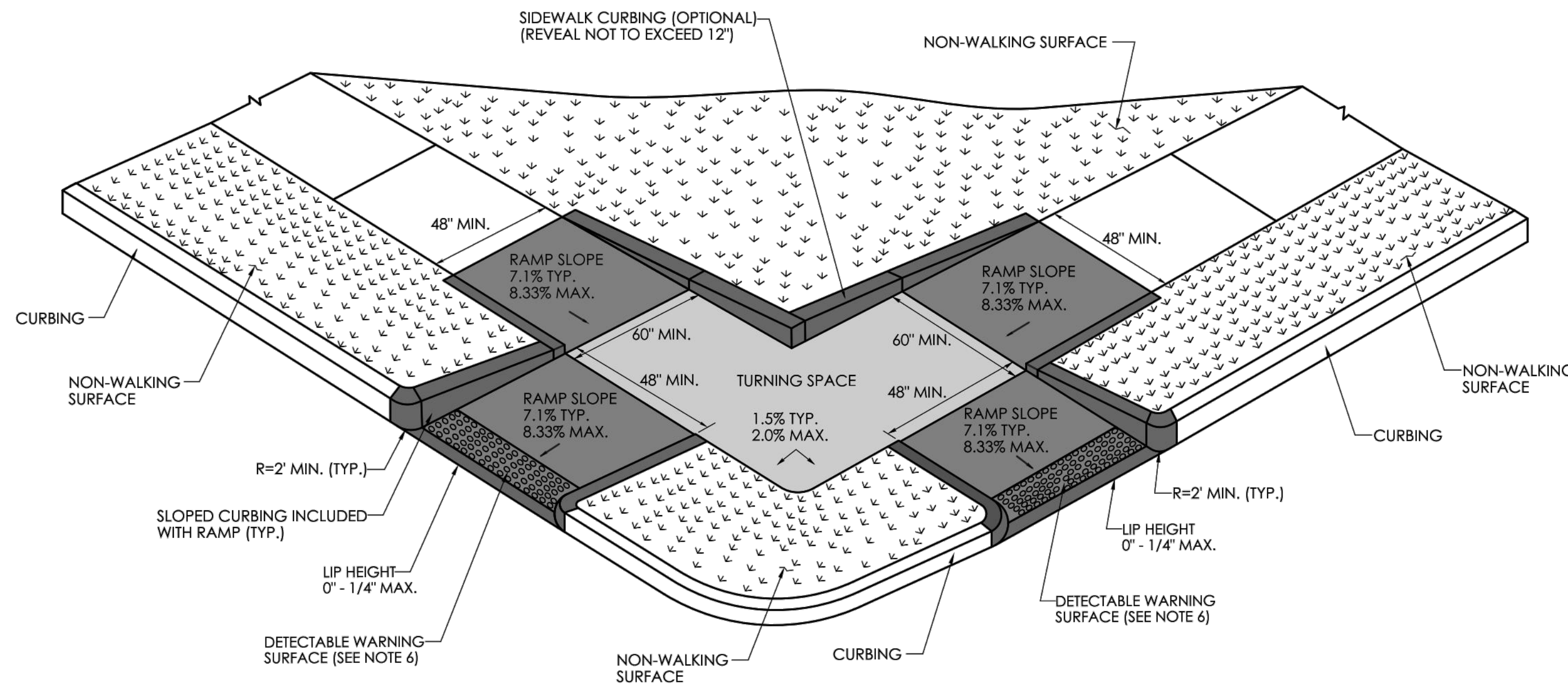
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PERPENDICULAR RAMP
WITH NON-WALKING SURFACE
(TYPE 18 LEFT)



PERPENDICULAR RAMP
WITH NON-WALKING SURFACE
(TYPE 18 RIGHT)



PERPENDICULAR RAMP
WITH NON-WALKING SURFACE
(TYPE 19)

GENERAL NOTES:

1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.33 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.
5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED ON SIDEWALK RAMPS AT PEDESTRIAN STREET CROSSINGS, PEDESTRIAN REFUGE ISLANDS AND RAILROAD CROSSINGS ALONG STREETS OR HIGHWAYS.
6. DETECTABLE WARNING SURFACES SHALL EXTEND 2 FEET MIN. IN THE DIRECTION OF PEDESTRIAN TRAVEL AND SPAN THE ENTIRE RAMP OPENING.
7. ALL GUIDESHEET VALUES COMPLY WITH PROWAG DRAFT 2013 GUIDELINES. UPDATED VALUES WILL BE MADE TO COMPLY WITH PROWAG FINAL RULE ONCE OFFICIALLY THE LAW.
8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.

MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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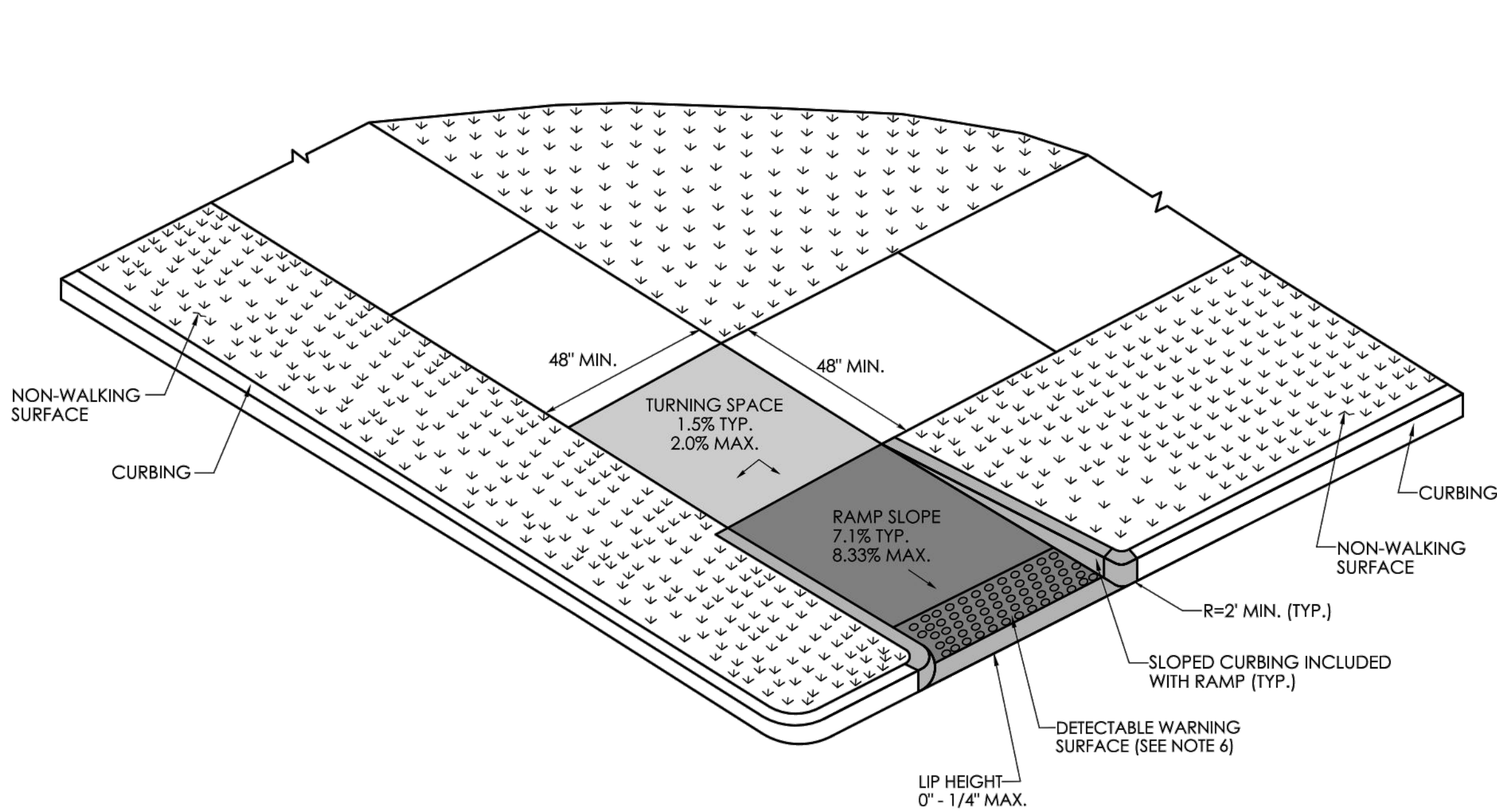
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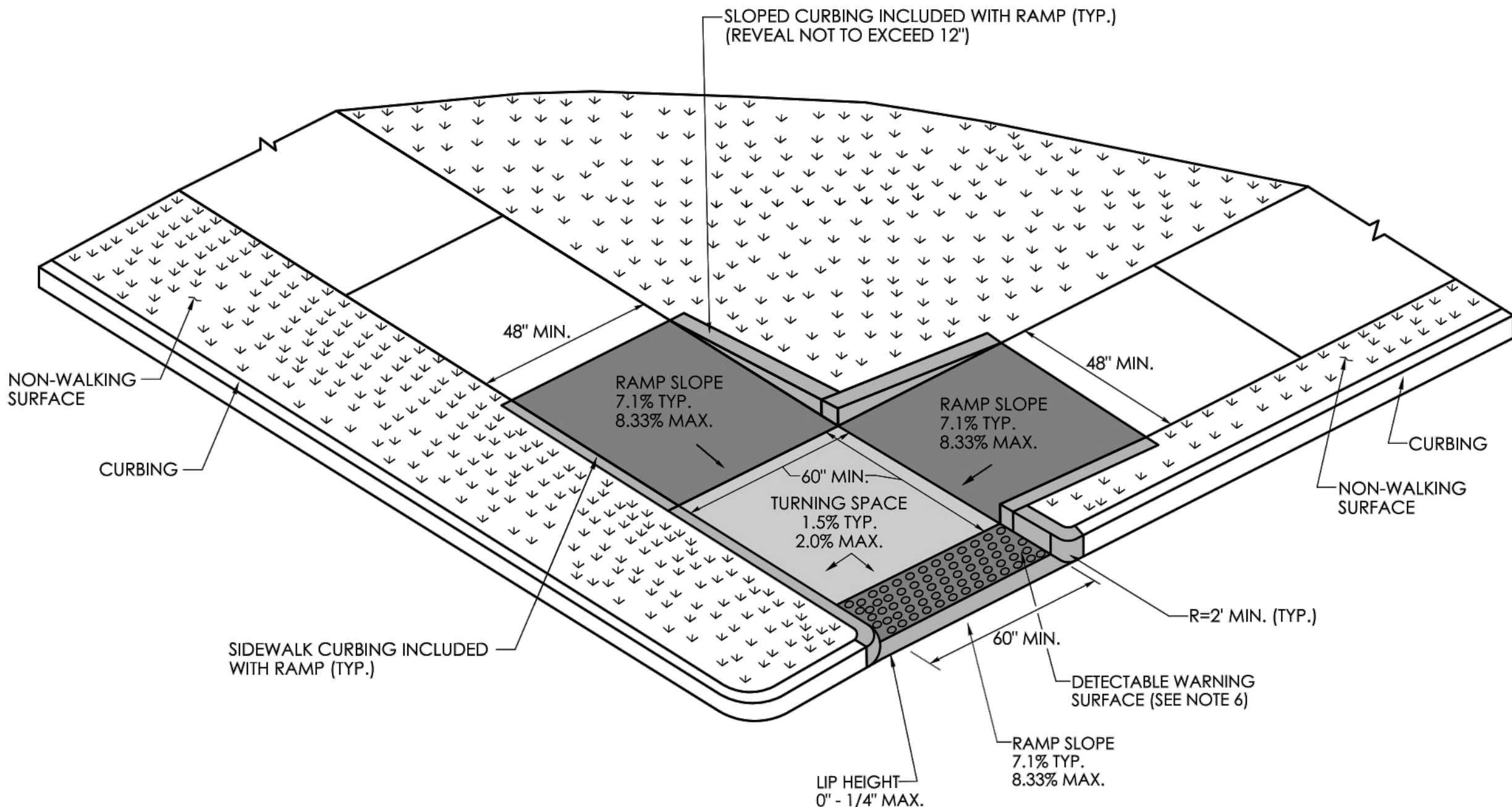
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- GENERAL NOTES:**
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP.
 2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
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 8. CTDOT DESIGN STANDARD USE THE PUBLIC RIGHT-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG) SEE ENGINEERING DIRECTIVE ED-2019-7.
 9. CURB RAMPS SHALL BE CONSTRUCTED SO THAT WATER WILL NOT ACCUMULATE WITHIN THE PEDESTRIAN ACCESS ROUTE.



**RESTRICTED PEDESTRIAN CROSSING SIDEWALK RAMP
WITH NON-WALKING SURFACE
(TYPE 20)**



**RESTRICTED PEDESTRIAN CROSSING
WITH LANDING AT BOTTOM AND NON-WALKING SURFACE
(TYPE 21)**

REV.	DATE	REVISION DESCRIPTION

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LASTED SAVED BY: RichardEH FILE NAME: W:\CT_CONNECT_DDE\CT_Configuration\Organization\Cel\CTDOT_Borden_Contract.cel
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SIGNATURE/
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2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

APPROVED BY:



STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION



PROJECT NUMBER: 0000-0000
PROJECT DESCRIPTION: SAMPLE PROJECT DESCRIPTION
TOWNS: SAMPLE TOWN(S)
DRAWING TITLE: CONCRETE SIDEWALK RAMPS SHEET 8 OF 11

DRAWING NO.

SHEET NO.

REVISIONS:

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

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<div>MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2</div> <div>STATE PRJ. NO. L138-0003 MAIN STREET (CT-113) IN STRATFORD CONNECTICUT</div> <div>STATE GUIDE SHEET DETAILS</div> <div>OCTOBER 2024</div>		
REVISIONS:		
<div>PREPARED FOR:</div> <div>METROCOG</div> <div>1000 LAFAYETTE BOULEVARD BRIDGEPORT, CT 06604</div>		
<div><div><div>BSC GROUP</div><div>BUILD SUPPORT CONNECT</div><div>665 Winding Brook Drive Glastonbury, Connecticut 06033</div></div><div>860 652 8227</div></div>		
<div>© 2024 BSC GROUP, INC.</div> <div>SCALE: AS SHOWN</div>		
FILE: 8387300\CIVIL_DRAWINGS\ DWG. NO: JOB. NO: 83873.00		GS-12

PROGRESS PRINT - 60% DESIGN

M1 - SERIES										M2 - SERIES										M3 - SERIES										M4 - SERIES										M5 - SERIES										M6 - SERIES																																																																																																																												
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<div>M1-5</div> <div><div><div><div><div></div><div>00</div></div><div>51-6616</div><div>51-6636</div><div>VARIABLE: 1 or 2 DIGITS</div></div><div><div><div><div><div></div><div>000</div></div><div>51-6647</div><div>51-6646</div><div>VARIABLE: 3 DIGITS</div></div></div></div><div>LEGEND - BLACK BACKGROUND - WHITE</div><table><tr><th>AREA (SQ. FT.)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>4.00</td><td>24X24</td><td>51-6616</td><td>1</td><td>.080</td></tr><tr><td>5.00</td><td>30X24</td><td>51-6647</td><td>1</td><td>.080</td></tr><tr><td>9.00</td><td>36X36</td><td>51-6636</td><td>2</td><td>.080</td></tr><tr><td>11.25</td><td>45X36</td><td>51-6646</td><td>2</td><td>.100</td></tr></table></div></div>										AREA (SQ. FT.)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	4.00	24X24	51-6616	1	.080	5.00	30X24	51-6647	1	.080	9.00	36X36	51-6636	2	.080	11.25	45X36	51-6646	2	.100	<div>M3-3</div> <div><div><div><div><div></div><div>S</div><div>OUTH</div></div></div></div></div>										<div>M6-3</div> <div><div><div><div><div></div><div></div></div></div></div><div>51-2685</div><div>51-2625</div><div>51-2686</div><div>51-2626</div></div>																																																																																																																																	
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MAIN STREET STREETSCAPE IMPROVEMENTS - PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP

BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033

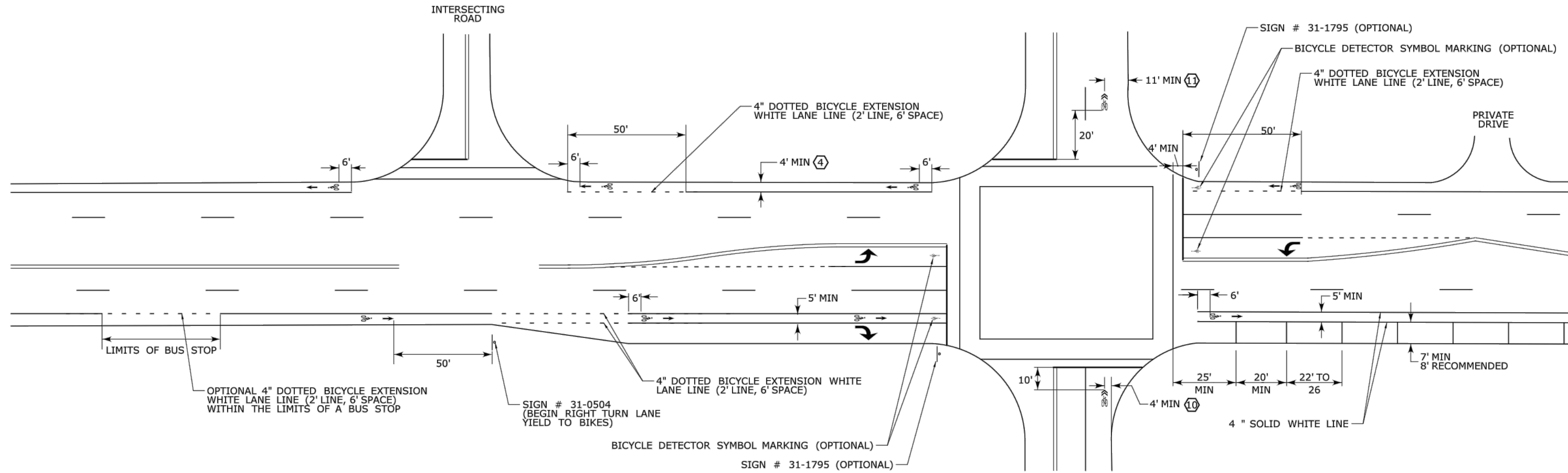
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SCALE: AS SHOWN

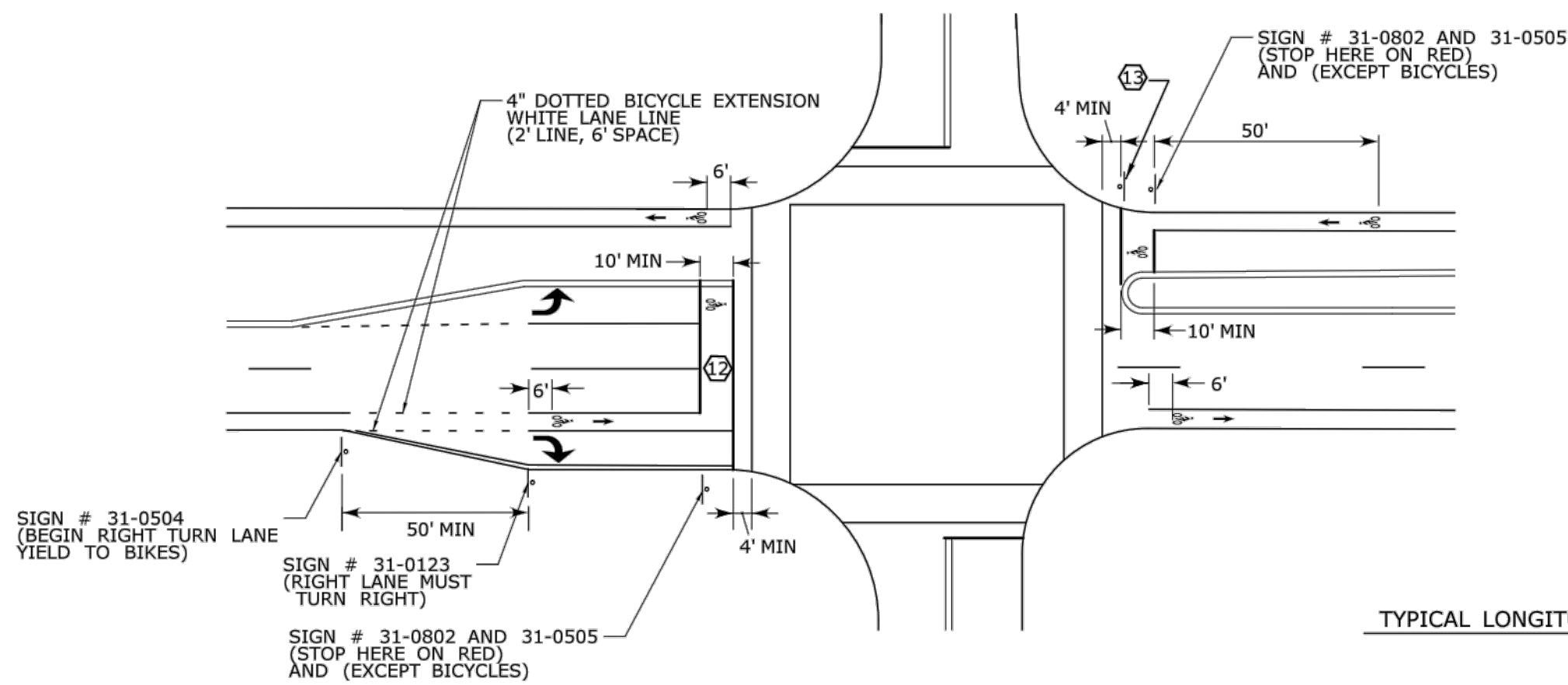
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DWG. NO:	GS-14
JOB. NO: 83873.00	

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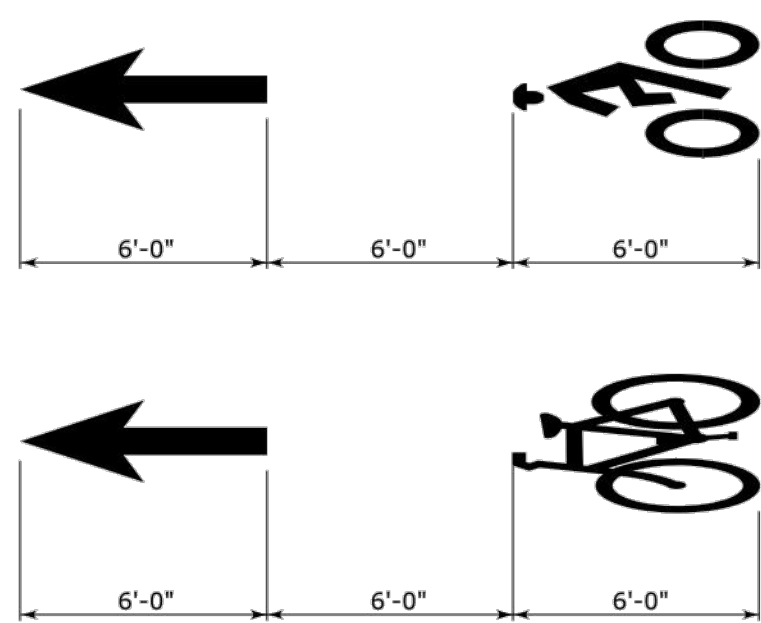
TYPICAL BICYCLE LANE AND SHARROW MARKING DETAIL



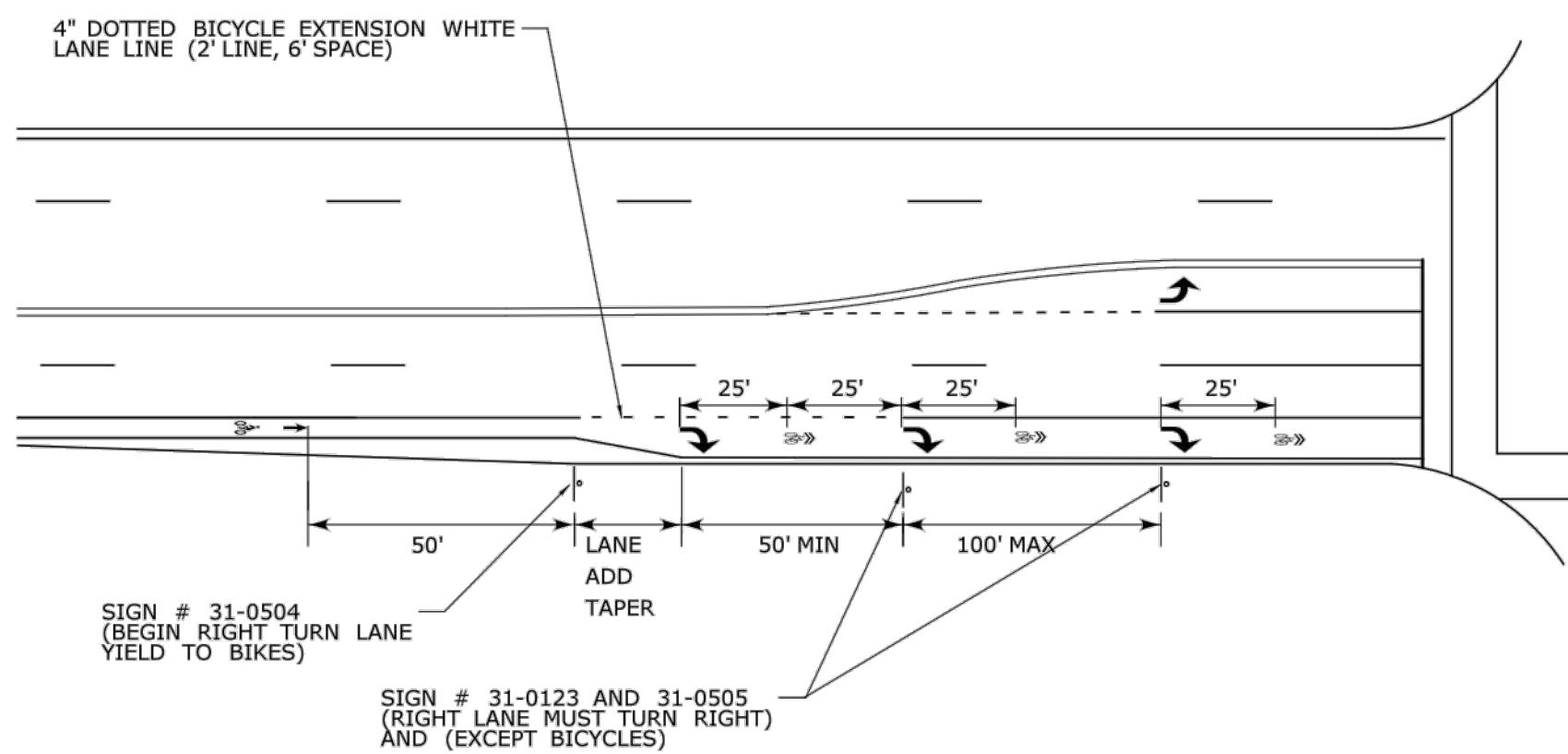
TYPICAL BICYCLE BOX DETAIL



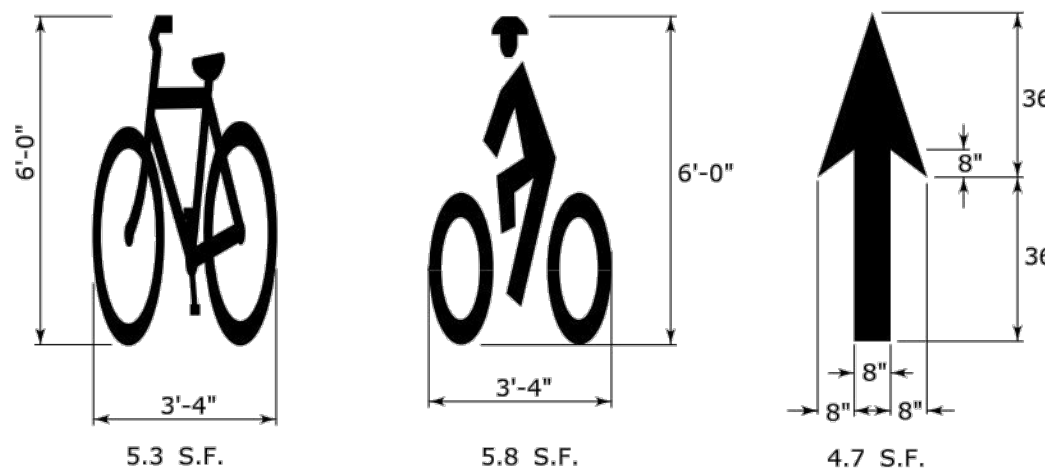
TYPICAL LONGITUDINAL SPACING



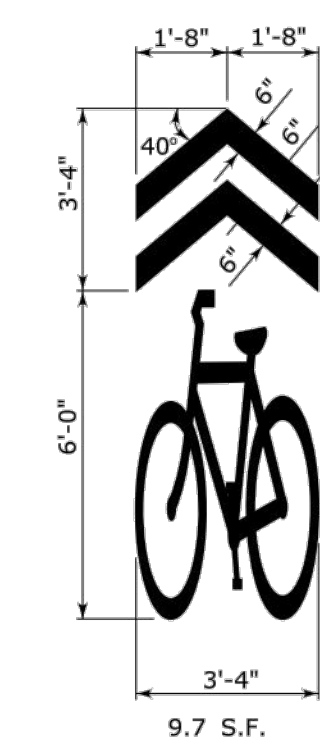
TYPICAL SHARED BICYCLE LANE AND TURN LANE DETAIL



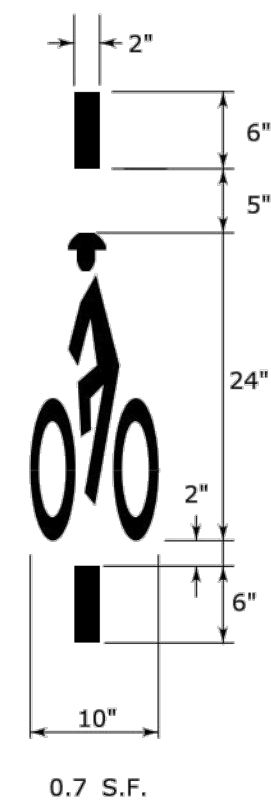
BICYCLE LANE SYMBOL MARKINGS



SHARED LANE SYMBOL MARKING



BICYCLE DETECTOR SYMBOL MARKING



NOTES:

1. BICYCLE PAVEMENT MARKINGS AND GREEN PAVEMENT TO BE OWNED AND MAINTAINED BY THE MUNICIPALITY.
2. MUNICIPALITY MUST OBTAIN FHWA INTERIM APPROVAL FOR BICYCLE BOX INSTALLATION AND COMPLY WITH THE REQUIREMENTS OF FHWA INTERIM APPROVAL No. 1A-14, OPTIONAL USE OF GREEN COLORED PAVEMENT FOR BIKE LANES AND FHWA INTERPRETATION LETTER 9(09)-96(1), CHROMATICITY REQUIREMENTS FOR GREEN-COLORED PAVEMENT. GREEN COLORED PAVEMENT MAY BE INSTALLED ONLY WITHIN A BICYCLE LANE OR WITHIN AN EXTENSION OF A BICYCLE LANE.
3. MUNICIPALITY MUST OBTAIN FHWA INTERIM APPROVAL FOR GREEN PAVEMENT INSTALLATION AND COMPLY WITH THE REQUIREMENTS OF FHWA INTERIM APPROVAL No. 1A-14, OPTIONAL USE OF GREEN COLORED PAVEMENT FOR BIKE LANES AND FHWA INTERPRETATION LETTER 9(09)-96(1), CHROMATICITY REQUIREMENTS FOR GREEN-COLORED PAVEMENT. GREEN COLORED PAVEMENT MAY BE INSTALLED ONLY WITHIN A BICYCLE LANE OR WITHIN AN EXTENSION OF A BICYCLE LANE.
4. 5 FEET WIDE BICYCLE LANES SHOULD BE USED IF IMMEDIATELY ADJACENT TO A CURB, GUIDE RAIL, OR OTHER VERTICAL SURFACES.
5. MARKINGS USED ON BIKEWAYS SHALL BE RETROREFLECTORIZED.
6. BICYCLE LANES MARKINGS SHOULD BE SPACED AT INTERVALS NO GREATER THAN 500 FT.
7. IF USED, SHARED LANE MARKINGS SHOULD BE SPACED AT INTERVALS NO GREATER THAN 250 FT.
8. SHARED LANE MARKINGS SHOULD NOT BE PLACED ON ROADWAYS THAT HAVE A SPEED LIMIT ABOVE 35 MPH.
9. BICYCLE DETECTOR SYMBOL MARKINGS MAY BE PLACED ON THE PAVEMENT INDICATING THE OPTIMUM POSITION FOR A BICYCLIST TO ACTUATE THE SIGNAL. THE LOCATION OF THE MARKINGS SHALL BE SPECIFIED ON THE SIGNAL PLAN. THE MARKINGS MAY BE SUPPLEMENTED BY SIDE MOUNTED SIGN # 31-1795.
10. ON STREETS WITHOUT ON-STREET PARKING AND WITH OUTSIDE TRAVEL LANES LESS THAN 14 FEET WIDE, THE CENTERS OF THE SHARED LANE MARKINGS SHOULD BE AT LEAST 4 FEET FROM THE FACE OF THE CURB, OR FROM THE EDGE OF THE PAVEMENT WHERE THERE IS NO CURB.
11. ON STREETS WITH ON-STREET PARKING THE CENTERS OF THE SHARED LANE MARKINGS SHOULD BE AT LEAST 11 FEET FROM THE FACE OF THE CURB, OR FROM THE EDGE OF THE PAVEMENT WHERE THERE IS NO CURB.
12. COUNTDOWN PEDESTRIAN SIGNAL HEADS ARE REQUIRED FOR BICYCLE BOXES LOCATED ACROSS MULTILANE APPROACHES.
13. TURNS ON RED SHALL BE PROHIBITED FROM THE APPROACH WHERE A BICYCLE BOX IS PLACED USING A NO TURN ON RED (R10-11 SERIES) SIGN.
14. AREA OF PAVEMENT MARKING SYMBOLS AS INDICATED IS APPROXIMATE.
15. REFER TO STANDARD SHEET TR-1210.04 FOR PAVEMENT MARKING LINE DETAILS.

MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
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OCTOBER 2024

REVISIONS:

PREPARED FOR:

METROCOG
1000 LAFAYETTE BOULEVARD
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BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
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SCALE: AS SHOWN

FILE: 8387300\CIVIL\DRAWINGS\

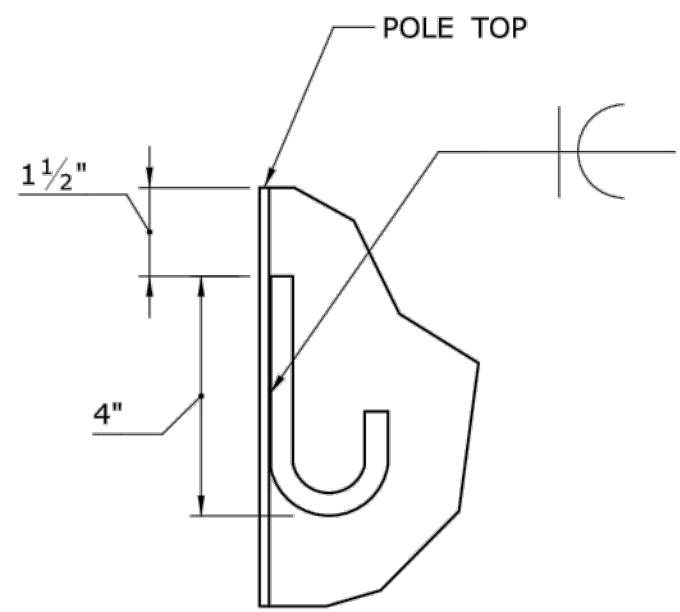
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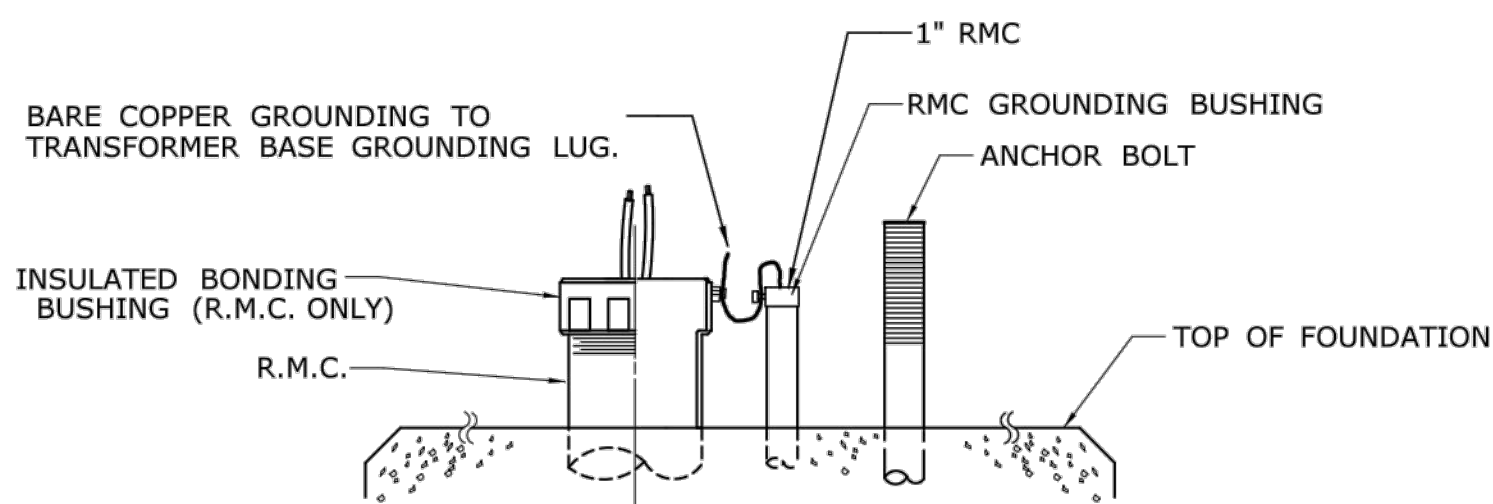
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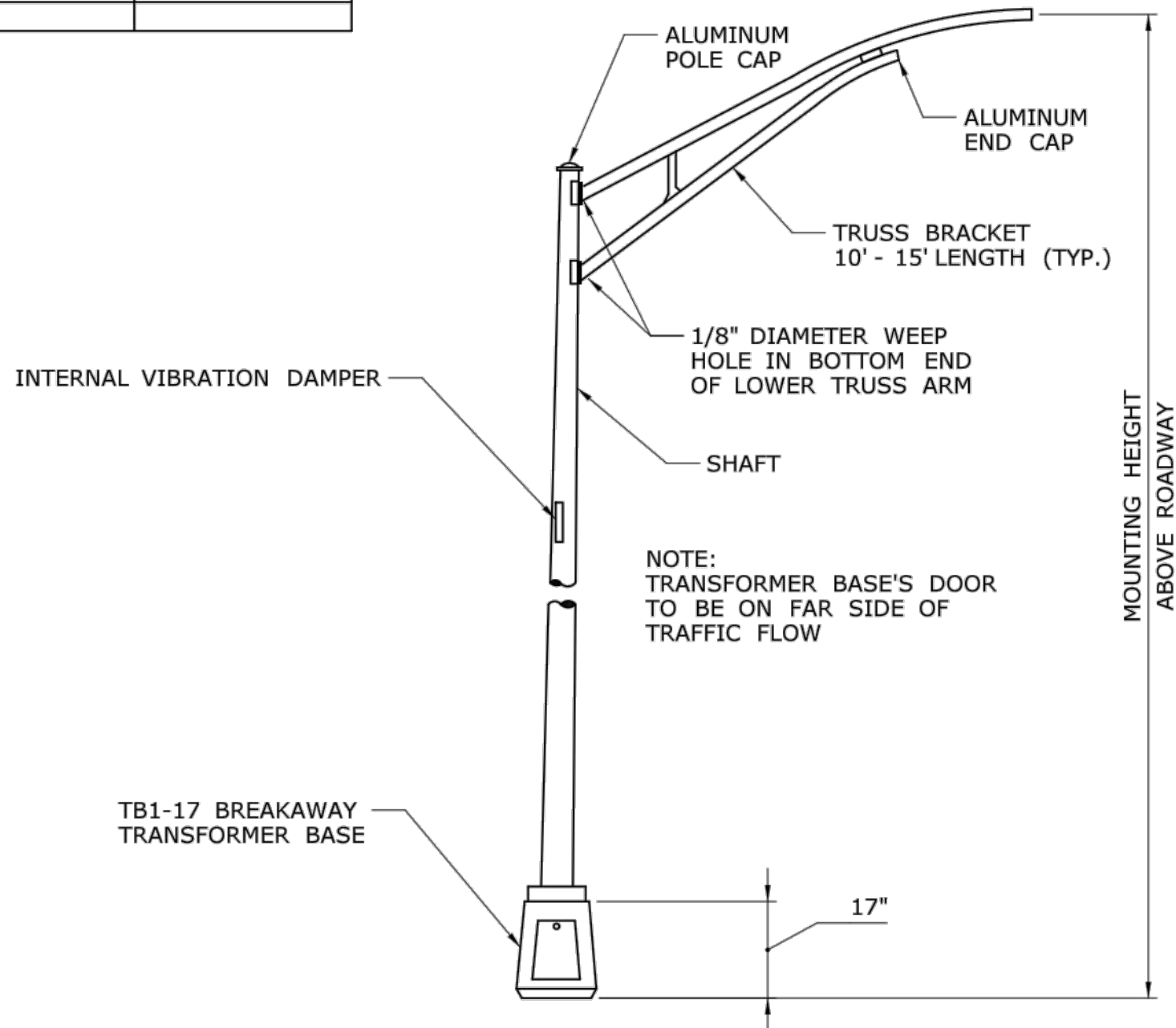
ALUMINUM LIGHT STANDARD - DIMENSION TABLE								
ITEM NO.	MOUNTING HEIGHT	BRACKET LENGTH	SHAFT DIAMETER		SHAFT WALL THICKNESS	BASE TYPE	ANCHOR BOLT SIZE	BOLT CIRCLE DIAMETER
			BOTTOM	TOP				
1003206	30'	15'	8"	6"	0.188"	TRANSFORMER	1"x40"	15"



J-HOOK MOUNTING
DETAIL

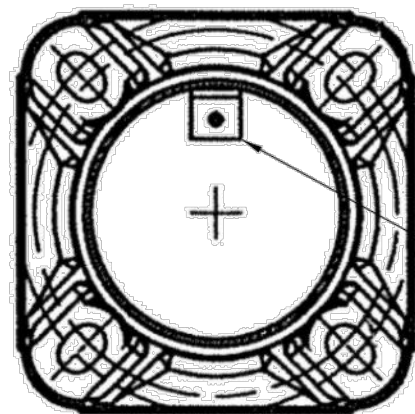


CONDUIT TERMINATION AT
VEHICLE DETECTOR LIGHT STANDARD BASE

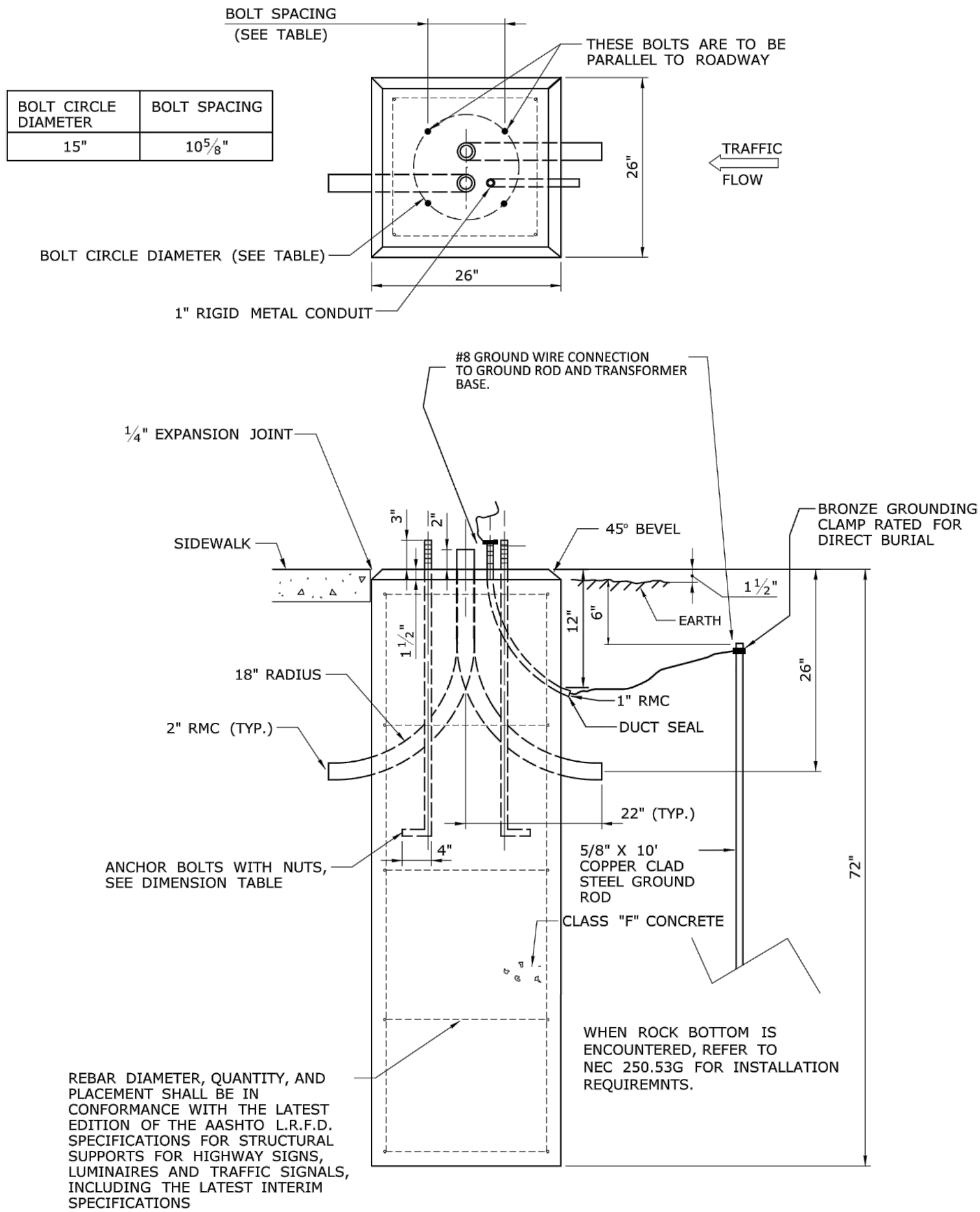


ALUMINUM LIGHT STANDARD FOR VEHICLE DETECTOR

NOTE:
TRANSFORMER BASE DOOR TO BE ON FAR SIDE OF TRAFFIC FLOW.
TOP 2'-6" OF SHAFT IS NOT TAPERED.
REFER TO SIGNAL PLANS FOR BRACKET LENGTHS.



ALUMINUM LIGHT STANDARD BASE



LIGHT STANDARD FOUNDATION - TYPE I

LIGHT STANDARD NOTES:

- ALUMINUM ALLOY SHALL BE 6063, T6 TEMPER.
- BOLT CIRCLE SHOWN IS FOR TRANSFORMER BASE BOTTOM.
- TO BE DESIGNED TO AASHTO "STANDARD SPECIFICATION FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" FOR 90 M.P.H. WINDS.
- WELDING DESIGN AND FABRICATION SHALL CONFORM TO THE LATEST EDITION OF THE ANSI/AWS D1.2, STRUCTURAL WELDING CODE - ALUMINUM.
- FOR BASE CONNECTION WELDS, FABRICATION INSPECTION AND TESTING SHALL BE PERFORMED AS NECESSARY PRIOR TO ASSEMBLY, DURING ASSEMBLY, DURING WELDING, AND AFTER WELDING, TO ENSURE THAT MATERIALS AND WORKMANSHIP MEET THE REQUIREMENTS OF THE CONTRACT DOCUMENTS. FABRICATION INSPECTION AND TESTING IS THE RESPONSIBILITY OF THE CONTRACTOR. VERIFICATION INSPECTION AND TESTING IS THE PREROGATIVE OF THE ENGINEER (CONNDOT).
- NON-DESTRUCTIVE TESTING FOR ALUMINUM SHALL BE AS FOLLOWS: A RANDOM 25% OF ALL BASE CONNECTION WELDS SHALL BE INSPECTED IN ACCORDANCE WITH ASTM E-165 STANDARD PRACTICE FOR LIQUID PENETRANT INSPECTION METHOD.

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MAIN STREET
STREETSCAPE
IMPROVEMENTS -
PHASE 2

STATE PRJ. NO. L138-0003

MAIN STREET (CT-113)

IN
STRATFORD
CONNECTICUT

STATE GUIDE SHEET
DETAILS

OCTOBER 2024

REVISIONS:

PREPARED FOR:
METROCOG
1000 LAFAYETTE BOULEVARD
BRIDGEPORT, CT 06604

BSC GROUP
BUILD | SUPPORT | CONNECT
665 Winding Brook Drive
Glastonbury, Connecticut
06033
860 652 8227

© 2024 BSC GROUP, INC.
SCALE: AS SHOWN


FILE: 8387300\CIVIL\DRAWINGS\
DWG. NO:
JOB. NO: 83873.00

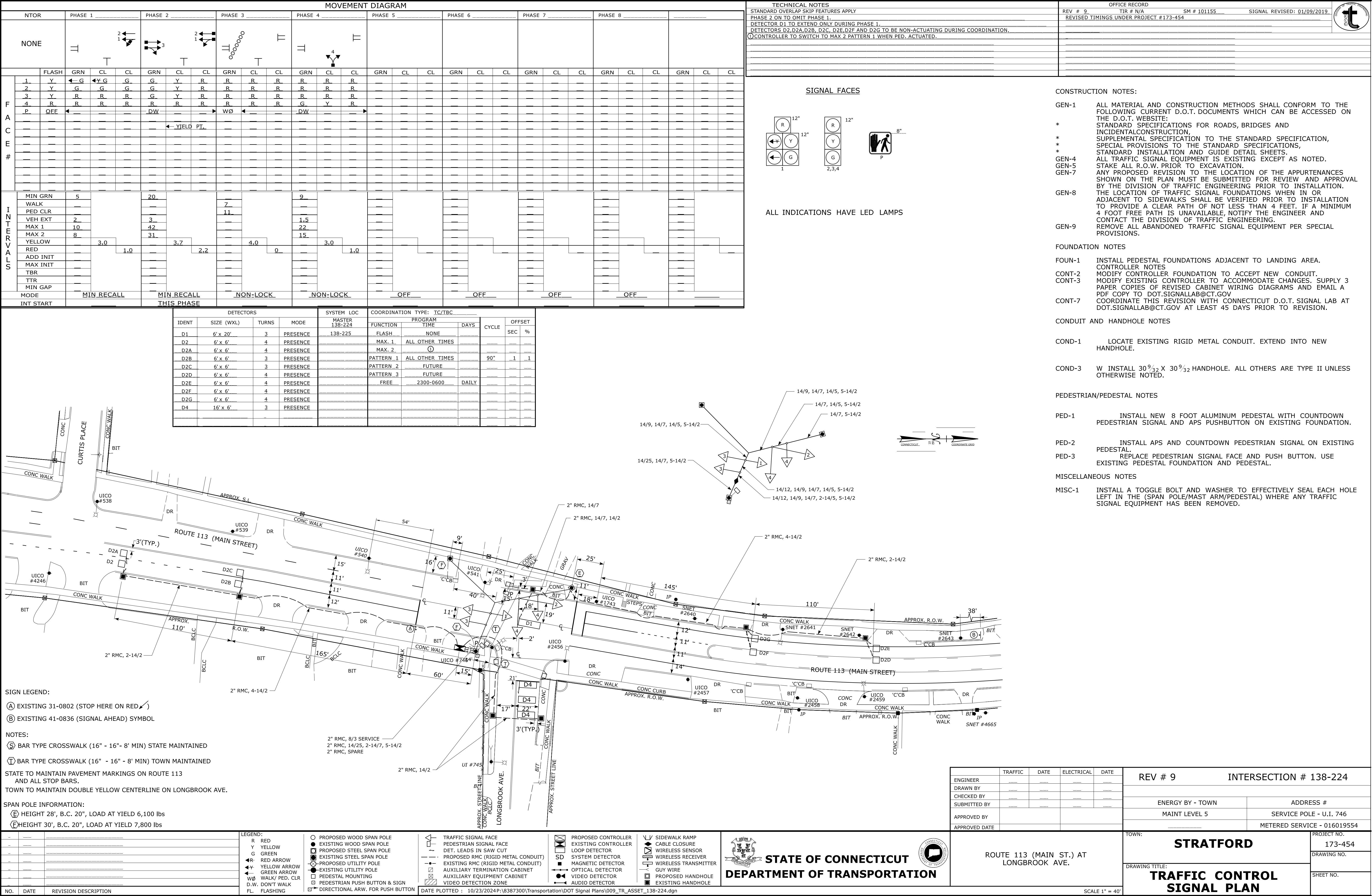
GS-16

PROGRESS PRINT - 60% DESIGN



NO.	DATE	INIT.	DESCRIPTION
REVISIONS			

	OFFICE OF ENGINEERING	TOWN:	STRATFORD	PROJECT NO. 173-375
		DRAWING TITLE:	TRAFFIC CONTROL SIGNAL PLAN	DRAWING NO.
				SHEET NO.

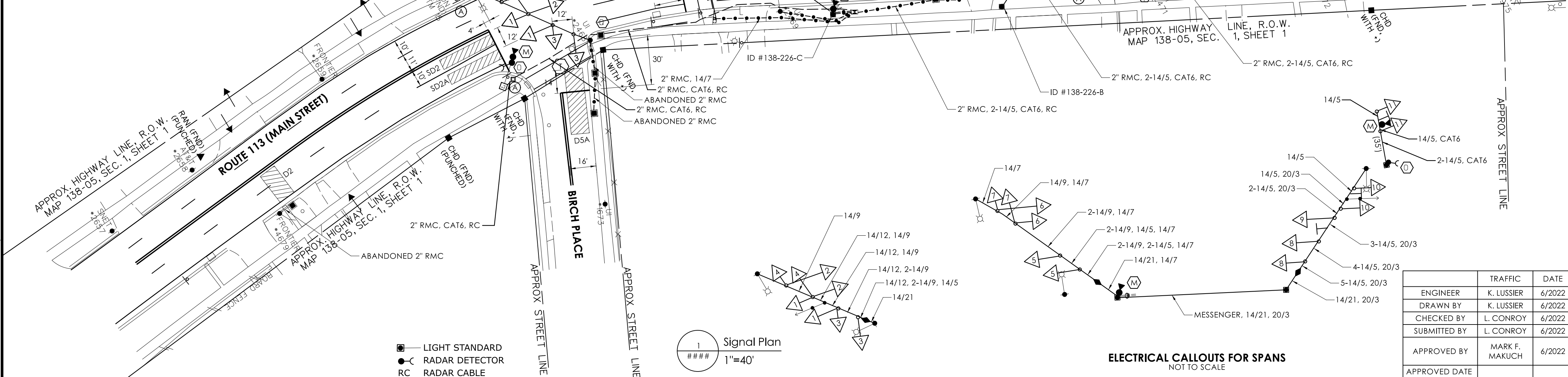




[illegible]

MOVEMENT DIAGRAM																												
NTOR		PHASE 1			PHASE 2 PRE-EMPT. 1			PHASE 3			PHASE 4			PHASE 5			PHASE 6			PHASE 7			PHASE 8			PHASE 9		
NONE																												
FACE #	FLASH	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL
	1	Y			G	Y	R	G	R	R	R	R	R	R	R	R	G	G	G									
	2	Y			G	G	G	R	R	R	R	R	R	R	R	R	G	G	G									
	3	R			R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R								
	4	R			R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R							
	5	Y			G	G	G	G	R	R	R	R	R	R	R	R	R	R	R	R	G	G						
	6	Y			G	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	G	G	G					
	7	R			G	R	R	R	G	R	R	R	R	R	R	R	R	G	Y	R	R	R	G	G				
	8	Y			G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G				
	9	R			G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G	G				
	10	Y			G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R				
	11	R				R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R				
	P	OFF																										

INTERVALS		DETECTORS		SYSTEM LOC		COORDINATION TYPE: NONE		MASTER		FUNCTION		PROGRAM		TIME		DAYS		CYCLE		OFFSET		SEC		%	
MIN GRN				16		5		7		COUNTDOWN		7													
WALK																									
PED CLR																									
VEH EXT																									
MAX 1																									
MAX 2																									
YELLOW																									
RED																									
ADD INIT																									
MAX INIT																									
TBR																									
TTR																									
MIN GAP																									
MODE																									
INIT START																									

SIGN LEGEND:		NOTES:	
(A) EXIST. 31-1702		STATE TO MAINTAIN SIGNS, PAVEMENT MARKINGS AND STOP BARS ON ROUTE 113 (MAIN ST.), AND STOP BARS ON FREEMAN AVE., BIRCH PLACE, AND HUNTINGTON RD.	
(B) EXIST. 31-1617		TOWN OF STRATFORD TO MAINTAIN CENTERLINE ON FREEMAN AVE., BIRCH PLACE, AND HUNTINGTON RD. AND SIGNS ON HUNTINGTON RD.	
(C) EXIST. 31-1618		SIGNS AND PAVEMENT MARKINGS ON PRIVATE DRIVEWAY TO BE MAINTAINED BY OWNER OF PVT. DR.	
(D) EXIST. 31-1092		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
(E) EXIST. 31-0135		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
(F) EXIST. 31-1526		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
(G) EXIST. 31-0802 (STOP HERE ON RED #1)		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
(H) EXIST. 31-1604		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
(I) EXIST. 31-0117		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
SPAN POLE INFORMATION		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
ID #138-226-B, HEIGHT 28", B.C. 20", LOAD AT YIELD 6100 LBS., YEAR INSTALLED 2010		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
ID #138-226-C, HEIGHT 28", B.C. 20", LOAD AT YIELD 6100 LBS., YEAR INSTALLED 2010		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
MAST ARM INFORMATION		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
ID #138-226-A		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
SHAFT HEIGHT 20"		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
ARM LENGTH 35"		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
360 CAMERA RISER 10"		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	
YEAR INSTALLED 2010		TOWN OF STRATFORD TO MAINTAIN EMERGENCY PRE-EMPTION EQUIPMENT.	



LEGEND:		D.W. DON'T WALK		○ PROPOSED WOOD SPAN POLE		□ PEDESTAL MOUNTING		☒ AUXILIARY TERMINATION CABINET		■ MAGNETIC DETECTOR		 <div>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</div>				PROJECT NUMBER: 0173-0501	
R, Y, G, RED, YEL, GRN	FL. FLASHING	● EXISTING WOOD SPAN POLE	□ PEDESTRIAN PUSH BUTTON & SIGN	☒ AUXILIARY EQUIPMENT CABINET	◀ OPTICAL DETECTOR	INTERSECTION DESCRIPTION: ROUTE 113 (MAIN STREET) AT HUNTINGTON ROAD, BIRCH PLACE, FREEMAN AVENUE											
◀ RED ARROW	~ DET. LEADS IN SAW CUT	☒ PROPOSED STEEL SPAN POLE	☒ DIRECTIONAL ARW. FOR PUSH BUTTON	☒ PROPOSED CONTROLLER	◀ VIDEO DETECTOR	TOWN: STRATFORD											
◀ YELLOW ARROW	— PROPOSED RMC	● EXISTING STEEL SPAN POLE	◀ TRAFFIC SIGNAL FACE	◀ EXISTING CONTROLLER	◀ AUDIO DETECTOR	DRAWING TITLE: TRAFFIC SIGNAL CONTROL PLAN											
◀ GREEN ARROW	● PROPOSED UTILITY POLE	● EXISTING UTILITY POLE	◀ PEDESTRIAN SIGNAL FACE	◀ LOOP DETECTOR	VC VIDEO CAMERA CABLE	◀ PROPOSED HANDHOLE											
WØ WALKY / PED. CLR	● EXISTING RMC	● EXISTING UTILITY POLE	◀ VIDEO DETECTION ZONE	SD SYSTEM DETECTOR	◀ CABLE CLOSURE	◀ EXISTING HANDHOLE											

***ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #**

****REVISED OR ADDED**

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-211_01	ANTI-TRACKING PAD	11-09-22
	HW-286_01	DRAINAGE TRENCH EXCAVATION	11-09-22
	HW-505_01a	STRAIGHT ENDWALLS	02-28-24
	HW-505_01b	STEEL REINFORCING FOR STRAIGHT ENDWALLS (2" DIFF BASE TO FLOW LINE)	01-05-24
	HW-505_01c	STEEL REINFORCING FOR STRAIGHT ENDWALLS (STANDARD RIPRAP APPLICATION)	01-05-24
	HW-505_02	TYPE "D-G" & "L" ENDWALLS	01-05-24
	HW-586_01	CATCH BASIN AND DROP INLET TYPES "C" AND "C-L" STRUCTURES	01-05-24
	HW-586_02	CATCH BASIN (TYPES "C" AND "C-L") FOR DOUBLE GRATE TYPE I STRUCTURES	01-05-24
	HW-586_03	CATCH BASIN (TYPES "C" AND "C-L") FOR DOUBLE GRATE TYPE II STRUCTURES	01-05-24
	HW-586_04	PRECAST CATCH BASIN AND ROUND STRUCTURE	11-08-22
	HW-586_05	PRECAST CATCH BASIN TYPES FOR DOUBLE GRATE TYPE I	11-08-22
	HW-586_06	PRECAST CATCH BASIN TYPES FOR DOUBLE GRATE TYPE II	11-08-22
	HW-586_07a	CATCH BASIN TYPE "C" AND "C-L" TOPS	01-05-24
	HW-586_07b	CATCH BASIN TYPE "C" AND "C-L" DOUBLE GRATE TYPE I TOPS	11-09-22
	HW-586_07c	CATCH BASIN TYPE "C" AND "C-L" DOUBLE GRATE TYPE II TOPS	11-08-22
	HW-586_07d	CATCH BASIN TYPE "C-G" AND "C-M" BARRIER CURB TOPS	11-09-22
	HW-586_08	CATCH BASIN FRAMES AND GRATES	11-09-22
	HW-586_09	CATCH BASIN LOCK DOWN TOPS	11-09-22
	HW-586_10a	MANHOLE FRAME AND COVER	01-05-24
	HW-586_10b	MANHOLE FRAME AND GRATE	01-05-24
	HW-586_10c	REINFORCED PRECAST CONCRETE MANHOLE	11-08-22
	HW-586_10d	MANHOLE NON-PRECAST CONCRETE UNIT	11-08-22
	HW-686_01a	CONCRETE PIPE CONNECTION SHEET 1	11-08-22
	HW-686_01b	CONCRETE PIPE CONNECTION SHEET 2	11-08-22
	HW-686_02a	DRAINAGE PIPE ENDS SHEET 1 [CORRUGATED METAL PIPE]	11-08-22
	HW-686_02b	DRAINAGE PIPE ENDS SHEET 2 [CONCRETE PIPE]	11-08-22
	HW-751_01	UNDERDRAINS AND UNDERDRAIN OUTLETS	02-28-24
	HW-803_01	PAVED APRONS	11-08-22
	HW-811_01	CONCRETE CURBING	11-08-22
	HW-813_01	GRANITE STONE TRANSITION CURBING	11-08-22
	HW-813_02	STONE CURBING	11-08-22
	HW-815_01	BITUMINOUS CONCRETE CURBING	11-08-22

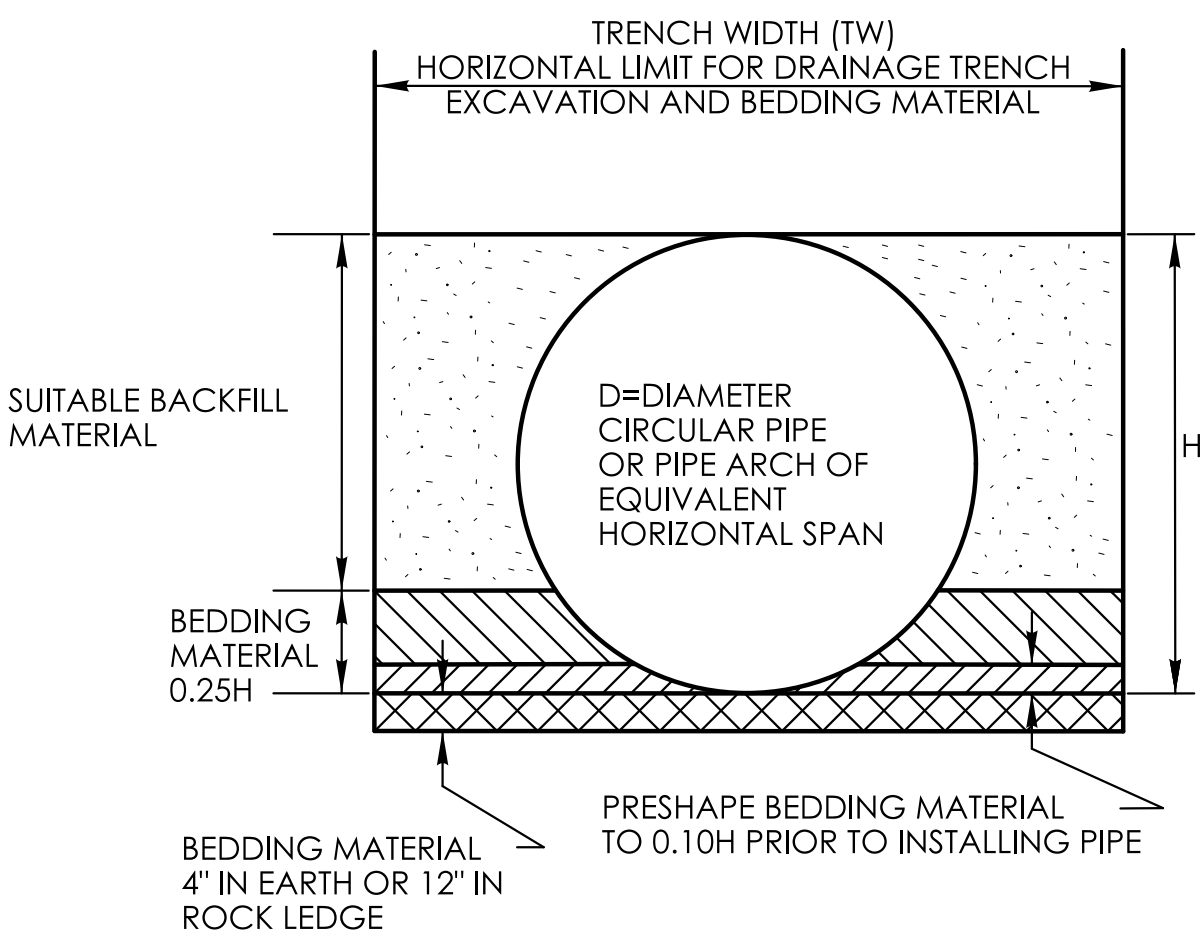
✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-821_01a	TRANSITION - 45" F-SHAPE TO 45" VERTICAL SHAPE SHEET 1	11-08-22
	HW-821_01b	TRANSITION - 45" F-SHAPE TO 45" VERTICAL SHAPE SHEET 2	11-08-22
	HW-821_01c	TRANSITION - 45" F-SHAPE TO 45" VERTICAL SHAPE SHEET 3	11-08-22
	HW-821_02a	45" F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 1	11-08-22
	HW-821_02b	45" F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 2	11-08-22
	HW-821_03a	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 1	11-08-22
	HW-821_03b	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 2	11-08-22
	HW-821_03c	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 3	11-08-22
	HW-821_03d	TRANSITION - 32" JERSEY SHAPE TO 45" VERTICAL SHAPE SHEET 4	11-08-22
	HW-821_03e	TRANSITION - 32" JERSEY SHAPE TO 45" F-SHAPE	11-08-22
	HW-821_04a	MERRITT PARKWAY NARROW MEDIAN BARRIER	11-08-22
	HW-821_04b	MERRITT PARKWAY - 2' WIDE MEDIAN BARRIER AND ROADSIDE BARRIER	11-08-22
	HW-821_05a	TRANSITION - 45" F-SHAPE TO 54" VERTICAL SHAPE SHEET 1	11-08-22
	HW-821_05b	TRANSITION - 45" F-SHAPE TO 54" VERTICAL SHAPE SHEET 2	11-08-22
	HW-821_06	54" VERTICAL SHAPE BARRIER	11-08-22
	HW-821_07	MISCELLANOUS DETAILS FOR BARRIER TRANSITIONS	11-08-22
	HW-821_08a	F-SHAPE CONC. BARRIER CURB (21"x45") TRANSITION FOR THRIE-BEAM	11-08-22
	HW-821_08b	F-SHAPE CONC. BARRIER CURB (21"x45") TRANSITION FOR THRIE-BEAM - REINF.	11-08-22
	HW-821_09a	SINGLE SLOPE CONC. BARRIER CURB (20"x42") TRANS. FOR THRIE-BEAM	11-08-22
	HW-821_09b	SINGLE SLOPE CONC. BARRIER CURB (20"x42") TRANS. FOR THRIE-BEAM - REINF.	11-08-22
	HW-821_10a	VERTICAL FACE CONC. (21"x54") TRANSITION FOR THRIE-BEAM	11-08-22
	HW-821_10b	VERTICAL FACE CONC. (21"x54") TRANSITION FOR THRIE-BEAM REINF.	11-08-22
	HW-821_11a	42" SINGLE SLOPE PRECAST CONCRETE BARRIER CURB -SHEET 1	01-05-24
	HW-821_11b	42" SINGLE SLOPE PRECAST CONCRETE BARRIER CURB -SHEET 2	01-05-24
	HW-822_01	TEMPORARY PRECAST CONCRETE BARRIER CURB	11-08-22
	HW-822_02a	TEMPORARY TRAFFIC BARRIER - DETAILS	11-08-22
	HW-822_02b	TEMPORARY TRAFFIC BARRIER (BOLTED)	02-02-24
	HW-822_02c	TEMPORARY TRAFFIC BARRIER & TEMPORARY TRAFFIC BARRIER (PINNED)	11-08-22
	HW-905_01	STONE WALL FENCE	11-09-22
	HW-906_01	WIRE FENCE	11-08-22

*ONLY STANDARD SHEETS MARKED WITH AN " ✓ " ARE IN THIS PROJECT #

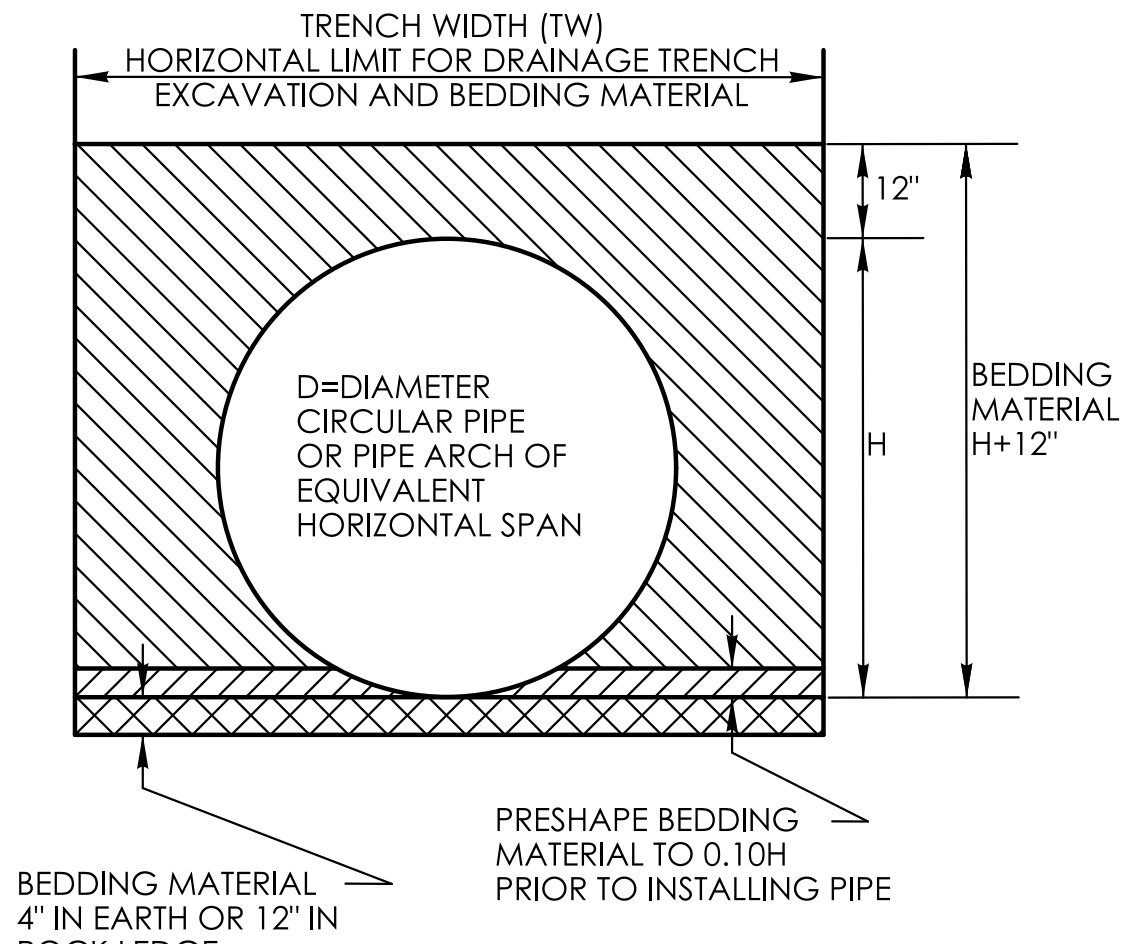
**REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-910_01	W-BEAM METAL BEAM RAIL HARDWARE	11-08-22
	HW-910_02	METAL BEAM RAIL (TYPE R-B 350) GUIDERAIL	11-08-22
	HW-910_03	METAL BEAM RAIL (TYPE MD-B 350) GUIDERAIL	11-08-22
	HW-910_04	METAL BEAM RAIL (TYPE R-B 350) SYSTEMS 5, 5A, & 6	11-08-22
	HW-910_05	METAL BEAM RAIL R-B 350 SPAN TYPE I, II, III SECTIONS	11-08-22
	HW-910_06	R-B 350 BRIDGE ATTACHMENT SAFETY SHAPE PARAPET	11-08-22
	HW-910_07	R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET	11-08-22
	HW-910 09a	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 1	11-08-22
	HW-910 09b	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 2	11-08-22
	HW-910 10	METAL BEAM RAIL 8" x 6" BOX BEAM	11-08-22
	HW-910 11	CURVED GUIDERAIL TREATMENT DETAIL	11-08-22
	HW-910_12a	MERRITT PARKWAY GUIDERAIL LEADING END ATTACHMENTS AND SYSTEMS 2&3	11-08-22
	HW-910_12b	MERRITT PARKWAY GUIDERAIL HARDWARE DETAILS	11-08-22
	HW-910_12c	MERRITT PARKWAY GUIDERAIL TRAILING END ATTACHMENTS	11-02-22
	HW-910_12d	MERRITT PARKWAY MEDIAN GUIDERAIL AND END ANCHOR	11-08-22
	HW-910_13a	THRIE-BEAM METAL BEAM RAIL HARDWARE	11-08-22
	HW-910_13b	THRIE-BEAM TRANSITIONS	11-08-22
	HW-910_14a	THRIE-BEAM 350 BRIDGE ATTACHMENT	11-08-22
	HW-910_14b	THRIE-BEAM 350 GUIDERAIL TRANSITION TO R-B 350 GUIDERAIL	11-08-22
	HW-910_15	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE I	11-08-22
	HW-910_16	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE II	11-08-22
	HW-910_17	R-B TERMINAL SECTION	11-08-22
	HW-910_18	METAL BEAM RAIL (TYPE MD-I) GUIDERAIL	11-08-22
	HW-910_19a	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE I	11-09-22
	HW-910_19b	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE II	11-08-22
	HW-910_19c	METAL BEAM RAIL (MODIFIED TYPE R-I) SYSTEMS 2 AND 3	11-08-22
	HW-910_20	MASH W-BEAM HARDWARE	01-05-24
	HW-910_21	METAL BEAM RAIL (R-B MASH) GUIDERAIL	01-05-24
	HW-910_22	METAL BEAM RAIL (MD-B MASH) GUIDERAIL	11-08-22
	HW-910_23	METAL BEAM RAIL (R-B MASH) HALF & QUARTER POST SPACING GUIDERAIL	11-08-22
	HW-910_24	METAL BEAM RAIL SPAN SECTION TYPES II AND III	11-08-22

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-910_25a	METAL BEAM RAIL TRANSITION 350 TO MASH	01-05-24
	HW-910_25b	METAL BEAM RAIL MEDIAN APPLICATION TRANSITION 350 TO MASH GUIDERAIL	01-05-24
	HW-910_26	THRIE-BEAM ATTACHMENT HARDWARE	11-08-22
	HW-910_27	THRIE-BEAM ATTACHMENT	11-08-22
	HW-910_29	THRIE-BEAM BRIDGE ATTACHMENT TRAILING END	02-02-24
	HW-911_01	R-B END ANCHORAGE TYPE I AND II	02-28-24
	HW-911_02	MD-B END ANCHORAGE TYPE I	02-28-24
	HW-911_03	ANCHOR IN EARTH CUT SLOPE & ANCHOR IN ROCK CUT SLOPE	01-05-24
	HW-911_05	MERRITT PARKWAY GUIDERAIL END ANCHORS	11-08-22
	HW-913_01a	CHAIN LINK FENCE	11-08-22
	HW-913_01b	CHAIN LINK FENCE HARDWARE	11-08-22
	HW-913_02	CHAIN LINK FENCE GATES	11-08-22
	HW-918_01a	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 1	11-08-22
	HW-918_01b	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 2	11-08-22
	HW-918_01c	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 3	11-08-22
	HW-921_01	CONCRETE SIDEWALKS	11-08-22
	HW-922_01	BITUMINOUS CONCRETE SIDEWALK AND BITUMINOUS CONCRETE DRIVEWAY	11-08-22
	HW-924_01	CONCRETE DRIVEWAY RAMPS	11-09-22
	HW-930_01	OBJECT MARKER (MAINTENANCE)	02-28-24
	HW-949_01a	LANDSCAPE PLANTING	11-09-22
	HW-949_01b	TREE STAKING	11-02-22
	HW-1800_01	GRADING PLAN FOR IMPACT ATTENUATION SYSTEMS (FLARED AND TANGENTIAL)	11-02-22
	HW-1800 02	GRADING PLAN FOR IMPACT ATTENUATION SYSTEMS (MEDIAN/GORE)	11-02-22



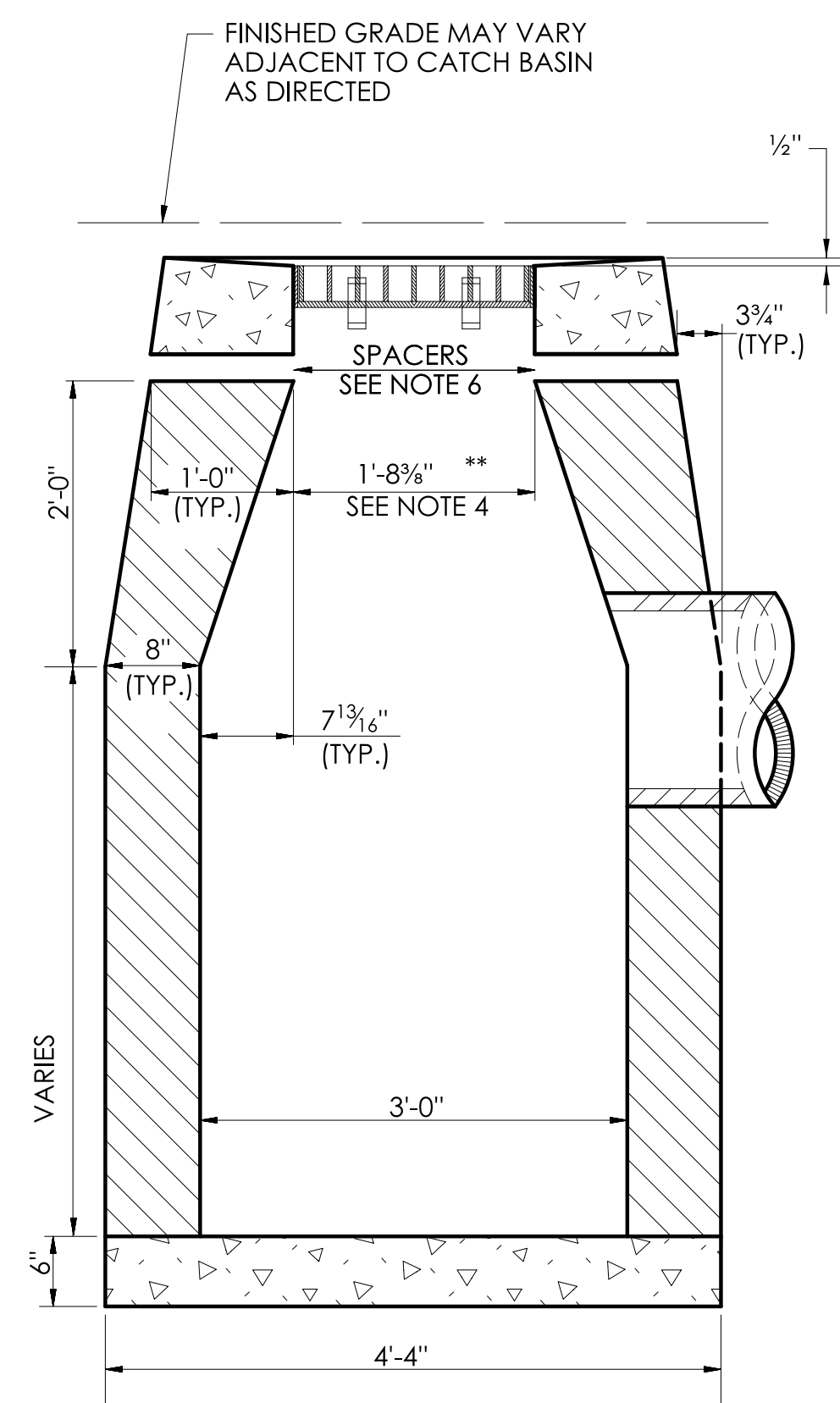
PIPE TRENCH
FOR PIPES LESS THAN 48"



PIPE TRENCH
FOR PIPES GREATER THAN
OR EQUAL TO 48"

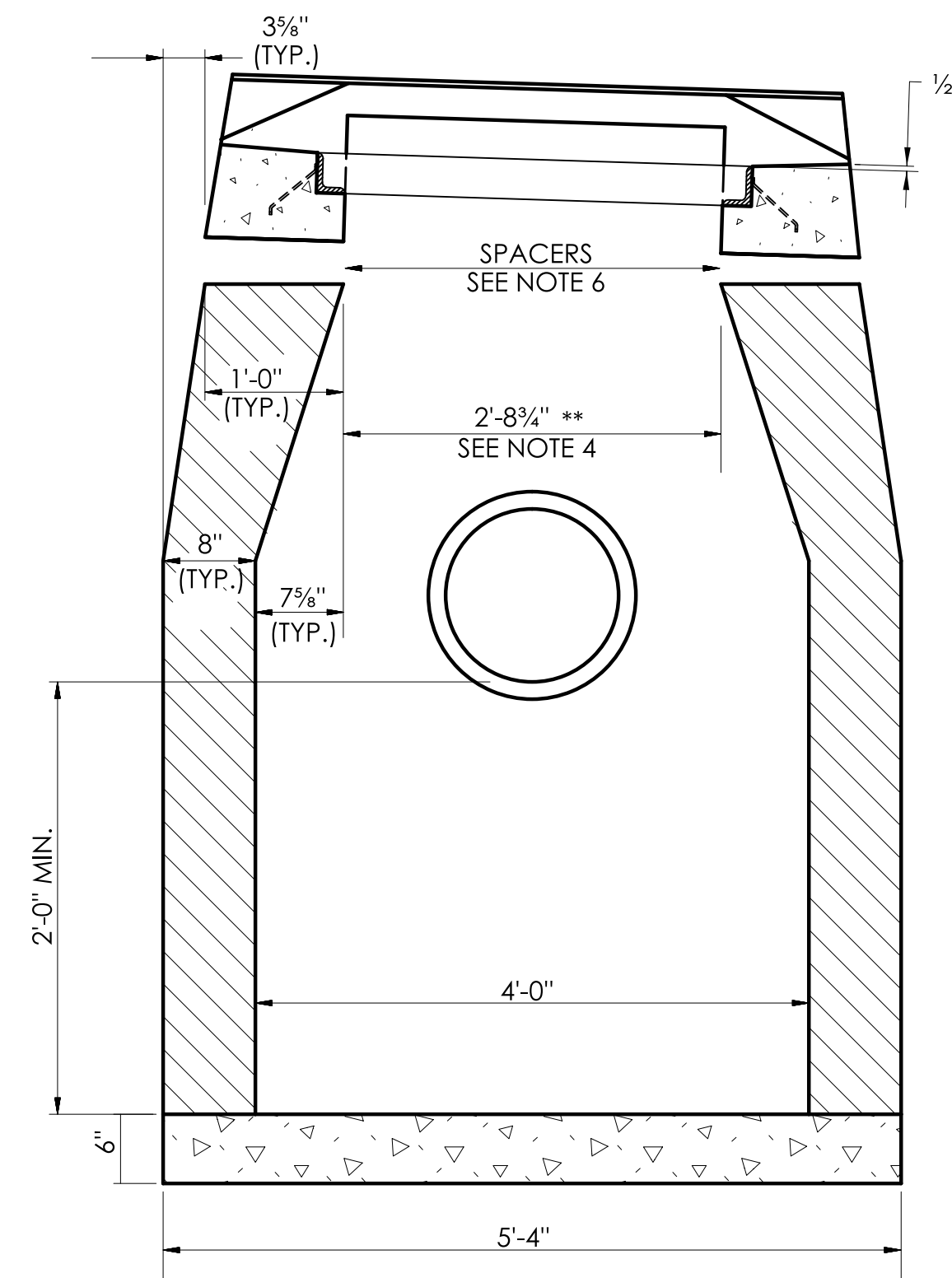
TRENCH WIDTH (TW) CHART

PIPE, PIPE-ARCH, OR DRAINAGE STRUCTURE	TRENCH WIDTH
PIPE OR PIPE-ARCH WITH NOMINAL INSIDE HORIZONTAL SPAN LESS THAN 30'	2' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
PIPE OR PIPE-ARCH WITH NOMINAL INSIDE HORIZONTAL SPAN GREATER THAN OR EQUAL TO 30'	3' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
PIPE OR PIPE-ARCH FABRICATED FROM STRUCTURAL PLATES	4' GREATER THAN NOMINAL INSIDE HORIZONTAL SPAN
DRAINAGE STRUCTURES	2' BEYOND ALL EXTERIOR OR FOUNDATION WALLS



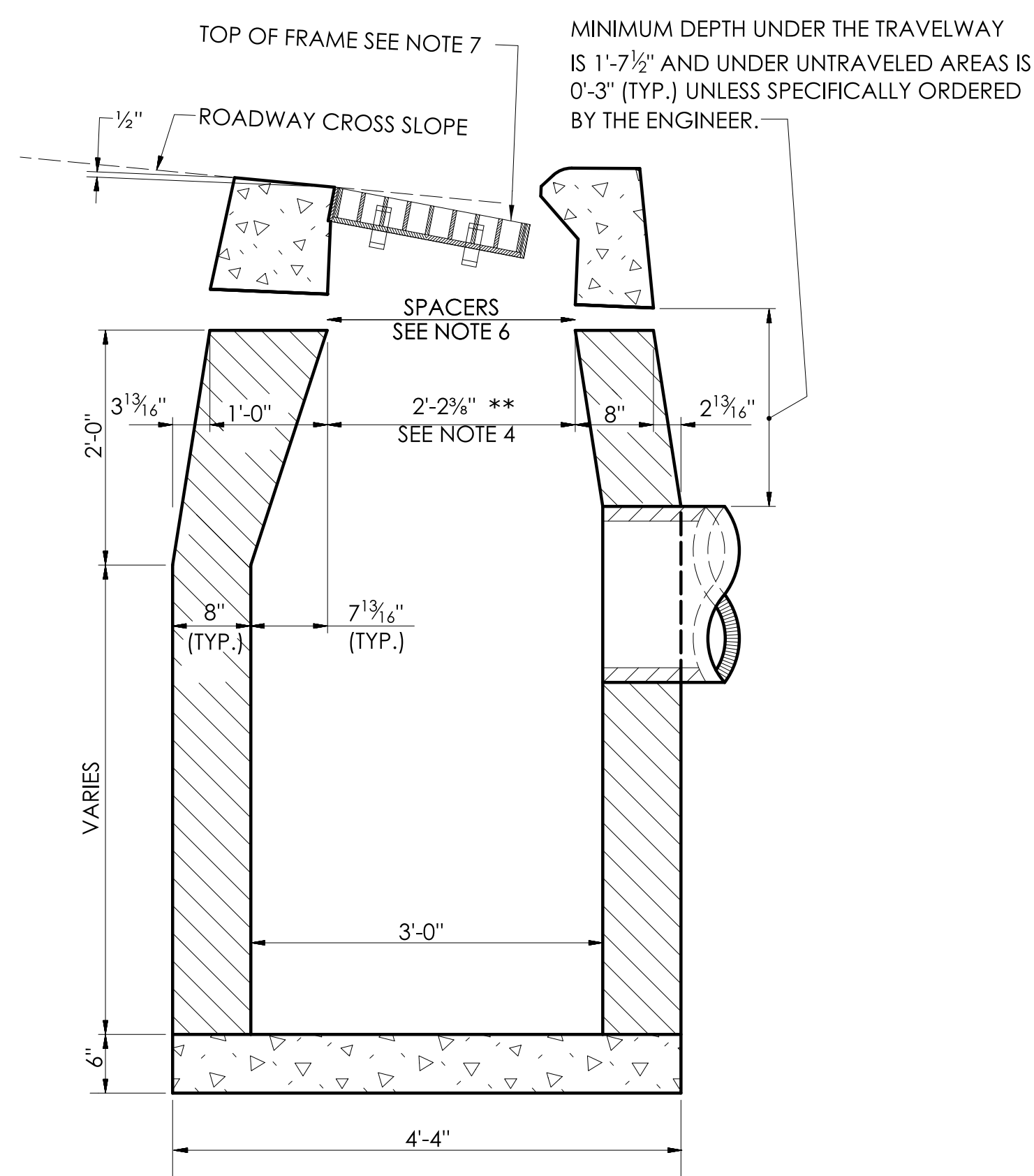
SECTION **B**

TYPE "C-L" CATCH BASIN



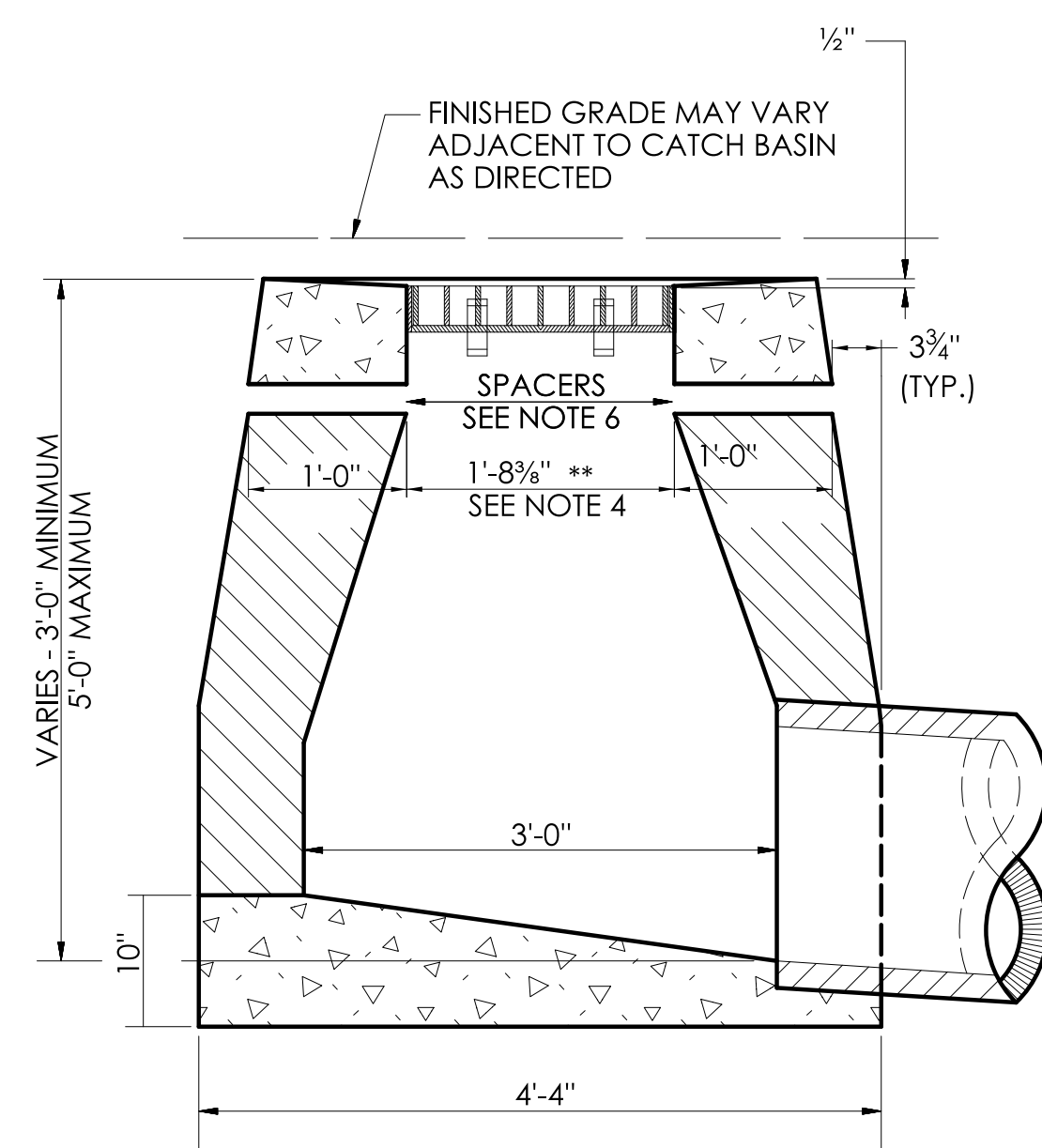
SECTION A

**TYPE "C" & "C-L" CATCH BASIN
(TYPE "C" TOP SHOWN)**



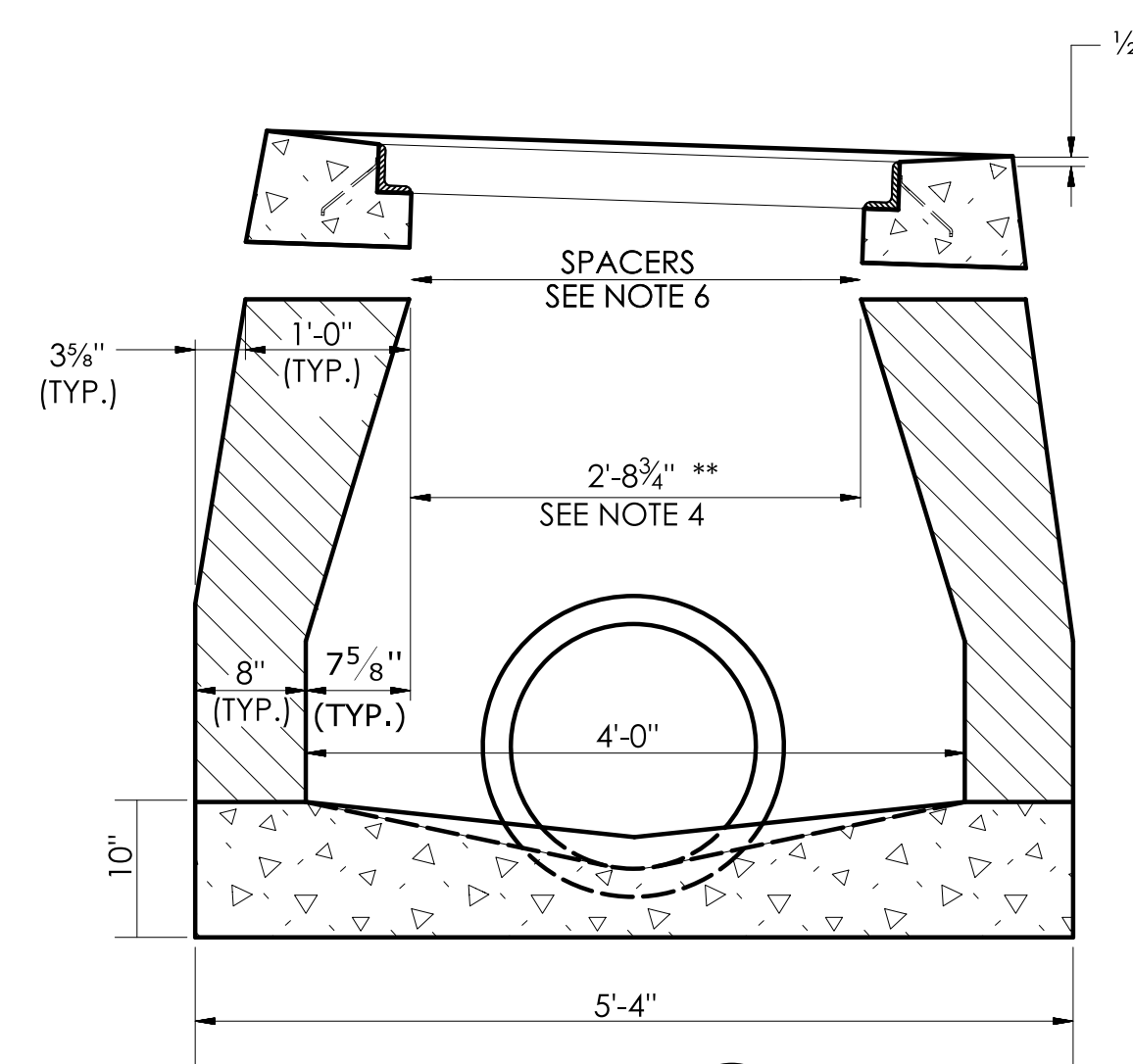
SECTION B

TYPE "C" CATCH BASIN



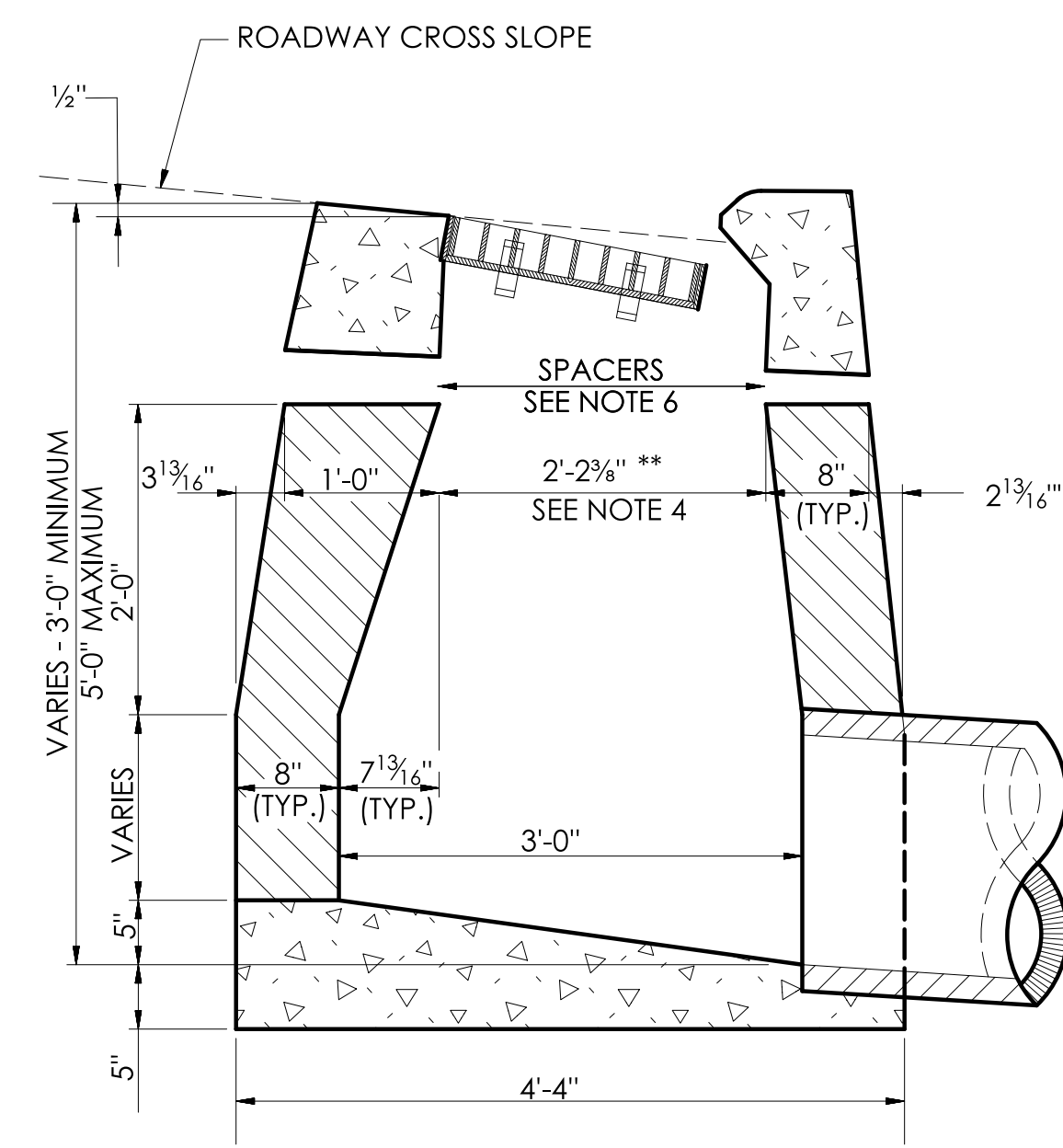
SECTION B


TYPE "C-L" DROP INLET

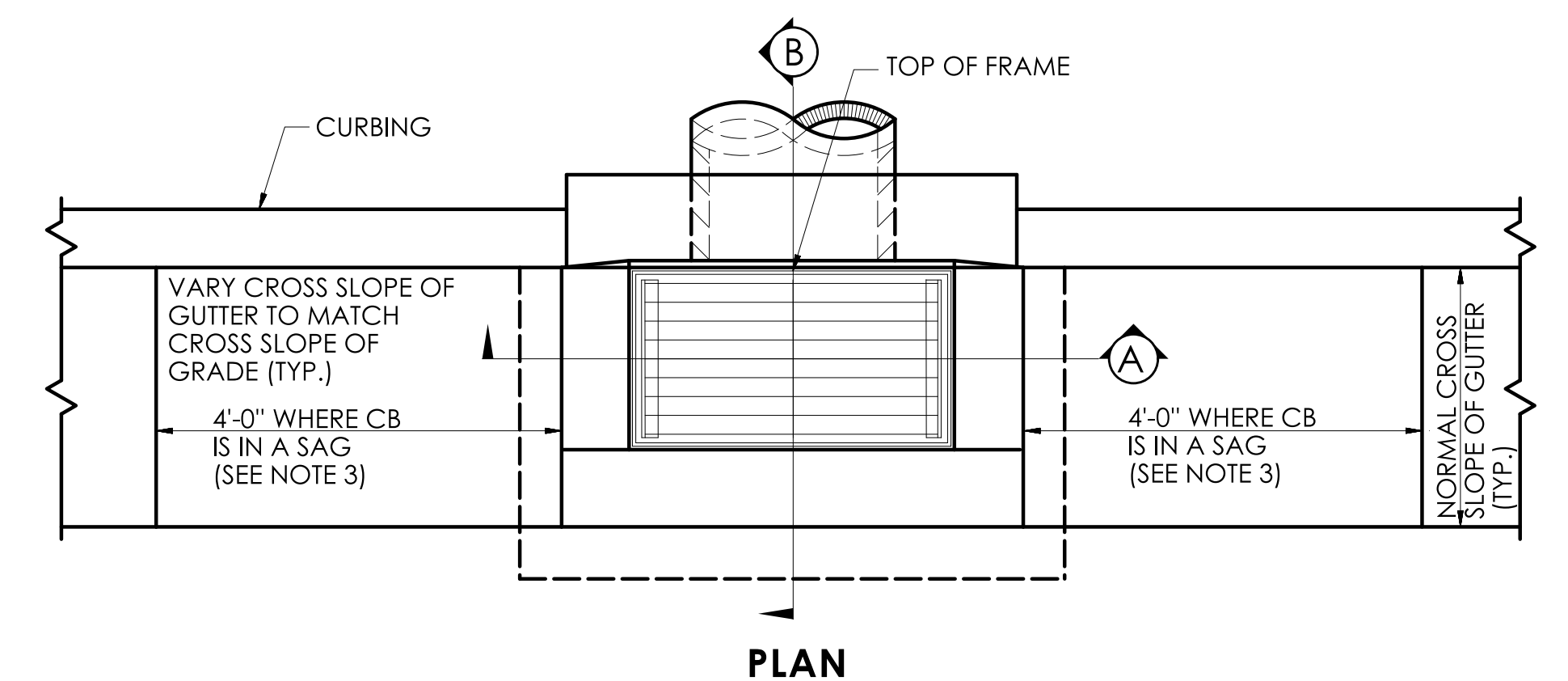


SECTION (A)

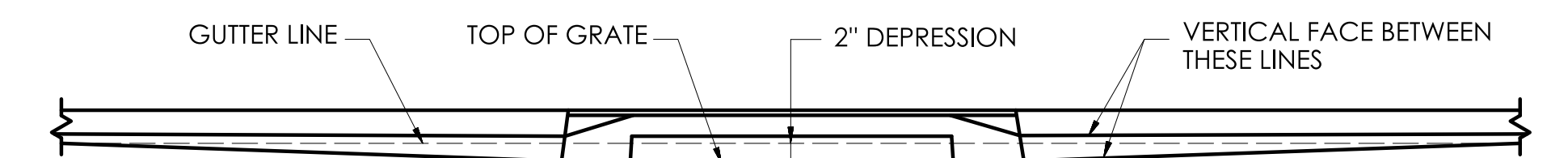
**TYPE "C" & "C-L" DROP INLET
(TYPE "C-L" TOP SHOWN)**



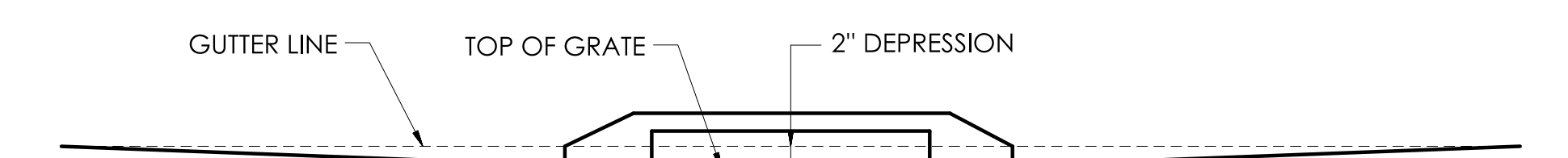
SECTION 
TYPE "C" DROP INLET



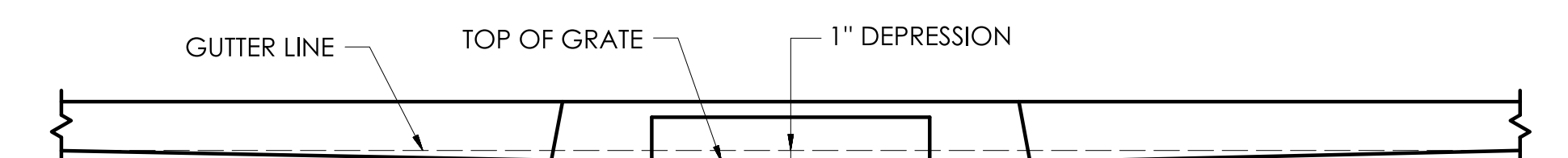
PLAN



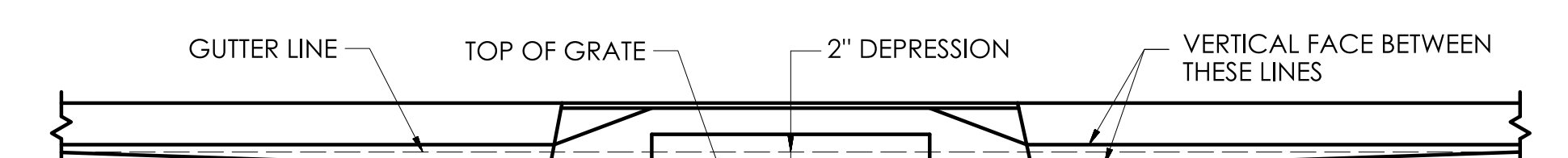
CATCH BASINS IN A LINE WITH 4" CONCRETE PARK CURBING OR 4" BITUMINOUS CONCRETE PARK CURBING



CATCH BASINS WHERE NO CURBING OF ANY TYPE EXISTS OR IS PROPOSED



CATCH BASINS IN A LINE WITH 6" CONCRETE CURBING OR 6" STONE CURBING



CATCH BASINS IN A LINE WITH 6" BITUMINOUS CONCRETE LIP CURBING (MACHINE FORMED)

DETAILS OF DEPRESSED GUTTER STRIP FOR TYPE "C" CATCH BASIN

GENERAL NOTES:

1. FOR CATCH BASIN TOPS, SEE SHEET NO. HW-586_07.
2. ALL FACES OF STRUCTURES IN CONTACT WITH CONCRETE PAVEMENT SHALL BE COVERED WITH A LAYER OF TAR PAPER OR APPROVED EQUAL.
3. USE 6'-0" ON UPGRADE SIDE (SEE PLAN VIEW) OF CONTINUOUS GRADE AND 1'-0" ON DOWNGRADE SIDE OF CONTINUOUS GRADE OR AS DIRECTED BY THE ENGINEER.
4. IF MASONRY UNITS ARE REQUIRED, THE BASIN SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE DIMENSIONS SHOWN. CORBELLING SHALL BE PERMITTED TO A MAXIMUM OF 3". NO PROJECTION SHALL EXTEND INSIDE THE LIMITS FOR THE CATCH BASIN OPENINGS SHOWN IN THE SECTION VIEWS **.
5. WALL THICKNESS OF ALL CATCH BASINS OVER 10' DEEP SHALL BE INCREASED TO 12" THICK. INSIDE DIMENSION SHALL REMAIN THE SAME. 12" THICKNESS SHALL START AFTER THE FIRST 10'.
6. FOR GRADE ADJUSTMENT OF THE CATCH BASIN TOP (TO MATCH THE ROADWAYS' PROFILE AND CROSS-SLOPE SHOWN ON THE PLANS) USE: SPACERS EITHER CONCRETE MASONRY UNIT OR PRECAST WITH THE REQUIRED REINFORCING (RECOMMENDED BY THE MANUFACTURER) COMBINED WITH MORTAR AS NEEDED TO PROVIDE THE PROPER GRADE ADJUSTMENTS.
7. TOP OF FRAME ELEVATION SHALL BE MEASURED IN THE CENTER OF GRATE AT GUTTER LINE.

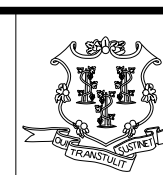
NOT TO SCALE

SIGNATURE BLOCK:

OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

SUBMITTED BY:

APPROVED BY:



STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION



CTDOT
STANDARD SHEET

STANDARD SHEET TITLE:

CATCH BASIN AND DROP INLET TYPES "C" AND "C-L" STRUCTURES

STANDARD SHEET NO.:

W- 586_01

PLOTTED DATE: 12/28/2023

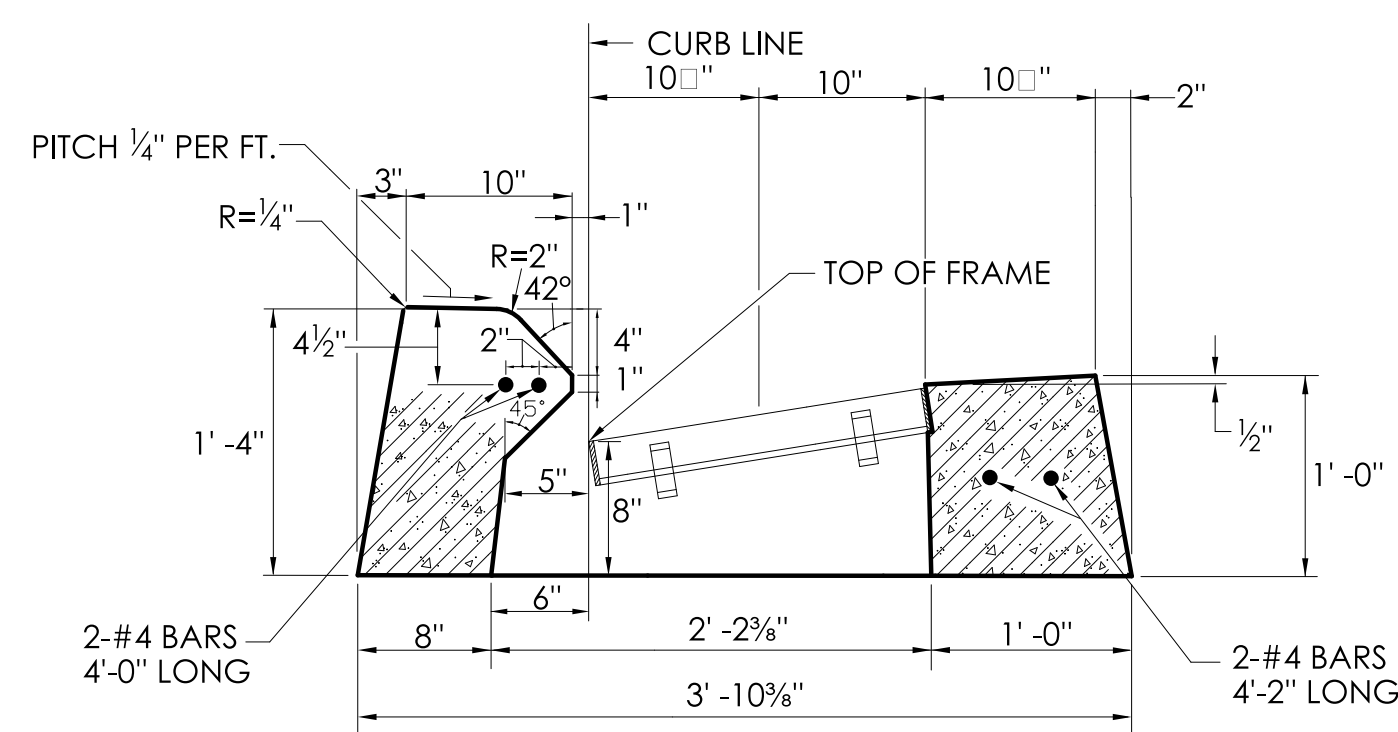
1. SEE SHEET HW-586_08, FOR CATCH BASIN FRAMES AND GRATES
AND HW-586_09 FOR CATCH BASIN LOCK DOWN TOPS.
2. SEE SHEET HW-586_01, CATCH BASIN AND DROP INLET TYPES "C" AND "C-L"
TO DETERMINE THE TOP OF FRAME DEPRESSION AT THE GUTTER.
3. ALL BARS SHALL HAVE A MINIMUM 2" COVER.
4. Manufacturing Dimensional Tolerance Table

A diagram of a rectangular structure, possibly a container or a frame, with a grid of small squares inside. The grid is 10 columns wide and 10 rows high. Two circular markers are present: one labeled 'A' on the right side and one labeled 'B' at the top center. Both markers have a horizontal line through the center and a shaded, arrow-like shape pointing to the right. The diagram is centered on a horizontal and vertical axis.

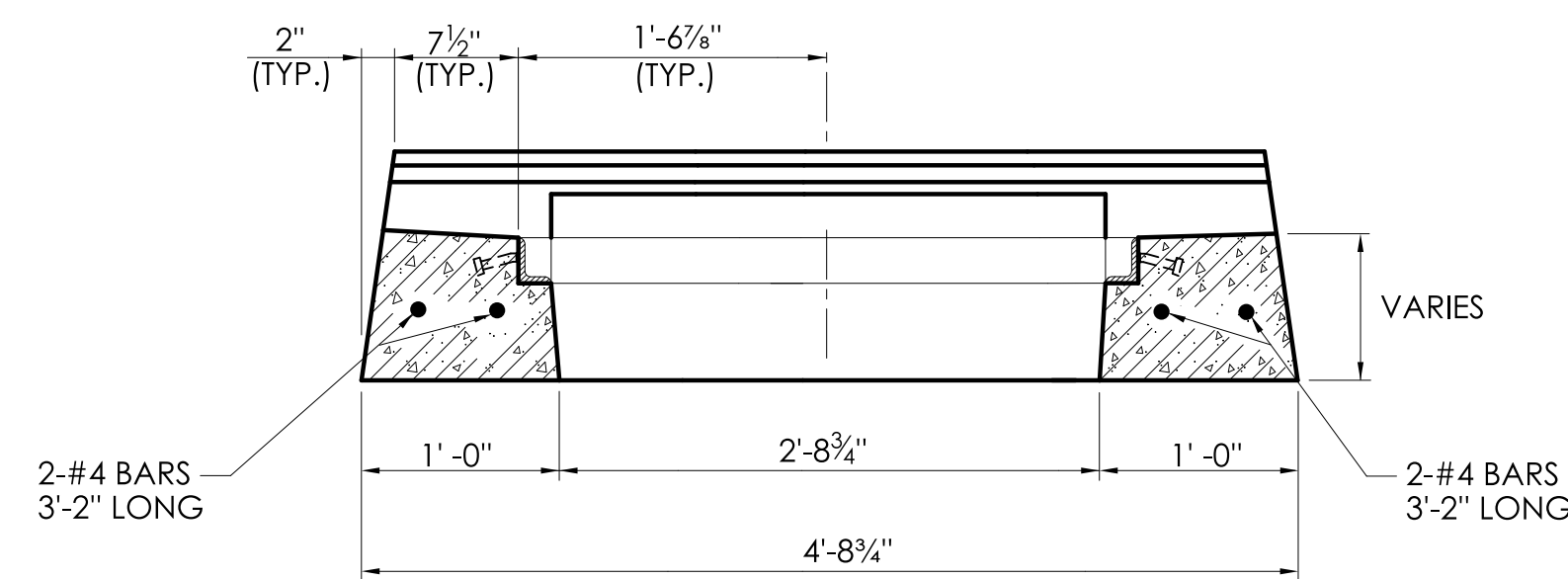
A 3D perspective drawing of a rectangular tray. The tray has a thick, raised rim and a flat base. The base is covered with a grid of small, square compartments, resembling a seedling tray or a baking dish. The drawing is done in a simple line-art style with black outlines on a white background.

[illegible]

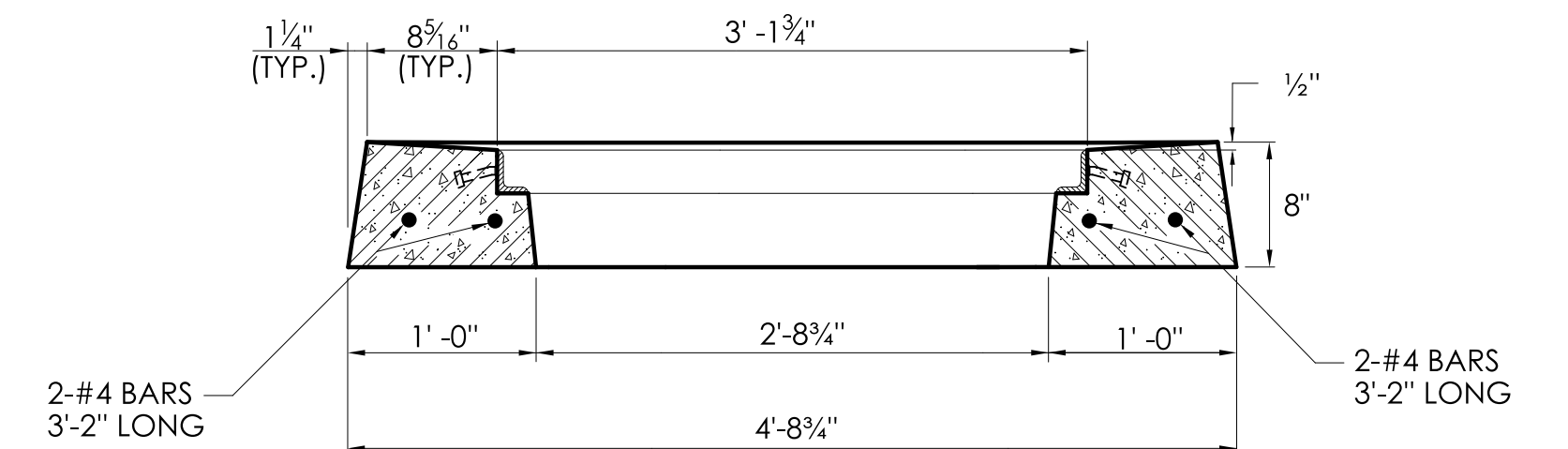
SECTION B



SECTION B

[illegible]

SECTION B

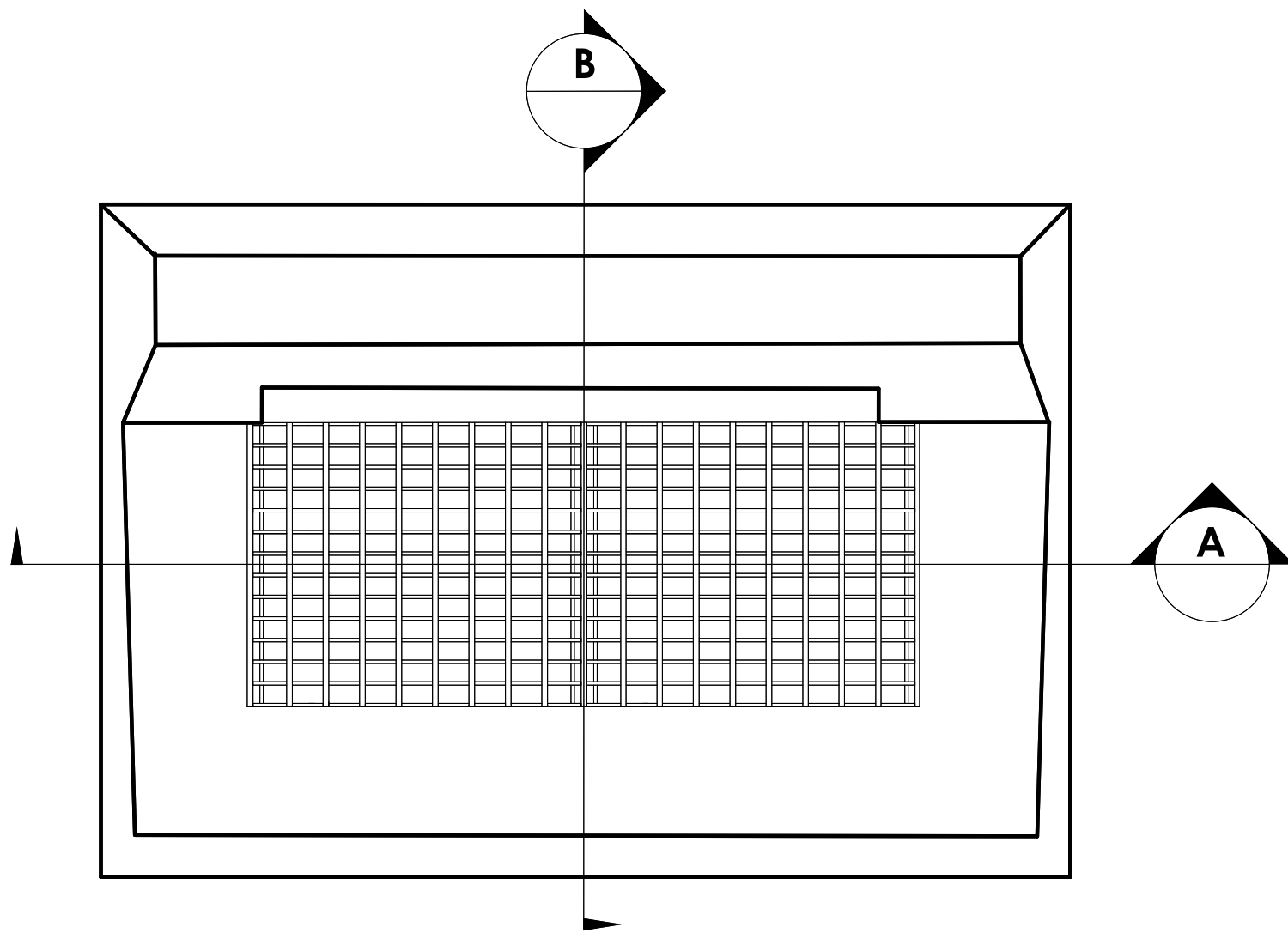
[illegible]

TYPE "C-L" CATCH BASIN TOP
SECTION D

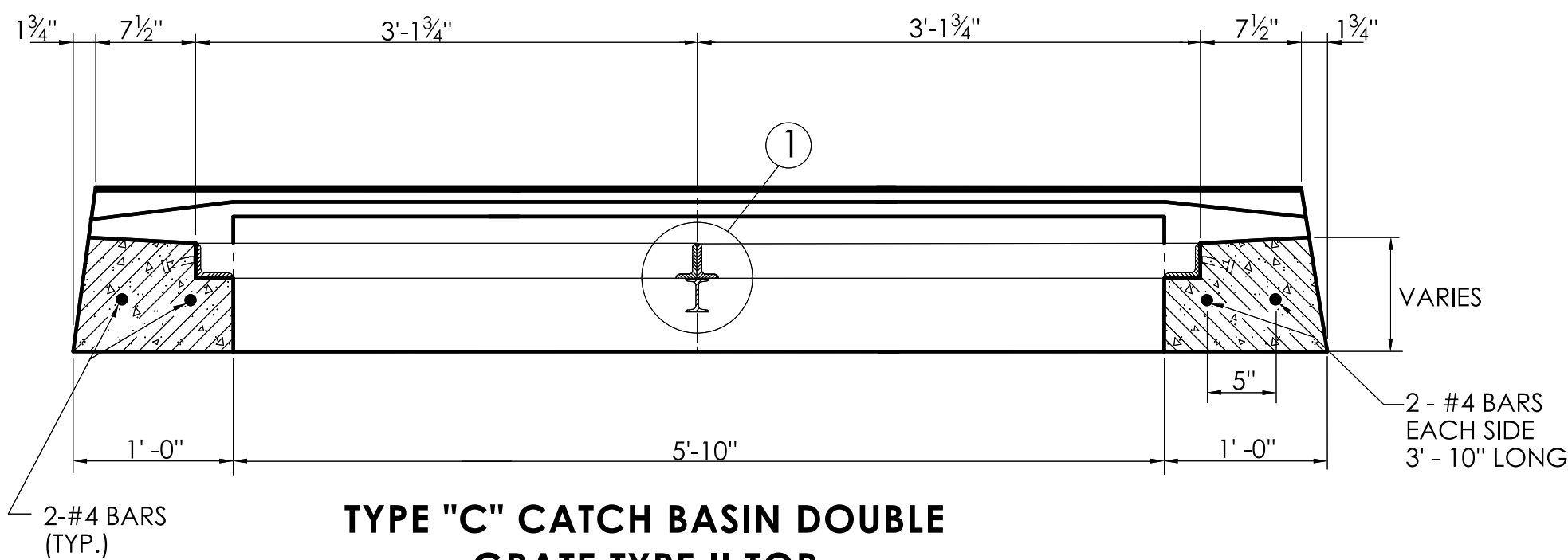
W- 586_07a

- GENERAL NOTES:**
- SEE SHEET HW-586_08 FOR CATCH BASIN FRAMES AND GRATES, AND HW-586_09 CATCH BASIN LOCK DOWN TOPS.
 - SEE SHEET HW-586_03 CATCH BASIN TOPS (TYPE "C" AND "C-L") FOR DOUBLE GRATE TYPE II, TO DETERMINE THE TOP OF FRAME AT THE GUTTER.
 - ALL BARS SHALL HAVE A MINIMUM 2" COVER
 - MANUFACTURING DIMENSIONAL TOLERANCE TABLE

Any Dimension (D)	Allowable Tolerance
D < 5"	± ¼"
5" ≤ D ≤ 10"	± ½"
D > 10"	± 1"

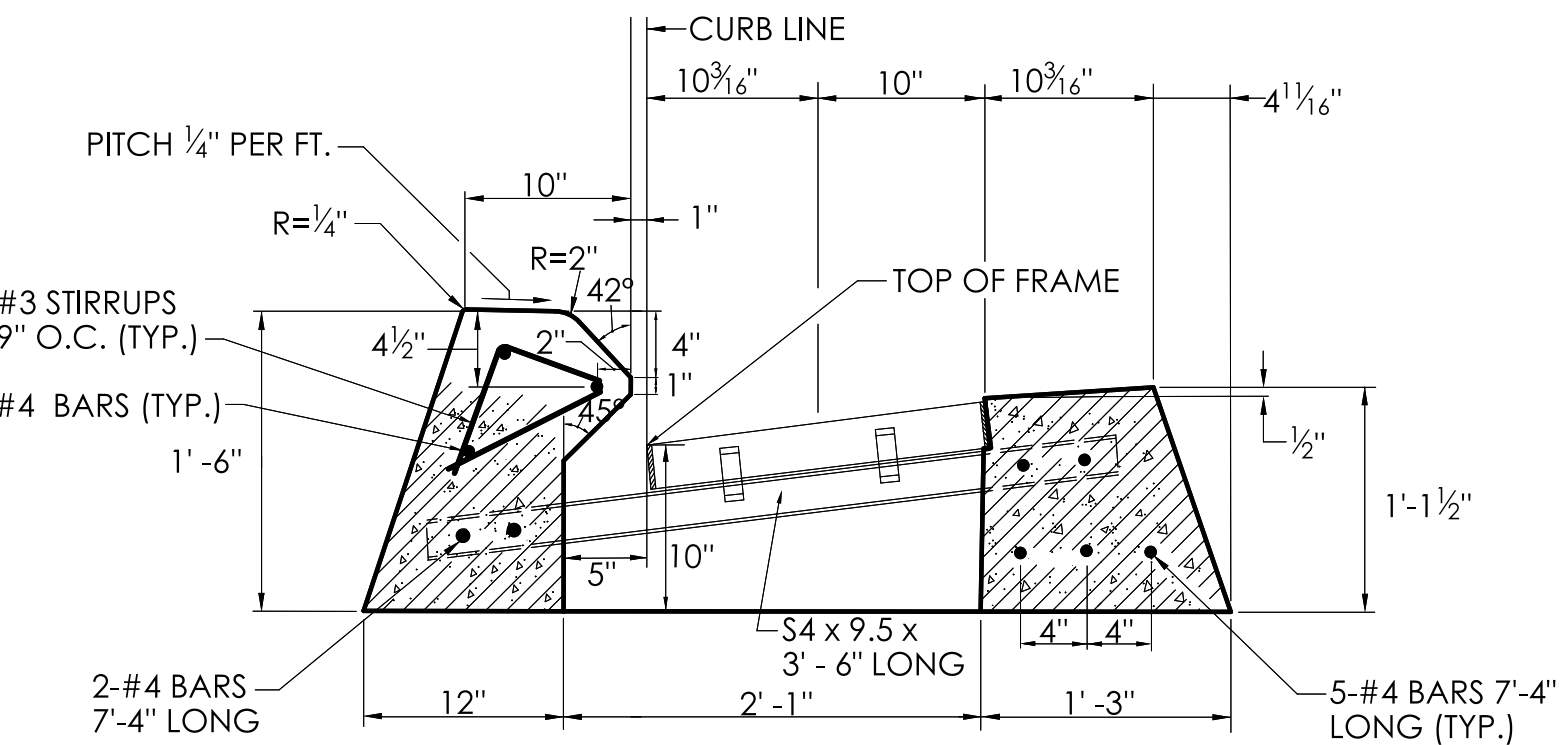


TYPE "C" CATCH BASIN DOUBLE
GRATE TYPE II TOP
PLAN



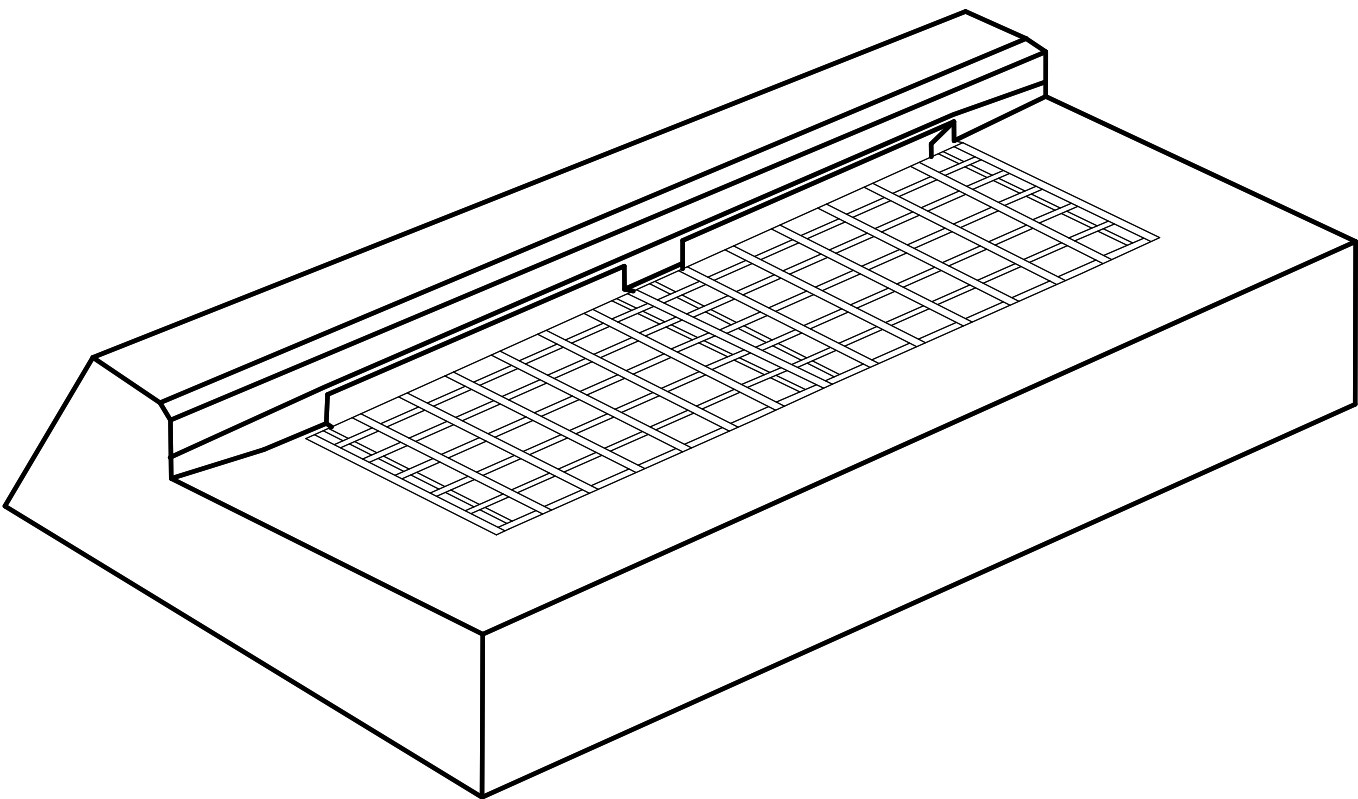
TYPE "C" CATCH BASIN DOUBLE
GRATE TYPE II TOP

SECTION A

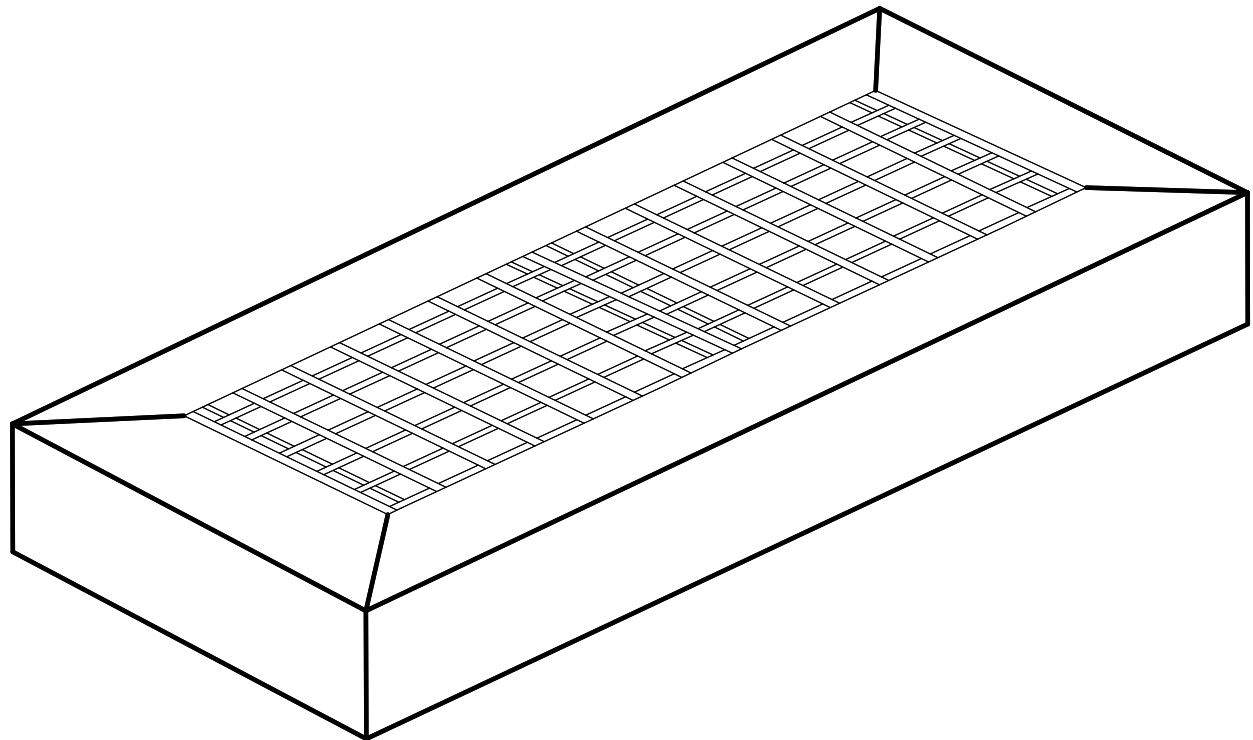


TYPE "C" CATCH BASIN DOUBLE GRATE TYPE II TOP FOR
6" BITUMINOUS CONCRETE LIP CURBING

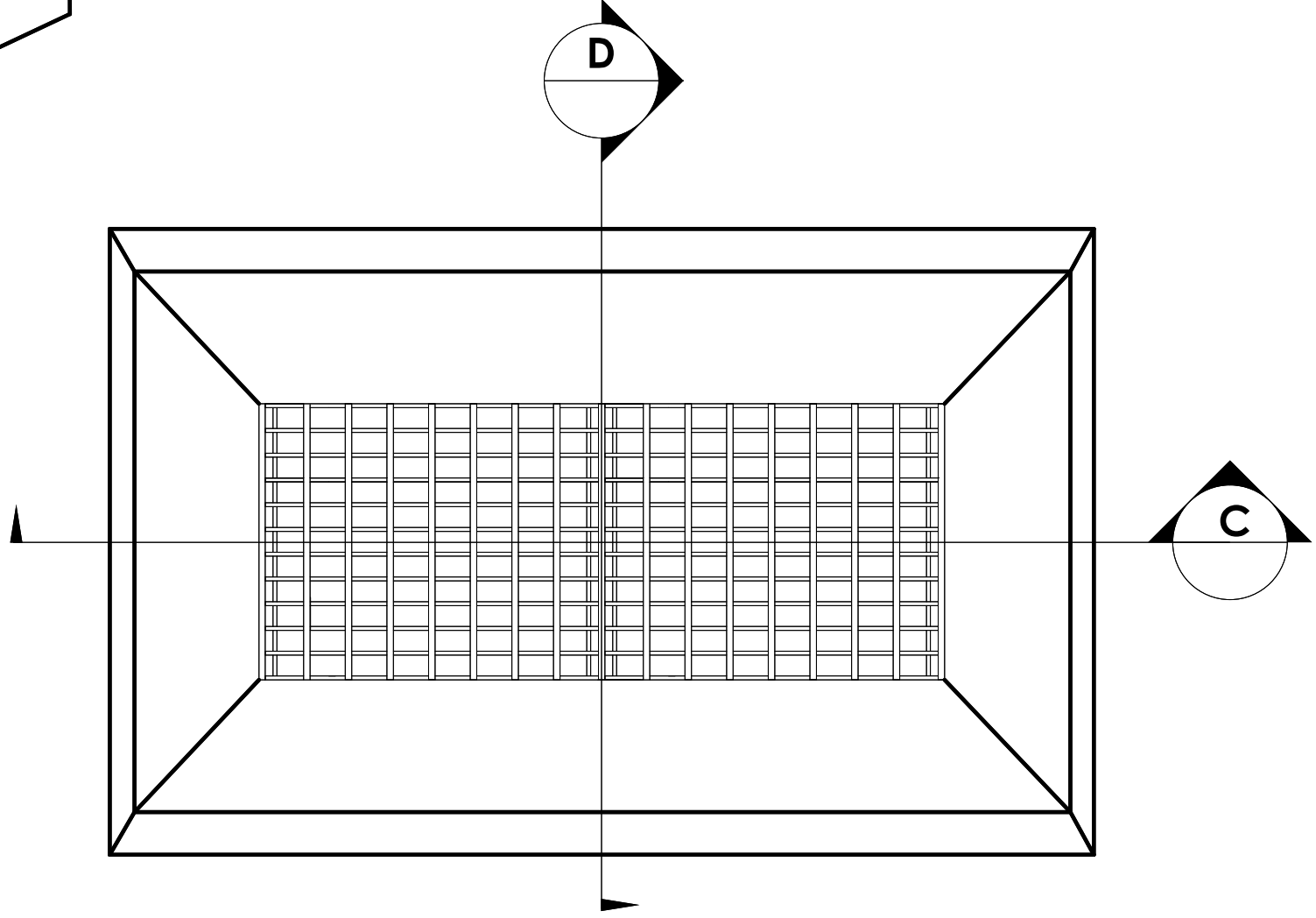
SECTION B



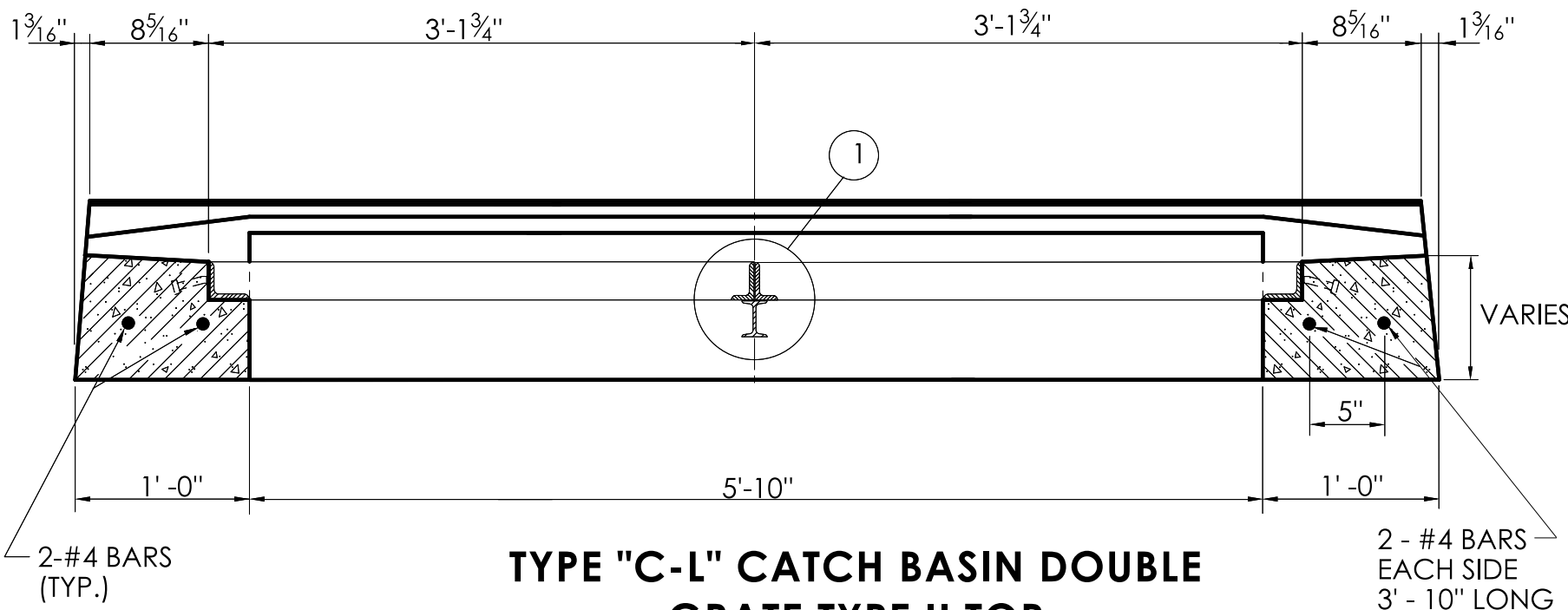
TYPE "C" CATCH BASIN DOUBLE
GRATE TYPE II TOP



TYPE "C-L" CATCH BASIN DOUBLE
GRATE TYPE II TOP

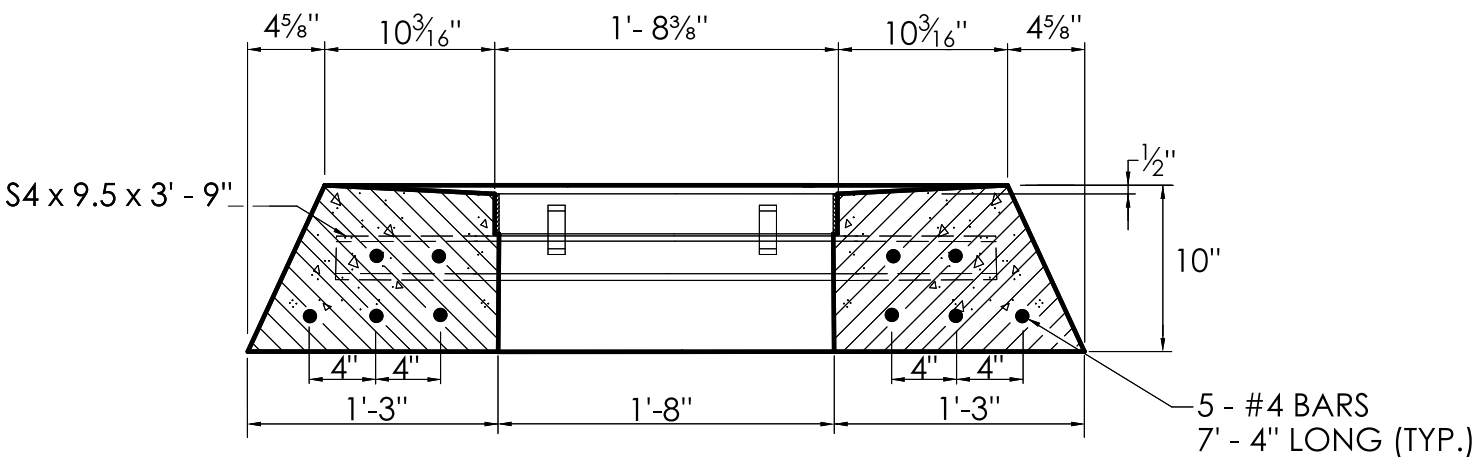


TYPE "C-L" CATCH BASIN DOUBLE
GRATE TYPE II TOP
PLAN



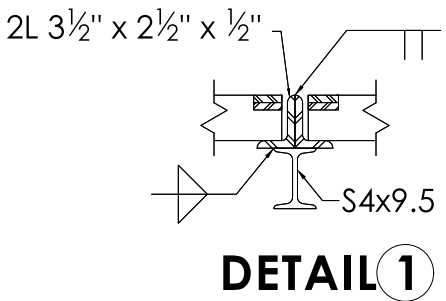
TYPE "C-L" CATCH BASIN DOUBLE
GRATE TYPE II TOP

SECTION C

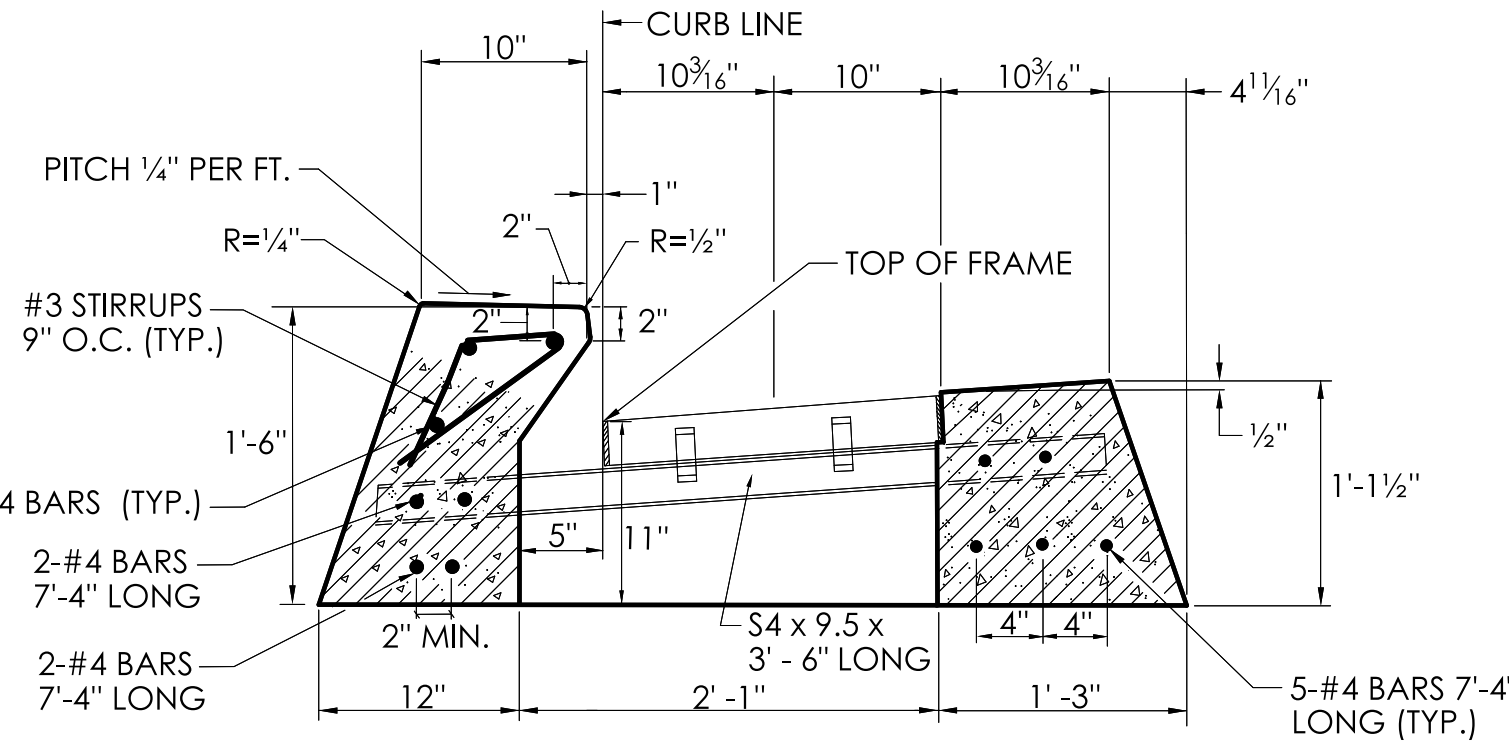


TYPE "C-L" CATCH BASIN DOUBLE
GRATE TYPE II TOP

SECTION D

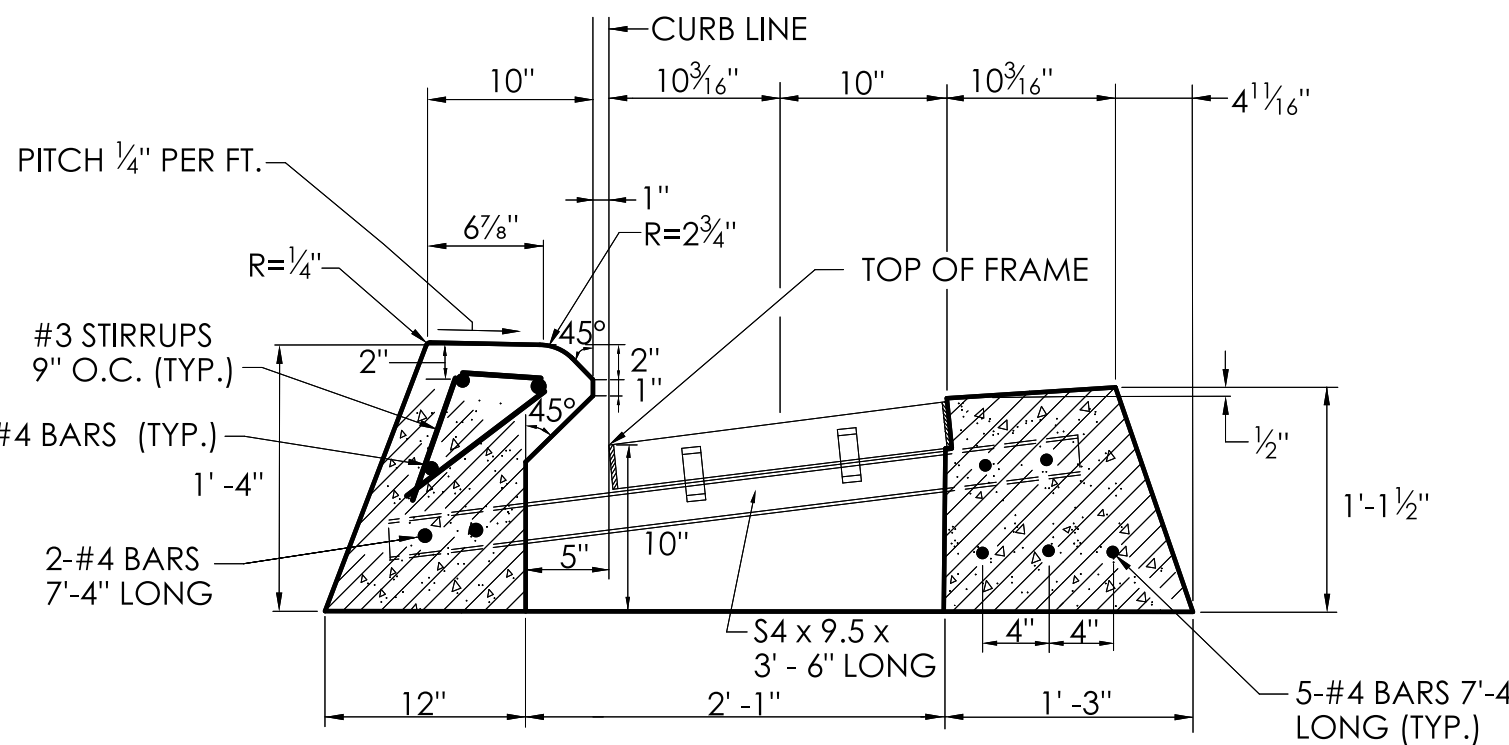


DETAIL 1



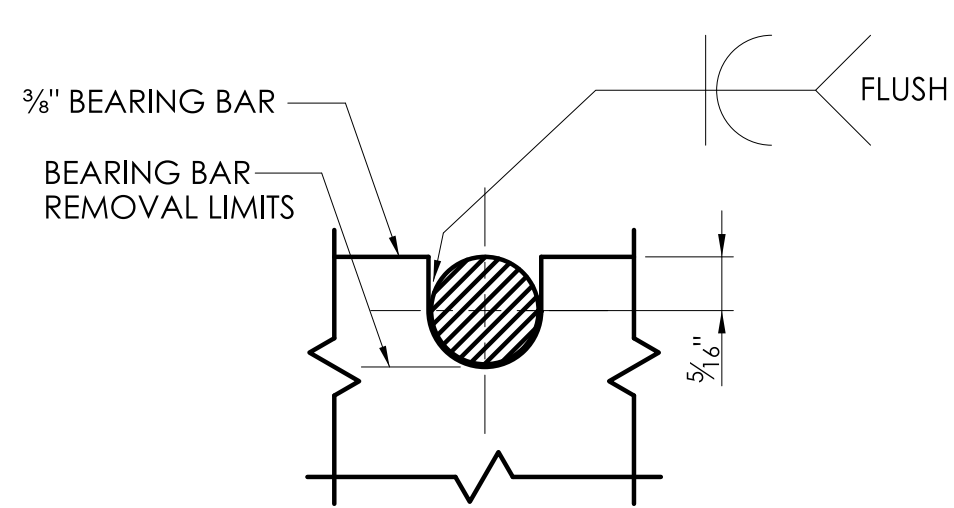
TYPE "C" CATCH BASIN DOUBLE GRATE TYPE II TOP FOR
6" CONCRETE CURBING OR 6" STONE CURBING

SECTION B



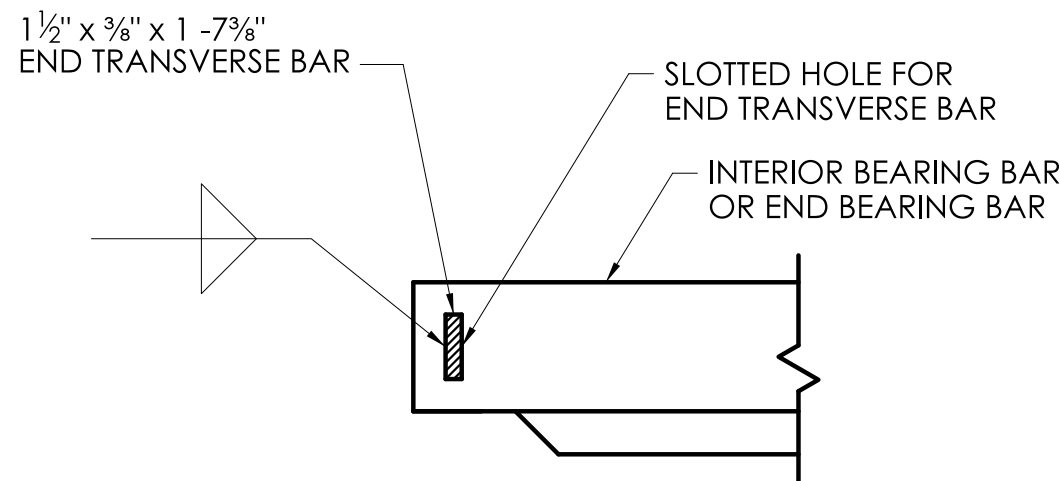
TYPE "C" CATCH BASIN DOUBLE GRATE TYPE II TOP FOR
4" CONCRETE PARK CURBING OR
4" BITUMINOUS CONCRETE PARK CURBING

SECTION B

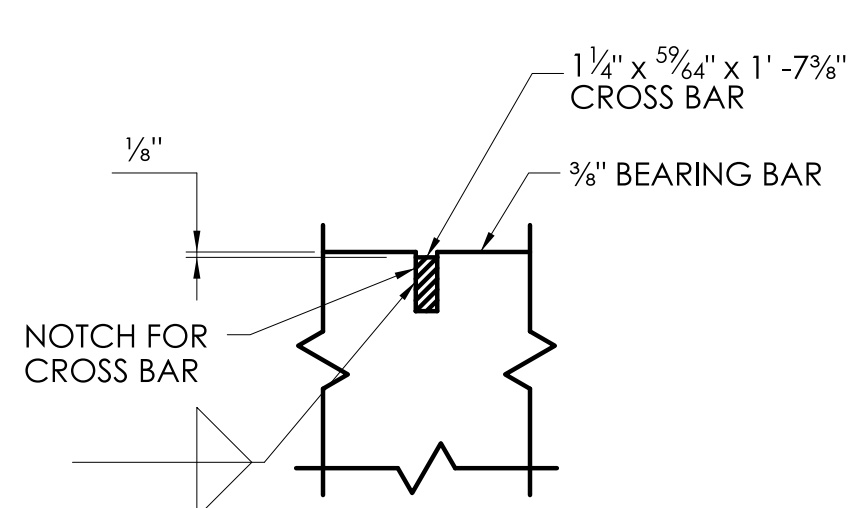


NOTE:
3/8" DIA. ROUND BAR SHALL CONTACT BEARING BAR AT BOTTOM AND BE FLUSH AT TOP.

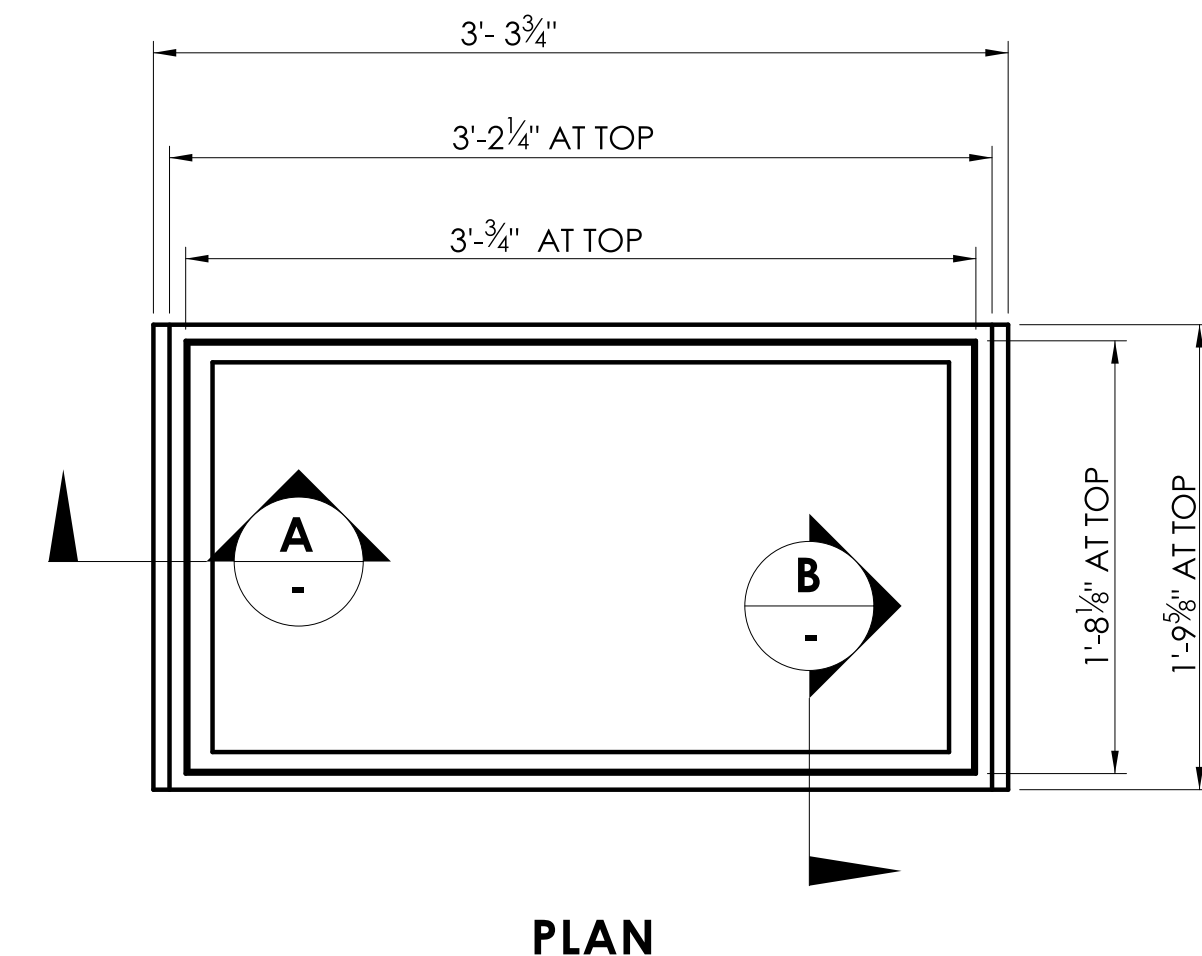
**ROUND BAR ATTACHMENT
CATCH BASIN GRATE TYPE A**



**END TRANSVERSE BAR ATTACHMENT
CATCH BASIN GRATE TYPE A AND B**

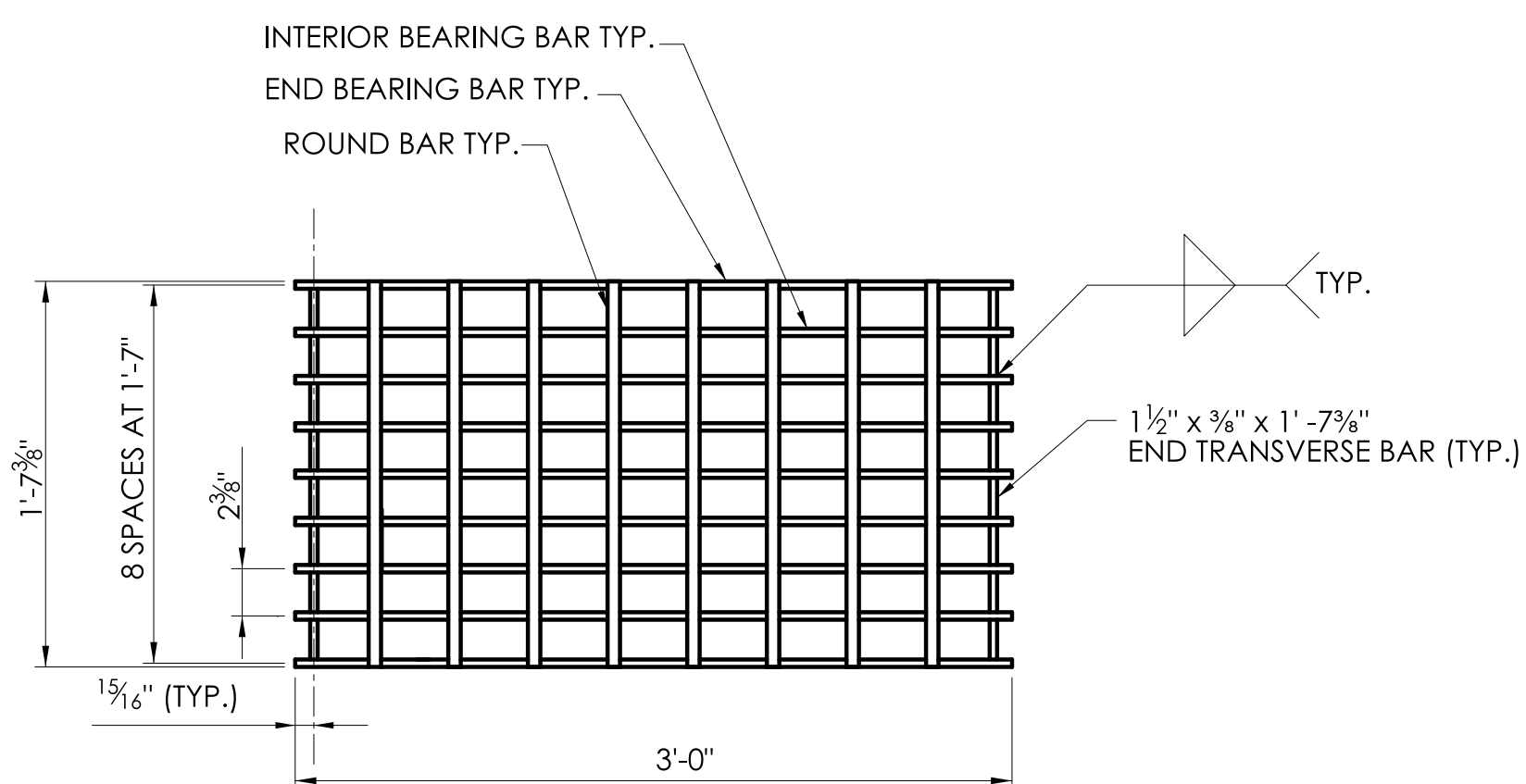


**CROSS BAR ATTACHMENT
CATCH BASIN GRATE TYPE B**

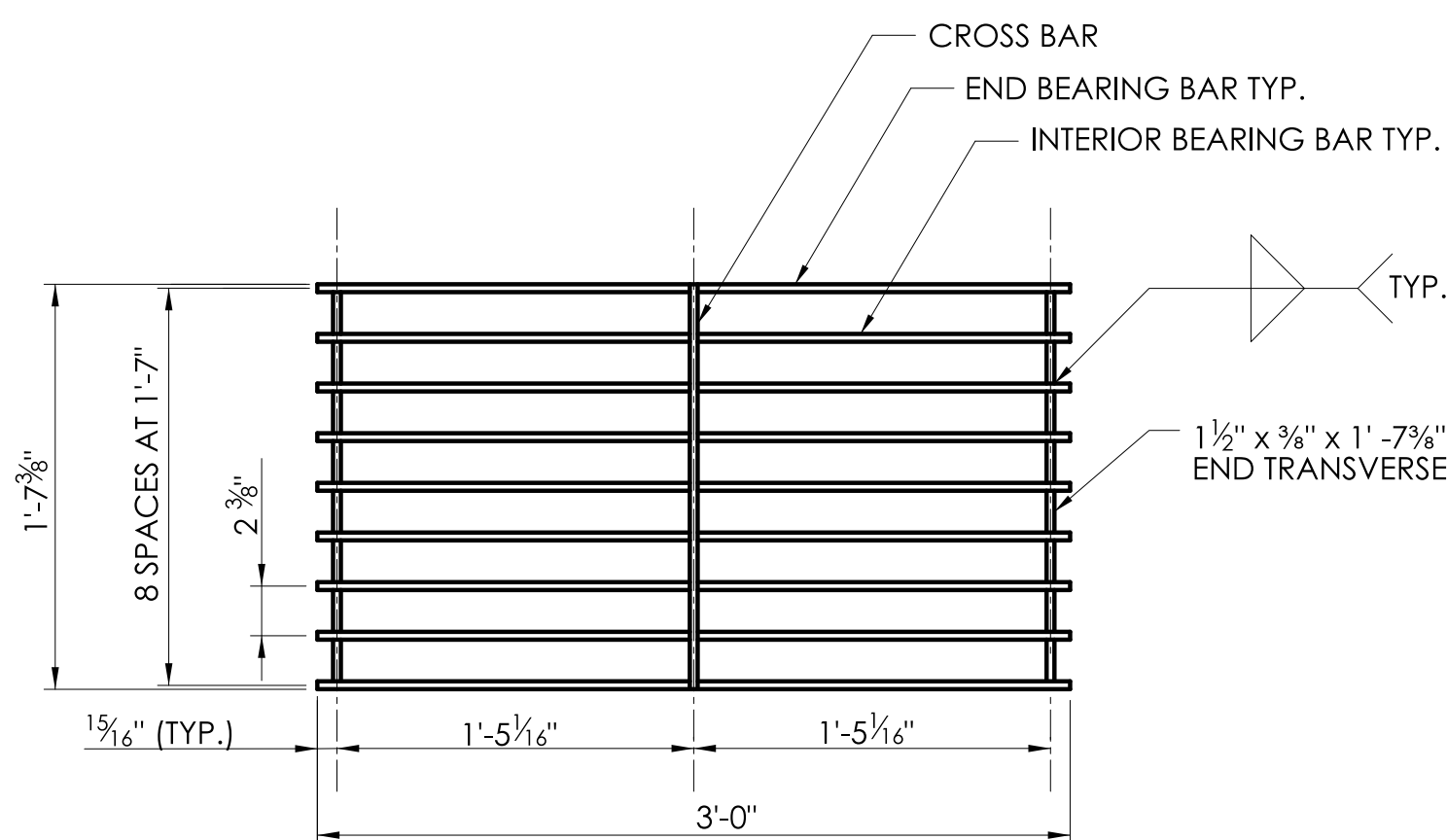


GENERAL NOTES:

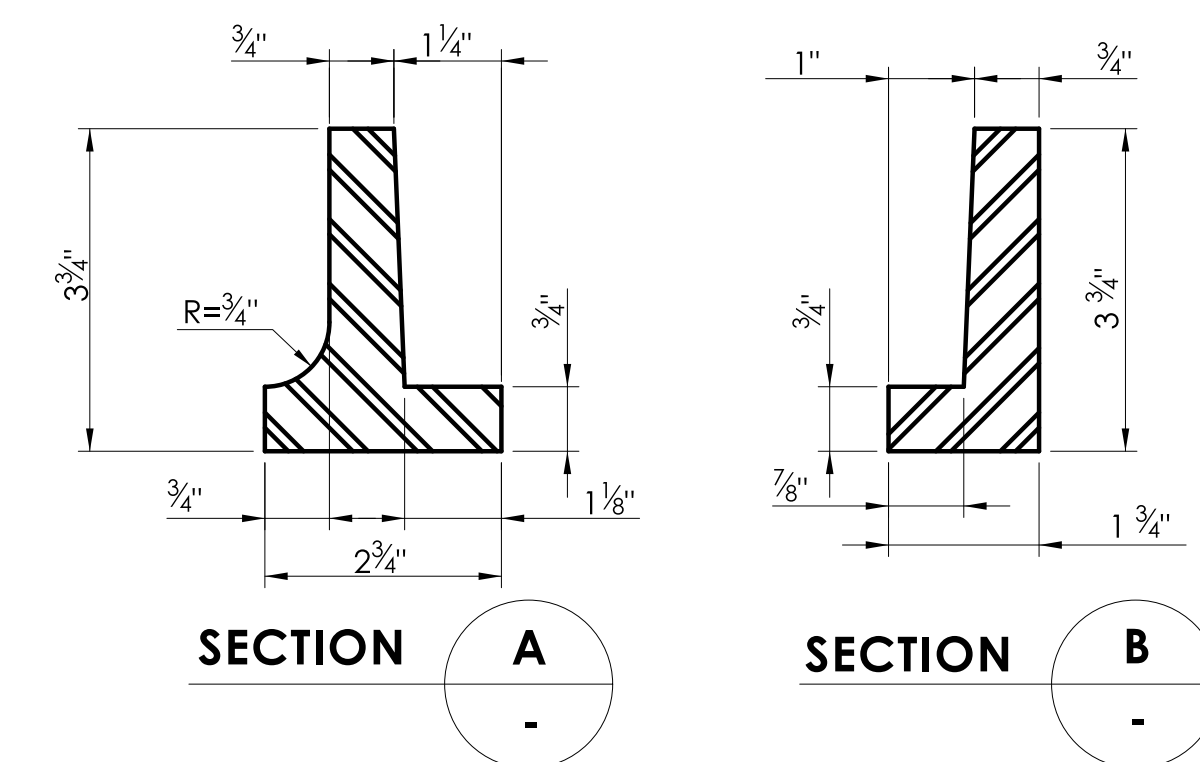
1. STEEL OR CAST IRON SHALL BE USED FOR FRAMES. STEEL SHALL BE USED FOR TYPE "A" AND "B" GRATES.
2. TYPE "A" GRATES SHALL BE USED ON ALL ROADWAYS WHERE BICYCLE TRAFFIC IS ALLOWED OR ON HEAVY DUTY LOCK DOWN TOPS AS DIRECTED BY THE ENGINEER.
3. TYPE "B" GRATES SHALL BE USED ON ALL LIMITED ACCESS HIGHWAYS, RAMP AND WHERE BICYCLE TRAFFIC IS NOT ALLOWED OR AS DIRECTED BY THE ENGINEER.
4. DO NOT GALVANIZE CAST IRON FRAMES.
5. DIMENSIONAL TOLERANCES SHALL BE $\pm 3/16$ ".
6. ALL STEEL BARS SHALL BE WELDED AT ALL INTERSECTIONS.



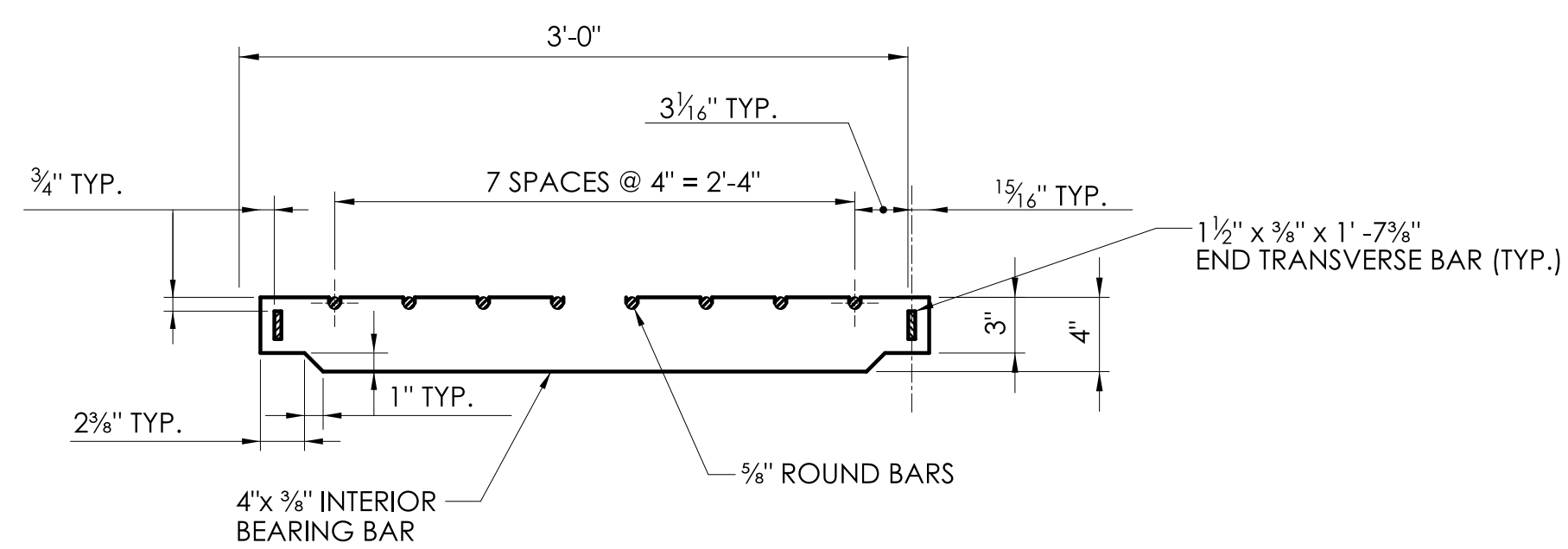
PLAN



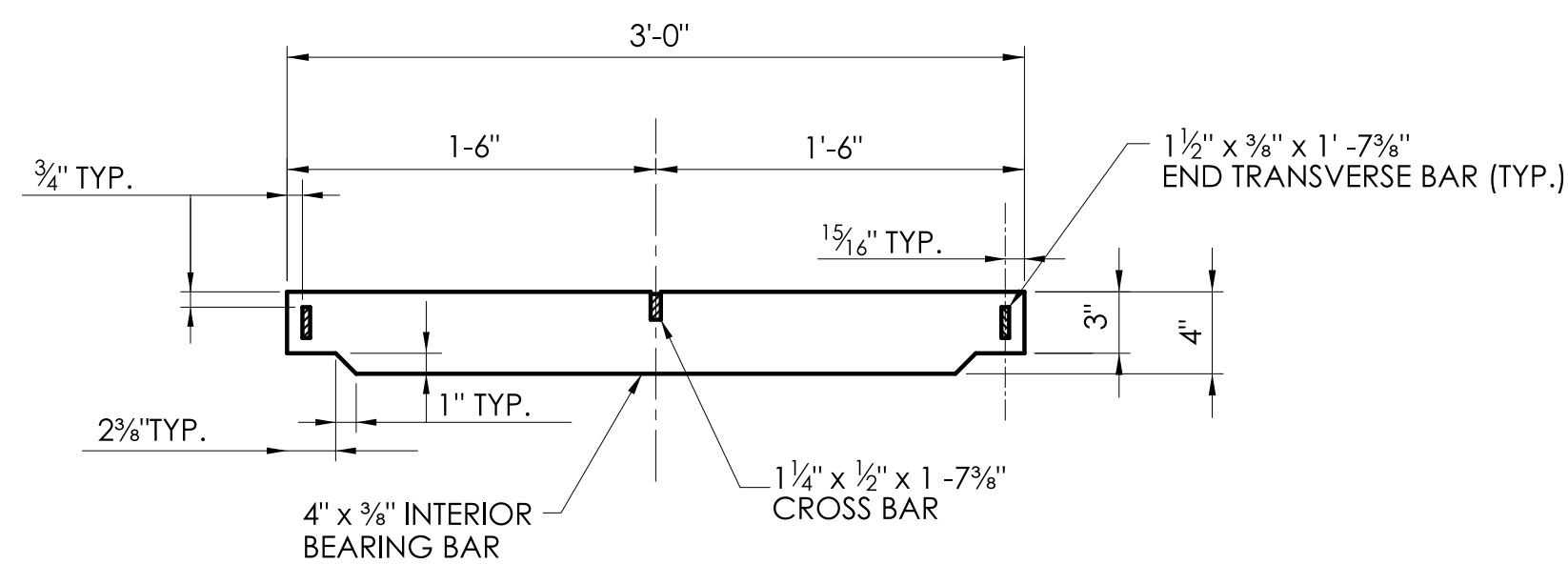
PLAN



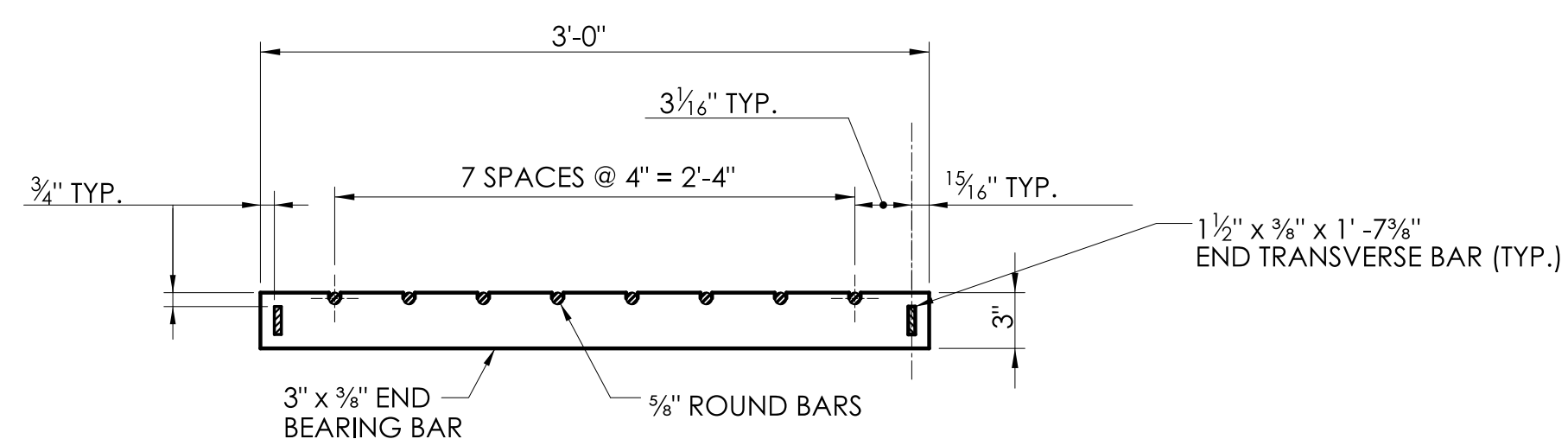
CAST IRON FRAME ALTERNATE



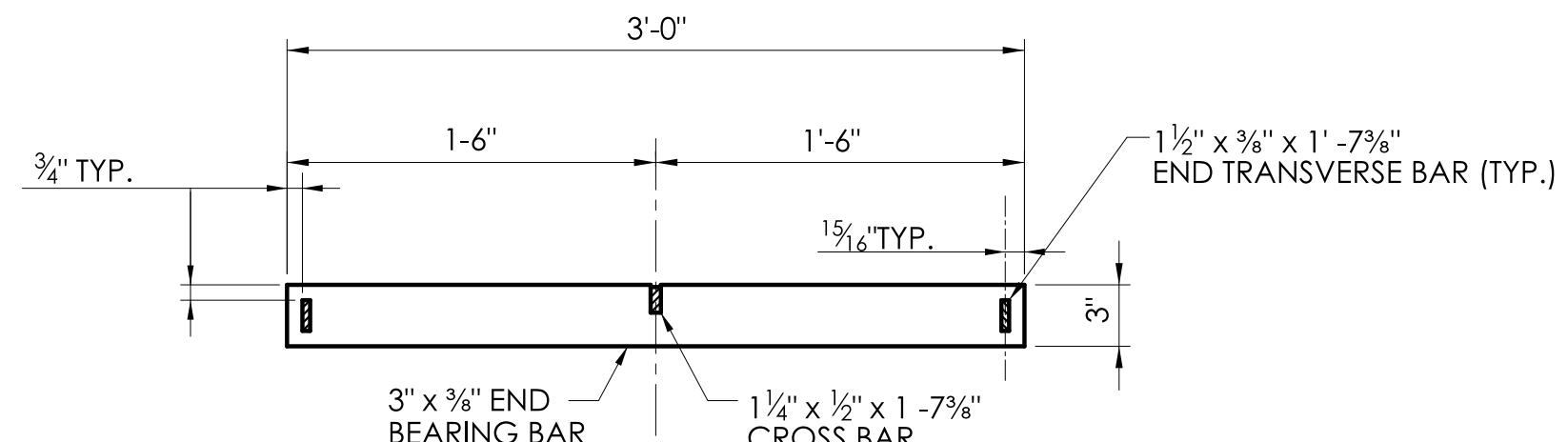
ELEVATION- INTERIOR BEARING BAR



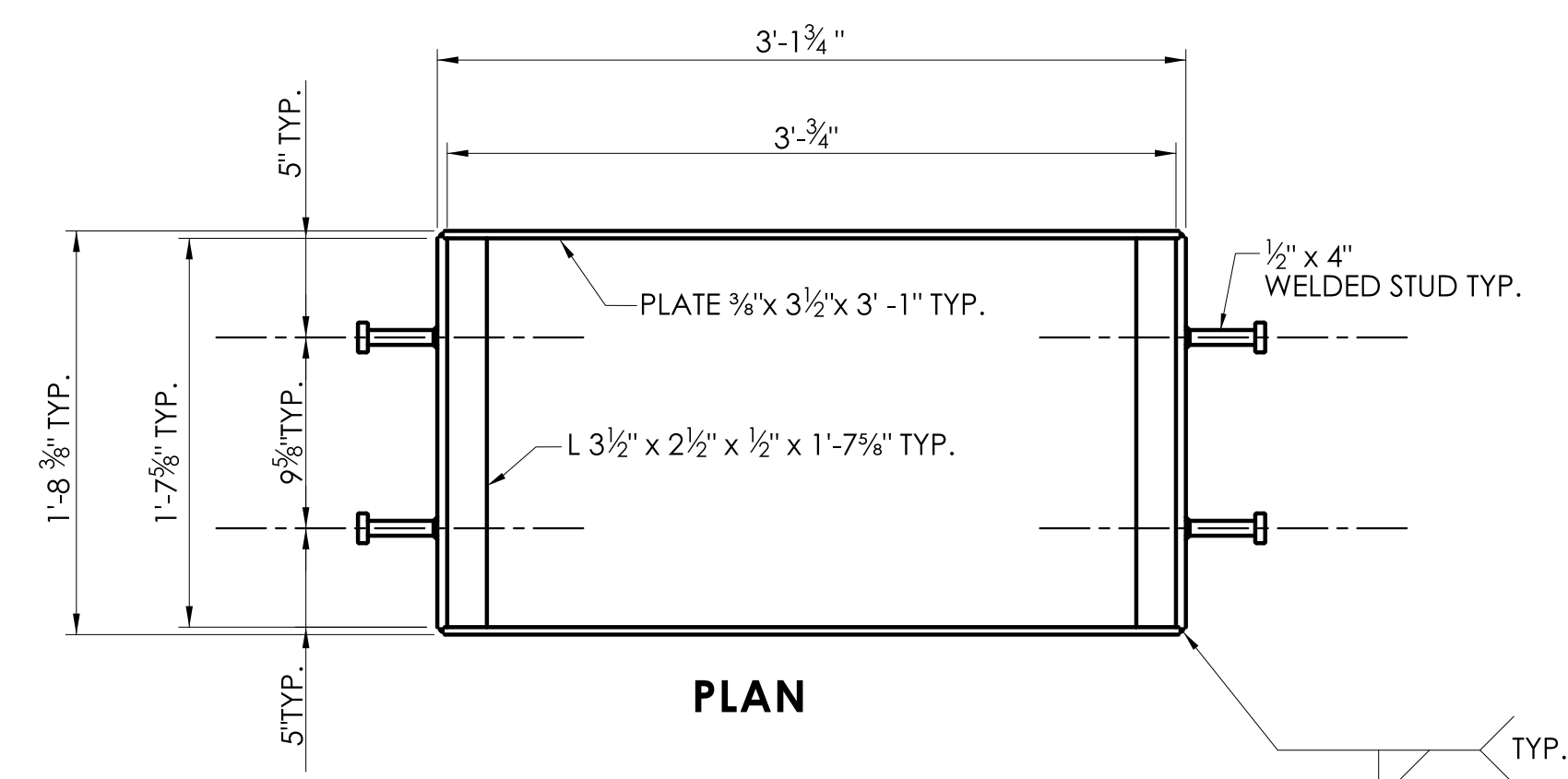
ELEVATION- INTERIOR BEARING BAR



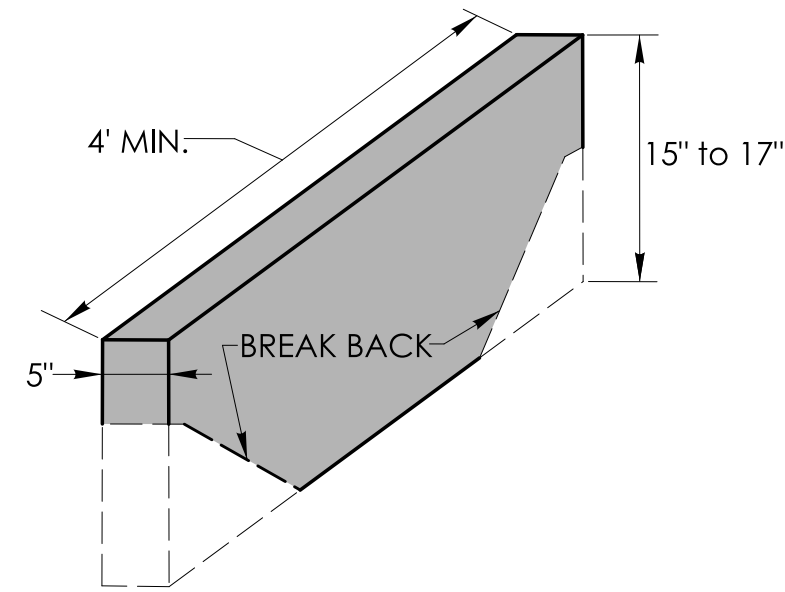
**ELEVATION- END BEARING BAR
CATCH BASIN GRATE TYPE A**



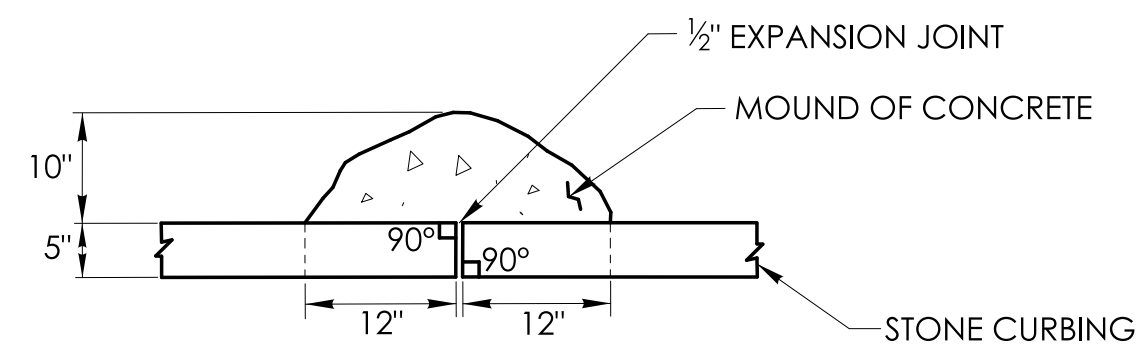
**ELEVATION- END BEARING BAR
CATCH BASIN GRATE TYPE B**



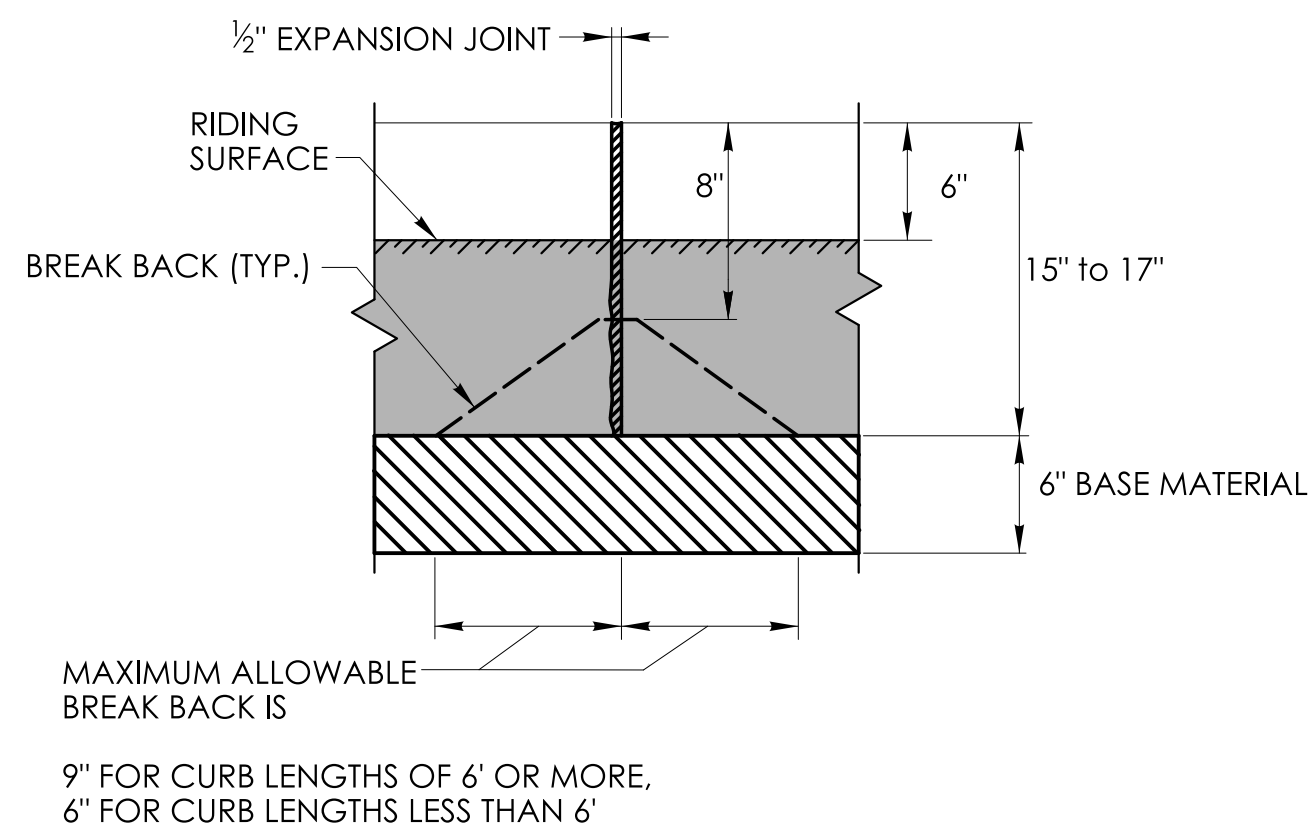
**WELDED STUD ANCHOR DETAILS
STEEL FRAME**



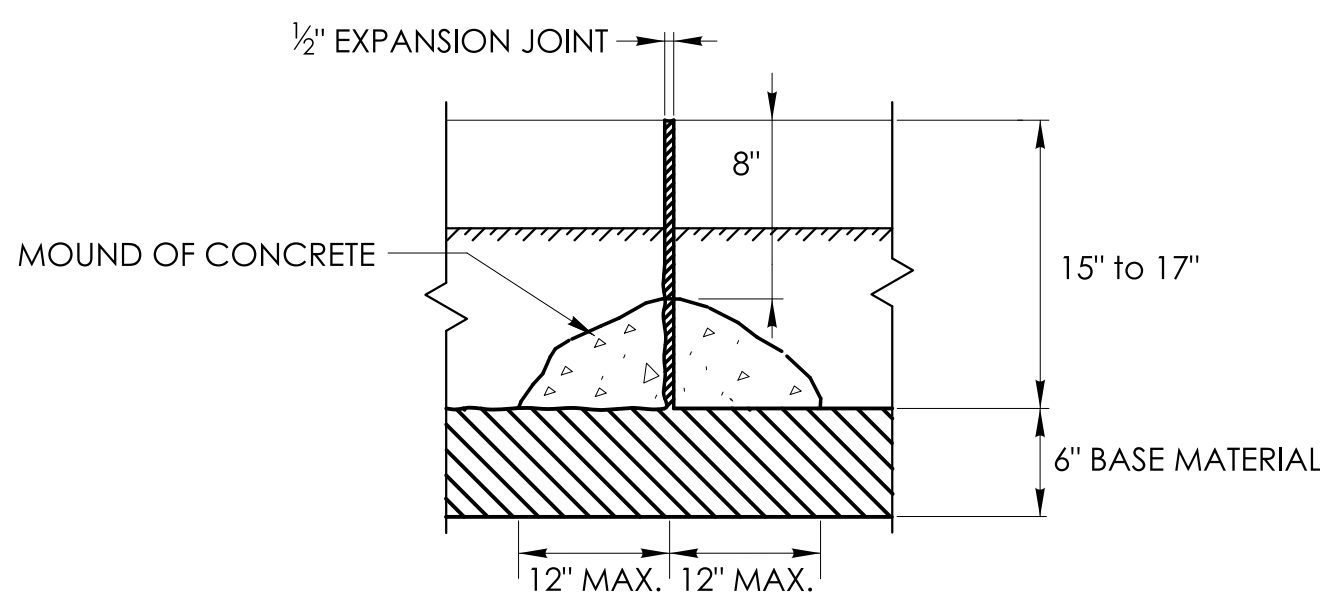
STONE CURBING



PLAN

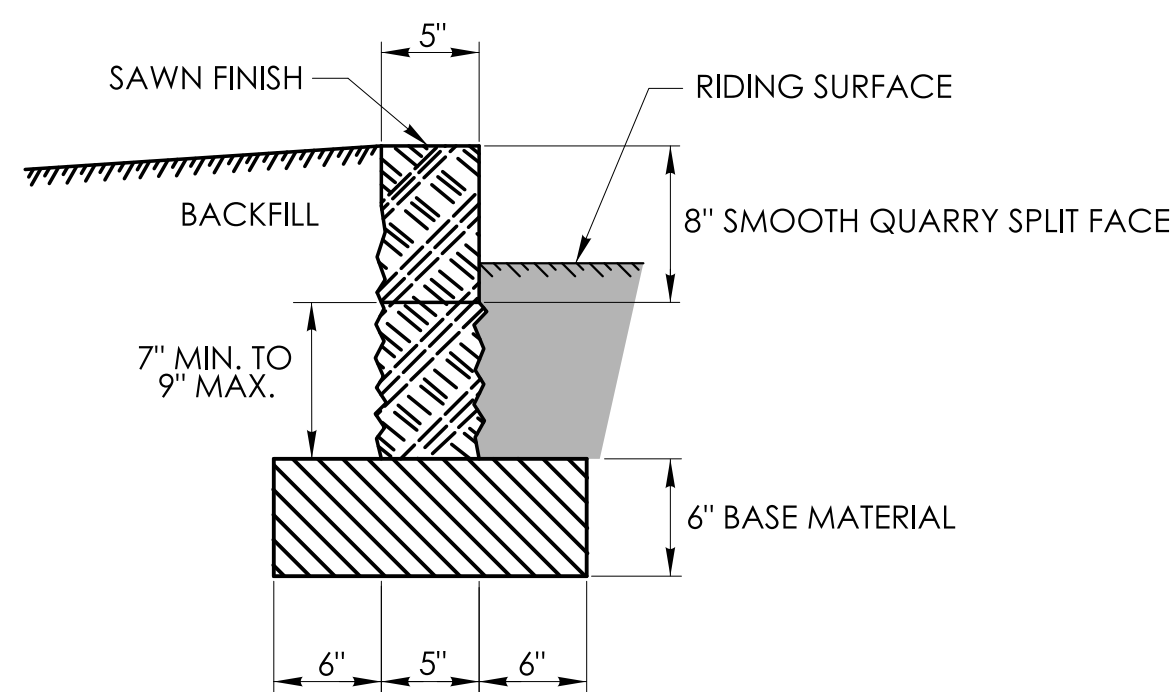


FRONT
ELEVATION

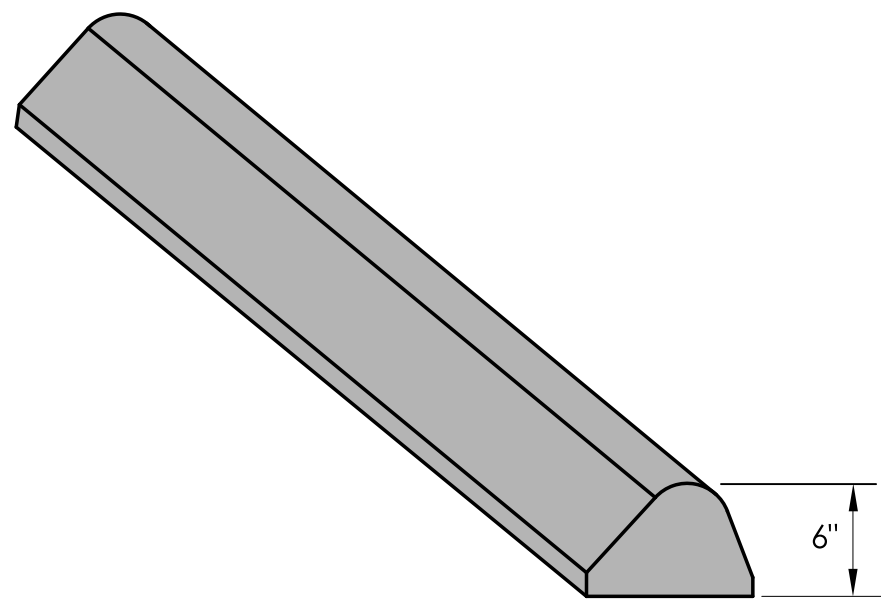


BACK
ELEVATION

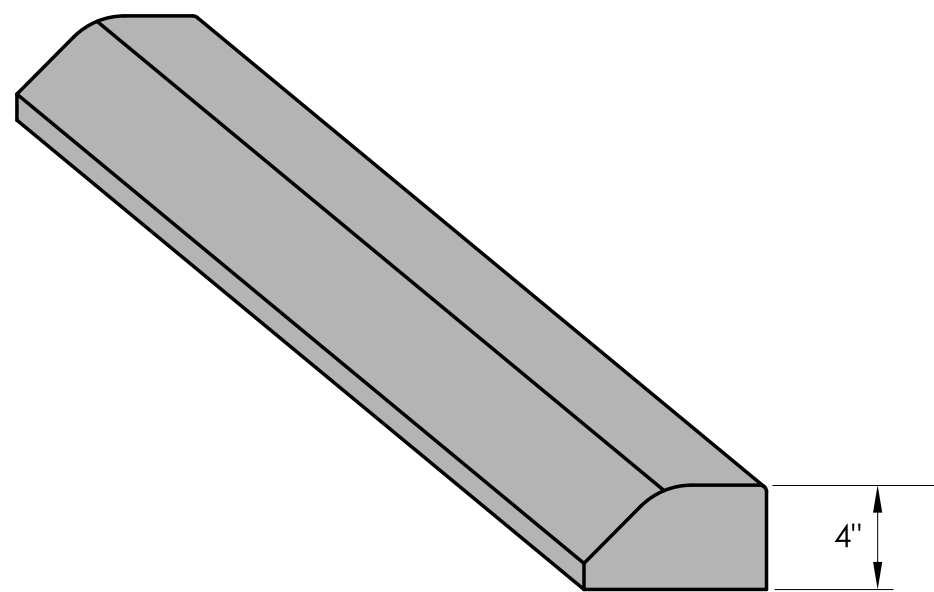
MOUND OF CONCRETE AT ALL JOINTS
FOR STONE CURBING



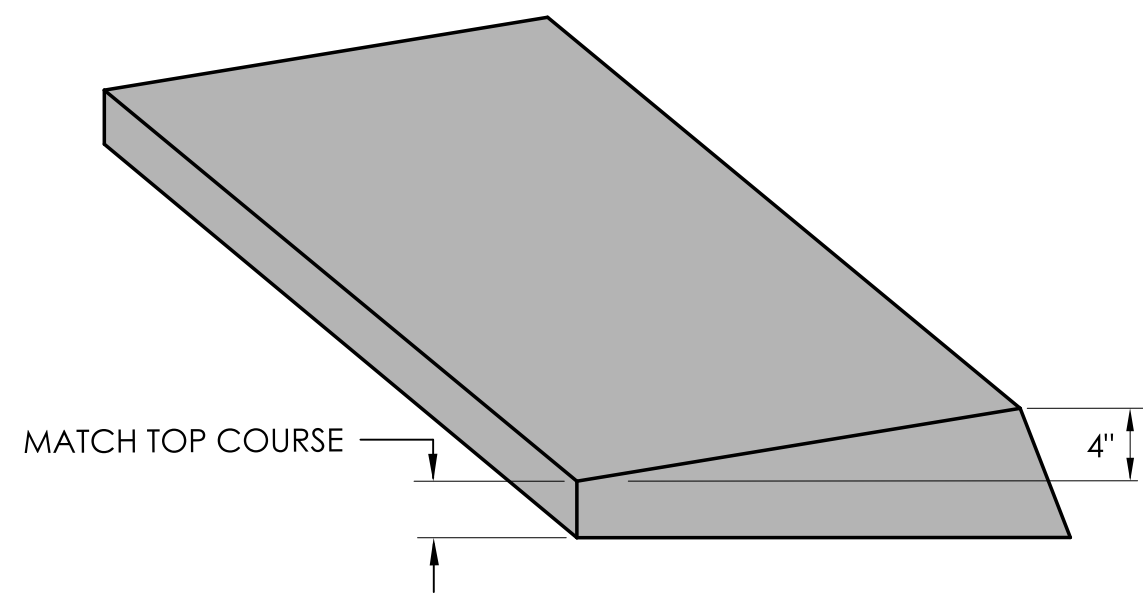
SECTION



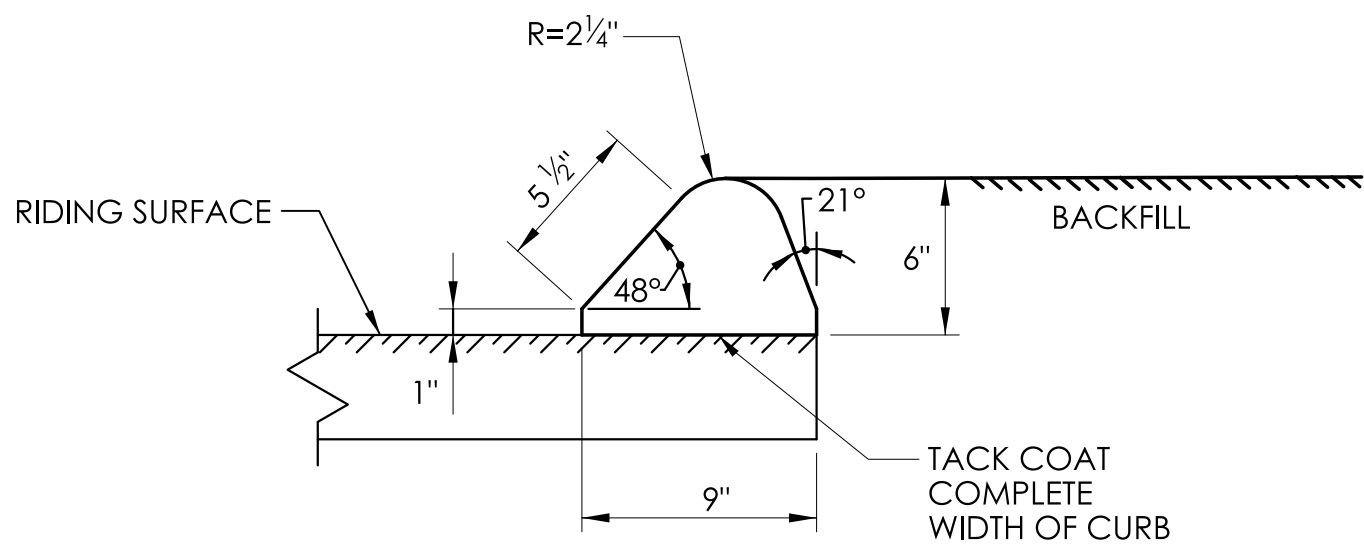
BITUMINOUS CONCRETE LIP CURBING
(6" HIGH)



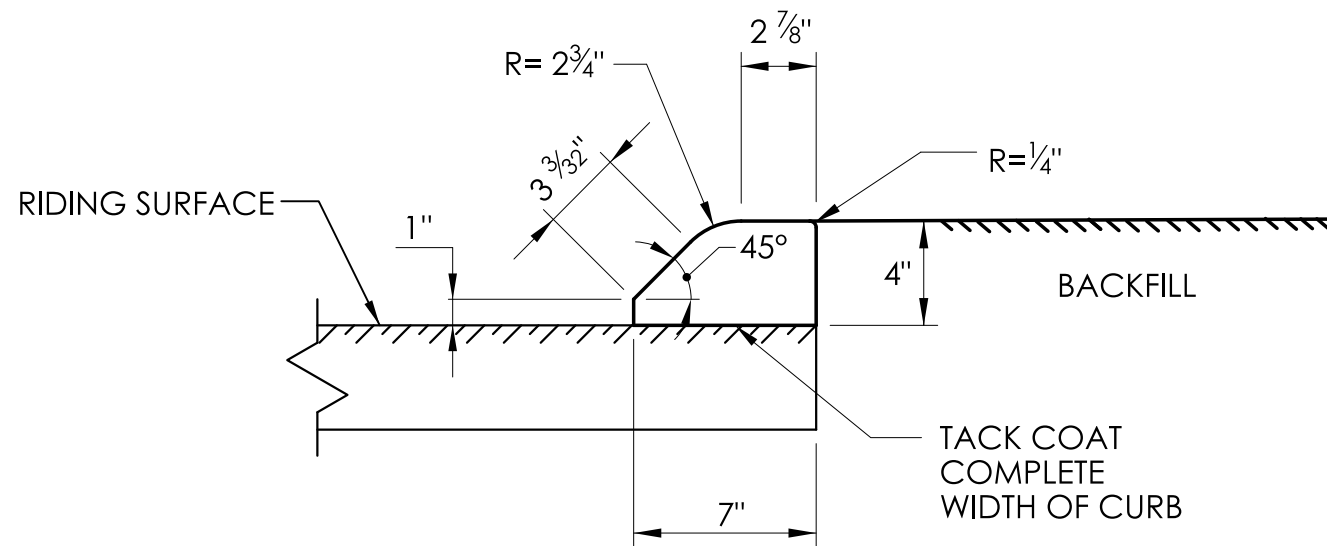
BITUMINOUS CONCRETE PARK CURBING
(4" HIGH)



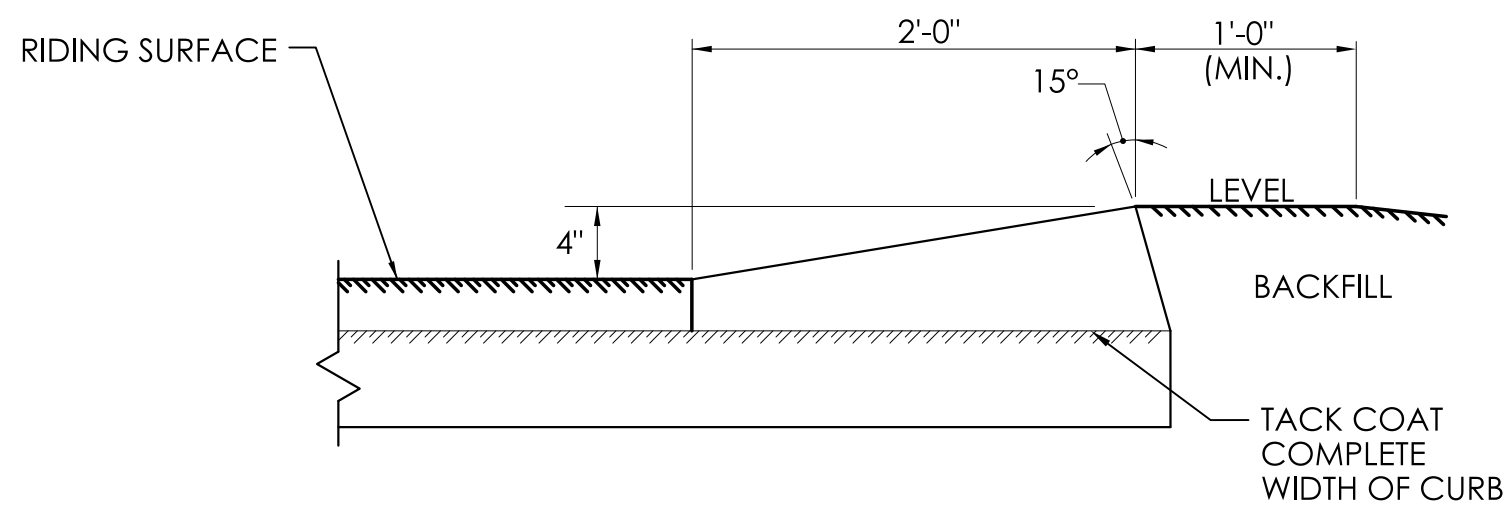
BITUMINOUS CONCRETE BERM CURBING
(4" HIGH)



SECTION



SECTION

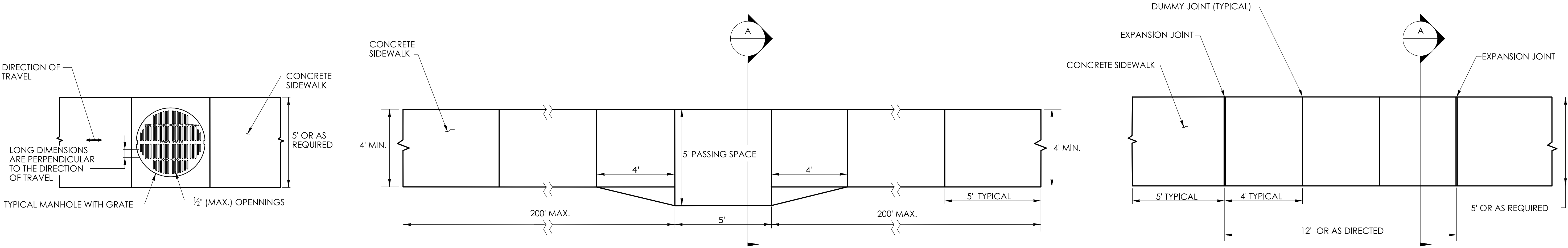


SECTION

GENERAL NOTES:

1. SEE CONCRETE SIDEWALK RAMPS GUIDE SHEETS FOR PEDESTRIAN RAMP TYPES.

2. ALL CURBING SHALL BE INSTALLED AS EITHER PRECAST OR CAST IN PLACE AS DIRECTED.



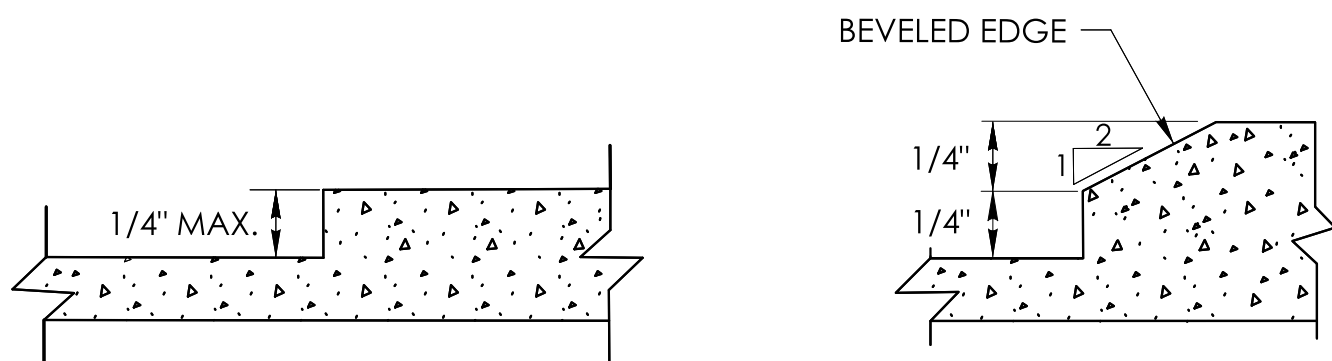
**PEDESTRIAN ACCESS ROUTE
OVER A MANHOLE WITH GRATE**

1. HORIZONTAL OPENINGS IN GRATES AND JOINTS MUST NOT BE MORE THAN 1/2 INCH
2. ELONGATED OPENINGS IN GRATES MUST BE PLACED SO THAT THE LONG DIMENSION IS PERPENDICULAR TO THE DIRECTION OF TRAVEL

**5' PASSING SPACE FOR 4' WIDE SIDEWALK
PLAN**

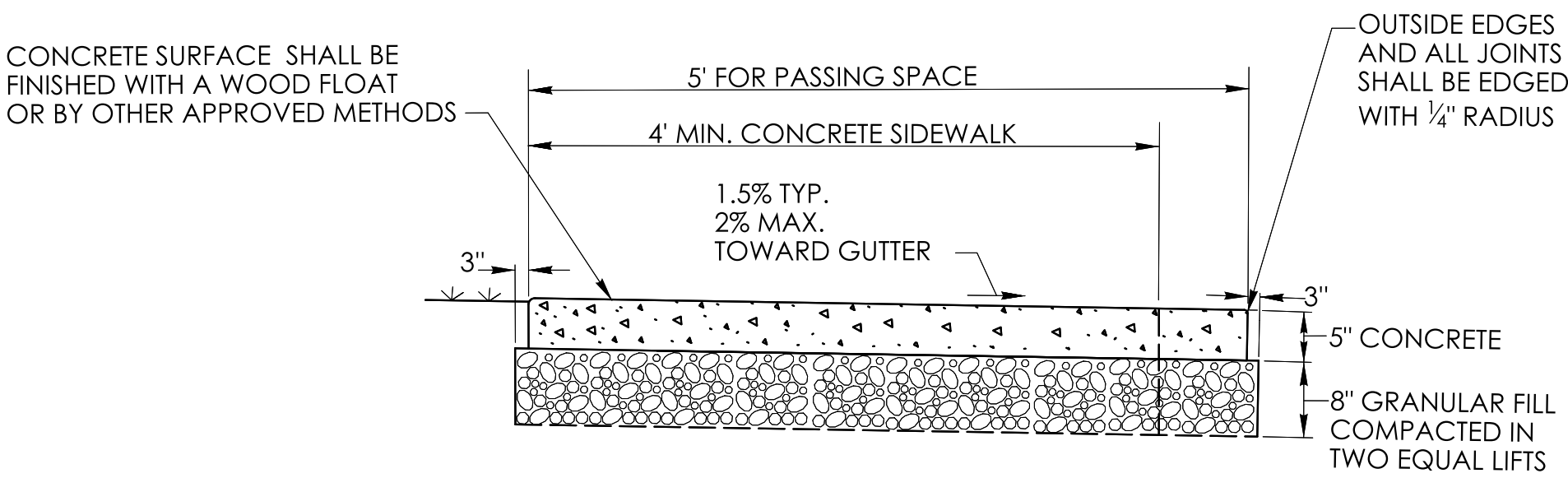
PASSING SPACES SHALL BE PROVIDED AT INTERVALS OF 200' MAXIMUM FOR SIDEWALKS LESS THAN 5' IN WIDTH

**5' WIDE SIDEWALK
PLAN**



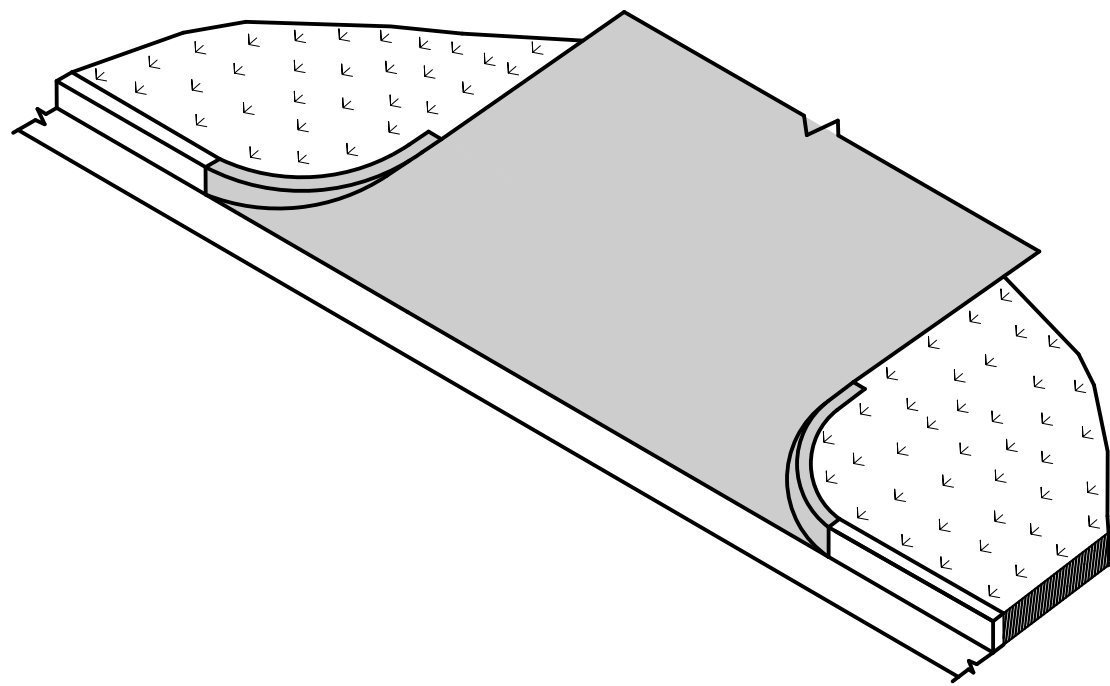
VERTICAL SURFACE DISCONTINUITIES

VERTICAL SURFACE DISCONTINUITIES MUST BE BEVELED TO A HEIGHT NOT GREATER THAN 1/4 INCH. THE BEVEL MUST BE THE ENTIRE WIDTH OF THE DISCONTINUITY

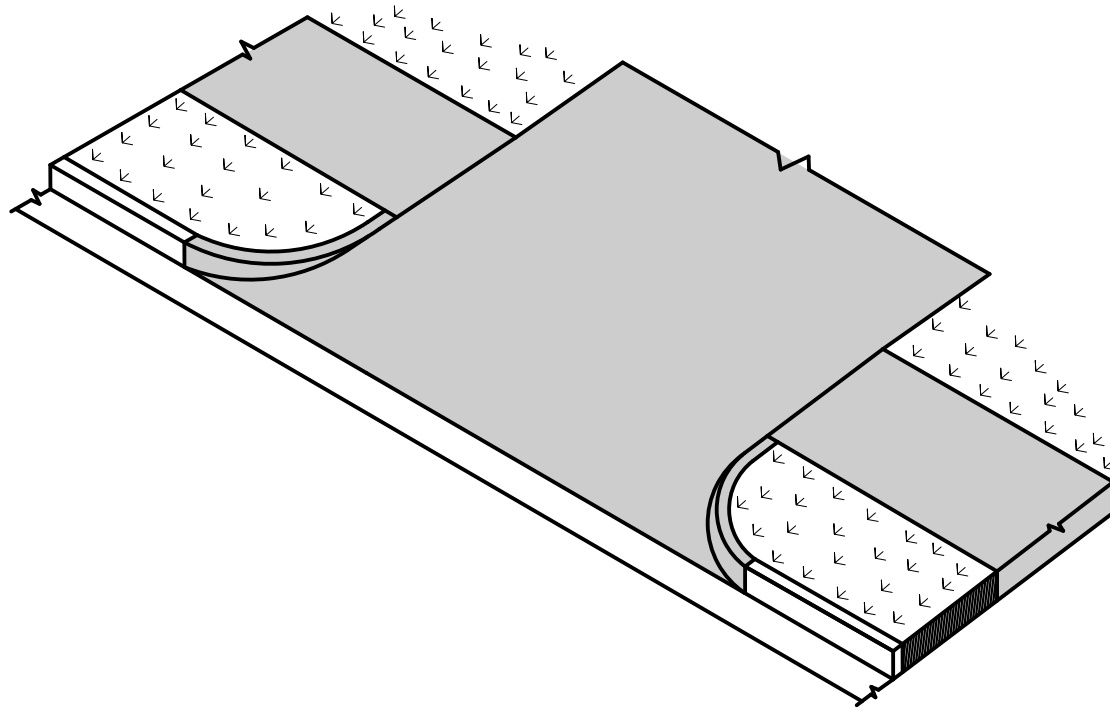
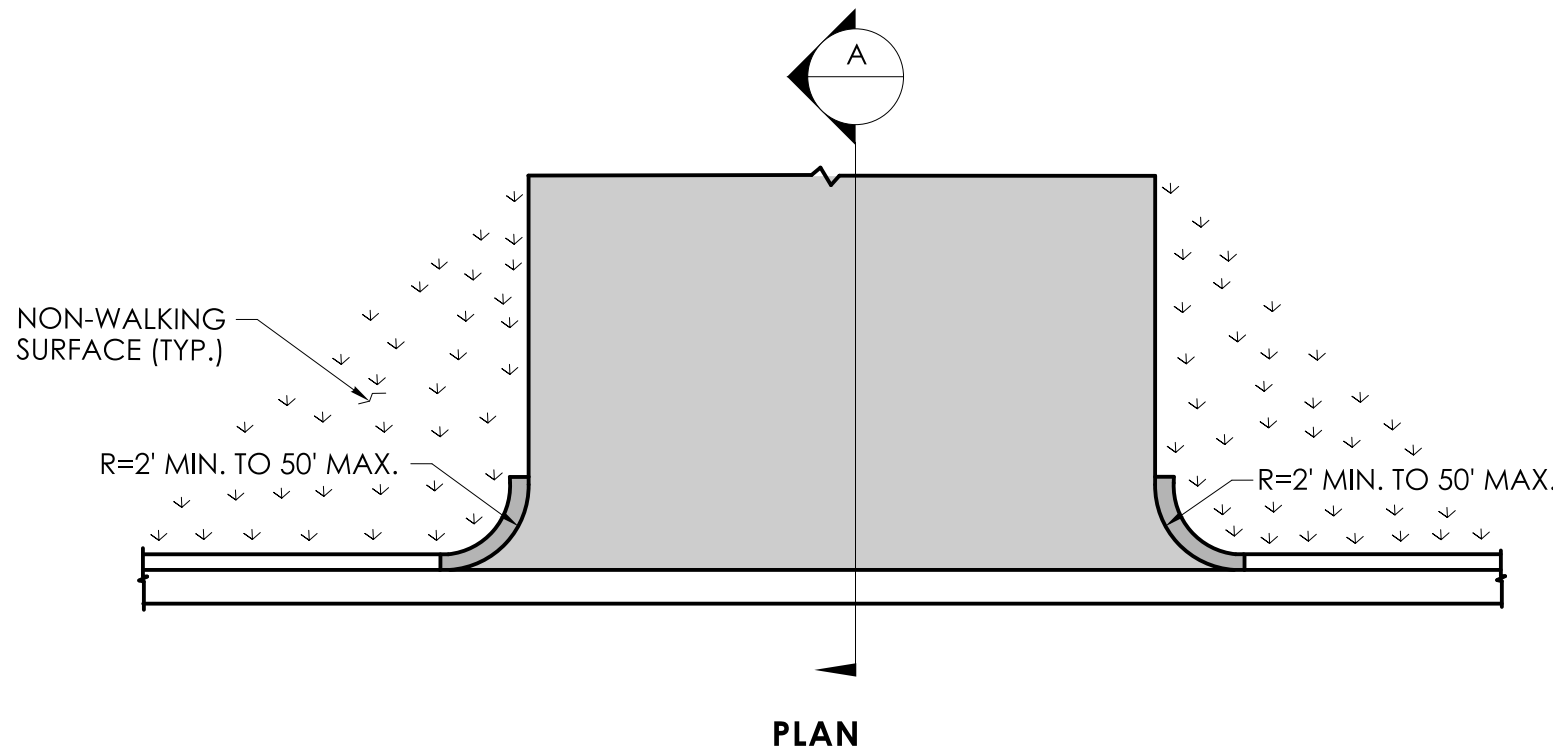


5' PASSING SPACE FOR 4' WIDE SIDEWALK

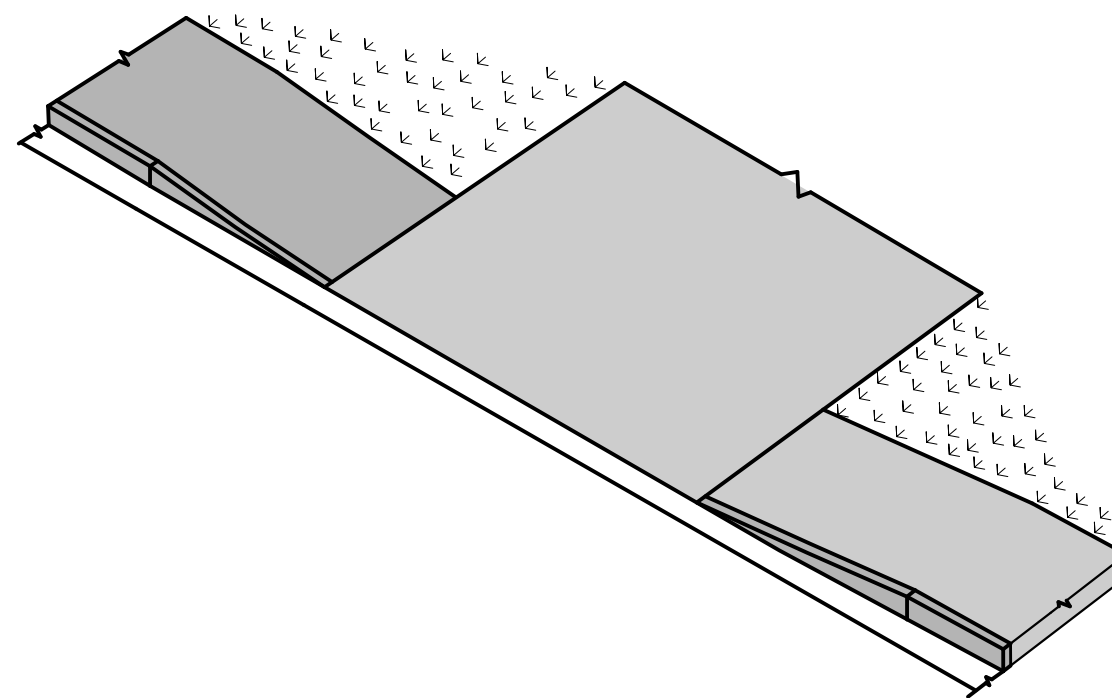
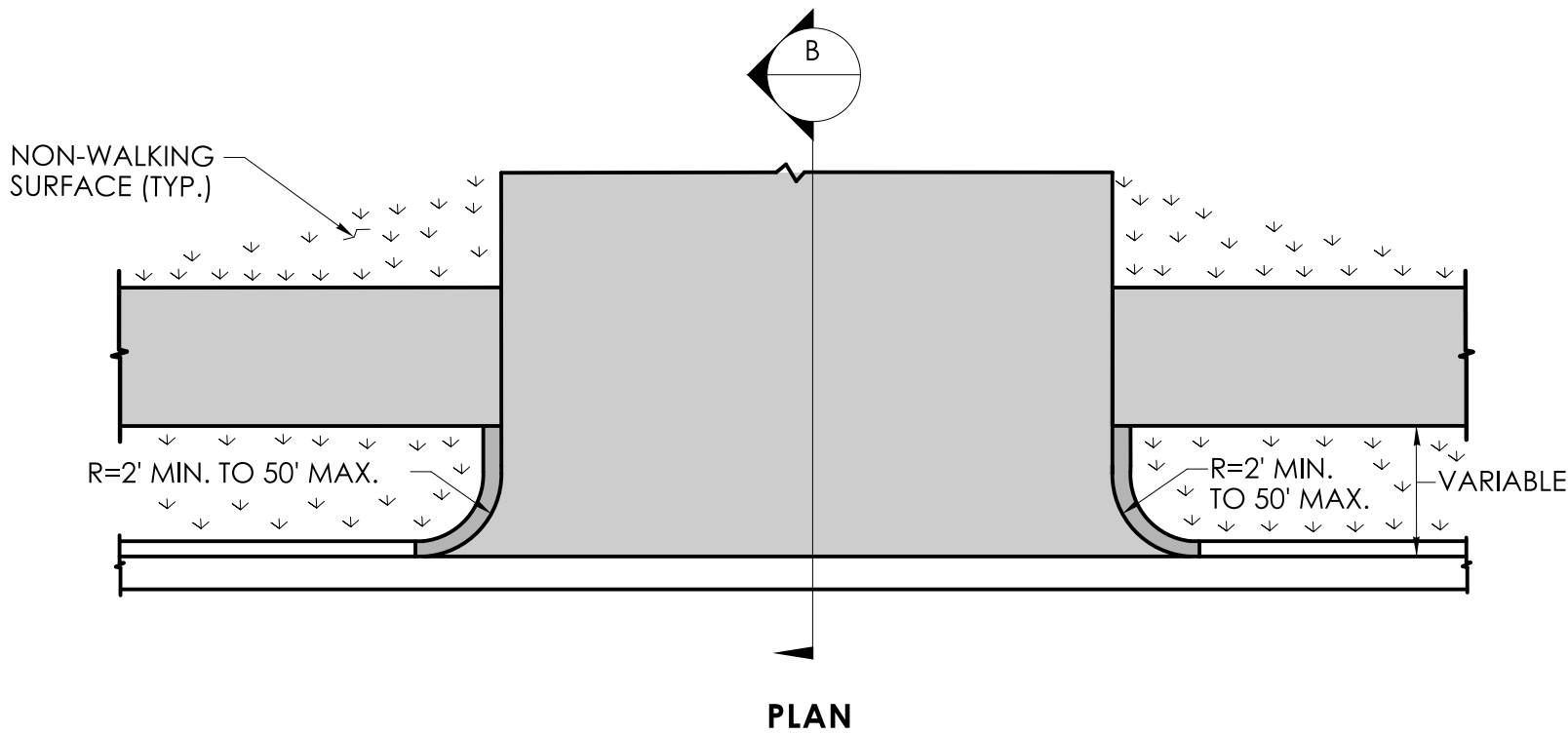
SECTION A



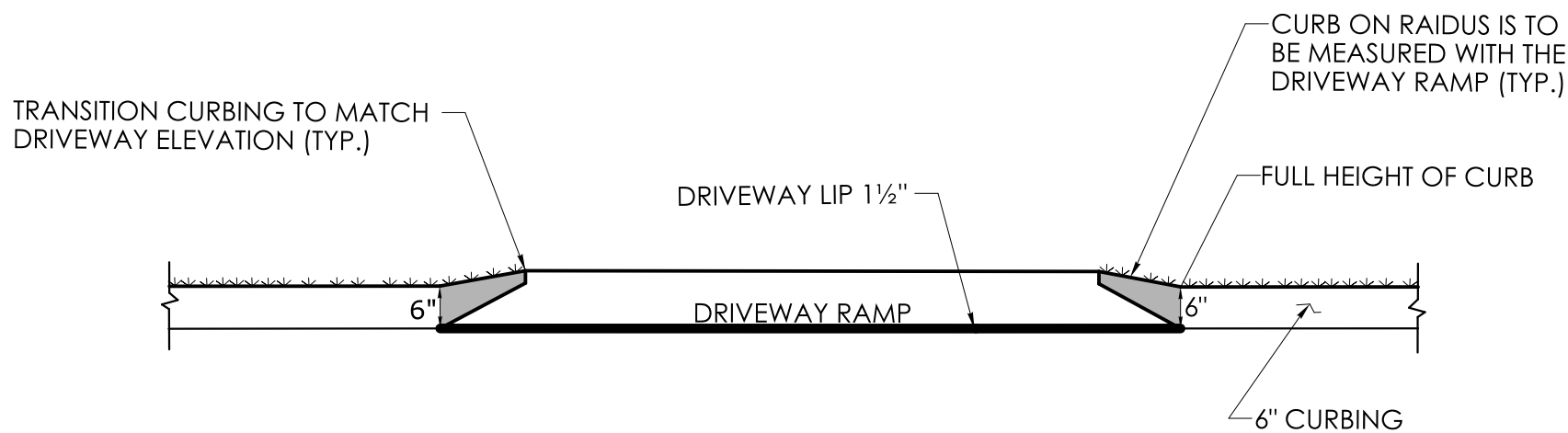
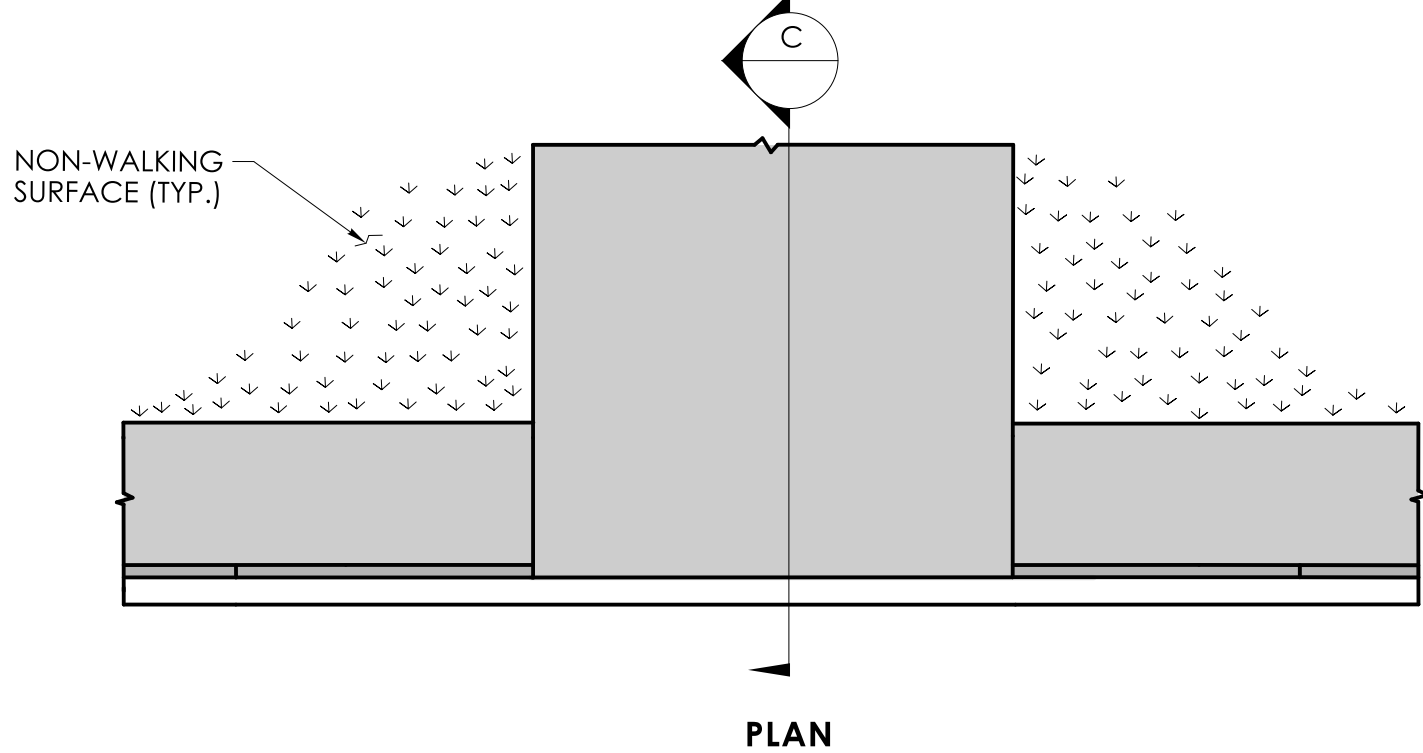
BITUMINOUS CONCRETE DRIVEWAY



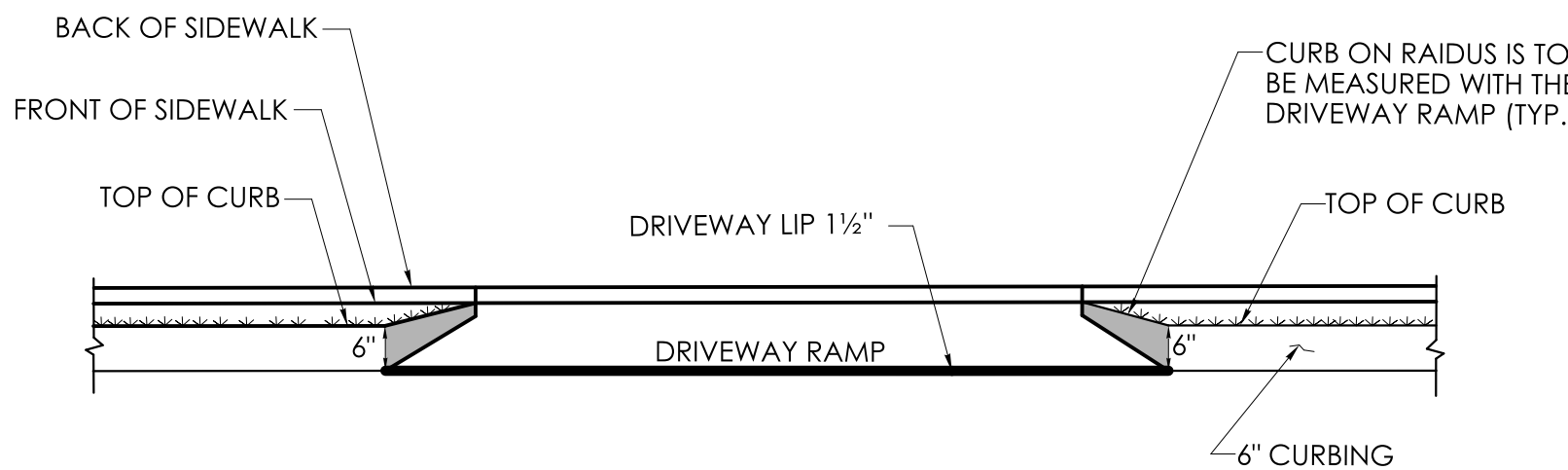
BITUMINOUS CONCRETE DRIVEWAY
WITH A PEDESTRIAN SIDEWALK SETBACK



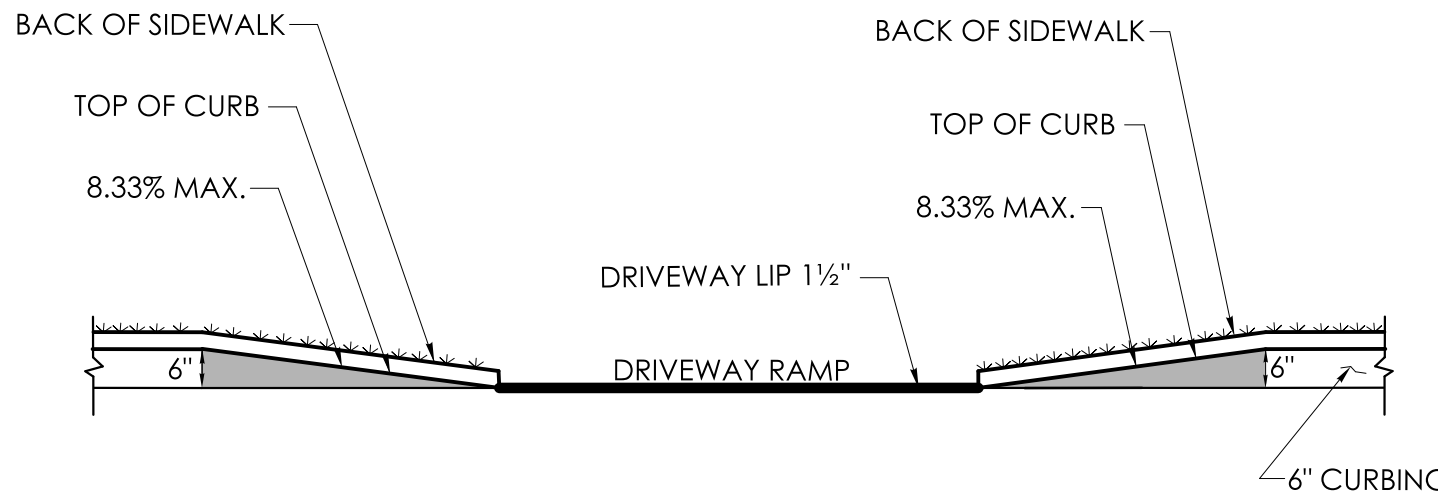
BITUMINOUS CONCRETE DRIVEWAY
WITH PEDESTRIAN SIDEWALK AT THE CURB



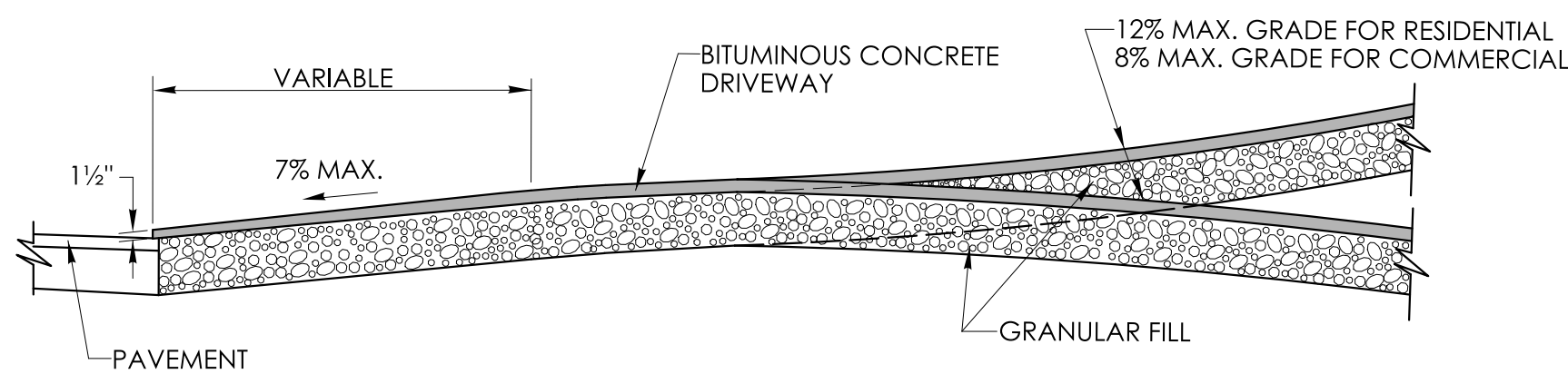
ELEVATION



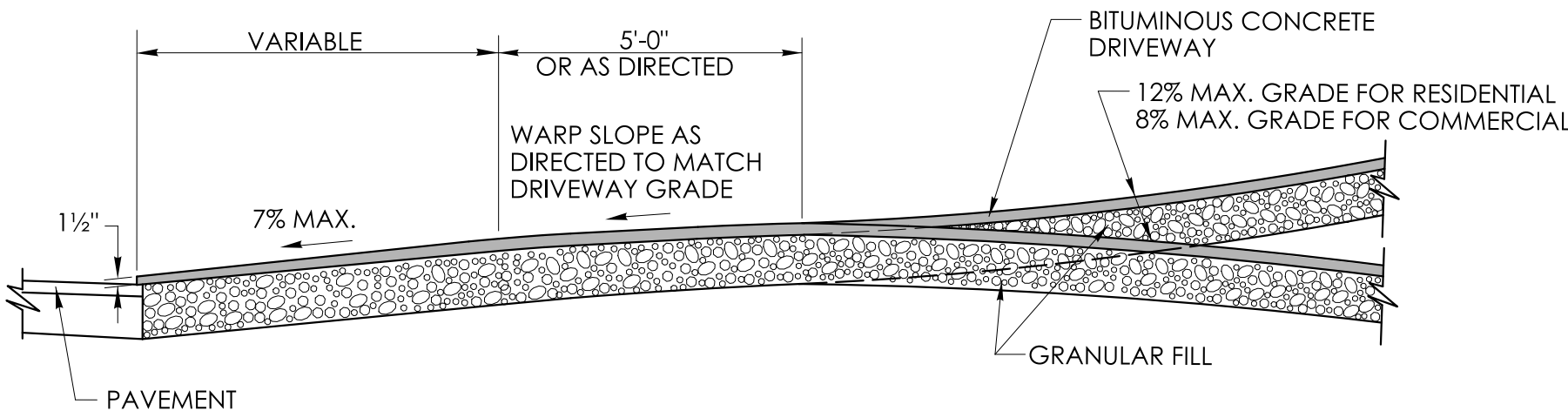
ELEVATION



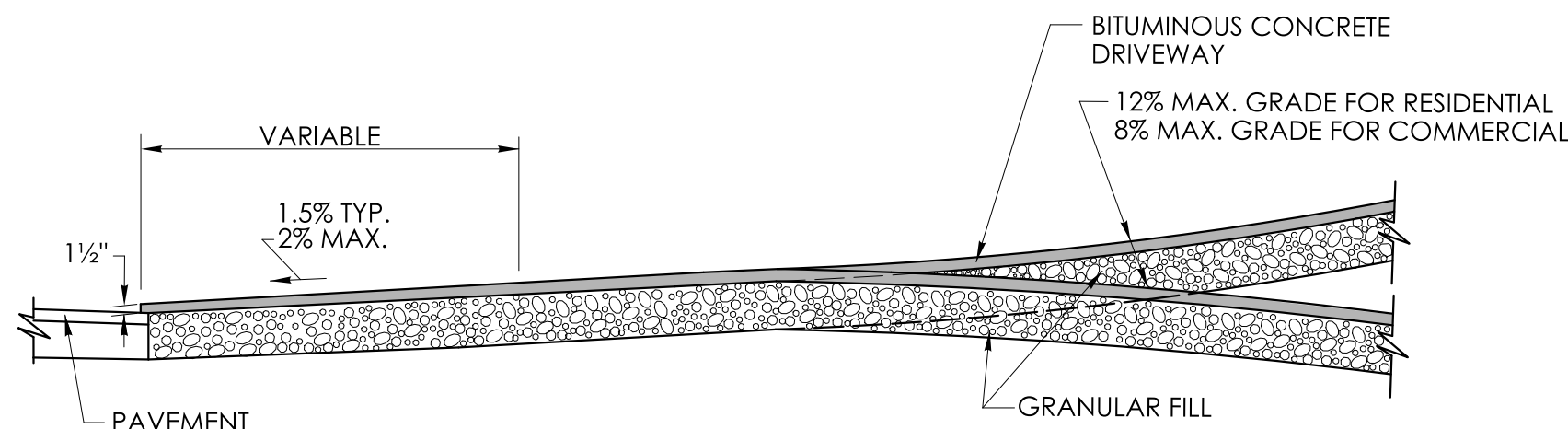
ELEVATION



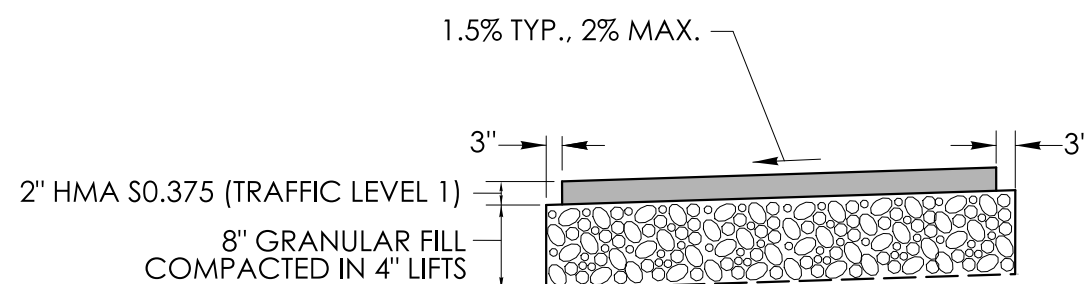
SECTION A



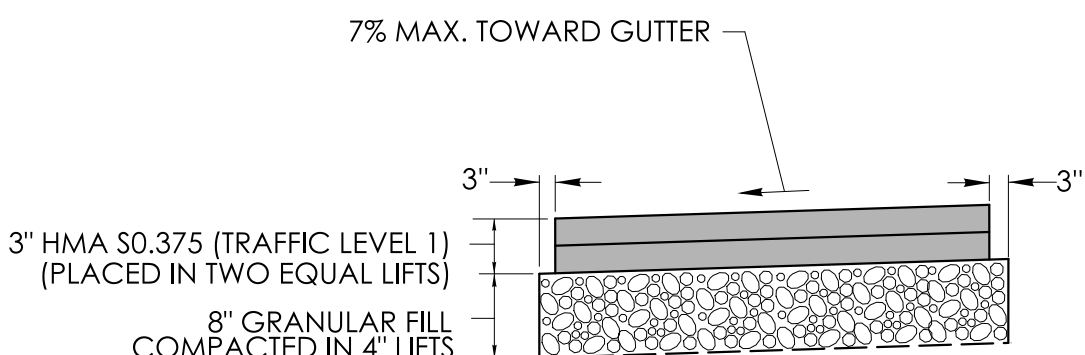
SECTION B



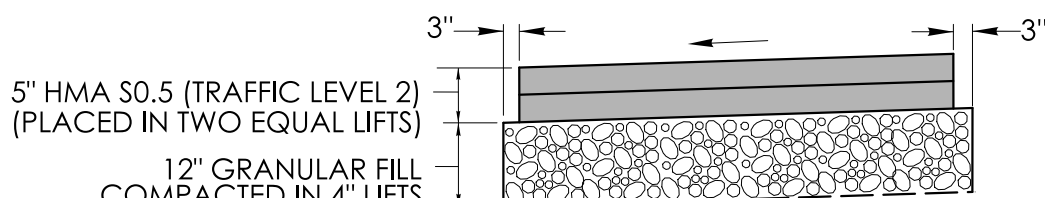
SECTION C



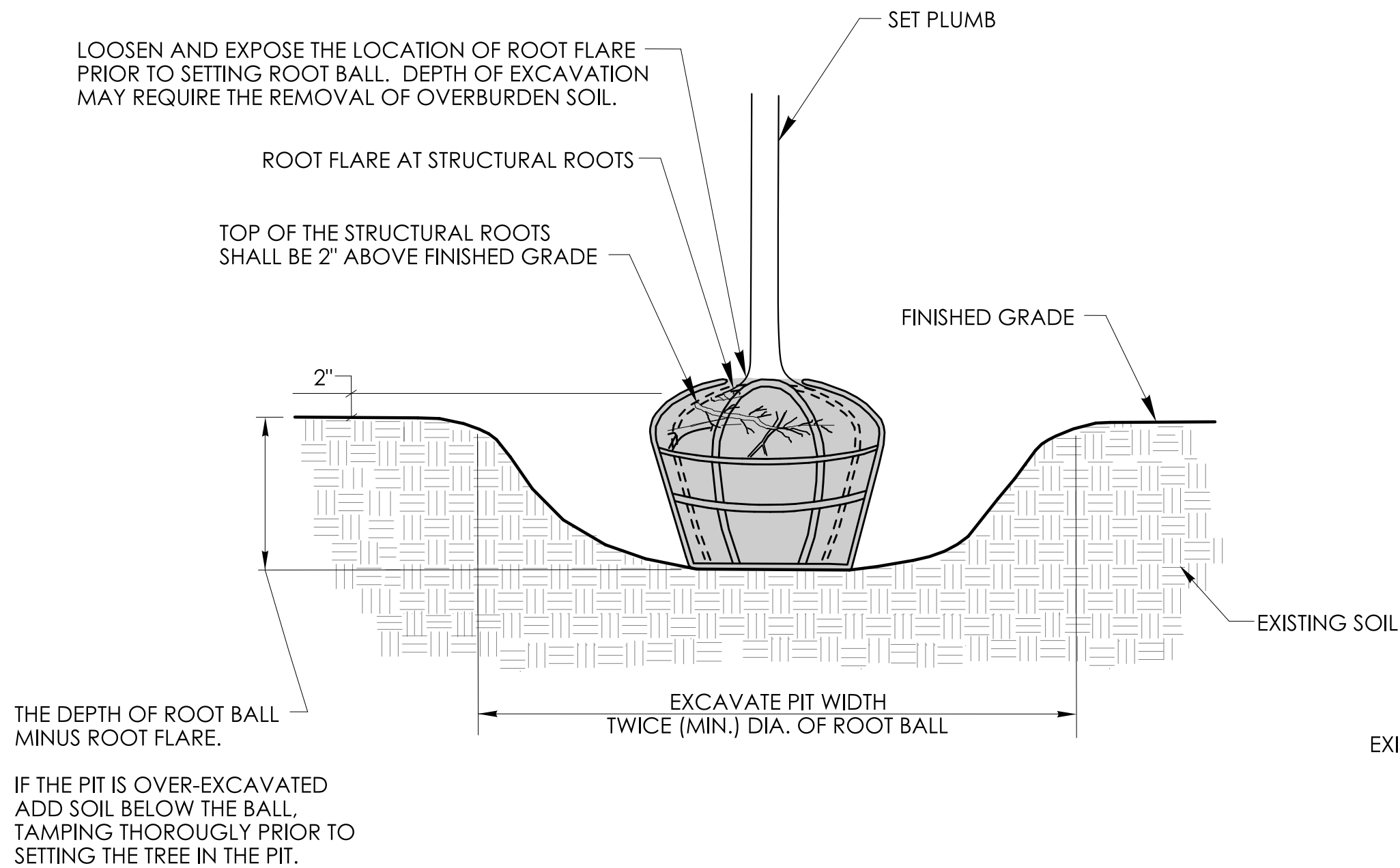
BITUMINOUS CONCRETE SIDEWALK



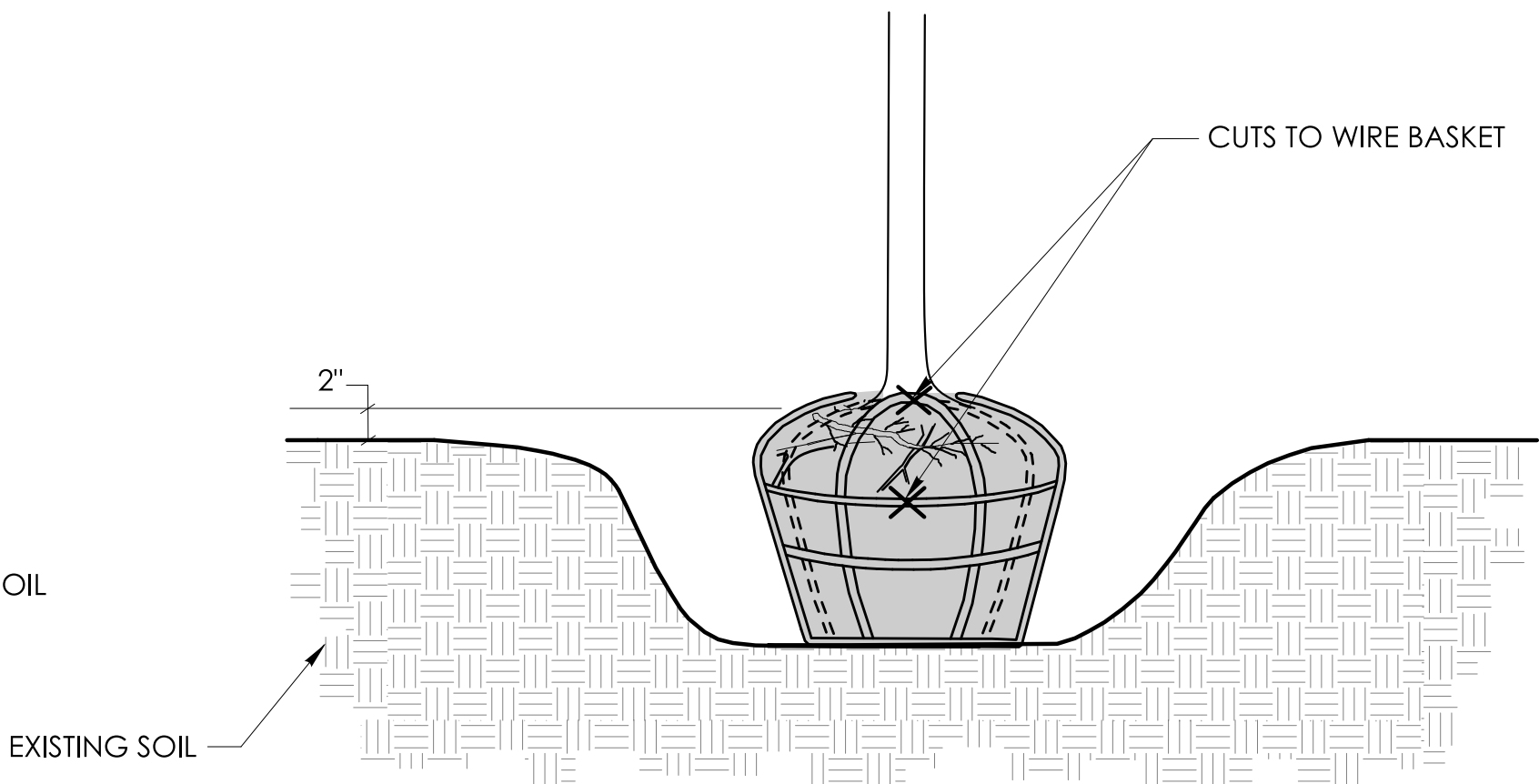
BITUMINOUS CONCRETE DRIVEWAY



COMMERCIAL
BITUMINOUS CONCRETE DRIVEWAY

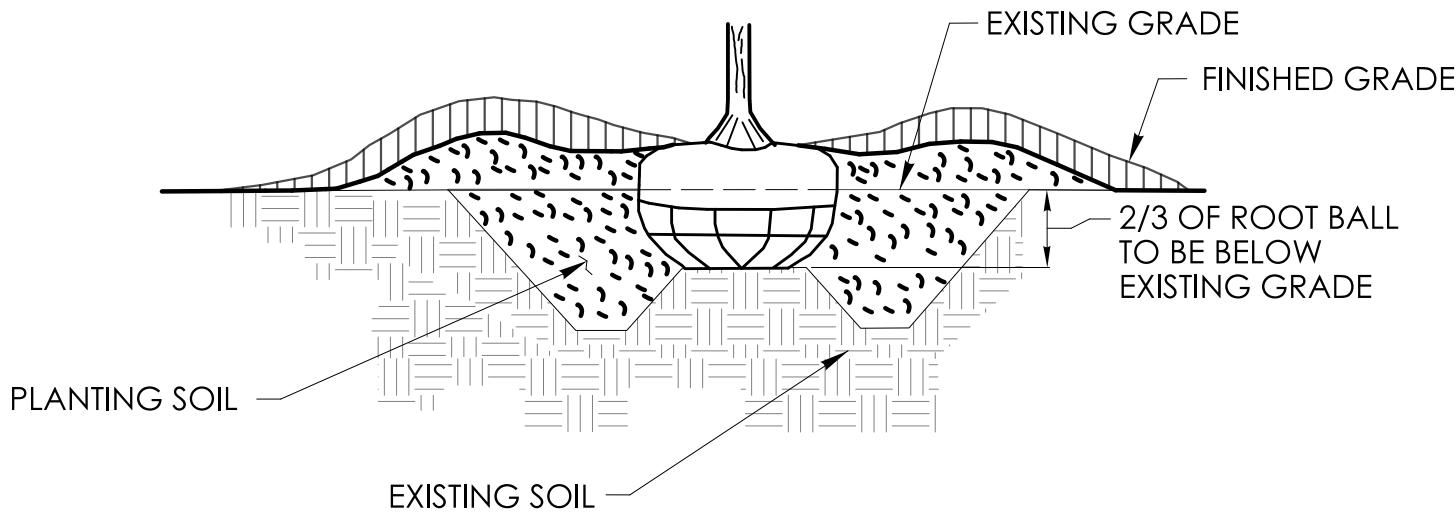


PIT EXCAVATION AND SETTING OF PLANTING

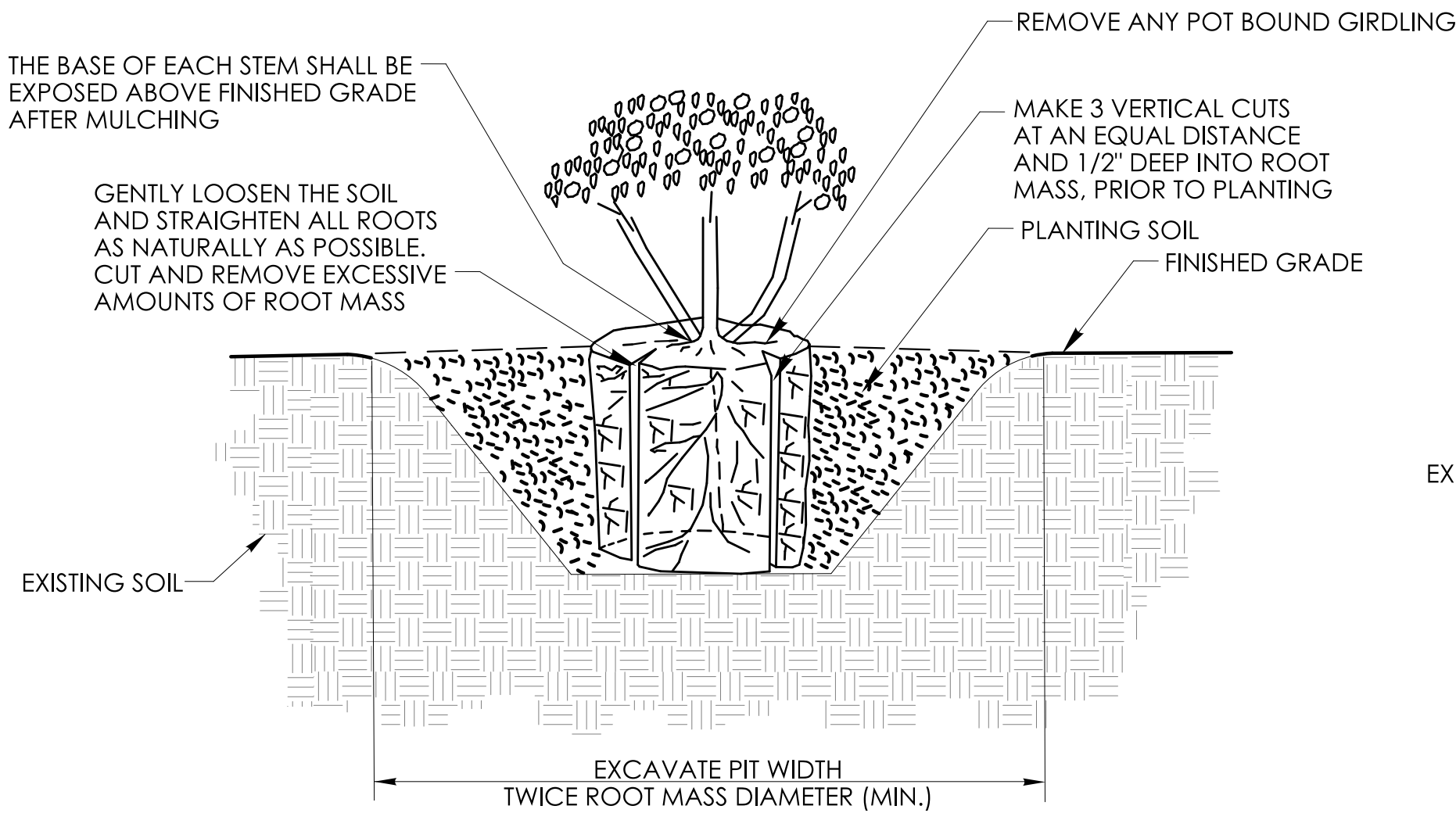


WIRE BASKET REMOVAL

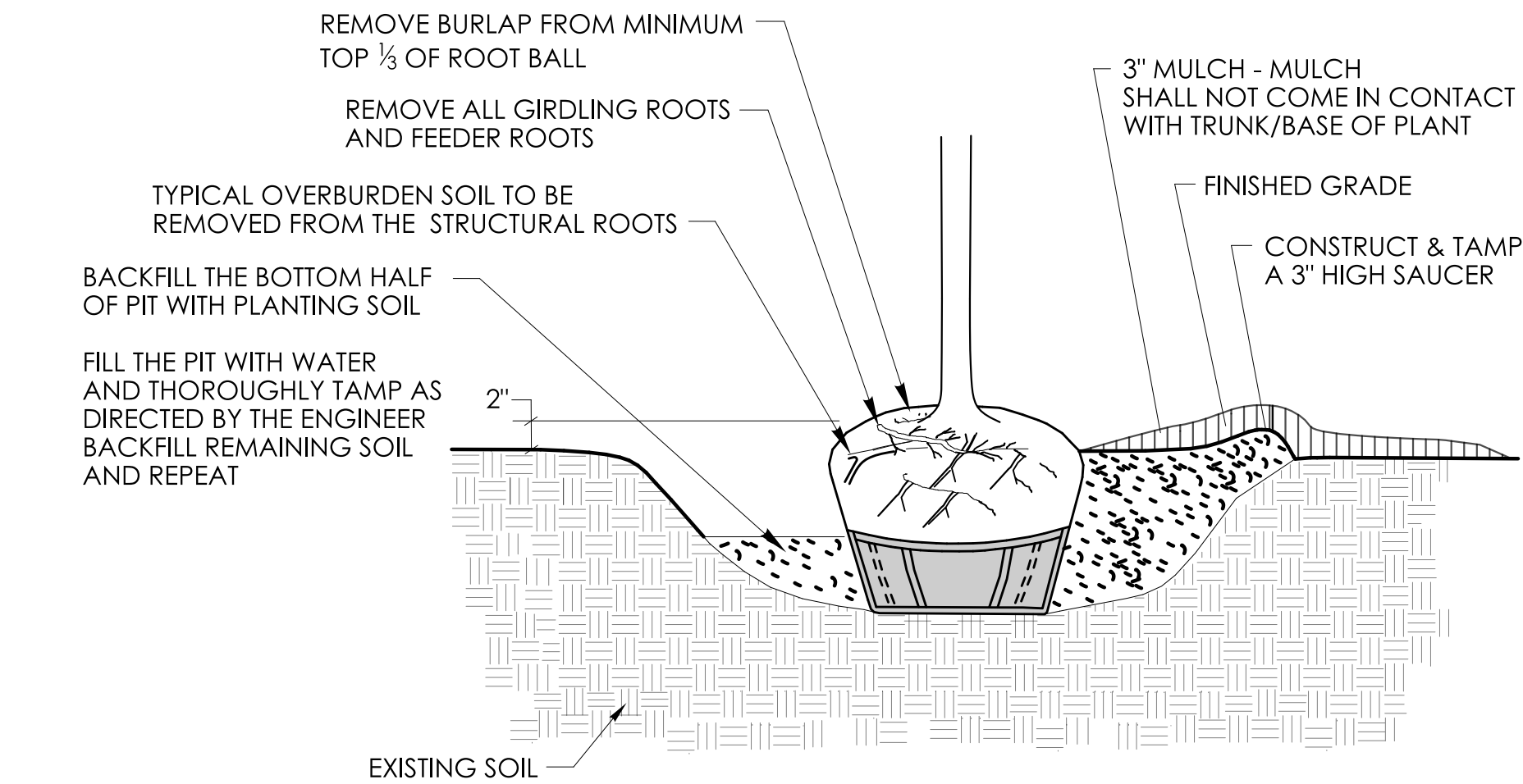
NOTE: IF WIRE BASKETS ARE USED, THE CONTRACTOR SHALL CUT ALL OF THE HORIZONTAL WIRES IN THE TOP 2/3 OF THE ROOT BALL AND BEND DOWN OR REMOVE THE TOP 1/3 OF THE WIRE BASKET



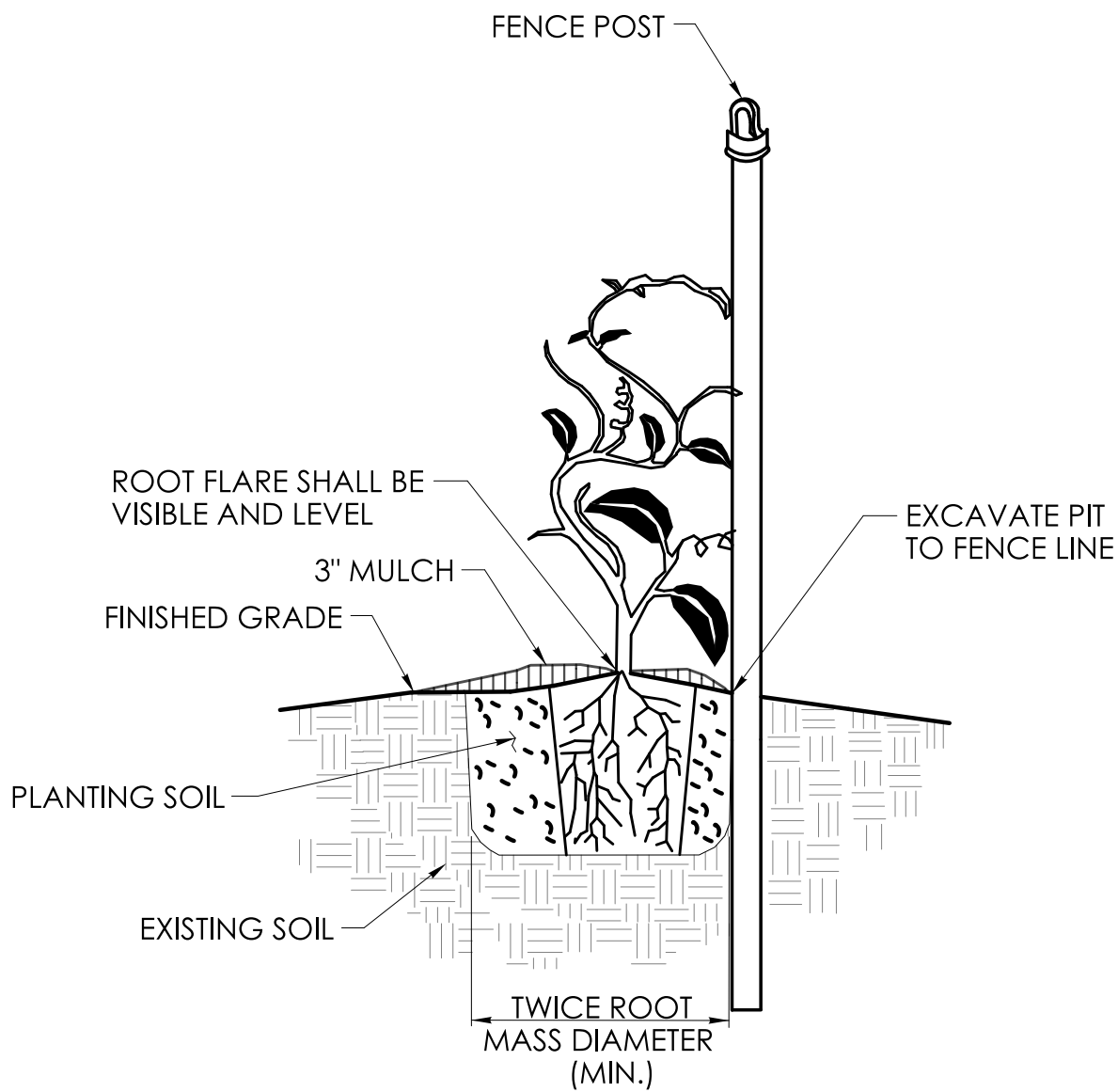
HEAVY CLAY PLANTINGS



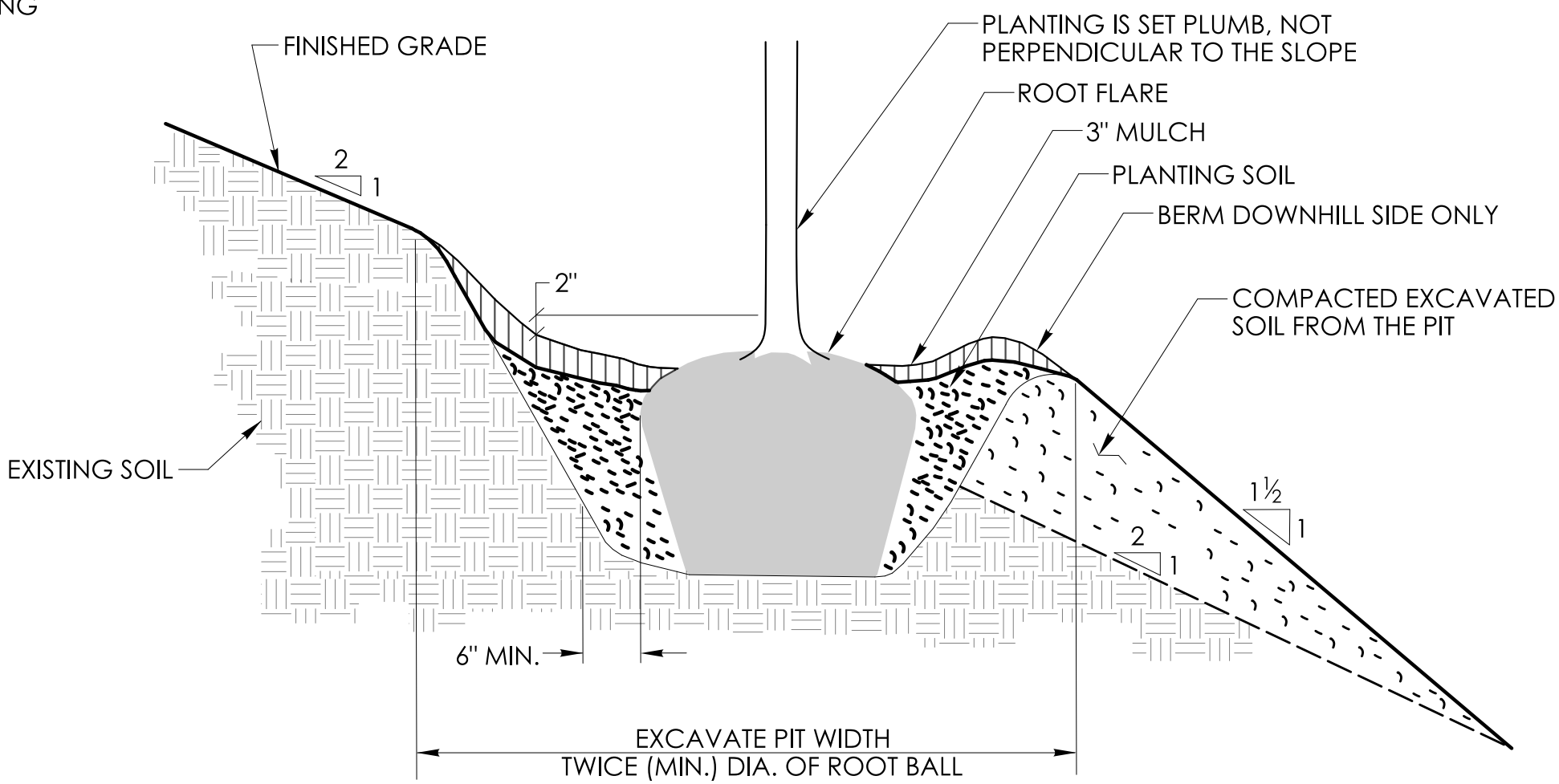
CONTAINER GROWN PLANTING



BACKFILL AND MULCH FOR PLANTING



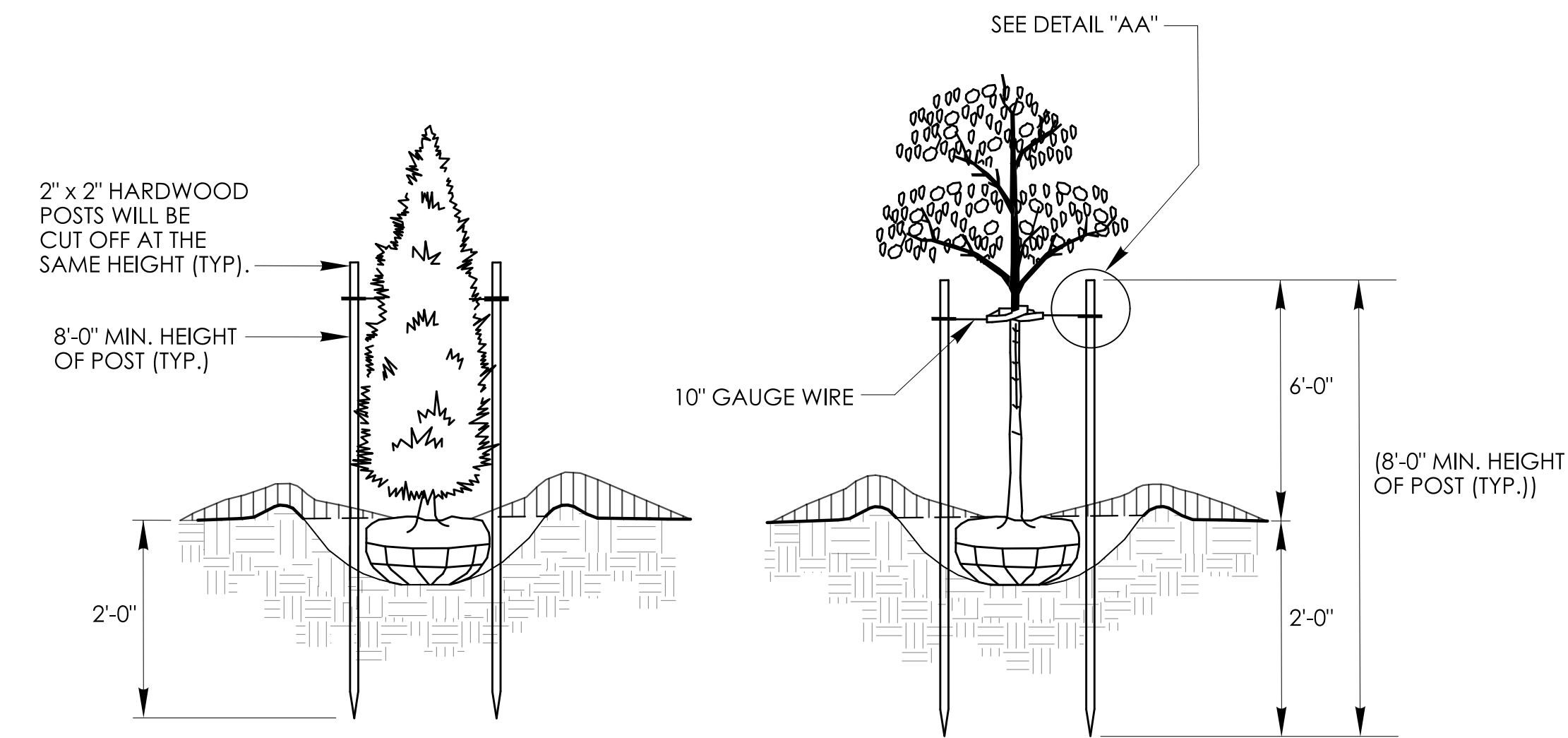
VINE PLANTING



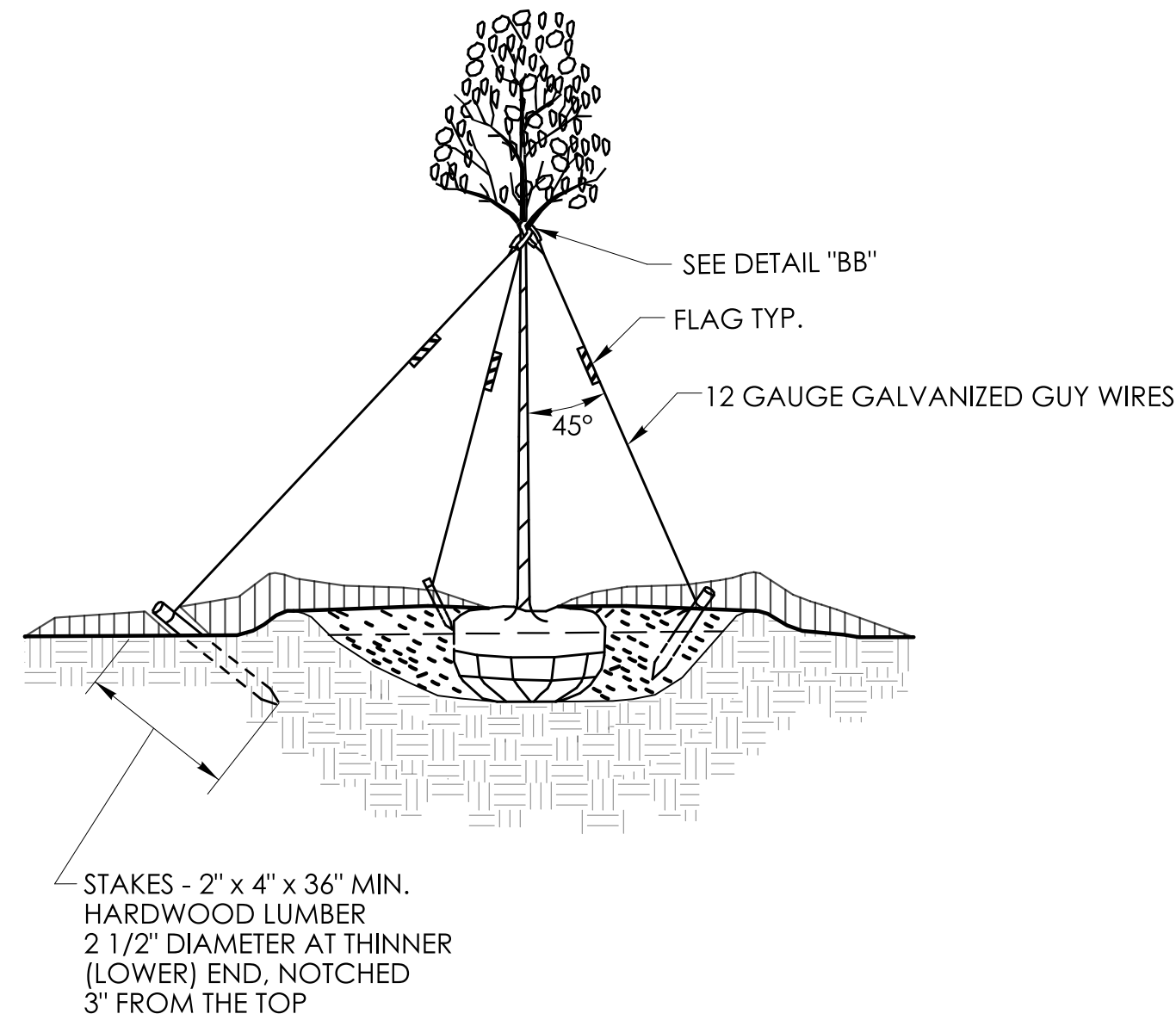
SLOPE PLANTING

GENERAL NOTES:

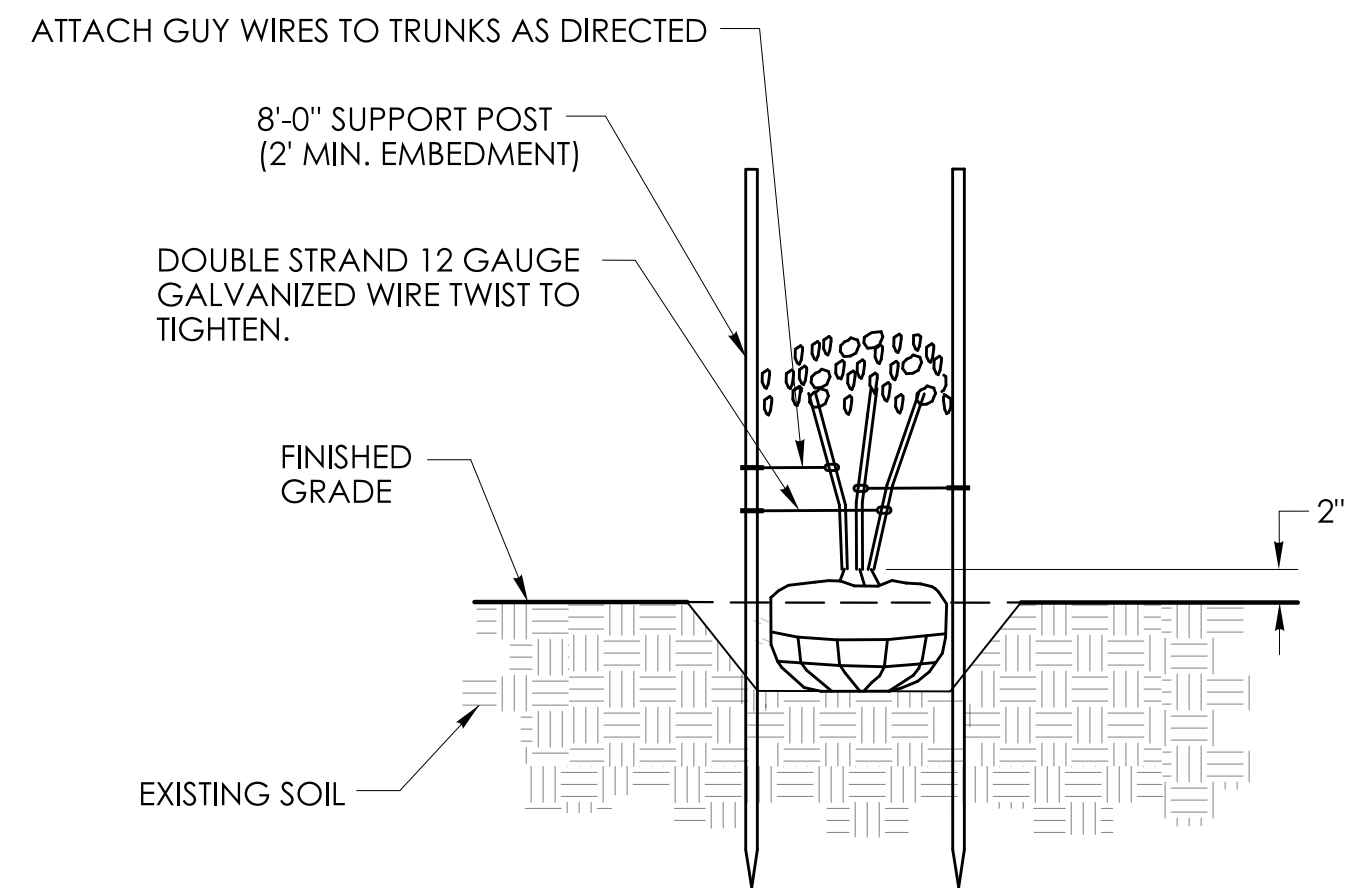
1. ALL EXTERIOR PACKAGING MATERIAL APPLIED TO PLANTS SHALL BE REMOVED AFTER THE PLANT IS LOCATED IN THE PIT EXCAVATION. CUT AND REMOVE TWINE, BURLAP OR WIRE BASKETS FROM THE TOP TWO-THIRDS OF THE ROOT BALL.
2. PLANT MALUS SPECIES (DECIDUOUS APPLE TREES OR SHRUBS) DEEP ENOUGH IN PIT TO COVER THE GRAFT TO PREVENT SPROUTING FROM THE ROOT STOCK.



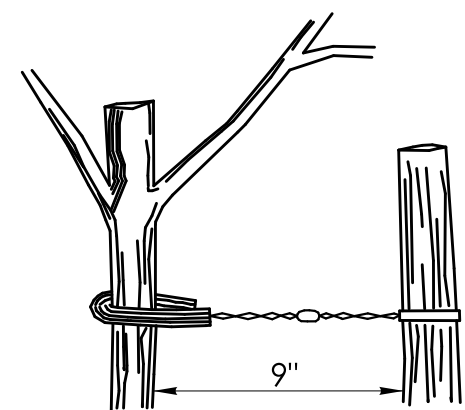
TWO STAKES



THREE GUYS AND STAKES



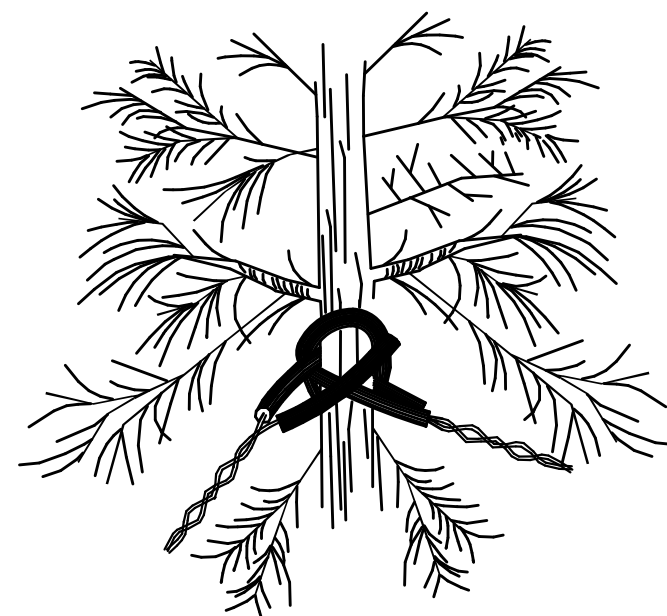
STAKING FOR MULTI-STEMMED TREES



DETAIL "AA"
POST AND GUY WIRE

ANCHOR TREE TO POST(S) USING GALVANIZED GUY WIRE AND 3/8" MIN. INSIDE DIAMETER RUBBER HOSE

GUY WIRES SHOULD BE PLACED AT LEAST HALF WAY UP THE TRUNK



DETAIL "BB"
GUY WIRES AROUND TRUNK

ANCHOR TREE TO STAKES USING GALVANIZED GUY WIRES AND 3/8" MIN. INSIDE DIAMETER RUBBER HOSE

GUY WIRES SHOULD BE PLACED AT LEAST HALF WAY UP THE TRUNK

GENERAL NOTES:



1. THE CONTRACTOR SHALL SUBMIT A STAKING PLAN FOR APPROVAL.
2. THE CONTRACTOR SHALL SUBMIT THE USE OF ANY OTHER MATERIALS FOR APPROVAL.
3. USE 3 POSTS FOR STAKING TREES 3" CALIPER OR GREATER AND EVERGREEN TREES 8' HIGH OR GREATER
4. USE DOUBLE STRAND 12 GAUGE GALVANIZED GUY WIRE FOR DECIDUOUS TREES GREATER THAN OR EQUAL TO 3" CALIPER AND USE DOUBLE STRAND 10 GAUGE GALVANIZED GUY WIRE FOR EVERGREEN TREES GREATER THAN OR EQUAL TO 8" CALIPER

ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

****REVISED
OR ADDED**

[illegible][illegible]

STANDARD SHEETS SHALL BE USED WITH STANDARD SPECIFICATIONS

	NOT TO SCALE	SIGNATURE BLOCK: OFFICE OF ENGINEERING 2800 BERLIN TURNPIKE NEWINGTON, CT 06111	SUBMITTED BY: _____	APPROVED BY: _____	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	 CTDOT STANDARD SHEET	STANDARD SHEET TITLE: TRAFFIC STANDARD SHEET INDEX	STANDARD SHEET NO.: TR-STD_INDEX
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DOCUMENT ALL LOOP DETECTOR VALUES BOTH CALCULATED AND MEASURED.

DEFINITIONS:

LOOP: #14 AWG WIRE IN SAWCUT, TERMINATED IN HANDHOLE, IMSA SPEC 51-7.
LEAD-IN: 14/2 SHIELDED TWISTED PAIR CABLE FROM HANDHOLE TO CONTROLLER, IMSA SPEC 50-2.
LOOP CIRCUIT: LOOP SAWCUT WIRE SPLICED TO 14/2 LEAD-IN CABLE.
AMPLIFIER: ELECTRONIC DEVICE CONNECTED TO LOOP CIRCUIT. SENSES CHANGE IN RESONANT FREQUENCY AND CREATES AN OUTPUT TO THE CONTROLLER.
MEGOHMETER: INSTRUMENT SPECIFICALLY DESIGNED TO TEST THE INSULATION RESISTANCE OF A CIRCUIT. COMMON MANUFACTURERS: AMEC®, AMPROBE®, FLUKE®, MEGGER®.

1: RESISTANCE:

- 1a: INSULATION RESISTANCE: PERFORM A 600 VOLT (MINIMUM) MEGOHMMETER TEST ON LOOP CIRCUIT. THE LOOP AMPLIFIER MUST BE DISCONNECTED FROM THE LOOP CIRCUIT OR THE LOOP AMPLIFIER WILL BE DAMAGED. THE RESISTANCE OF THE LOOP WIRE TO GROUND MUST BE GREATER THAN 100 MEG OHMS.
- 1b: WIRE RESISTANCE: MEASURE THE DC RESISTANCE OF THE LOOP CIRCUIT. THE LOOP CIRCUIT MUST BE DISCONNECTED FROM THE AMPLIFIER. USING AN OHMMETER CONNECTED ACROSS THE LOOP CIRCUIT, MEASURE THE DC RESISTANCE OF THE CONDUCTORS. THE RESISTANCE SHOULD BE LESS THAN 4 OHMS.
- NOTE: ALL TESTS SHALL BE DONE AT THE CONTROLLER ASSEMBLY (CA), HOWEVER IT IS RECOMMENDED TO PERFORM A PRELIMINARY MEGOHMMETER TEST AT THE HANDHOLE PRIOR TO SEALING THE SAWCUT AND SPLICING TO THE LEAD-IN. IF A DEFECTIVE LOOP WIRE IS FOUND, IT MAY BE EASILY REPLACED.

2: LOOP CIRCUIT INDUCTANCE:

- 2a: CALCULATE INDUCTANCE OF LOOP (L_{LOOP}) AND LEAD-IN CABLE (L_{14/2}).
- LOOP INDUCTANCE (ENGLISH) LOOP INDUCTANCE (METRIC)
- L_{LOOP} = (P/4) (N² + N) L_{LOOP} = (3.28P/4) (N² + N)
- LEAD-IN INDUCTANCE LEAD-IN INDUCTANCE
- L_{14/2} = (0.24µH/FT) (D) L_{14/2} = (0.78µH/m) (D)

WHERE:

L_{LOOP} = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS IN MICROHENRIES (µH).
L_{14/2} = INDUCTANCE OF LEAD-IN CABLE.
P = PERIMETER OF INDIVIDUAL LOOP SEGMENT, IN FEET OR METERS.
N = NUMBER OF TURNS.
D = LENGTH OF LEAD-IN CABLE FROM SPLICE IN HANDHOLE TO CONTROLLER, IN FEET OR METERS.

L_T = L₁ + L₂ + L₃ etc.,
(TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN SERIES.)

L_T = 1 / [(1 / L₁) + (1 / L₂) + (1 / L₃) + etc.],
(TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN PARALLEL.

WHERE:

L_T = TOTAL INDUCTANCE OF THE SEGMENTED ARRANGEMENT.
L₁, L₂, L₃ = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS.

EXAMPLE: (IN ENGLISH)

6' x 6', 4 TURNS, APPROXIMATELY 300' FROM THE CONTROLLER

L_{LOOP} = (24/4) (4² + 4) L_{14/2} = (0.24µH/FT) (300)
L_{LOOP} = (6) (20) L_{14/2} = (0.24) (300)
L_{LOOP} = 120 µH L_{14/2} = 72 µH

- 2b: MEASURE INDUCTANCE OF LOOP AND LEAD-IN AT CONTROLLER. USE INSTRUMENT DESIGNED TO MEASURE LOOP CIRCUIT INDUCTANCE.

3: POWER INTERRUPTION:

AFTER THE AMPLIFIER HAS TUNED AND IS OPERATING, DISCONNECT POWER BY REMOVING FUSE OR HARNESS CONNECTOR. RETURN POWER TO THE AMPLIFIER AND CONFIRM IT RE-TUNES AUTOMATICALLY WITHOUT ANY MANUAL ADJUSTMENTS.

INDUCTIVE LOOP TEST PROCEDURE

PIN	COLOR	FUNCTION
A	WHITE	110 VAC Neutral
B	BROWN	Output Relay Common (moving contact)
C	BLACK	110 VAC (Fused)
D	RED	Loop
E	ORANGE	Loop
F	YELLOW	Output Relay Contact (Closes with moving contact when detecting vehicle)
G	BLUE	Output Relay Contact (Opens with moving contact when detecting vehicle)
H	GREEN	Chassis Ground
J	GREY	110 VAC Delay/Extend Override
Shell		Ground (shall be connected to pin H in the connector)

DETECTOR AMPLIFIER PIN DESIGNATION

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:		
	INDUCTIVE LOOP DETECTOR	
	SAW CUT	
	RIGID METAL CONDUIT	
	HANDHOLE	

REV.	DATE	REVISION DESCRIPTION
2	1-2014	REVISED GROUND RESISTANCE NOTES.
1	4-2012	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.
Plotted Date: 1/7/2014

DIMENSIONS ARE IN ENGLISH ("") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.
NOT TO SCALE

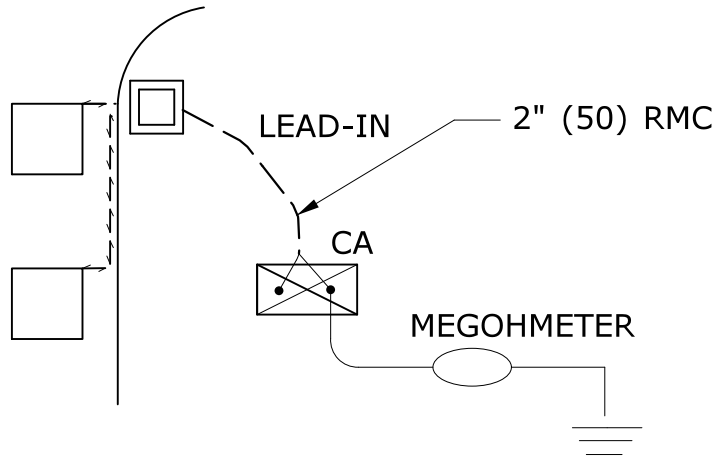
	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	
Filename: CTDOT_TRAFFIC_STD.DGN	Model: TR-1000_01	

SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

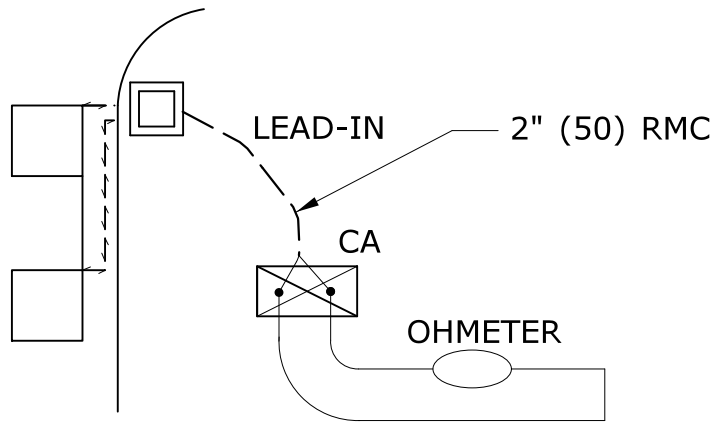
CTDOT STANDARD SHEET
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:	GENERAL CLAUSES (TEST PROCEDURES)
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STANDARD SHEET NO.:	TR-1000_01
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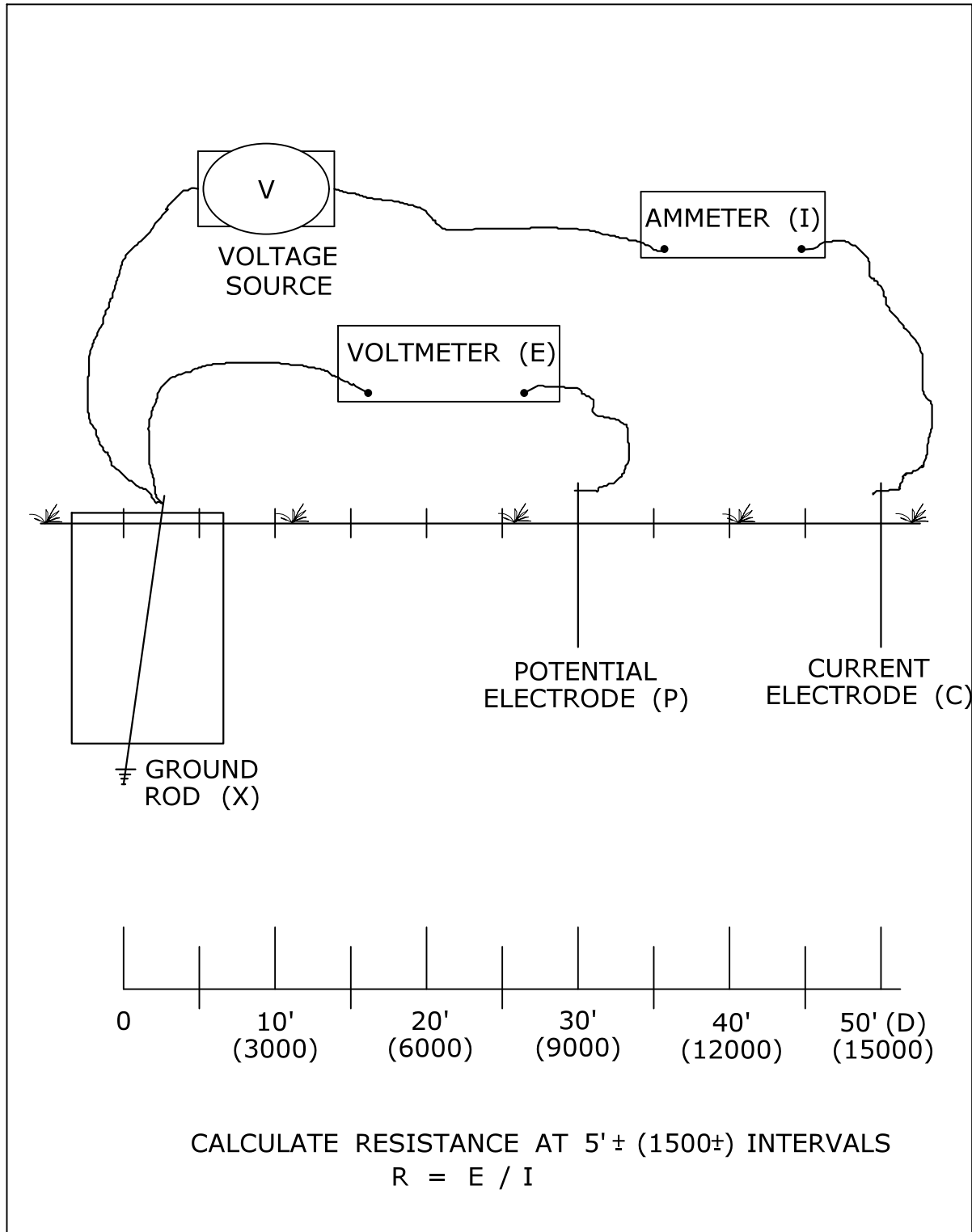
TEST 1a



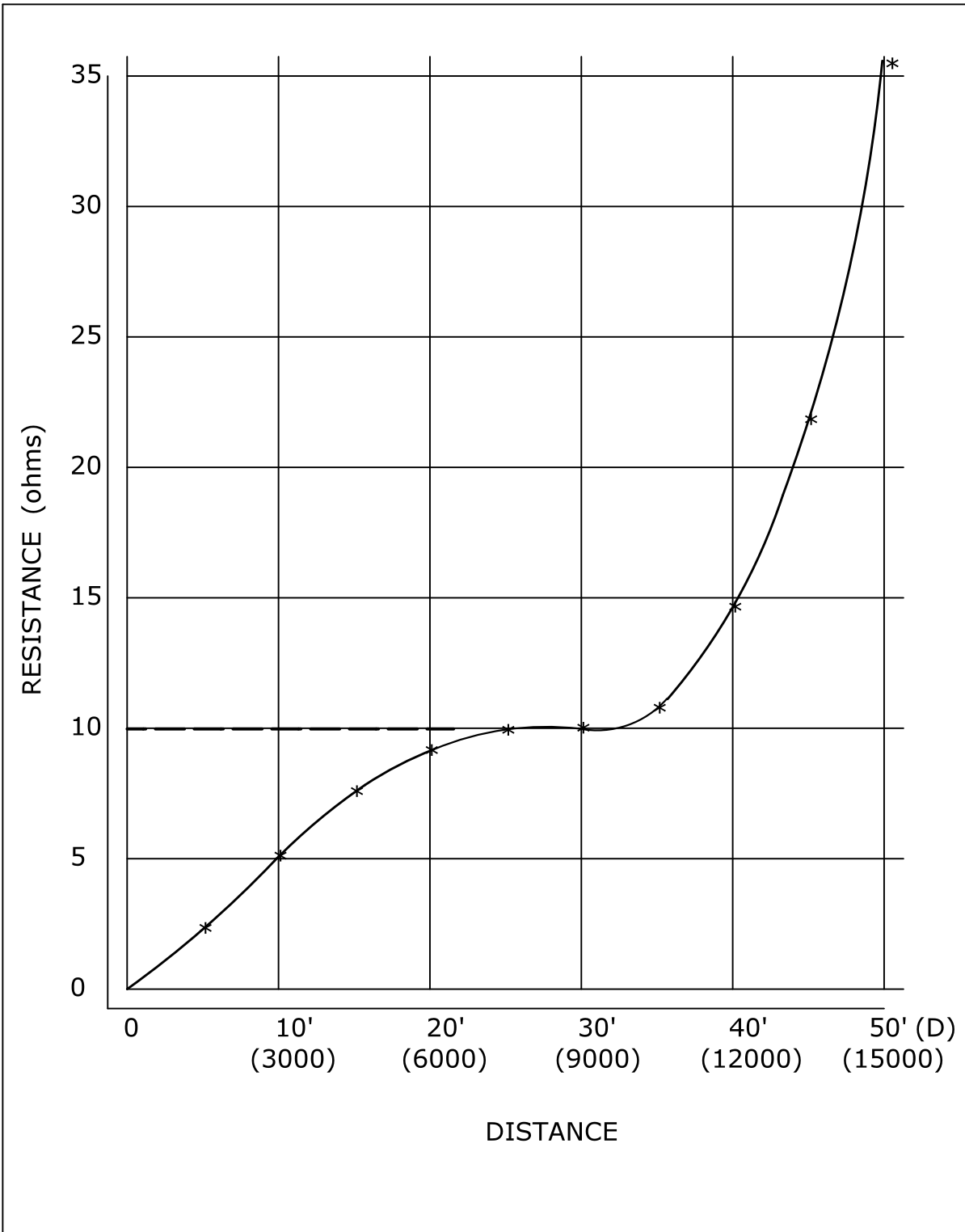
TEST 1b

PROJECT:		LOCATION:			
TOWN:					
LOOP NUMBER	RESISTANCE OHMS		INDUCTANCE MICROHENRIES (µH)		AMPLIFIER POWER INTERRUPTION PASS/FAIL (3)
	TO GROUND (1a)	LOOP WIRE (1b)	CALCULATED (2a)	MEASURED (2b)	
D1 FRONT					
D1 REAR					
D2A					
D2B					
D4A FRONT					
D4B REAR					
D5					
D6A					
D6B					

LOOP CIRCUIT TEST DATA (EXAMPLE)



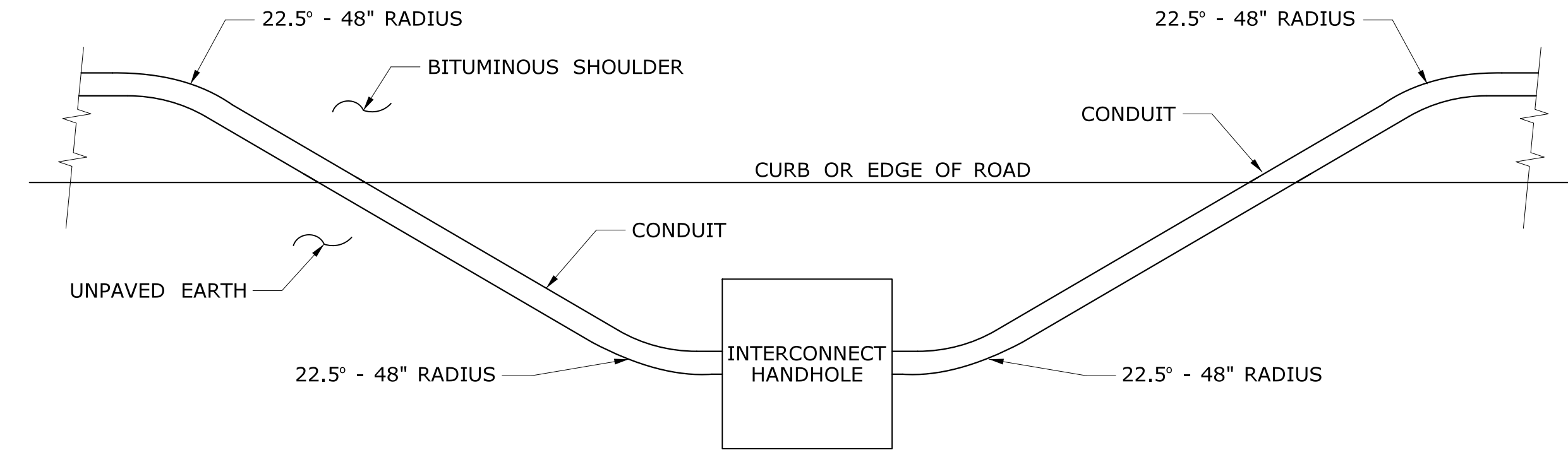
3 POINT GROUND RESISTANCE TEST CIRCUIT



GROUND RESISTANCE CHART (EXAMPLE)

- NOTES:
- WHEN REQUESTED BY THE ENGINEER, MEASURE RESISTANCE-TO-GROUND OF GROUND ROD AT TRAFFIC CONTROL FOUNDATIONS. SEE FALL-OF-POTENTIAL METHOD. IF LESS THAN 10 ohms, INSTALL SUPPLEMENTAL ELECTRODES AS REQUIRED. NEC ARTICLE 250.
 - DURING THE TEST, THE GROUND ROD SHOULD NOT BE BONDED TO ANY RMC IN THE FOUNDATION.
 - THE VOLTAGE SOURCE, VOLTMETER, AMMETER, ELECTRODES P AND C, AND CONNECTING CABLES ARE AVAILABLE AS A SPECIALIZED TEST INSTRUMENT.
 - REFER TO NATIONAL ELECTRICAL SAFETY CODE (NEC) SECTION 09, GROUNDING METHODS FOR ELECTRIC SUPPLY AND COMMUNICATIONS FACILITIES.
 - REFER TO NATIONAL ELECTRICAL CODE (NEC) CHAPTER 2, ARTICLE 250, GROUNDING.

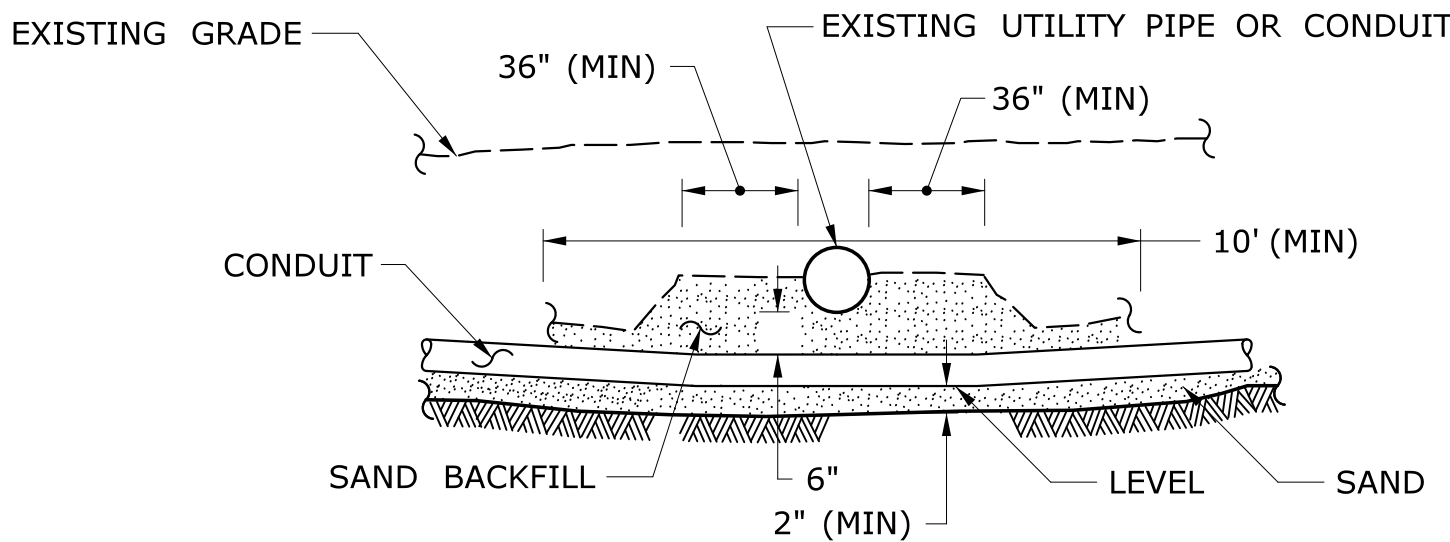
3 POINT FALL-OF-POTENTIAL GROUND RESISTANCE TEST



INTERCONNECT TYPICAL CURB CROSSING

NOTES:

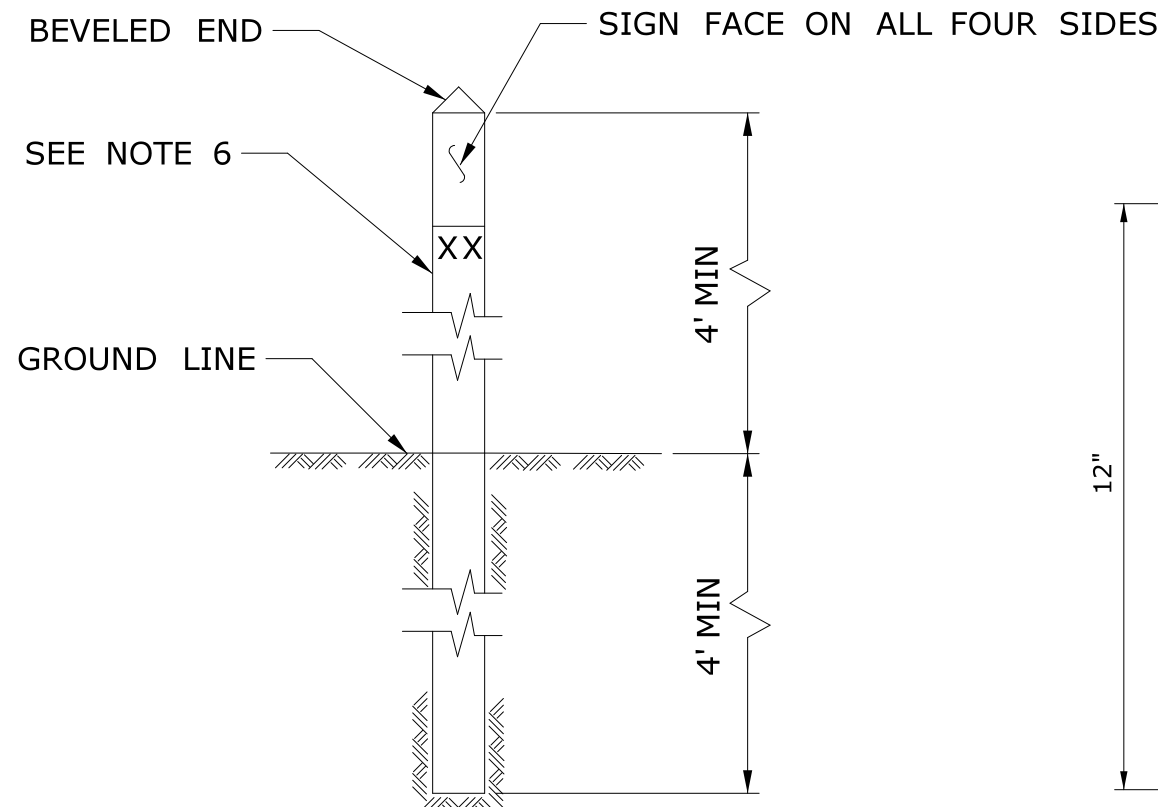
1. RESTORE AREAS DISTURBED BY TRENCH TO ORIGINAL CONDITION.



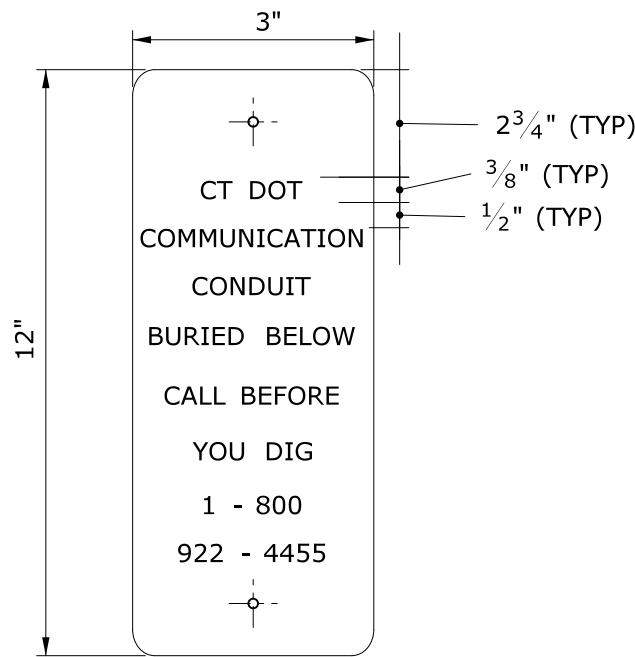
CROSSING UNDER EXISTING UTILITY

NOTES:

1. WHEN ENCOUNTERED AT APPROXIMATELY THE SAME DEPTH, CROSS BENEATH.
2. PROTECT & SUPPORT EXPOSED EXISTING UTILITY.

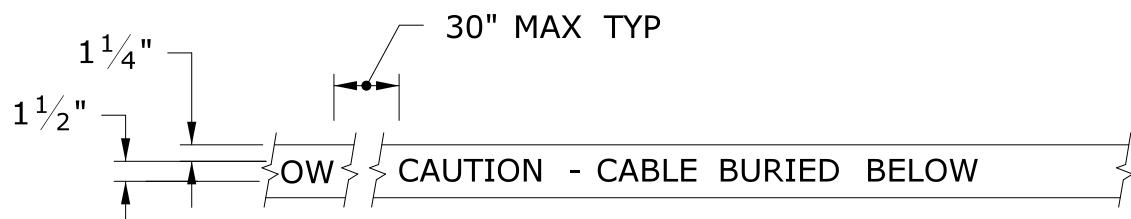


INTERCONNECT CONDUIT IDENTIFICATION POST



SIGN FACE DETAIL

SIGN # 41-4669



DETECTABLE WARNING TAPE

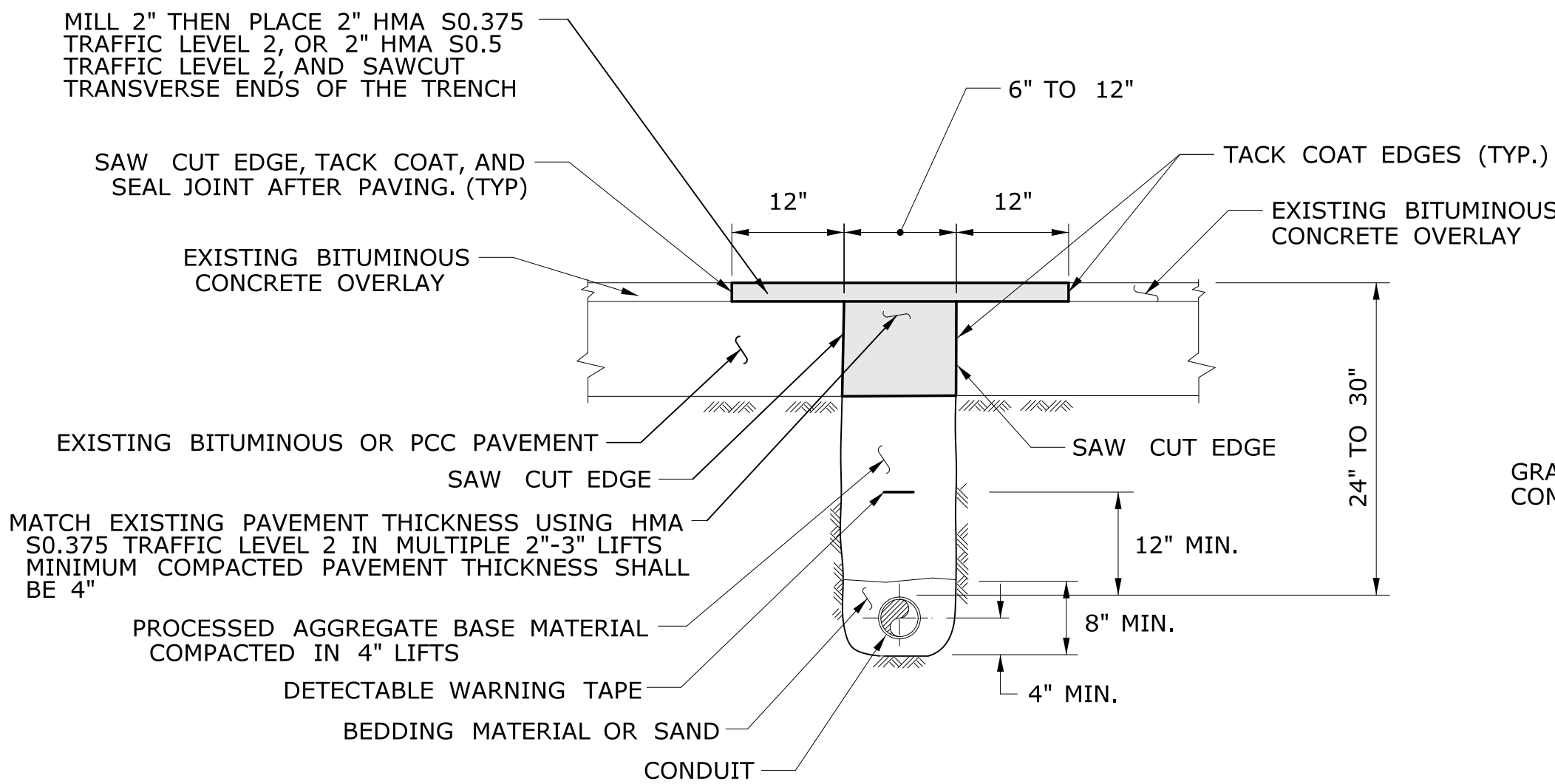
NOTE:

STANDARD SPECIFICATIONS, ARTICLE: 1.05.15

1. TAPE COLORS:
COMMUNICATION - ORANGE BACKGROUND / BLACK LEGEND
POWER - RED BACKGROUND / BLACK LEGEND

NOTES:

1. 4" x 4" NOMINAL, PRESSURE TREATED WOOD POST.
2. ATTACH SIGN TO POST WITH 1/4" x 1 1/4" STAINLESS STEEL LAG SCREW WITH NYLON WASHER ON FACE OF SIGN.
3. SIGN COLORS: BACKGROUND - ORANGE (RETROREFLECTIVE) LEGEND - BLACK (OPAQUE).
4. INSTALL POST APPROX 24" FROM RMC IN VICINITY OF EACH INTERCONNECT HANDHOLE.
5. INSTALL ADDITIONAL POST(S) WHERE SPACING BETWEEN INTERCONNECT HANDHOLES IS GREATER THAN 1500' APART.
6. PERMANENTLY ATTACH STAINLESS STEEL NUMBERS INDICATING DISTANCE IN FEET TO TRENCH CONTAINING COMMUNICATION CABLE. ATTACH NUMBERS TO SIDE OF POST FACING CONDUIT.
7. IDENTIFICATION POSTS WITH SIGNS SHALL BE PAID UNDER THE COMMUNICATION CABLE ITEM.

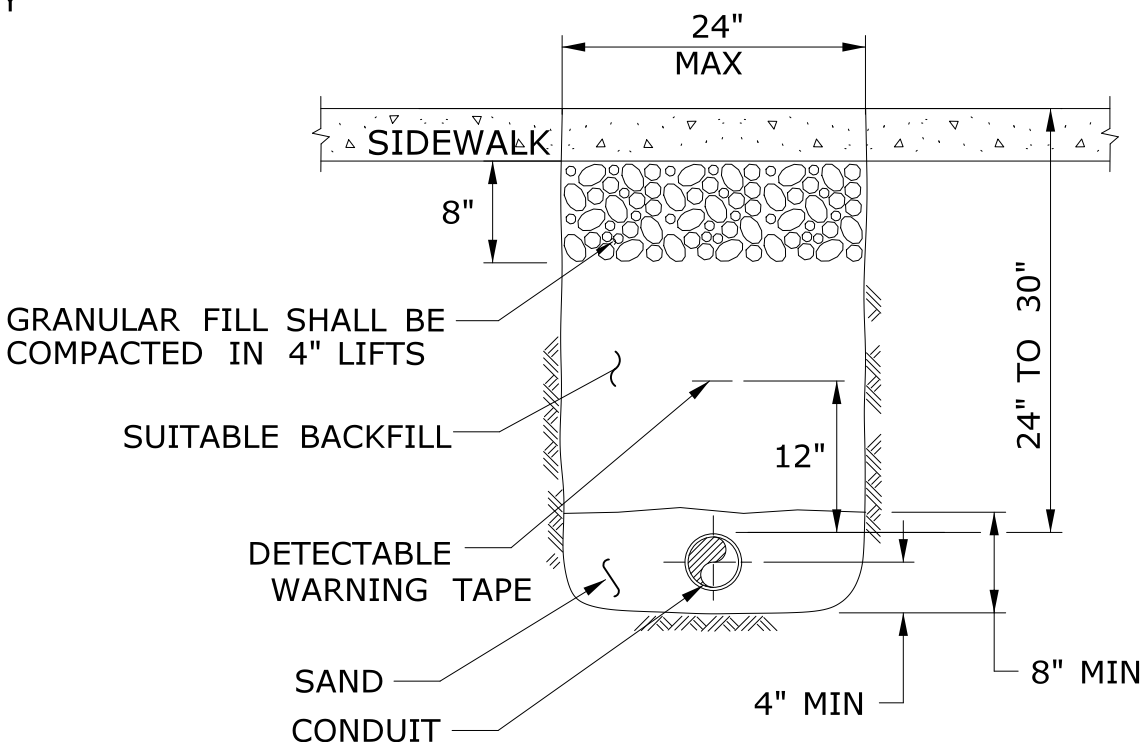


PAVEMENT - BITUMINOUS CONCRETE OR OVERLAYED PORTLAND CEMENT CONCRETE

NOTES:

STANDARD SPECIFICATION, SECTION 3.04 AND STANDARD SPECIFICATION, ARTICLE 4.06.03

1. TOTAL HOT MIX ASPHALT (HMA) THICKNESS TO MATCH EXISTING BITUMINOUS CONCRETE AND PORTLAND CEMENT CONCRETE (PCC) THICKNESS.
2. WHEN ALLOWED BY ENGINEER, USE CONTROLLED LOW STRENGTH MATERIAL (CLSM) AS BEDDING MATERIAL. TOP OF CLSM SHALL BE AT LEAST 20" BELOW SURFACE.

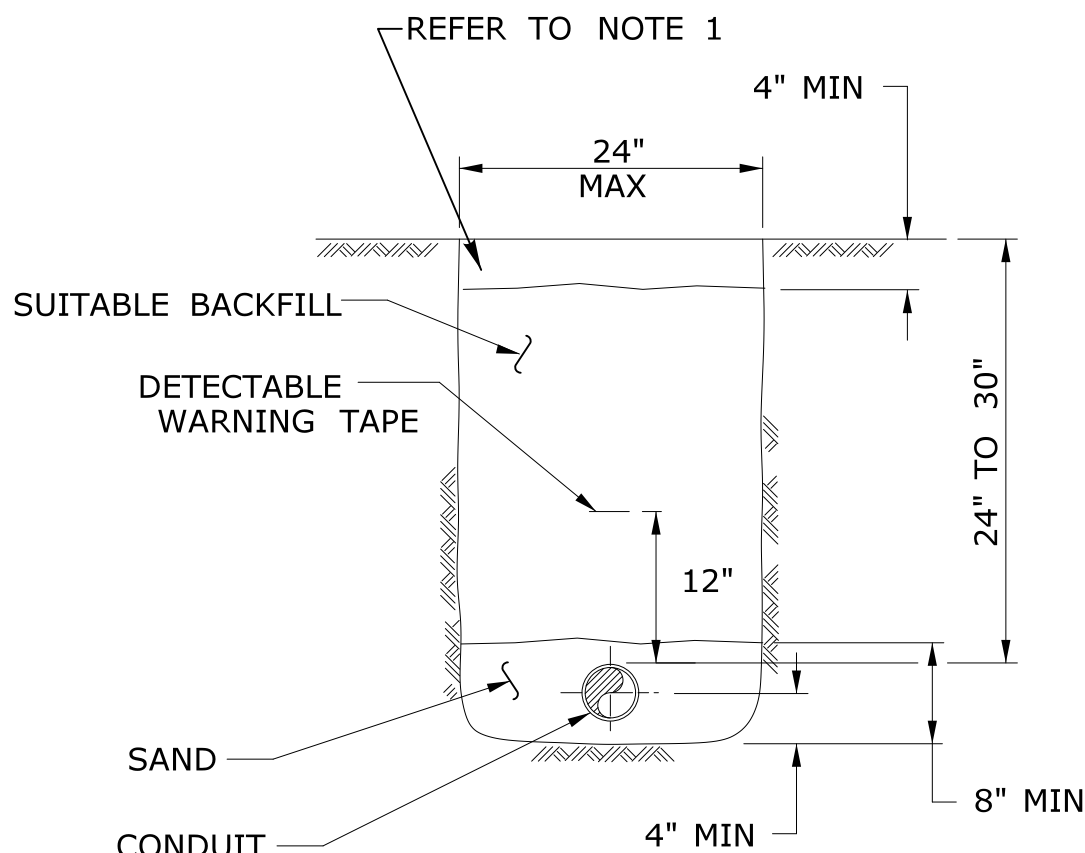


SIDEWALK

NOTES:

STANDARD SPECIFICATIONS, SECTIONS 9.21 & 9.22

1. WHERE CONCRETE SIDEWALK IS DAMAGED OR CUT, REPLACE THE ENTIRE SECTION BETWEEN JOINTS. REPLACEMENT SIDEWALK IS PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK".



EARTH

NOTES:

STANDARD SPECIFICATIONS, SECTION 9.50 OR SEEDING TYPE CALLED OUT ON THE PLANS.

1. IN MOWED AREAS: PLACE TOPSOIL, FERTILIZER, SEED, & MULCH.

GENERAL NOTES:

1. TOP OF CONDUIT AT LEAST 24" DEEP.
2. COMPACT BACKFILL IN ≤6" LIFTS. HAND COMPACTION NOT PERMITTED.

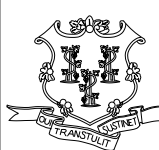
LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
---- PROPOSED RMC (RIGID METAL CONDUIT)
-●-●- EXISTING RMC (RIGID METAL CONDUIT)

NOT TO SCALE

SIGNATURE BLOCK:
OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

SUBMITTED BY:

APPROVED BY:



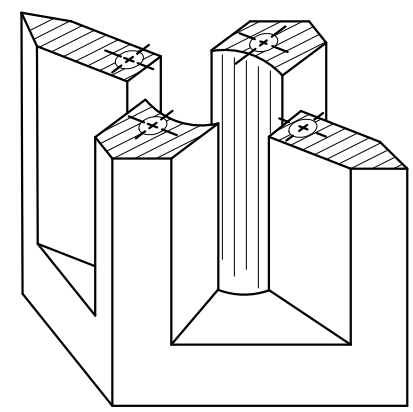
STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION



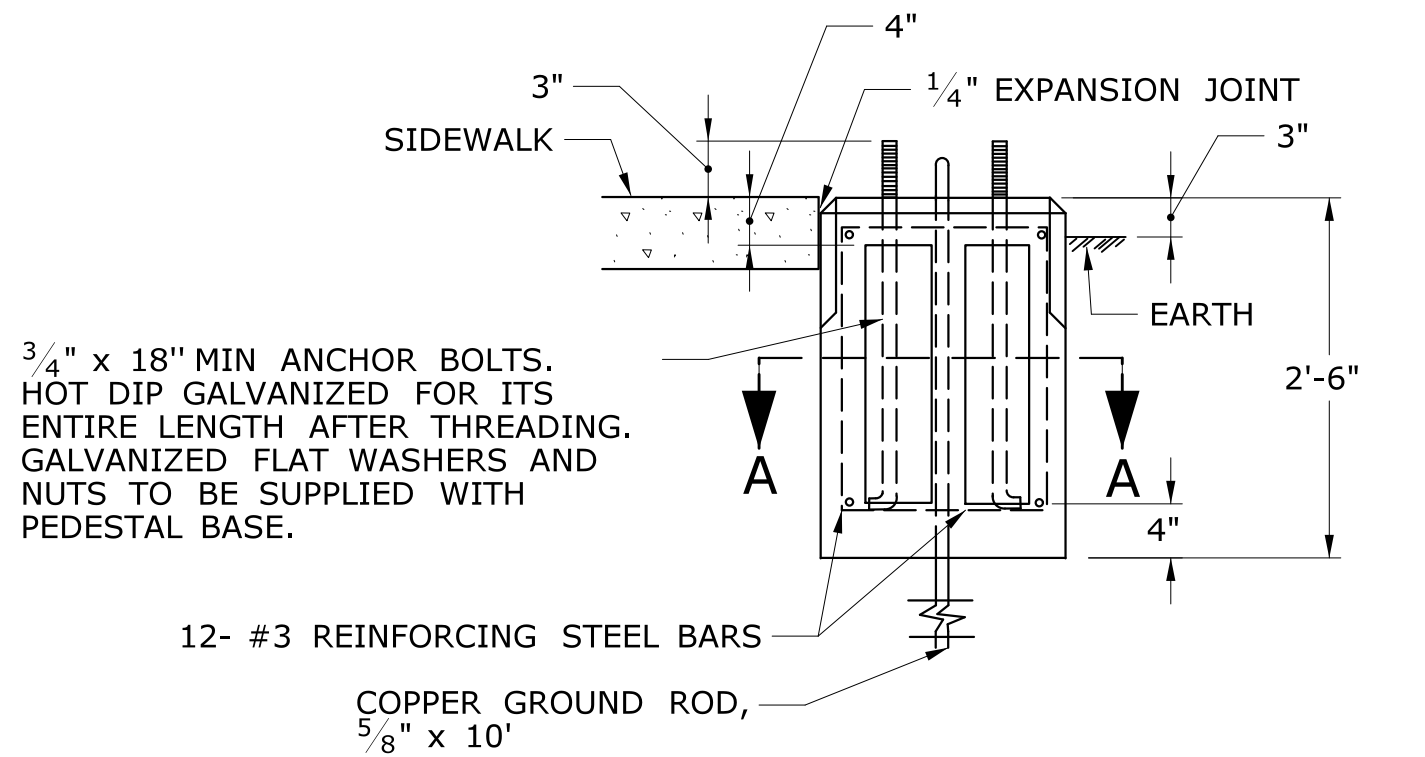
CTDOT
STANDARD SHEET

STANDARD SHEET TITLE:
TRENCHING & BACKFILLING, ELECTRICAL CONDUIT

STANDARD SHEET NO.:
TR-1001_01



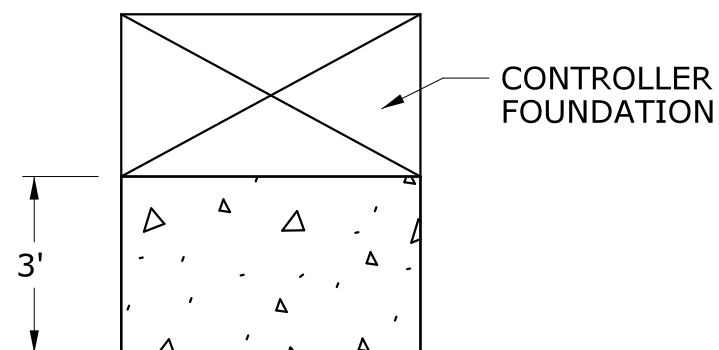
**PICTORIAL
SECTION A-A**



**TRAFFIC CONTROL FOUNDATION
PEDESTAL - TYPE I - PRECAST**

TRAFFIC CONTROL FOUNDATION PEDESTAL TYPE I NOTES:

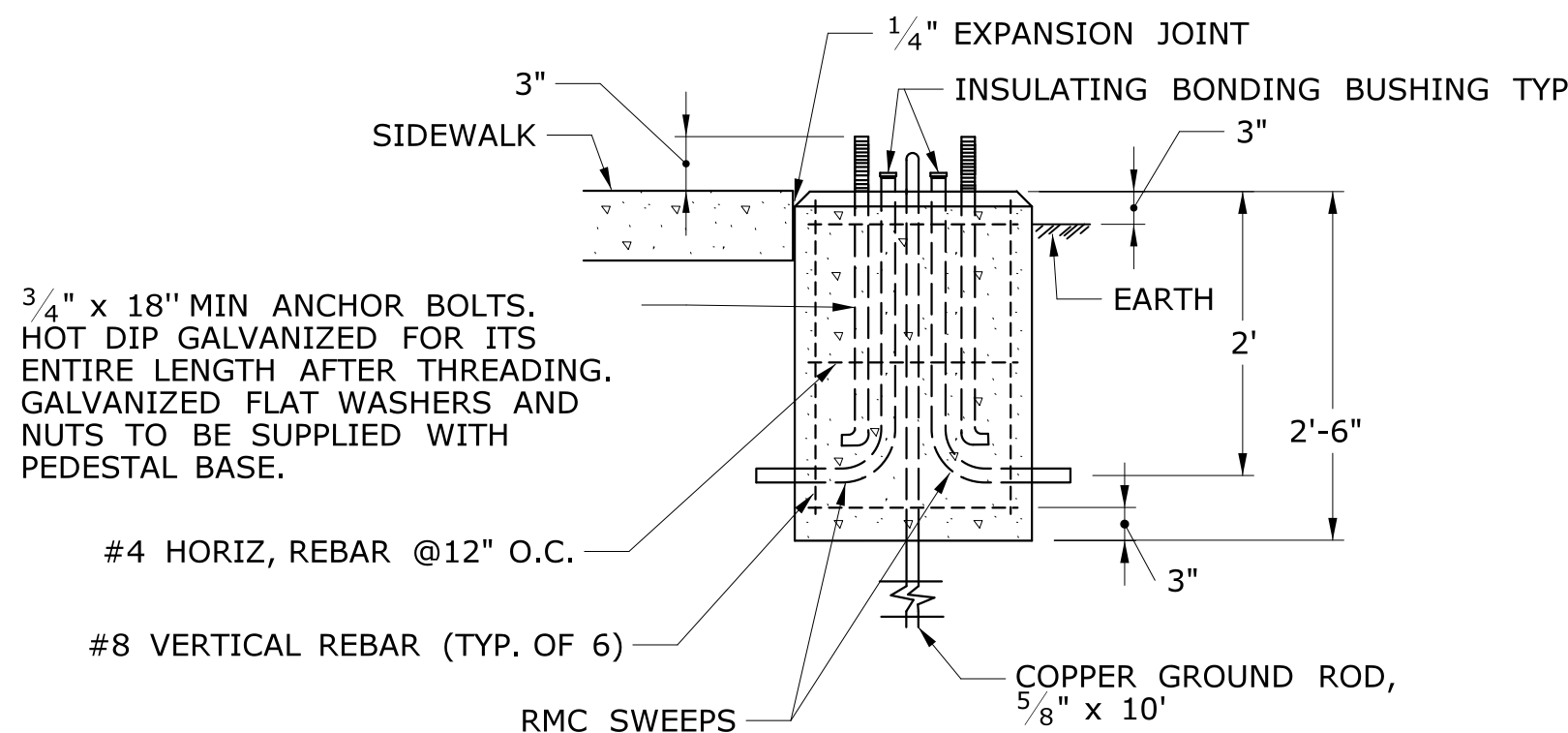
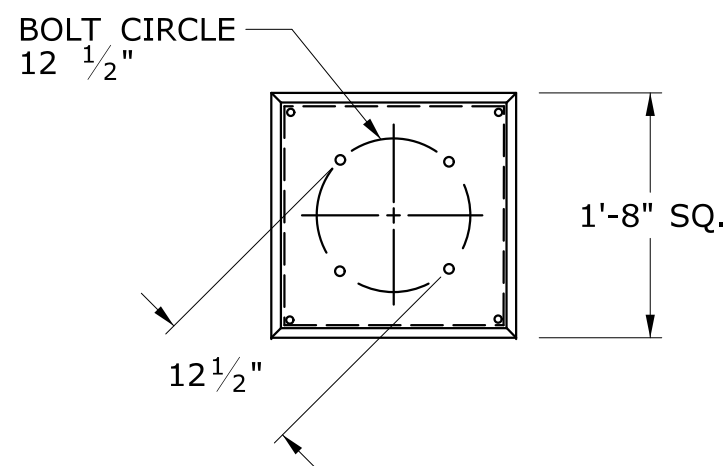
INSTALL FOUNDATION ON 12" OF COMPACTED GRANULAR FILL IN ACCORDANCE WITH SECTION 2.13
LEVEL FOUNDATION WITH A PROJECTION OF 3" ABOVE FINISHED GRADE.
INSTALL COPPER GROUND ROD: 5/8" X 10'
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND
GROUND ROD HAVE BEEN INSTALLED.
CONDUITS SHALL NOT PROJECT MORE THAN 2" ABOVE FOUNDATION.
CONCRETE: (CAST-IN PLACE) PCC04460 IN ACCORDANCE WITH SECTION M.03
CONCRETE: (PRECAST) PRC04060 IN ACCORDANCE WITH ARTICLE M.14.01



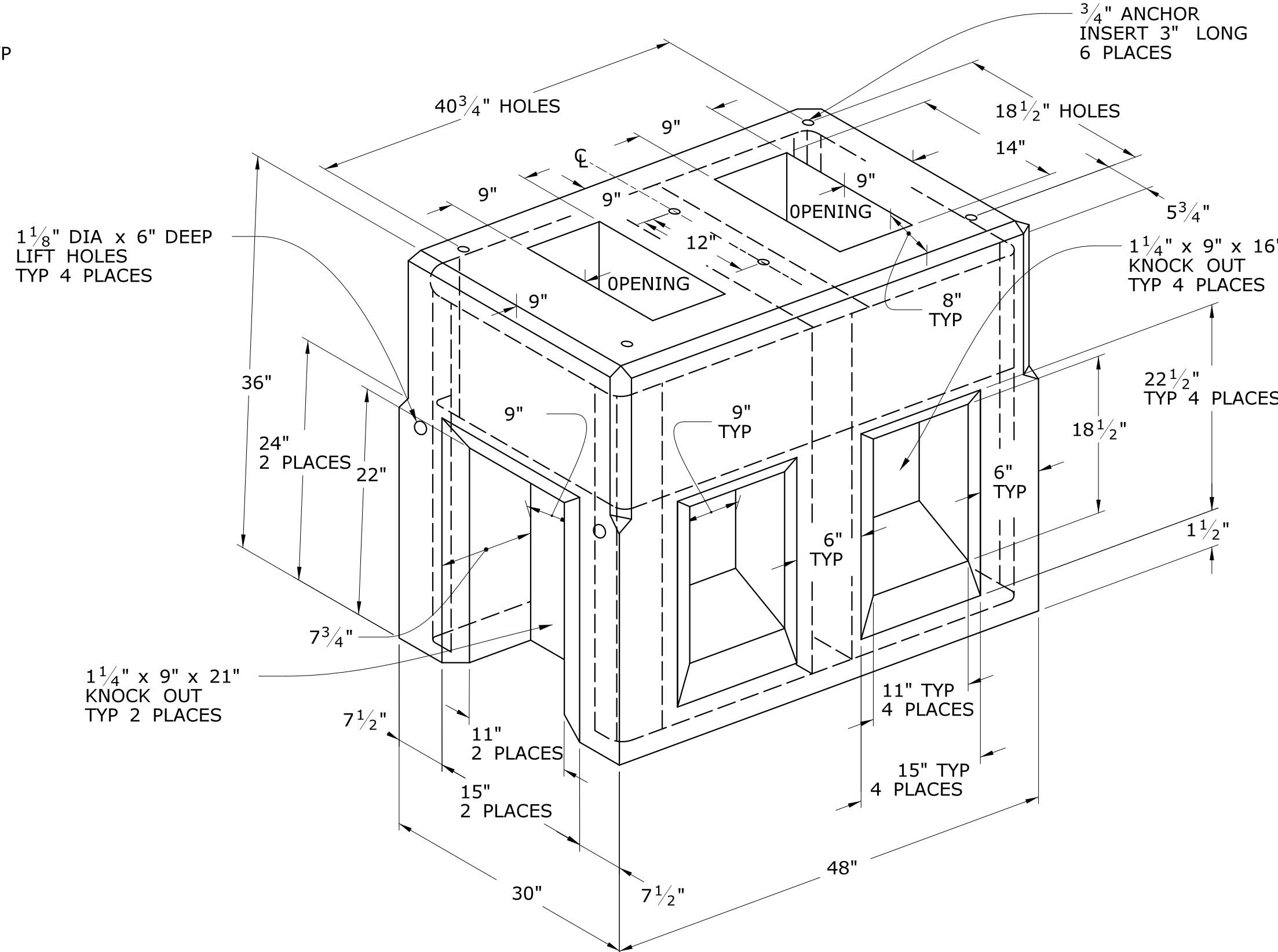
INSTALL PRECAST OR CAST IN PLACE CONCRETE SIDEWALK ON CABINET
DOOR SIDE OF CONTROLLER FOUNDATION.
PITCH SIDEWALK 1/4" PER FOOT AWAY FROM THE CONTROLLER FOUNDATION.
REFER TO HIGHWAY STANDARD SHEET HW-921-01 FOR SIDEWALK CONSTRUCTION.

**TYPICAL CONCRETE SIDEWALK
AT CONTROLLER FOUNDATION**

NOT TO SCALE



**TRAFFIC CONTROL FOUNDATION
PEDESTAL - TYPE I - CAST IN PLACE**



**TRAFFIC CONTROL FOUNDATION
CONTROLLER - TYPE IV - PRECAST**

TRAFFIC CONTROL FOUNDATION CONTROLLER TYPE IV NOTES:

INSTALL FOUNDATION ON 12" OF COMPACTED GRANULAR FILL IN ACCORDANCE WITH SECTION 2.13
LEVEL FOUNDATION WITH A PROJECTION OF 4" ABOVE FINISHED GRADE.
INSTALL COPPER GROUND ROD: 5/8" X 10'
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND
GROUND ROD HAVE BEEN INSTALLED. THE OPENINGS SHALL BE CAPPED WITH A 2"
GROUT LEVEL WITH THE TOP OF THE FOUNDATION AND NEATLY FINISHED. THE GROUT
SHALL CONFORM WITH THE REQUIREMENTS OF ARTICLE M.03.05.
#4 REBAR 2" MIN COVER AROUND ALL OPENINGS, 3-#4 REBARS IN ALL CORNERS.
CONDUITS SHALL NOT PROJECT MORE THAN 2" ABOVE FOUNDATION.
CONCRETE: (PRECAST) PRC04060 IN ACCORDANCE WITH ARTICLE M.14.01

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

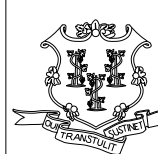
- PROPOSED CONTROLLER
- EXISTING CONTROLLER
- PEDESTAL MOUNTING

NOT TO SCALE

SIGNATURE BLOCK:
OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

SUBMITTED BY:

APPROVED BY:



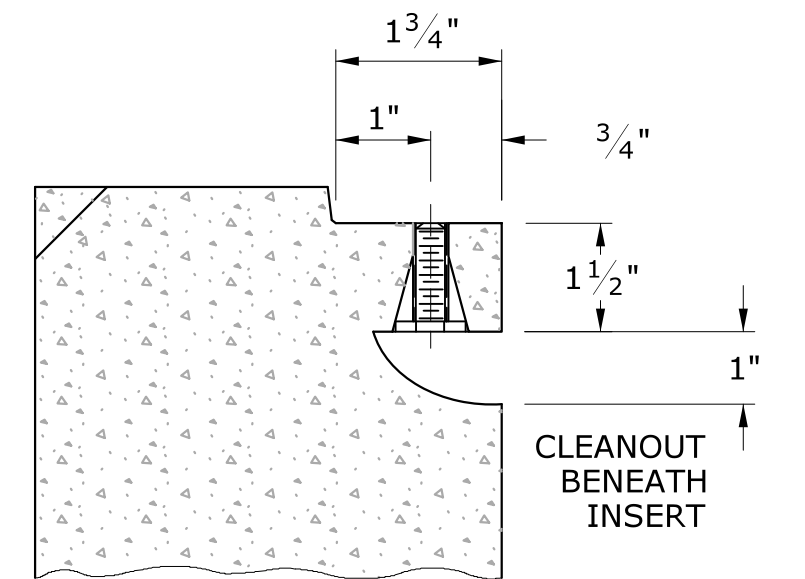
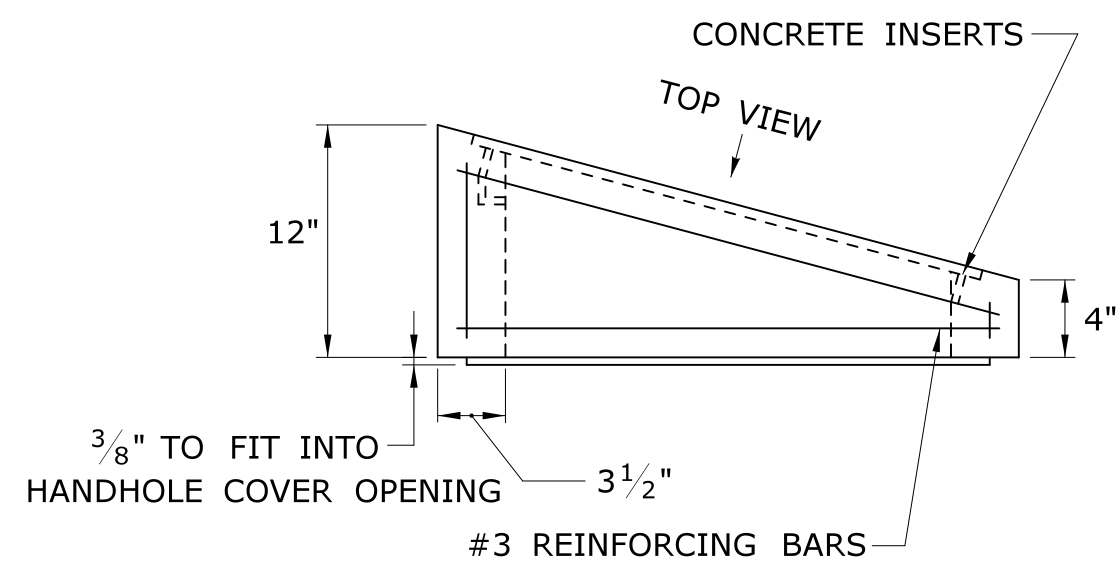
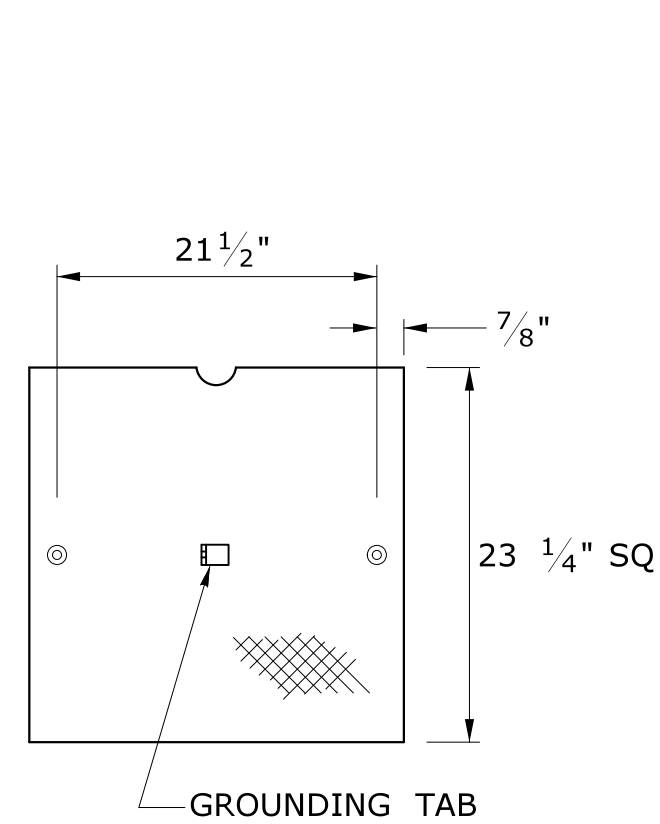
STATE OF CONNECTICUT
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OF
TRANSPORTATION



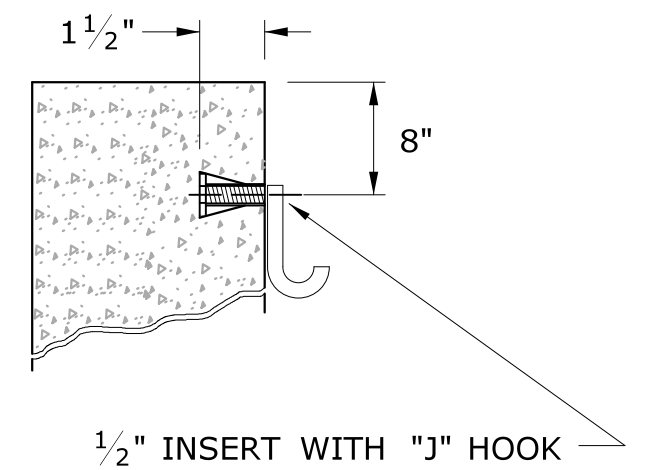
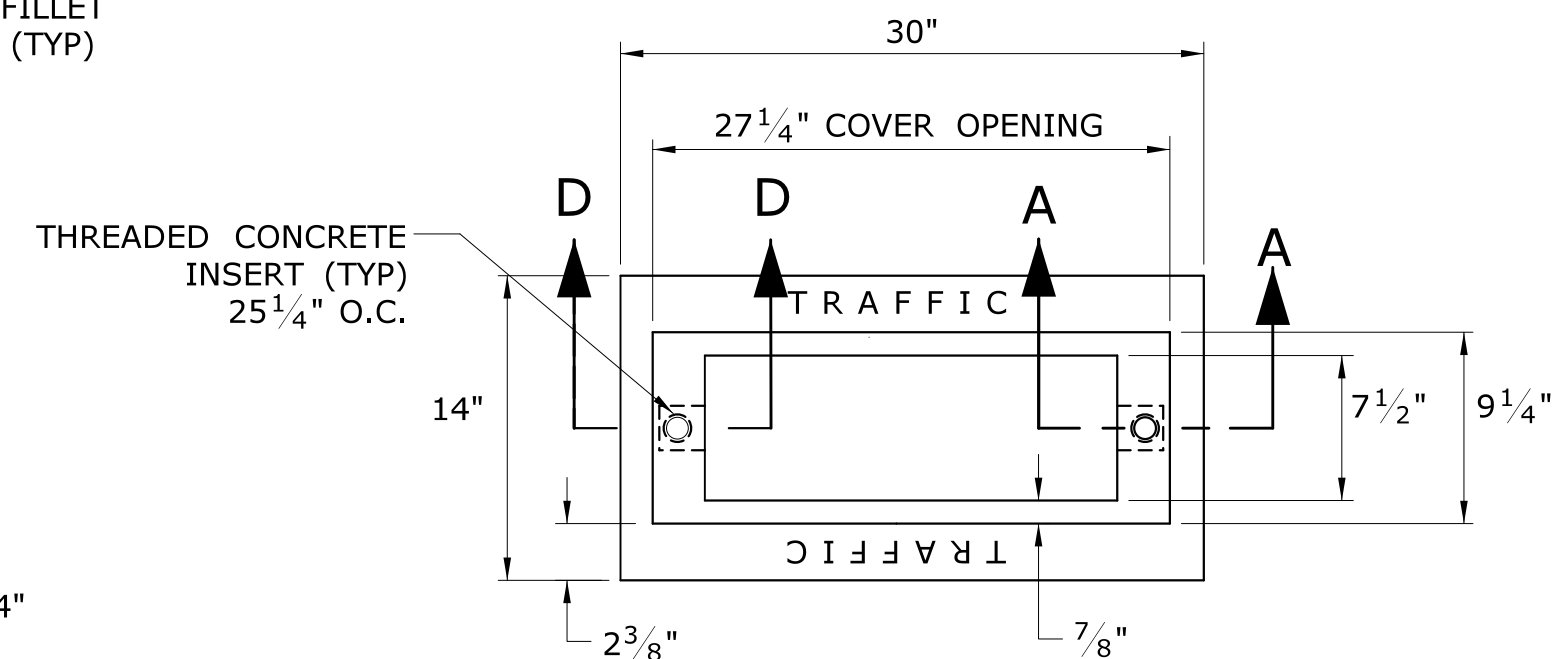
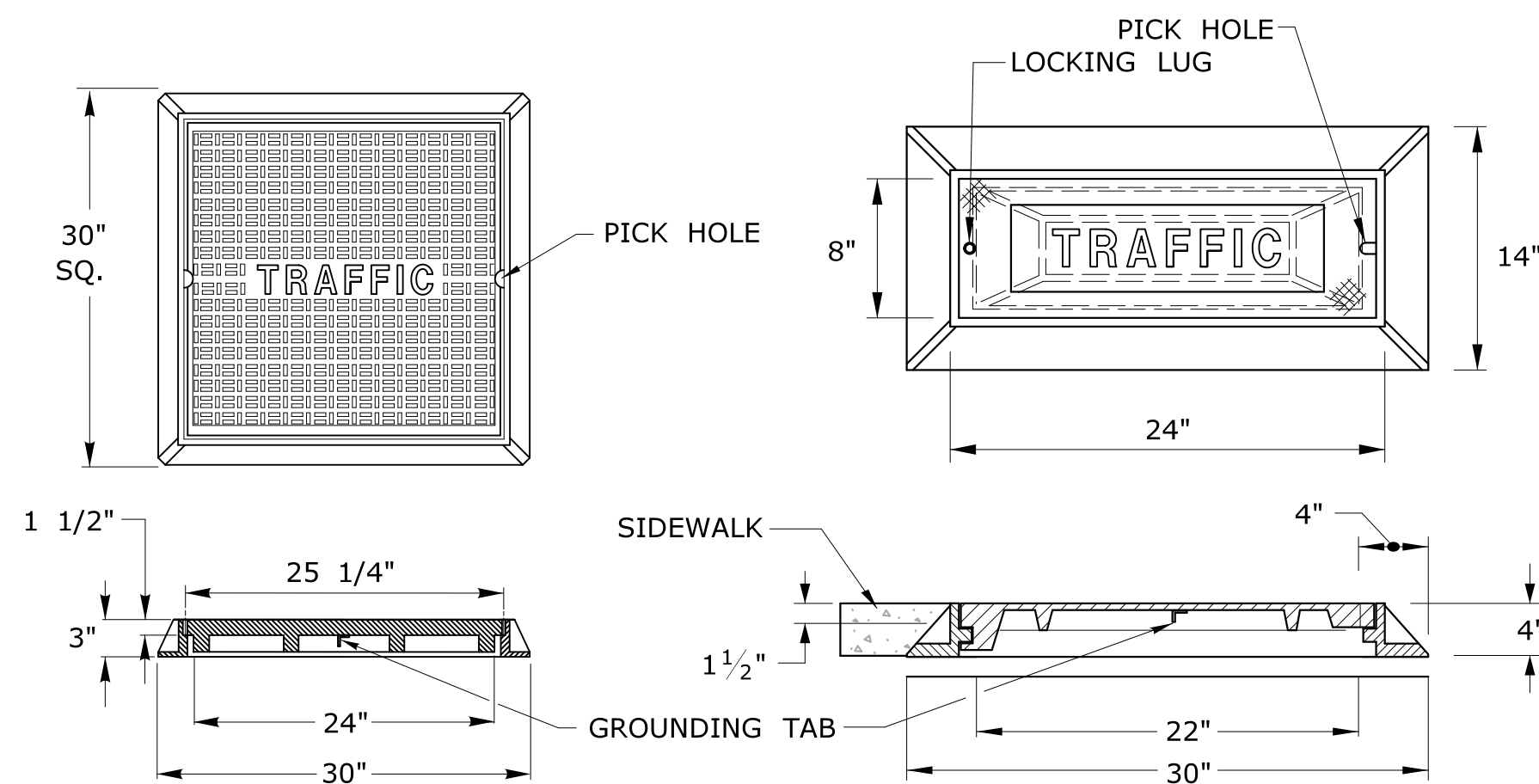
CTDOT
STANDARD SHEET

STANDARD SHEET TITLE:
TRAFFIC CONTROL FOUNDATIONS

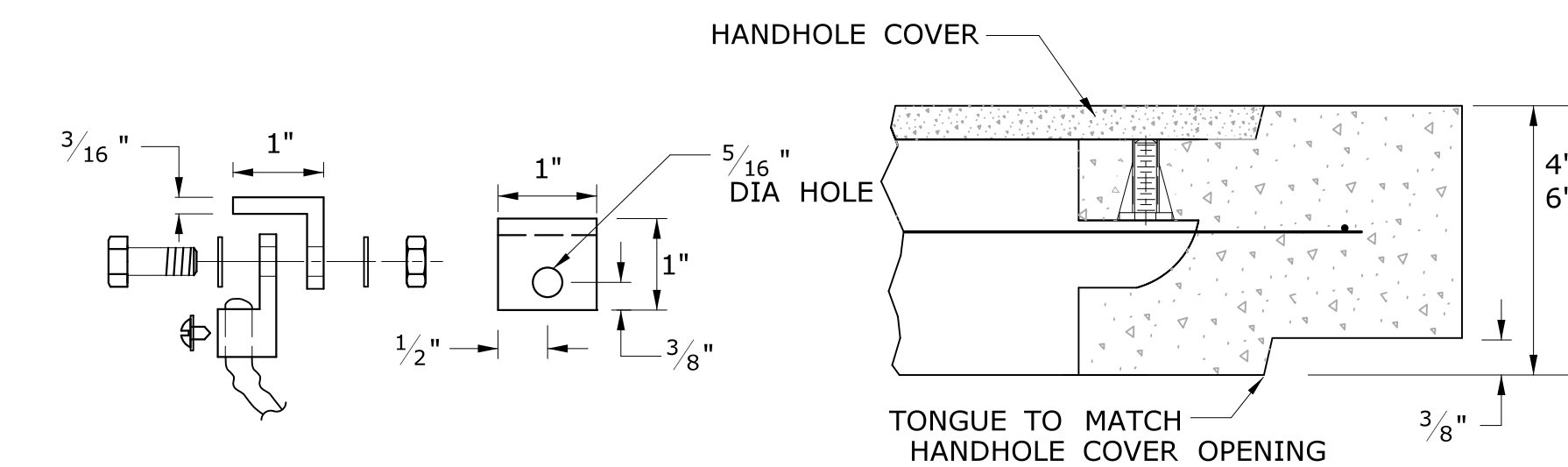
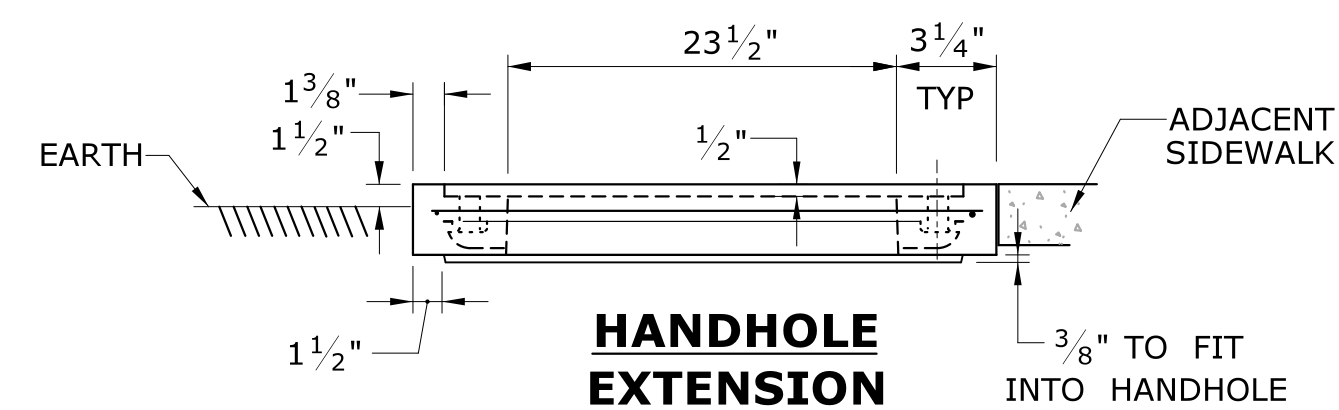
STANDARD SHEET NO.:
TR-1002_01



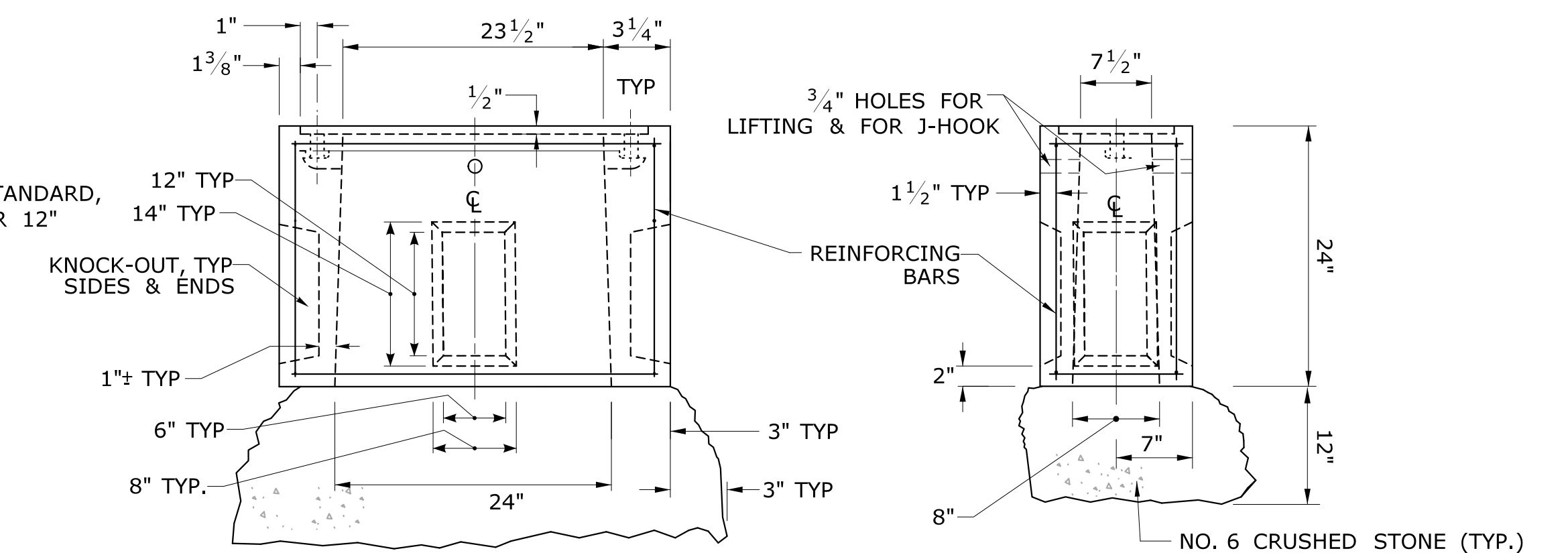
BANK ADAPTER



PLAN VIEW



SECTION A-A HANDHOLE EXTENSIONS



BASE SECTION
CONCRETE HANDHOLE TYPE II

6. INSTALL HANDHOLES APPROX. 12" BEHIND CURB OR
IF NO CURB, 24" BEHIND EDGE OF ROAD UNLESS OTHERWISE SPECIFIED.
7. CAST THE WORD "TRAFFIC" INTO TOP EDGE OF HANDHOLE, 1½" LETTERS.
8. WHERE AN EXISTING CONCRETE SIDEWALK SLAB ABUTTING A HANDHOLE IS DAMAGED OR
CUT DURING INSTALLATION, REPLACE THE ENTIRE SIDEWALK SECTION.
9. 12-#3 REINFORCING BARS REQUIRED FOR ALL HANDHOLES. (8 HORIZONTAL, 4 VERTICAL)
10. FOR TYPE I INTERCONNECT HANDHOLE 2" RMC ENTERING AND EXITING HANDHOLE MUST BE
INSTALLED PERPENDICULAR TO THE SIDEWALL OF THE HANDHOLE.
11. CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT THE RADIUS OF THE INSTALLED RMC
IS NOT GREATER THAN THE MINIMUM ALLOWED FOR FIBER OPTIC CABLE.

1. CONCRETE: (PRECAST) PRC04060 IN ACCORDANCE WITH ARTICLE M.14.01
2. COMPLETE TYPE II HANDHOLE:

IN EARTH AREAS, CONSISTS OF A BASE SECTION WITH HANDHOLE EXTENSION IF
NEEDED, AND STEEL COVER.

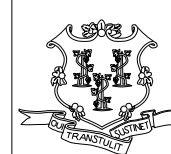
IN SIDEWALK AREAS, CONSISTS OF A BASE SECTION WITH HANDHOLE EXTENSION
IF NEEDED, AND CAST IRON COVER PAID UNDER A SEPARATE ITEM.
3. PLAN VIEW DIMENSIONS, SECTION VIEW, & DETAILS, SAME FOR BASE SECTION,
EXTENSIONS & BANK ADAPTER.
4. GROUT AROUND ALL CONDUITS.
5. INSTALL 30" SIDE PARALLEL TO ROAD UNLESS OTHERWISE NOTED.

NOT TO SCALE

SIGNATURE BLOCK:
OFFICE OF ENGINEERING
2800 BERLIN TURNPIKE
NEWINGTON, CT 06111

SUBMITTED BY:

APPROVED BY:



STATE OF CONNECTICUT
DEPARTMENT
OF
TRANSPORTATION



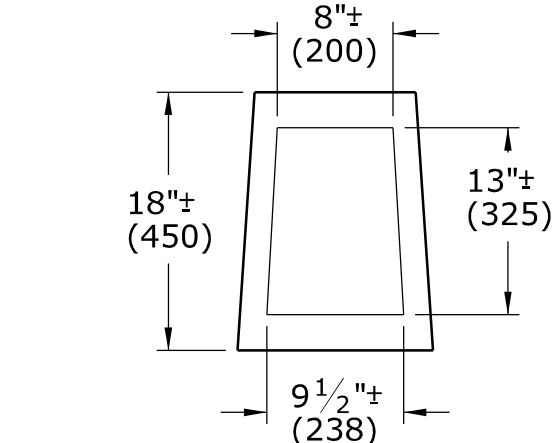
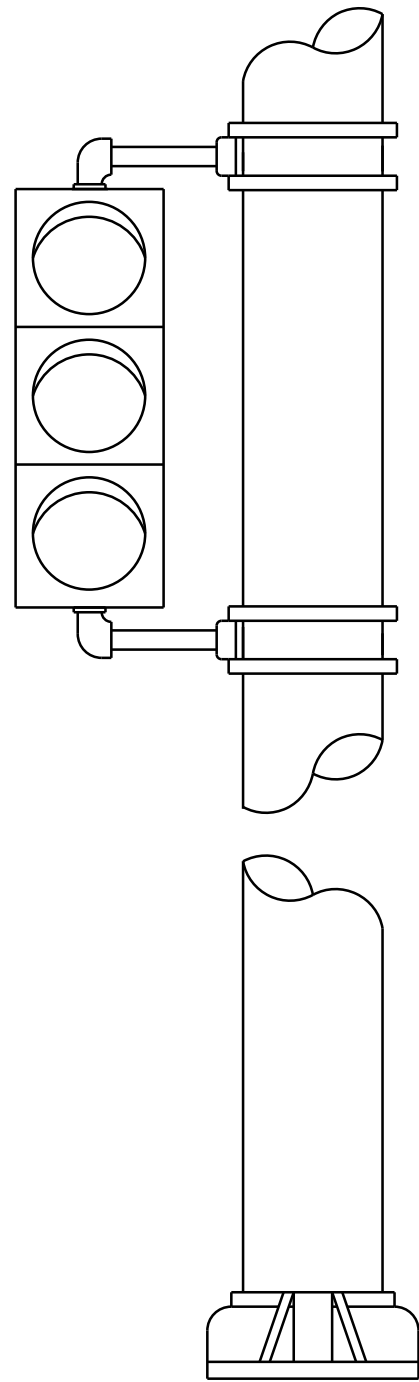
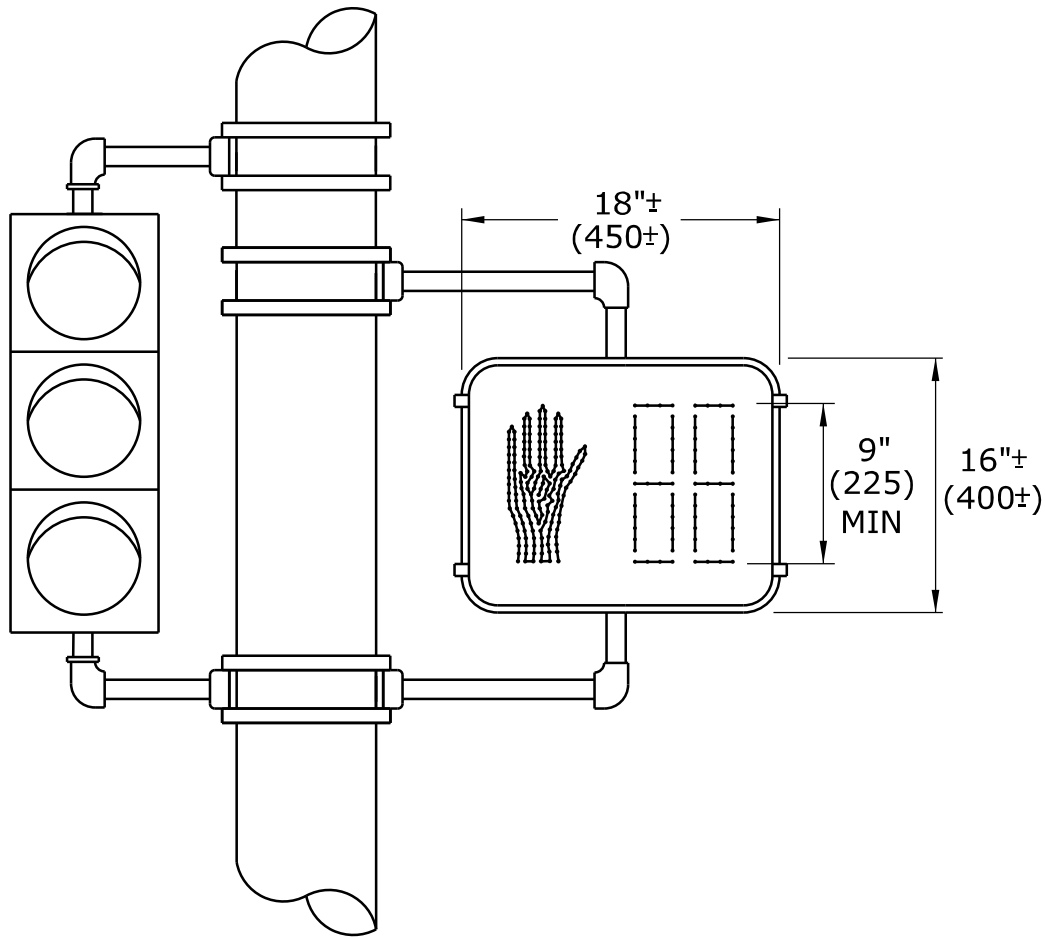
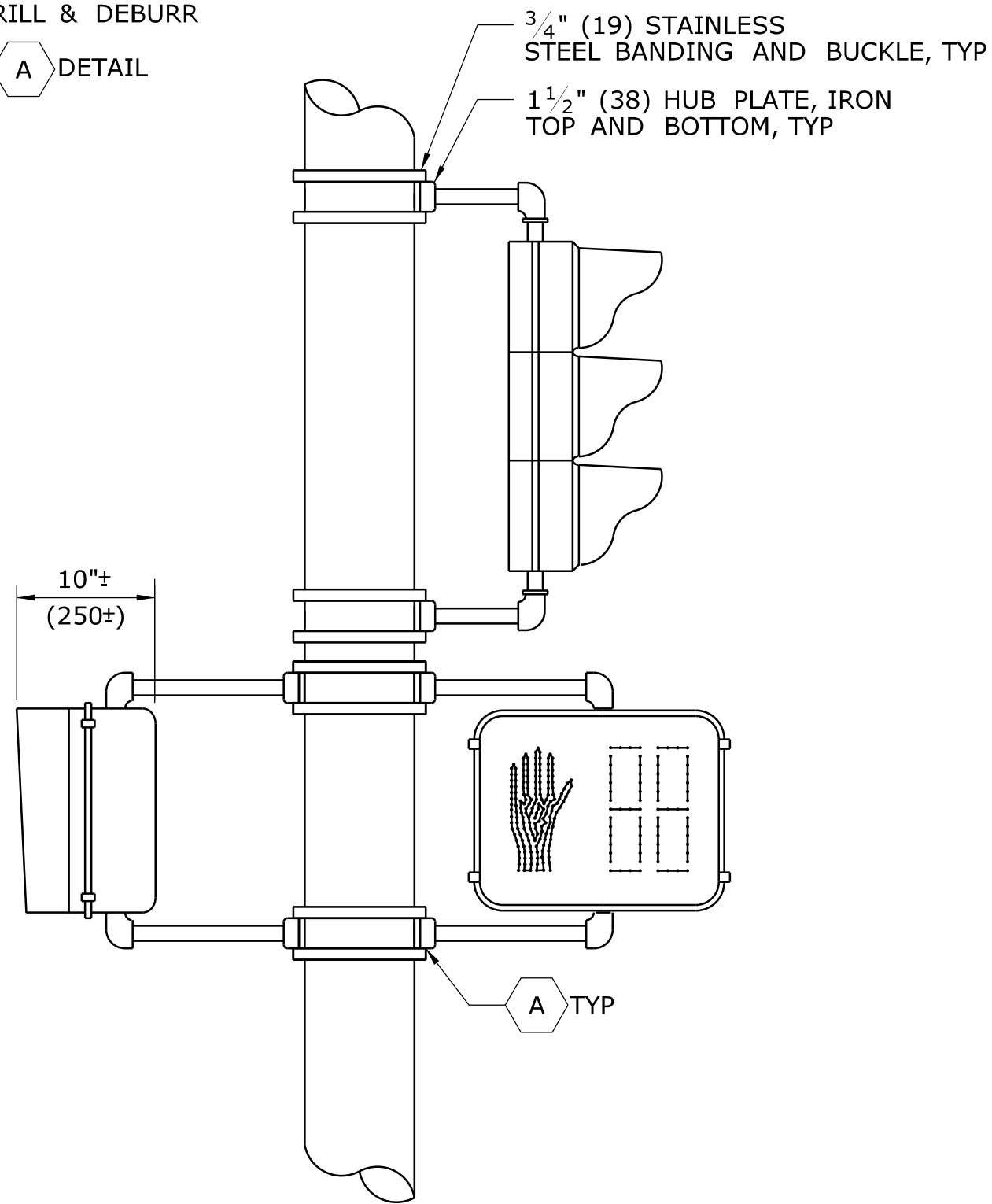
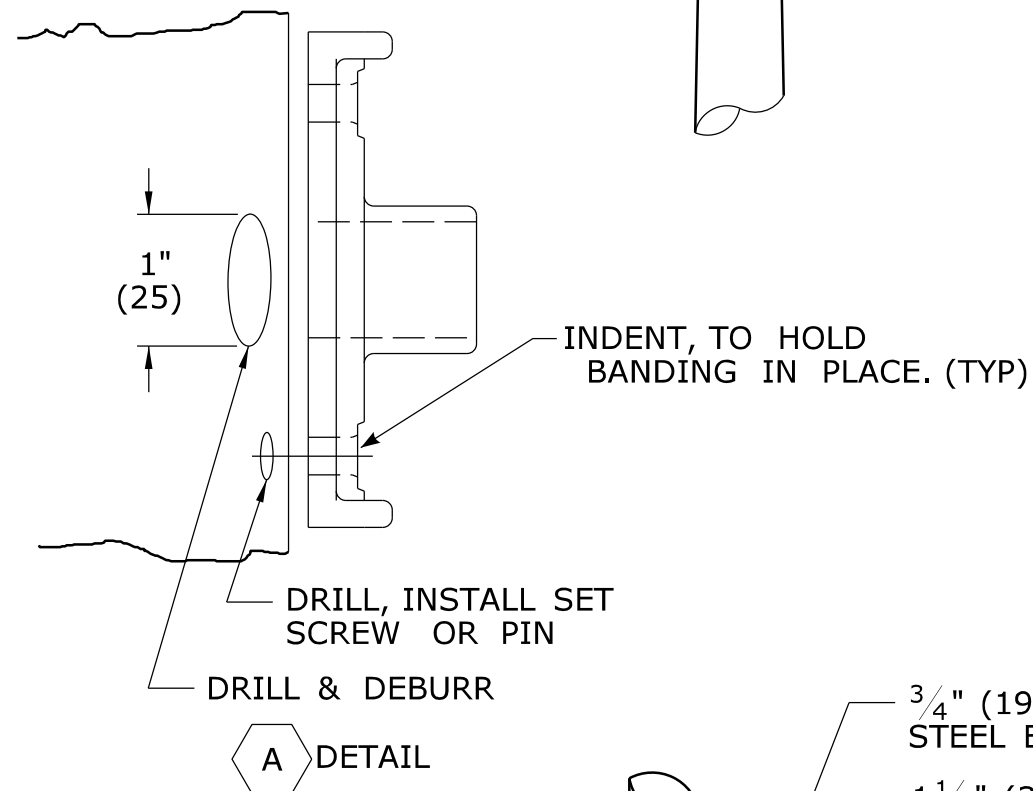
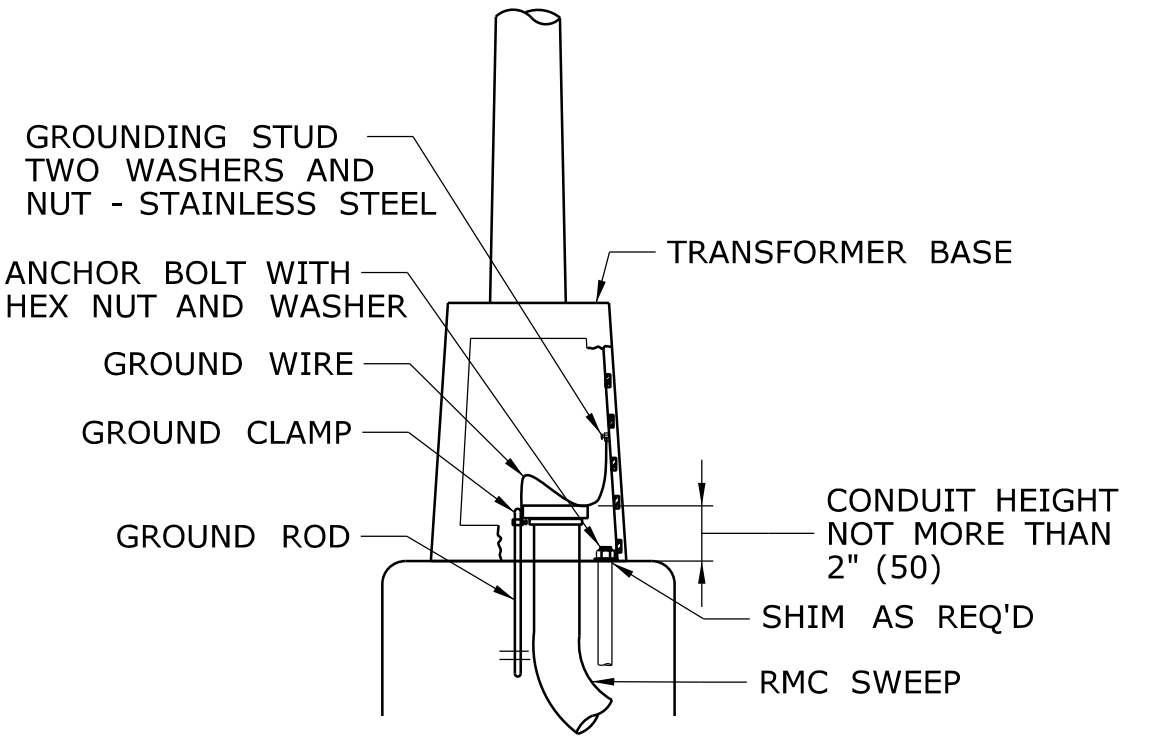
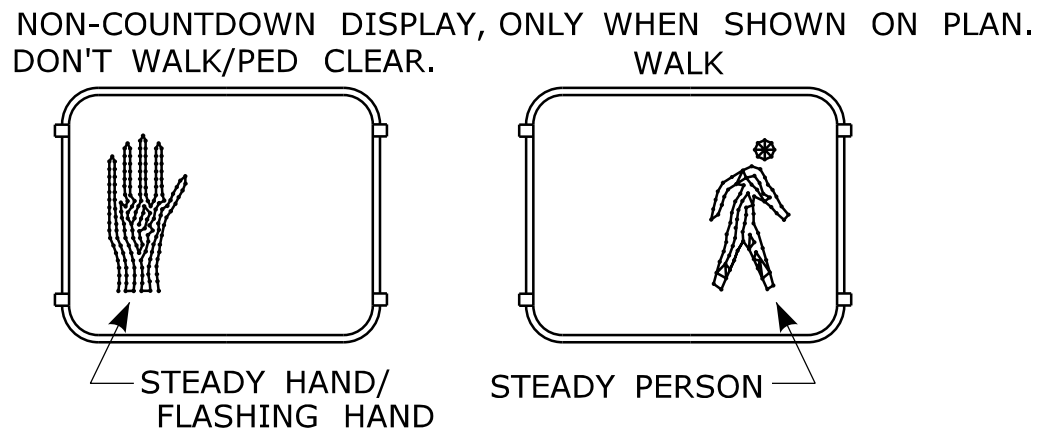
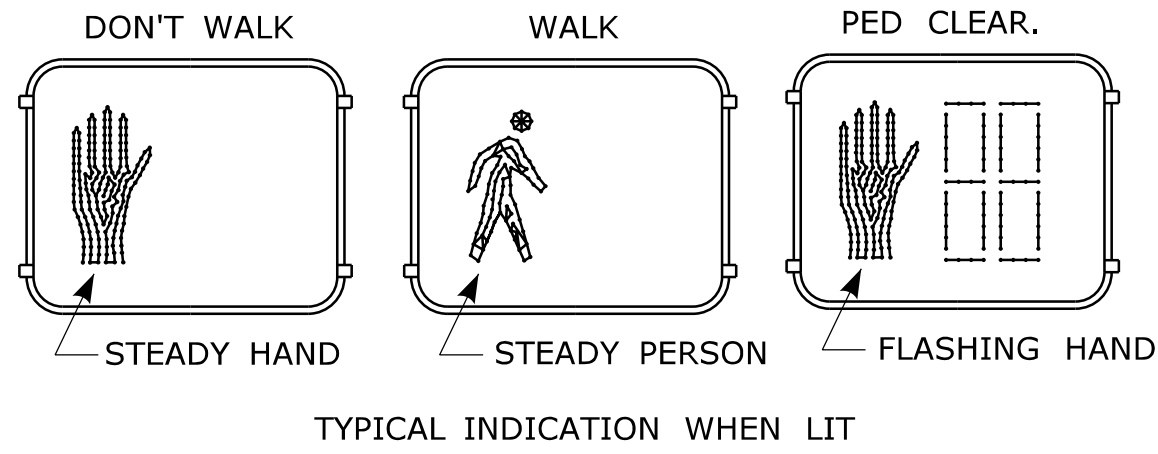
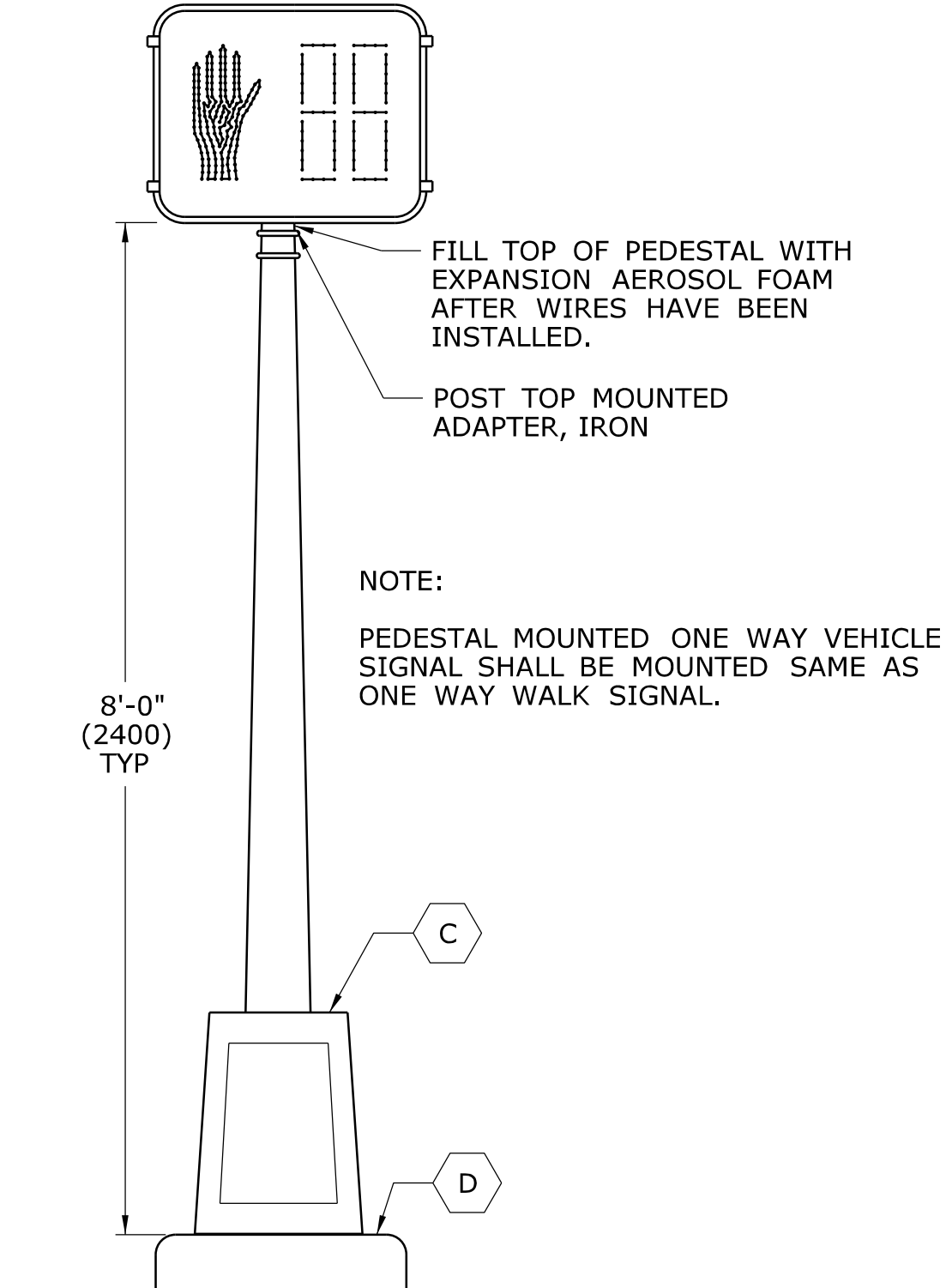
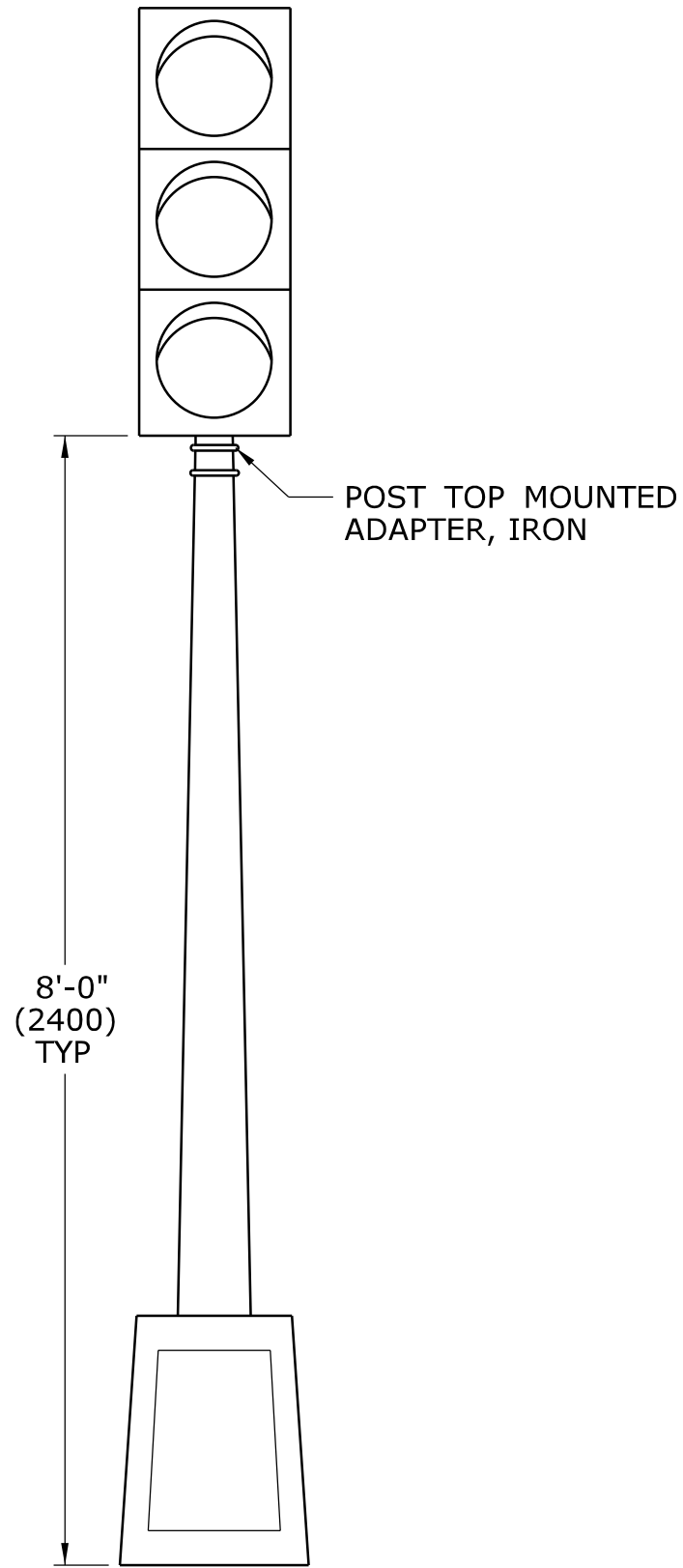
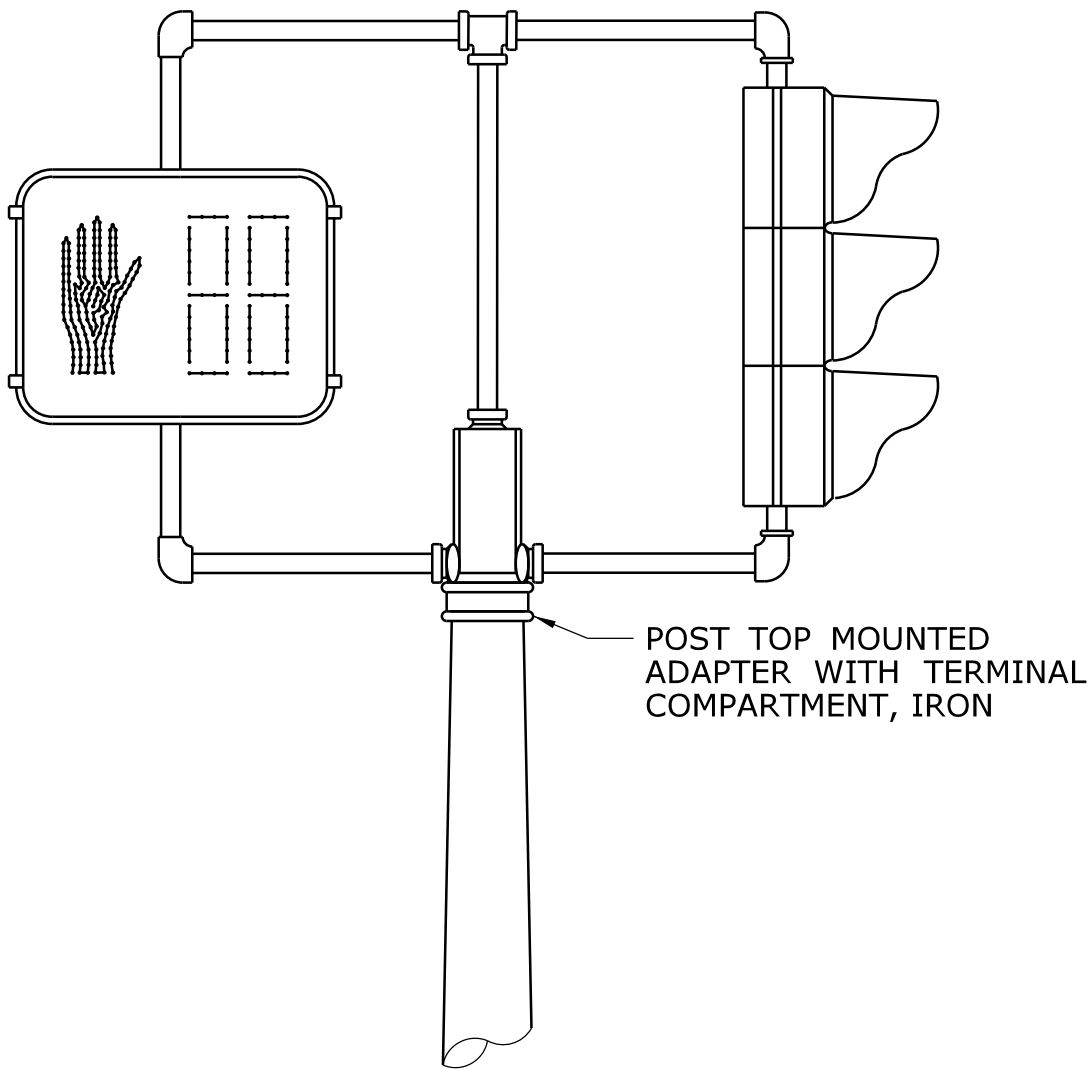
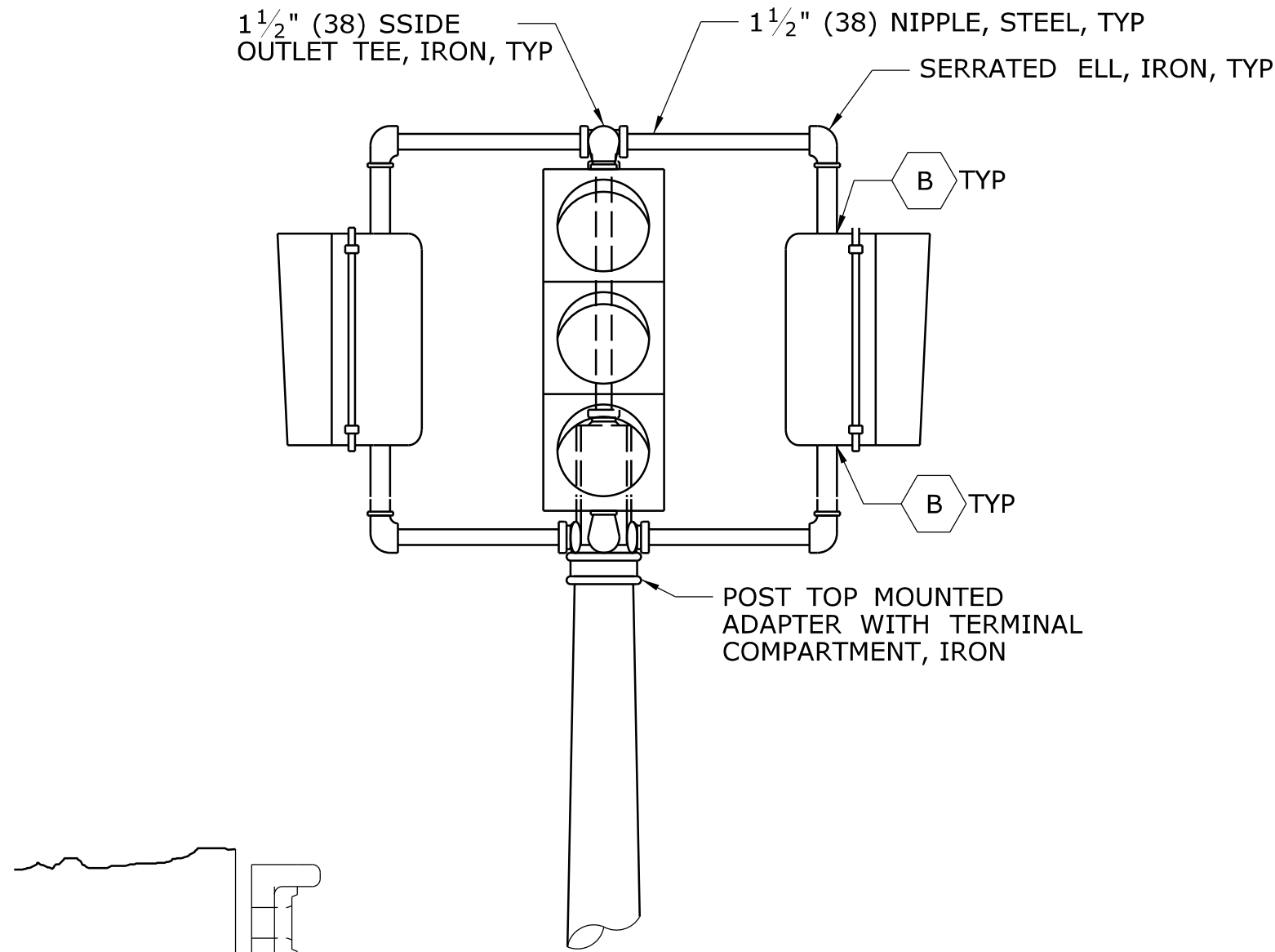
**CTDOT
STANDARD SHEET**

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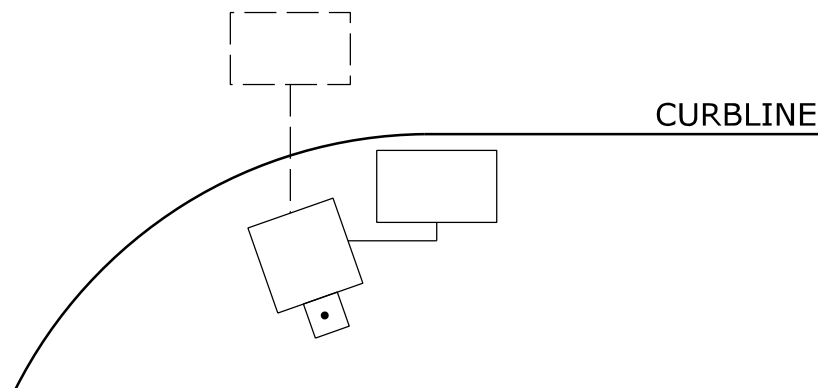
CONCRETE HANDHOLE

STANDARD SHEET NO.:

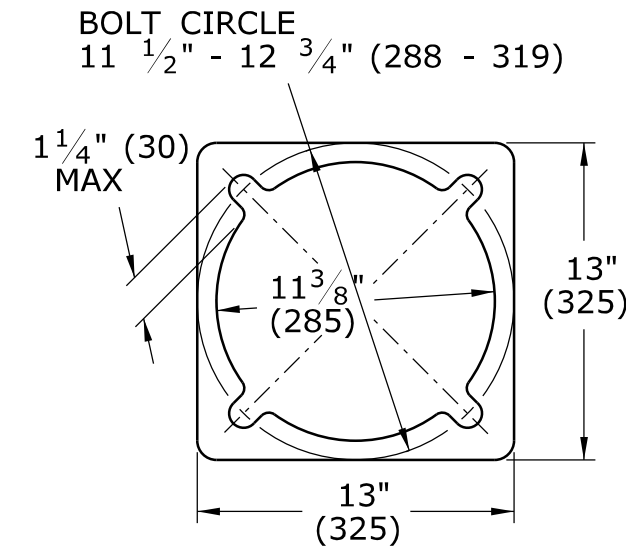
TR-1010_01



ALUMINUM PEDESTAL DOOR OPENING DETAIL









WHEN PEDESTALS OR SPAN POLES ARE INSTALLED CLOSE TO THE CURB, SIDE MOUNT PEDESTRIAN OR TRAFFIC SIGNALS TO AVOID VISOR DAMAGE FROM TURNING VEHICLES.



- NOTES:
- A SECURE LOWER HUB PLATE WITH STAINLESS STEEL SET SCREW OR PIN PRIOR TO BANDING TO PREVENT MOVEMENT. INSTALL CABLE THROUGH BOTTOM OF HUB PLATE.
 - B REFER TO CTDOT TRAFFIC STANDARD SHEET, TR-1105-01, TRAFFIC SIGNALS & CABLE ASSIGNMENTS.
 - C IF THREADED, MIN 1" (25) THREADED INTO BASE, SECURED WITH STAINLESS STEEL SET SCREWS.
 - D BASE DESIGNED AS BREAK-AWAY.

INCANDESCENT WALK SIGNAL LAMPS ARE 67 WATTS, RATED AT 8000 HOURS LAMP LIFE. LED WALK SIGNAL LAMPS ARE MAXIMUM 15 WATTS, WARRANTED AT 5 YEAR LIFE.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:			
	STEEL SPAN POLE, MAST ARM ASSEMBLY SHAFT		PEDESTRIAN SIGNAL
	ALUMINUM PEDESTAL		PEDESTAL MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS
	TRAFFIC SIGNAL		POLE MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS

REV.	DATE	REVISION DESCRIPTION
2	4-2012	MINOR REVISIONS.
1	1-2010	INCLUDED COUNTDOWN PEDESTRIAN SIGNALS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/14/2012

DIMENSIONS ARE IN ENGLISH ("') & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm - UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE



STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

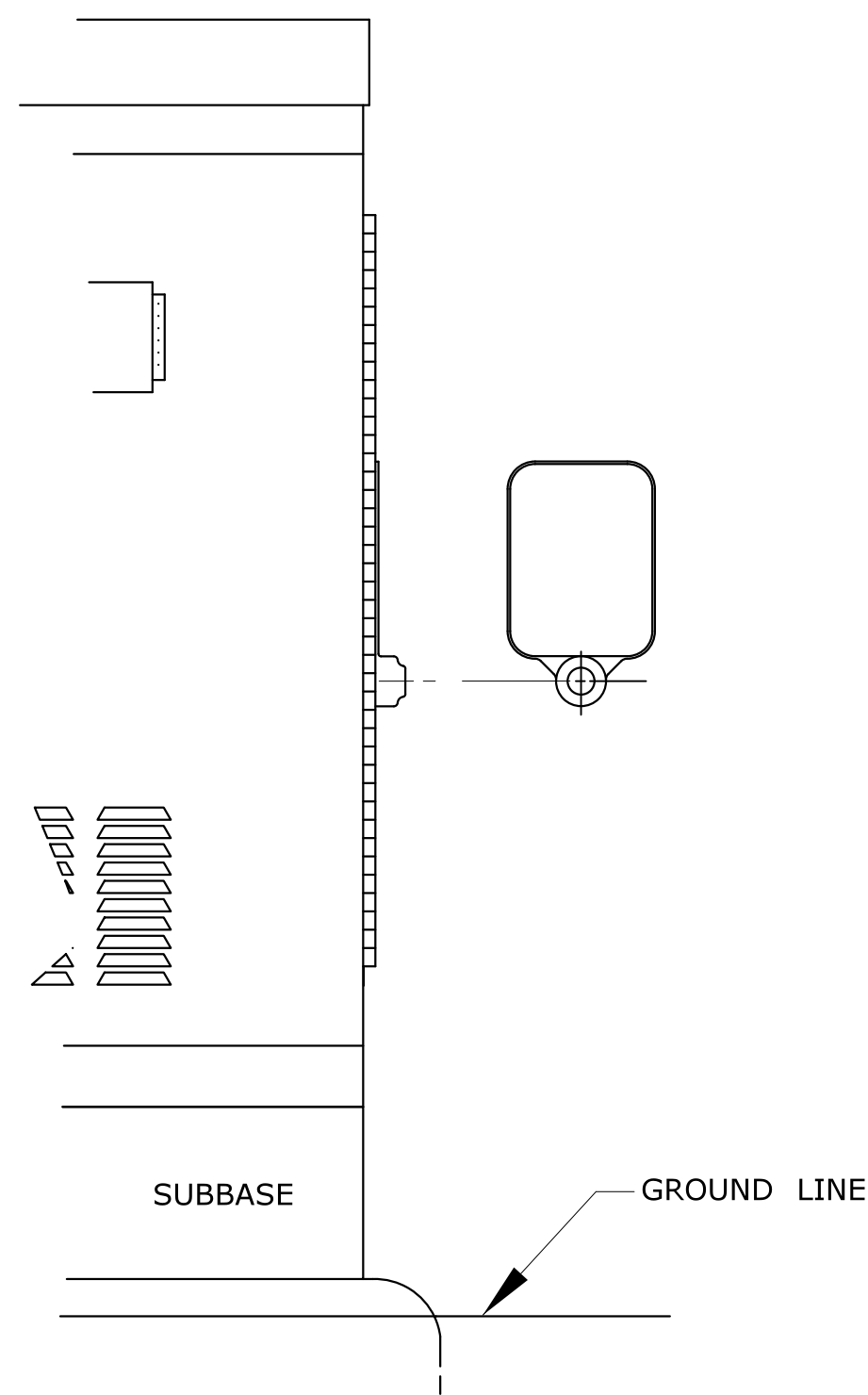
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Model: TR-1102_01

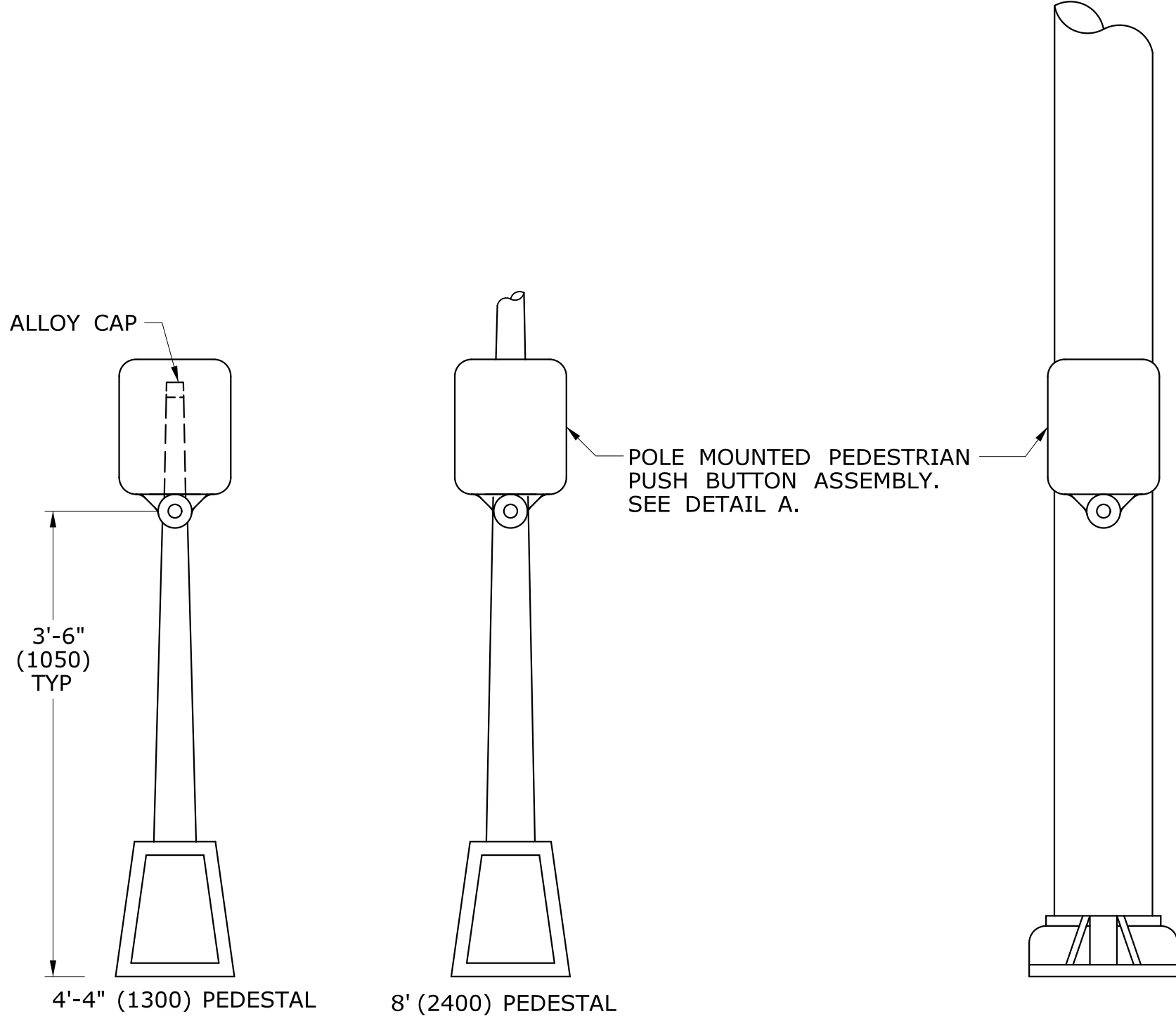
SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

CTDOT STANDARD SHEET
OFFICE OF ENGINEERING

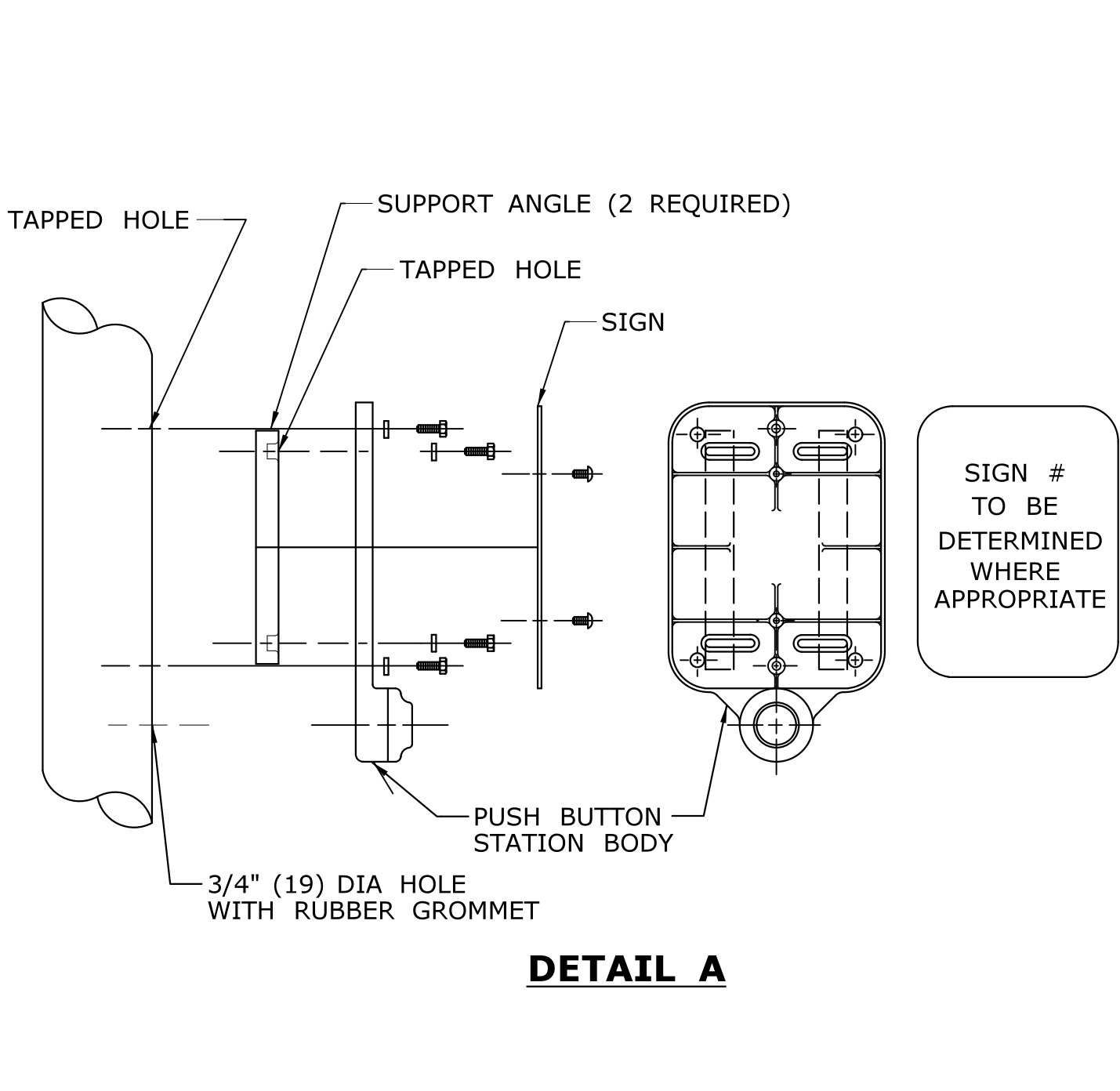
STANDARD SHEET TITLE:	STANDARD SHEET NO.:
PEDESTALS, PEDESTRIAN SIGNALS	TR-1102_01



SURFACE MOUNTED



PEDESTAL MOUNTED



SPAN POLE/MAST ARM MOUNTED



SIGN # 31-0833
USE APPROPRIATE LEFT OR RIGHT ARROW

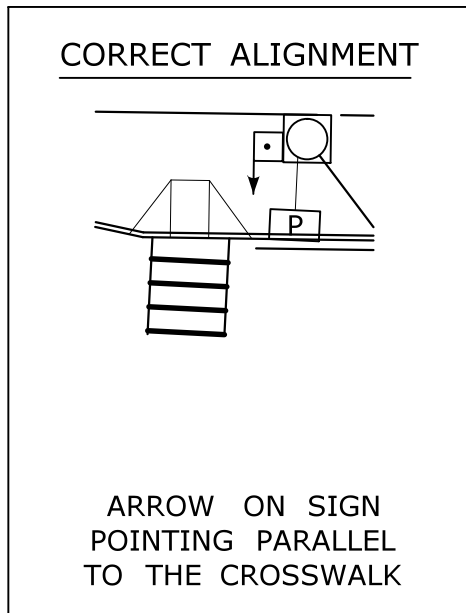


SIGN # 31-0835

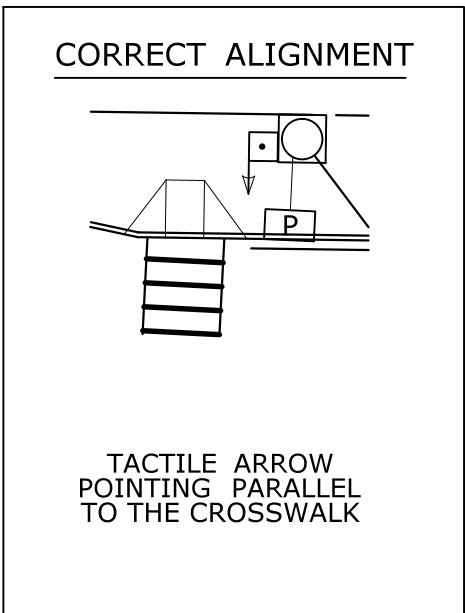
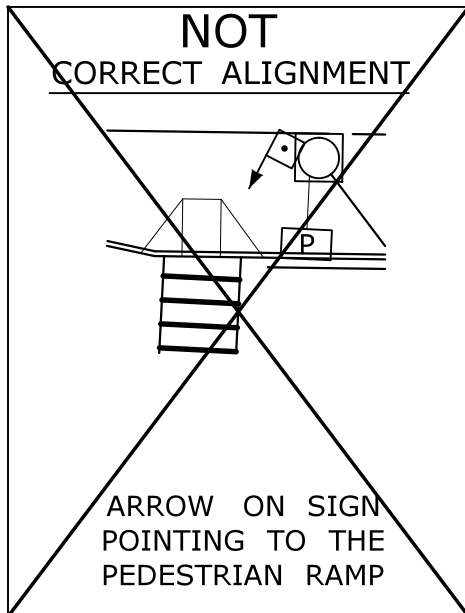
FOR CROSSING WITH SIDE STREET GREEN

GENERAL NOTES:

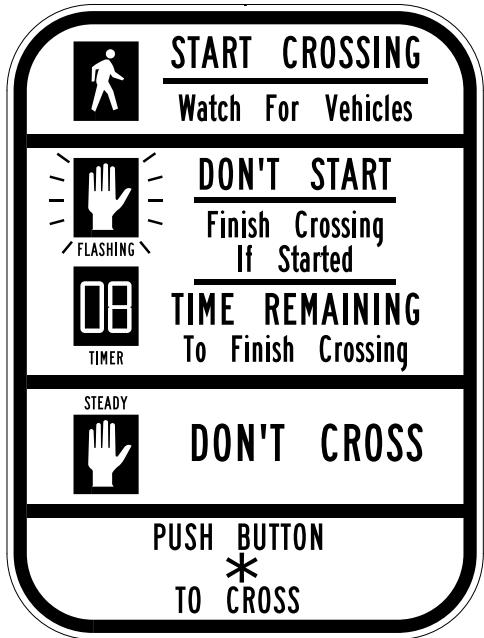
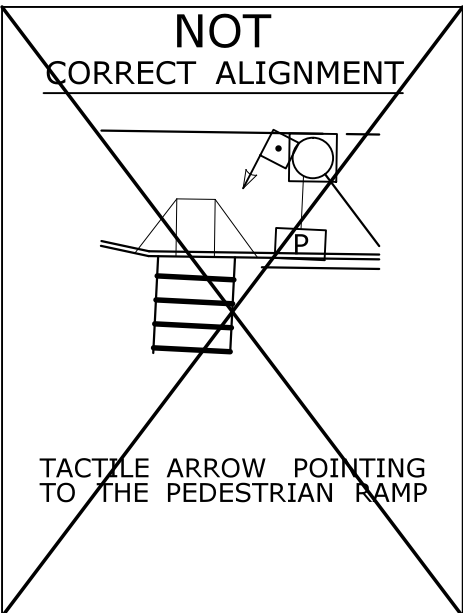
3'-6" (1050) FROM FINISHED GRADE SUCH AS SIDEWALK TO CENTER OF PUSH BUTTON.
PUSH BUTTON INSTALLATIONS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, CURRENT EDITION GOVERNS.
4'-4" (1300) PEDESTAL TO INCLUDE ALLOY CAP SECURED WITH STAINLESS STEEL SET SCREW.



PEDESTRIAN PUSH BUTTON ALIGNMENT



ACCESSIBLE PEDESTRIAN SIGNAL AND DETECTOR





*USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

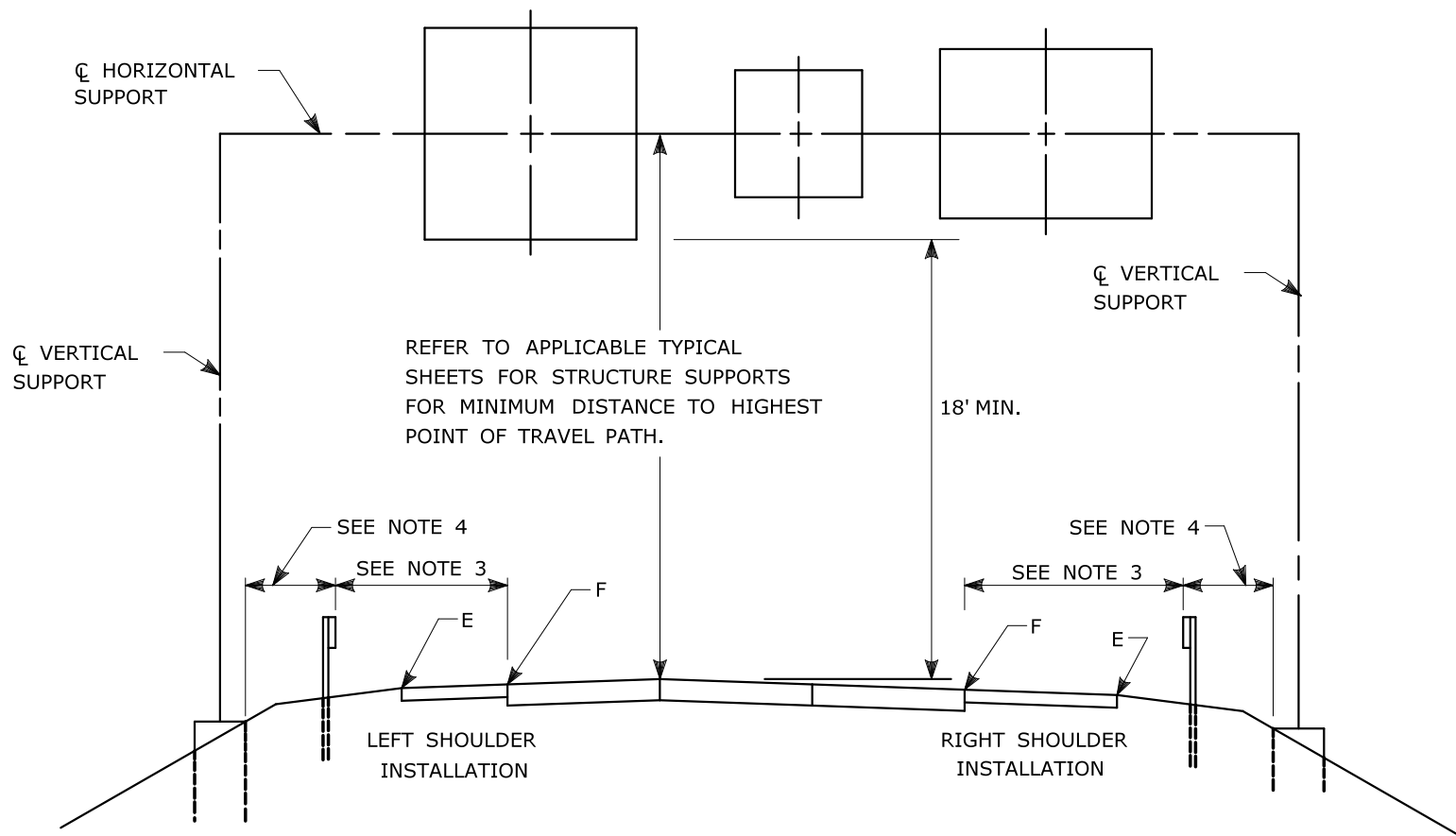
FOR NEW PUSHBUTTON HOUSING, USE 9" x 15" SIGN NO. 31-0856.

FOR EXISTING PUSHBUTTON HOUSING, WITH 9" x 12" SIZE, USE SIGN NO. 31-0845.

EXAMPLE ALIGNMENTS FOR EXCLUSIVE PEDESTRIAN PHASE

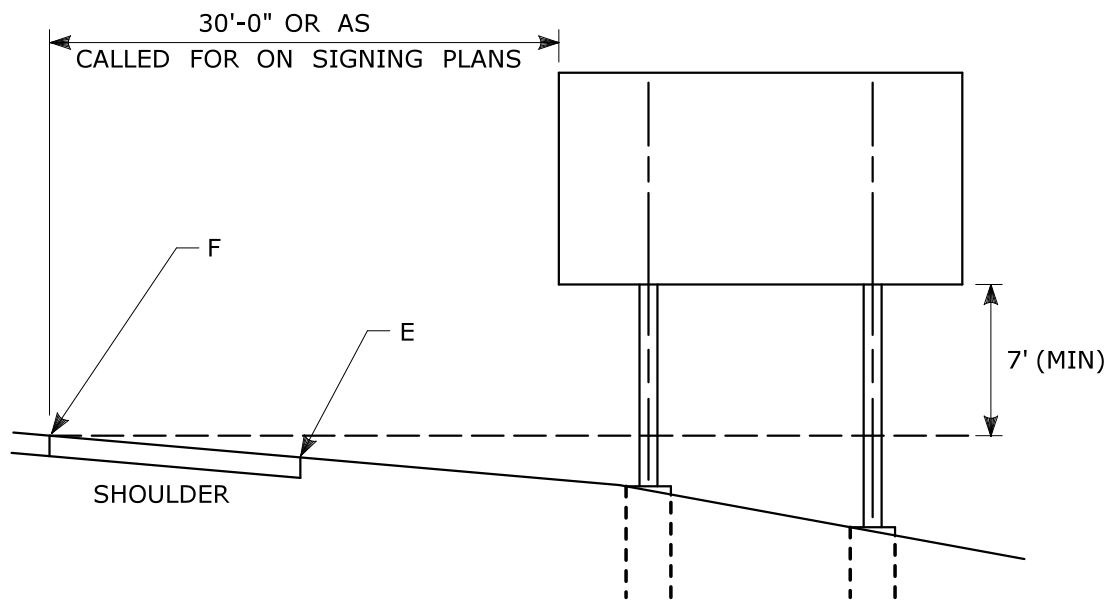
LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:		
	PEDESTRIAN PUSH BUTTON	
	PEDESTRIAN PUSH BUTTON, PEDESTAL MOUNTED	
	PEDESTRIAN PUSH BUTTON, POLE MOUNTED	

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	DIMENSIONS ARE IN ENGLISH ("") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION 	SUBMITTED BY: NAME/DATE/TIME:		CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: PEDESTRIAN PUSH BUTTONS	STANDARD SHEET NO.: TR-1107_01
3	8-2018	UPDATED PEDESTRIAN SIGN LEGENDS AND NOTES.				APPROVED BY: NAME/DATE/TIME:				
2	4-2014	ADDED PEDESTRIAN EXAMPLE ALIGNMENTS								
1	4-2012	MINOR REVISIONS & UPDATED SIGN #31-0845.								
REV.	DATE	REVISION DESCRIPTION								
			Plotted Date: 8/9/2018	NOT TO SCALE	Filename: CTDOT_TRAFFIC_STD_2018-01-25.dgn Model: TR-1107_01					



TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS

- NOTES:
- 1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
 - 2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANS.
 - 3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
 - 4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
 - 5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.



TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

- NOTES:
- 1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
 - 2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
 - 3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
 - 4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.

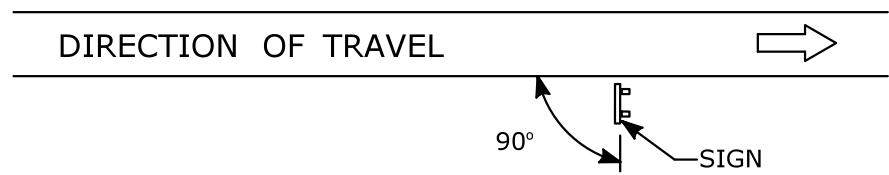


DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

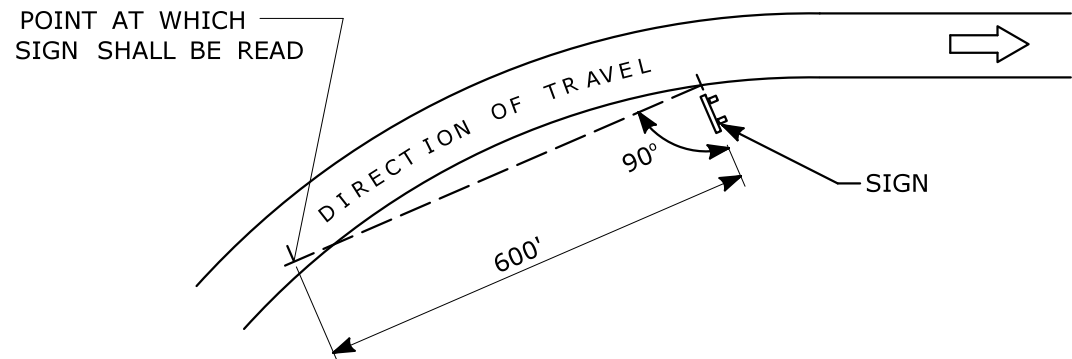
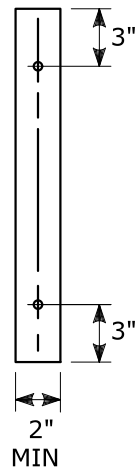


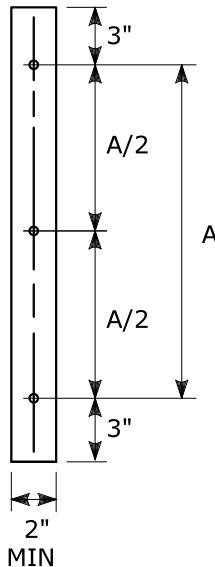
DIAGRAM "B"

SIGN ORIENTATION DETAILS FOR SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

RETROREFLECTIVE STRIPS 48" LONG OR LESS:



RETROREFLECTIVE STRIPS OVER 48" LONG:

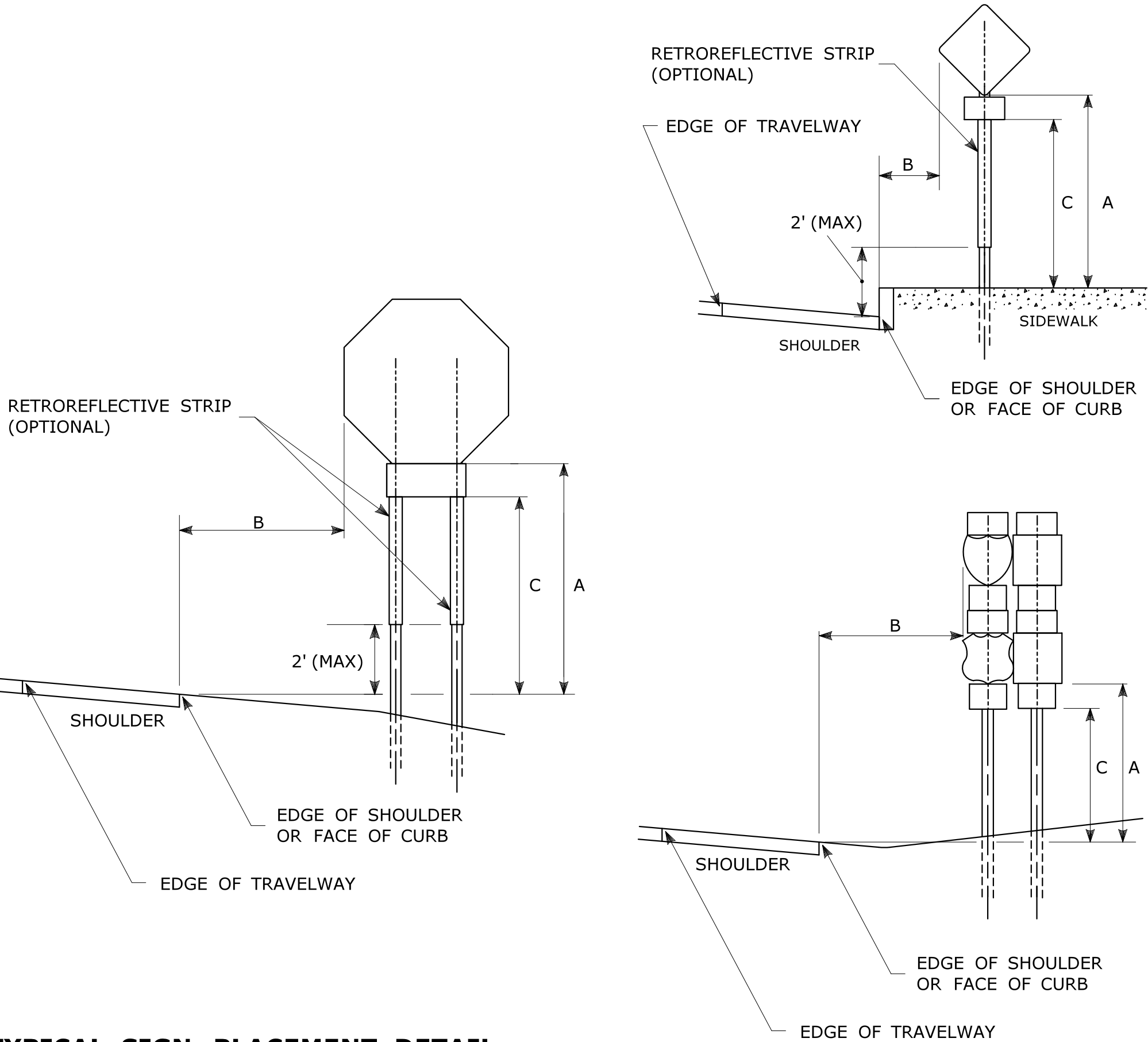


RETROREFLECTIVE STRIP DETAIL

NOTES:

RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS.

RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.



TYPICAL SIGN PLACEMENT DETAIL

NOTES:

ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY.

REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING.

IF A RETFOREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY.

PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET ①	DIM."C" MIN PLAQUE HEIGHT ①	ASSEMBLY LOCATION
7' ②	6' 12' ③	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMP • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMP, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMP, AND IN RURAL AREAS
4'	6' 12' ③	N/A	INCIDENT MANAGEMENT SIGNS AND MILE POST MARKER ASSEMBLIES LOCATED ON FREEWAYS AND EXPRESSWAYS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' ④	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' ④	7'	SIDEWALKS ⑤

① OR AS DIRECTED BY THE ENGINEER

② 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.

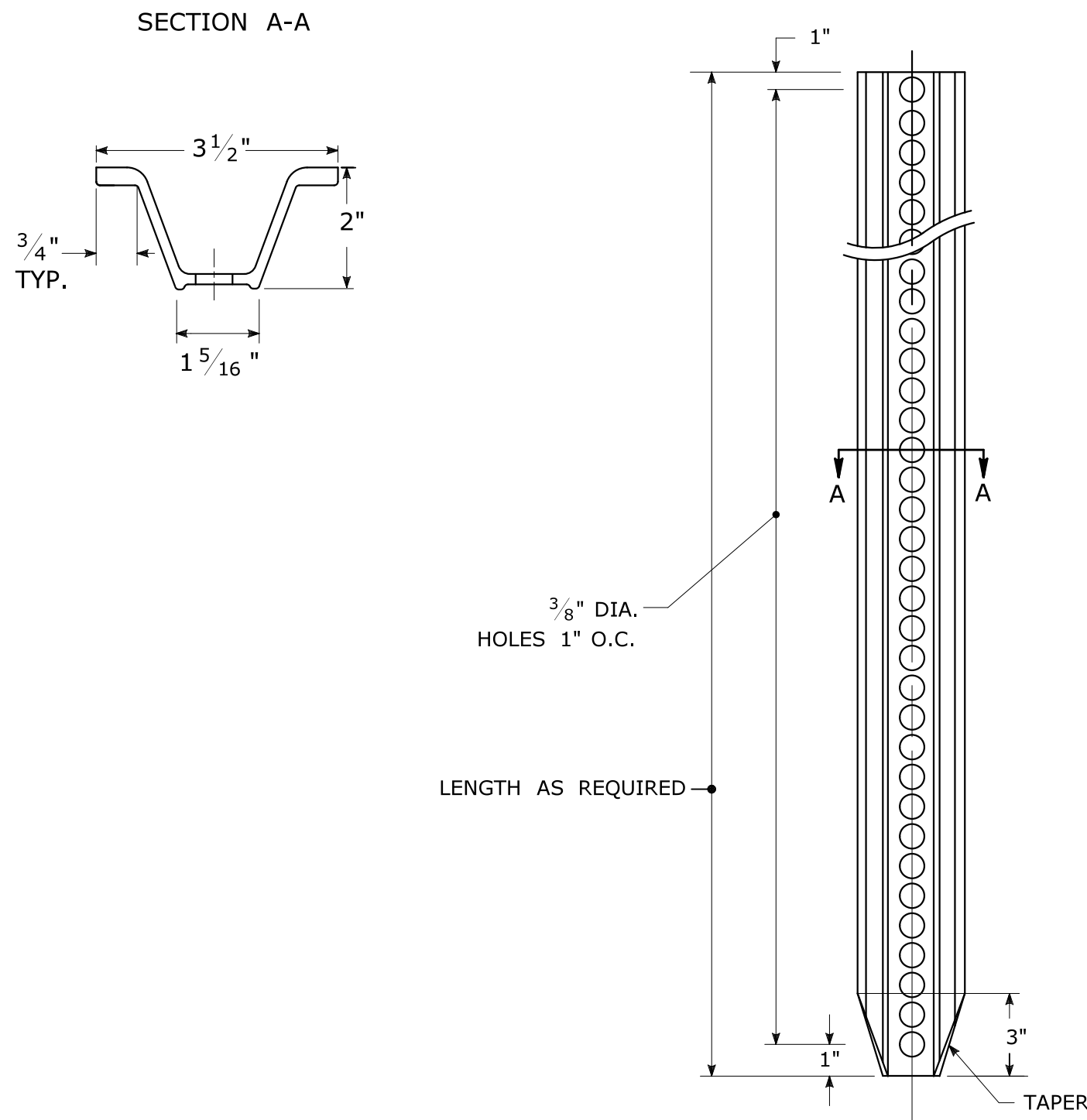
③ 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.

④ A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.

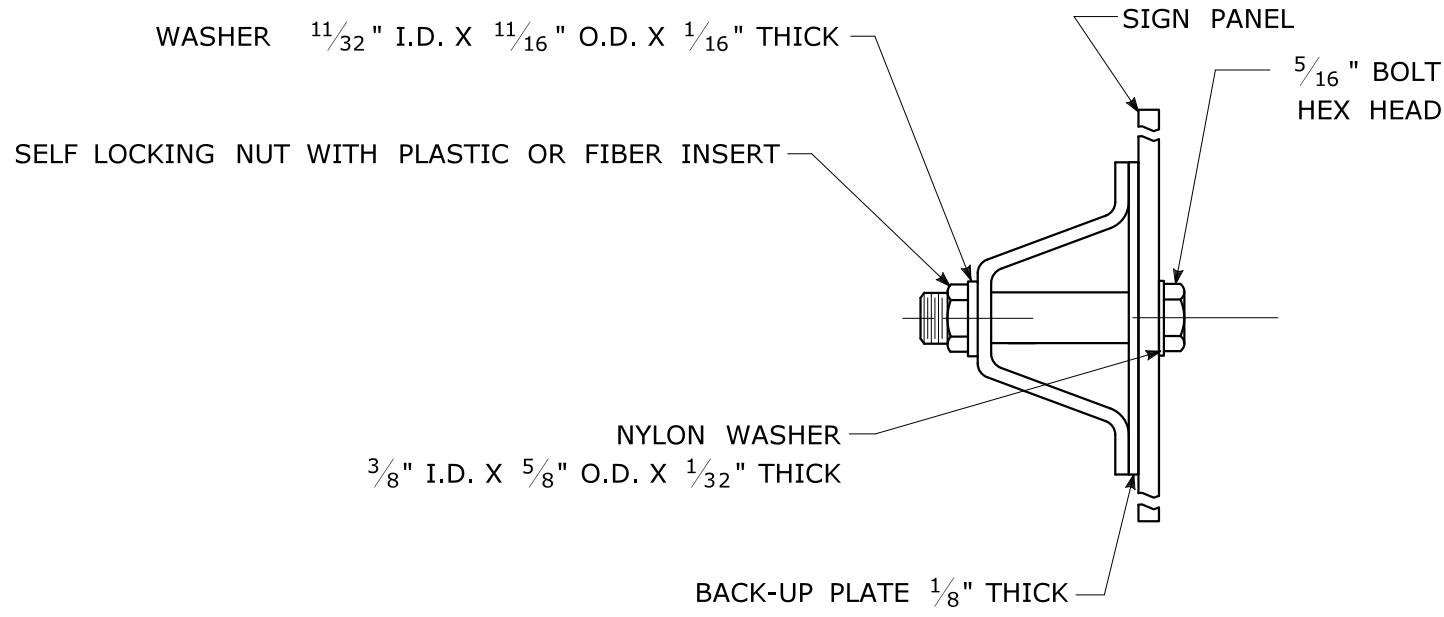
⑤ A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.			STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	FILENAME: TR_1208_01_1_2018.dgn MODEL: TR-1208_01	SUBMITTED BY: NAME/DATE/TIME: APPROVED BY: NAME/DATE/TIME: 	CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS	STANDARD SHEET NO.: TR-1208_01
3	8-2018	INCLUDED INCIDENT MANAGEMENT AND MILE MARKER SIGNS.									
2	4-2017	MINOR REVISIONS.									
1	2-2011	MINOR REVISIONS.									
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 8/10/2018	NOT TO SCALE							

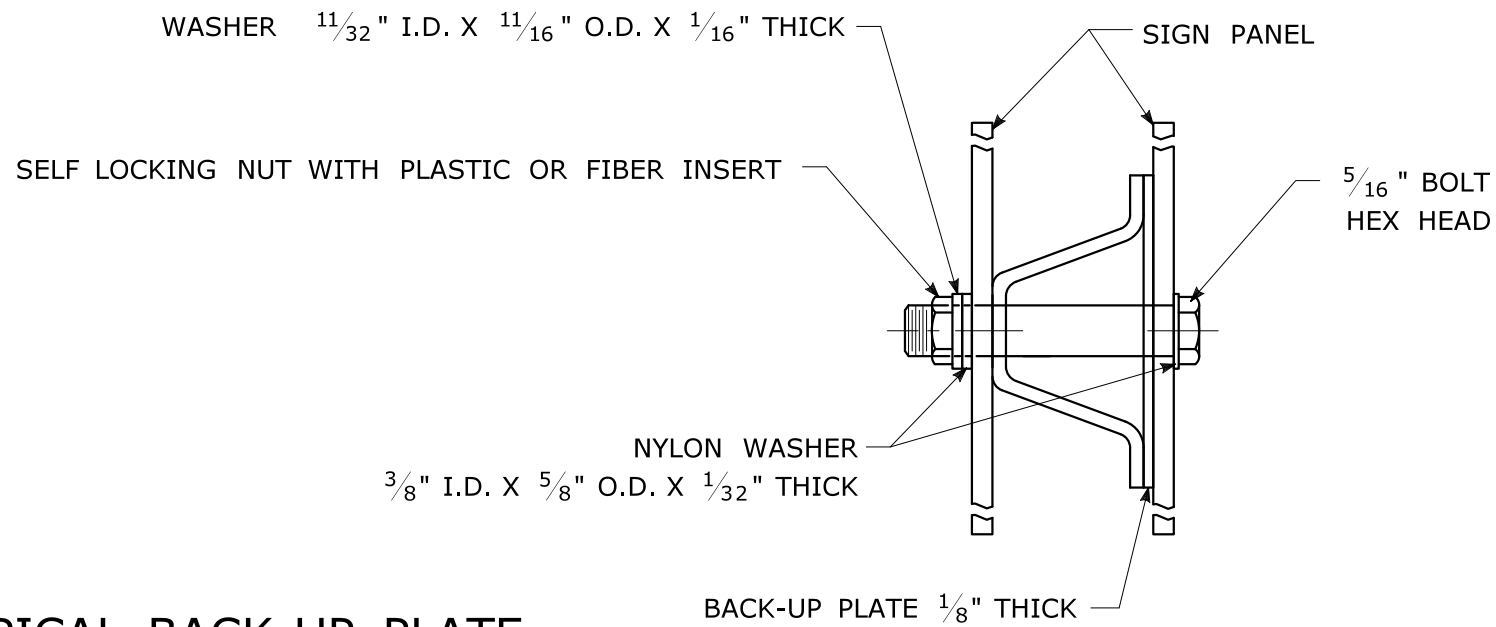
TYPICAL METAL SIGN POSTS



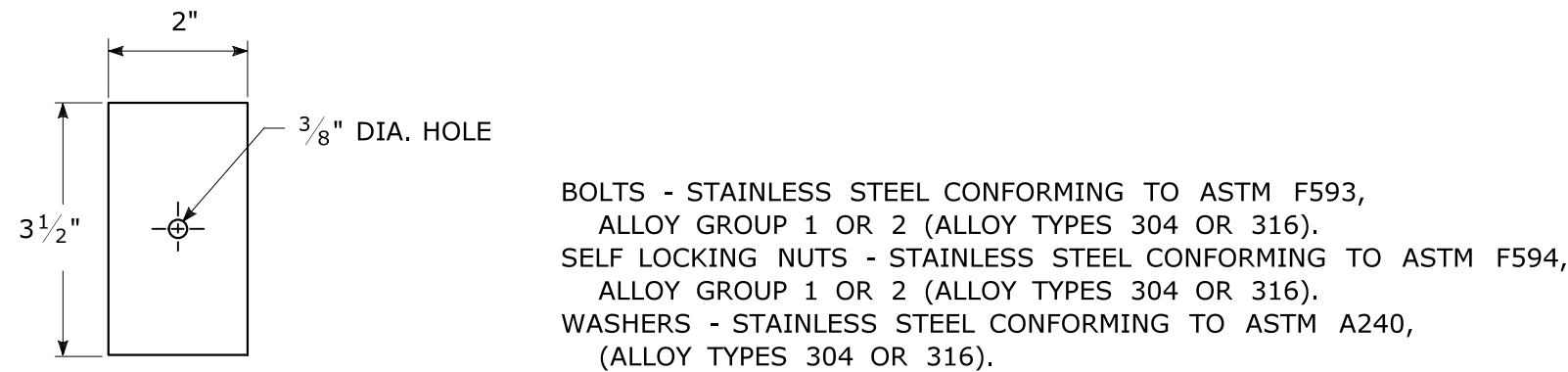
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT

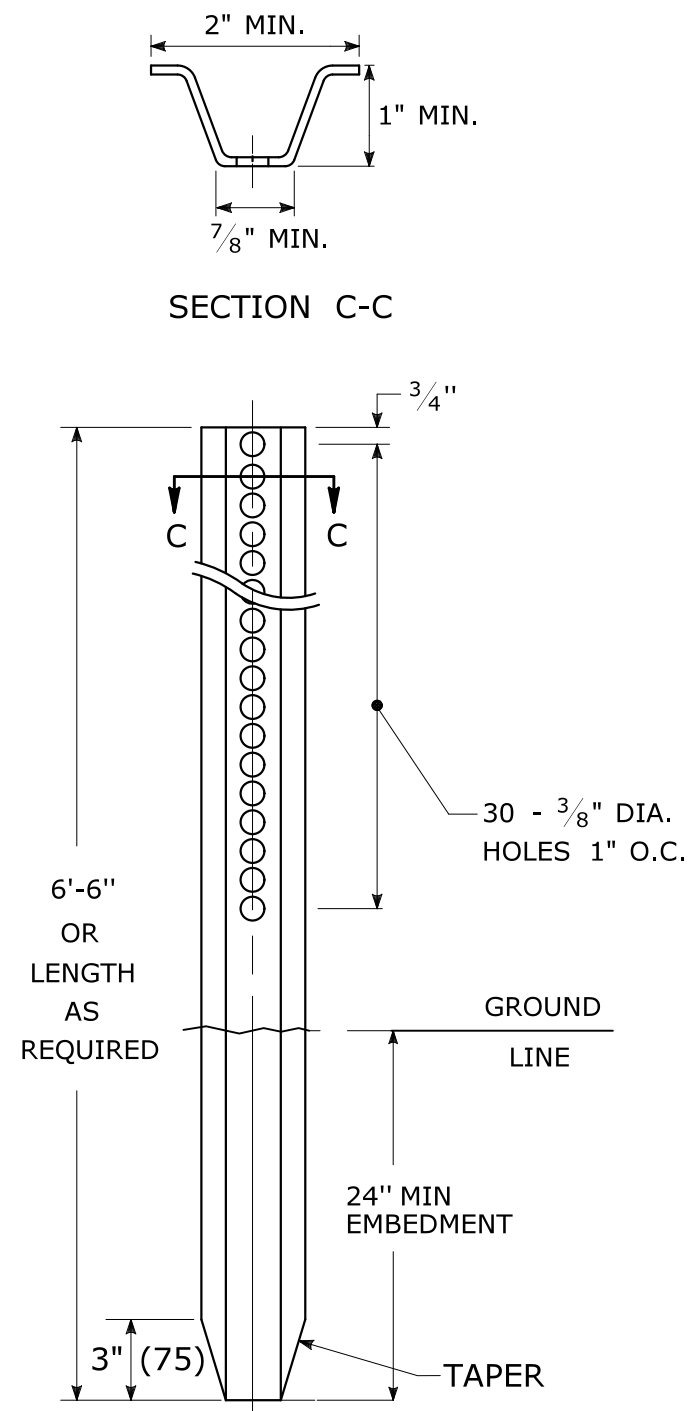


TYPICAL BACK-UP PLATE



METAL DELINEATOR POST

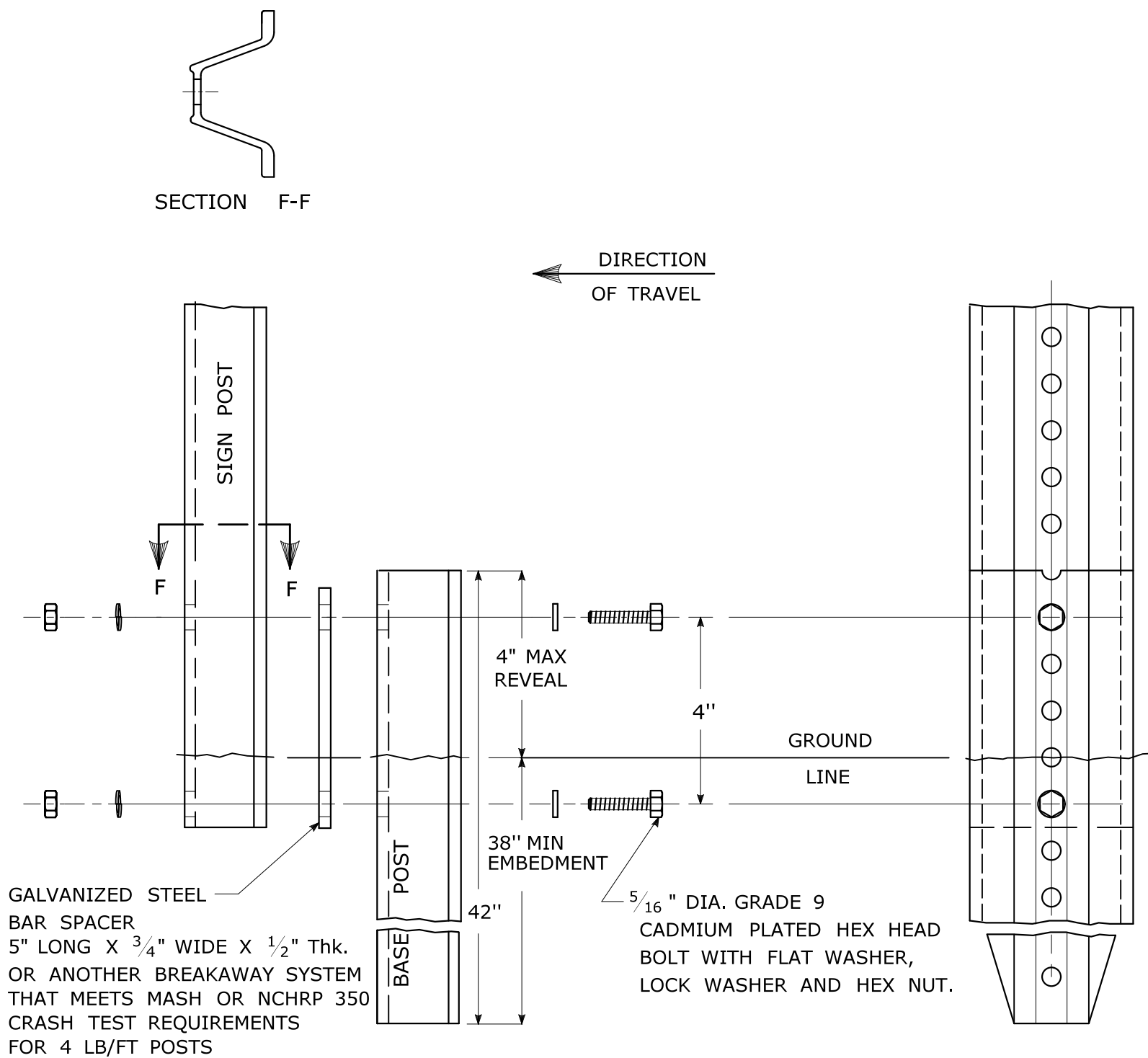
WT./FT. = 1.12 LBS./FT. MIN.



GENERAL NOTES:

1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

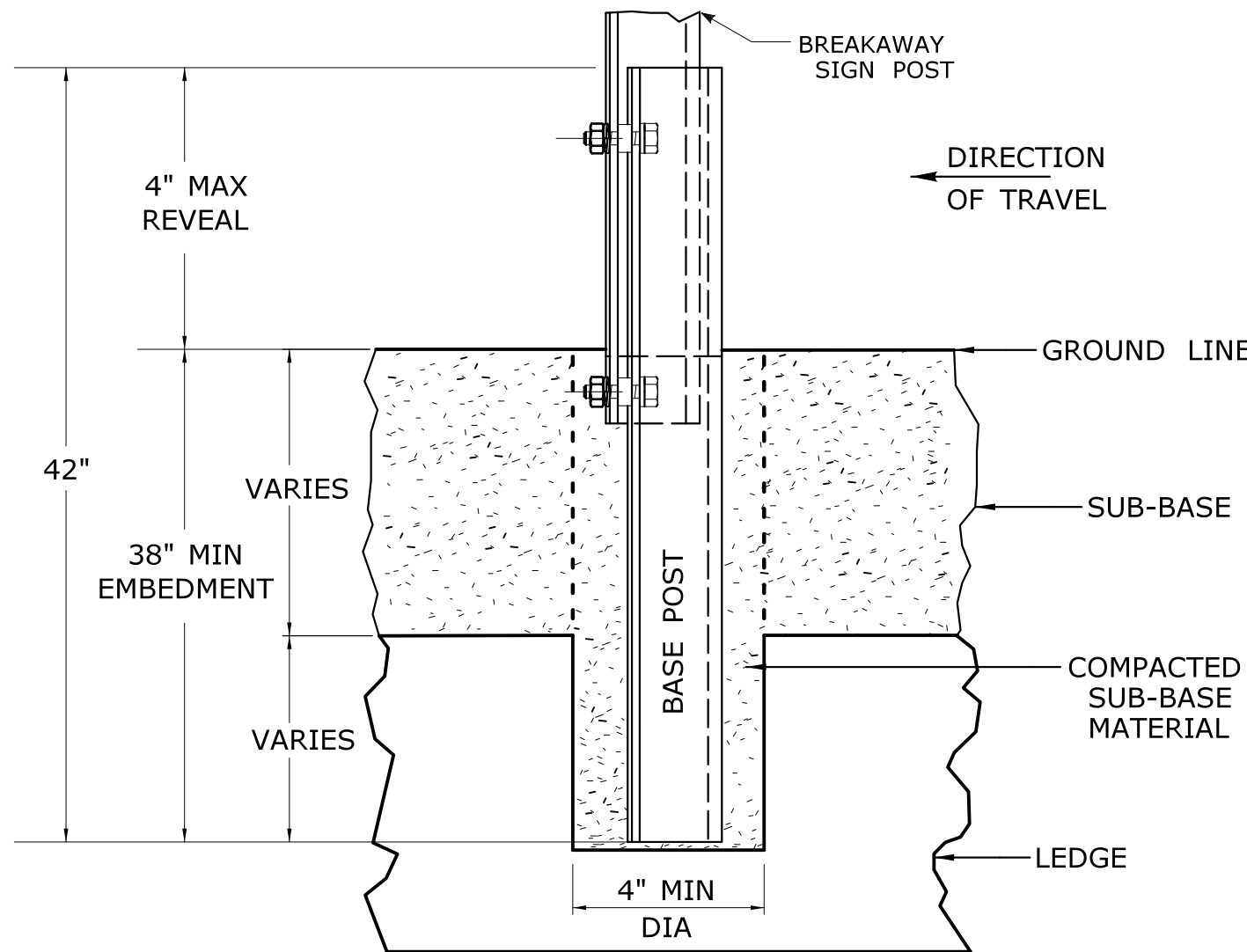
BREAKAWAY INSTALLATION
FOR 4 LBS./FT. POSTS



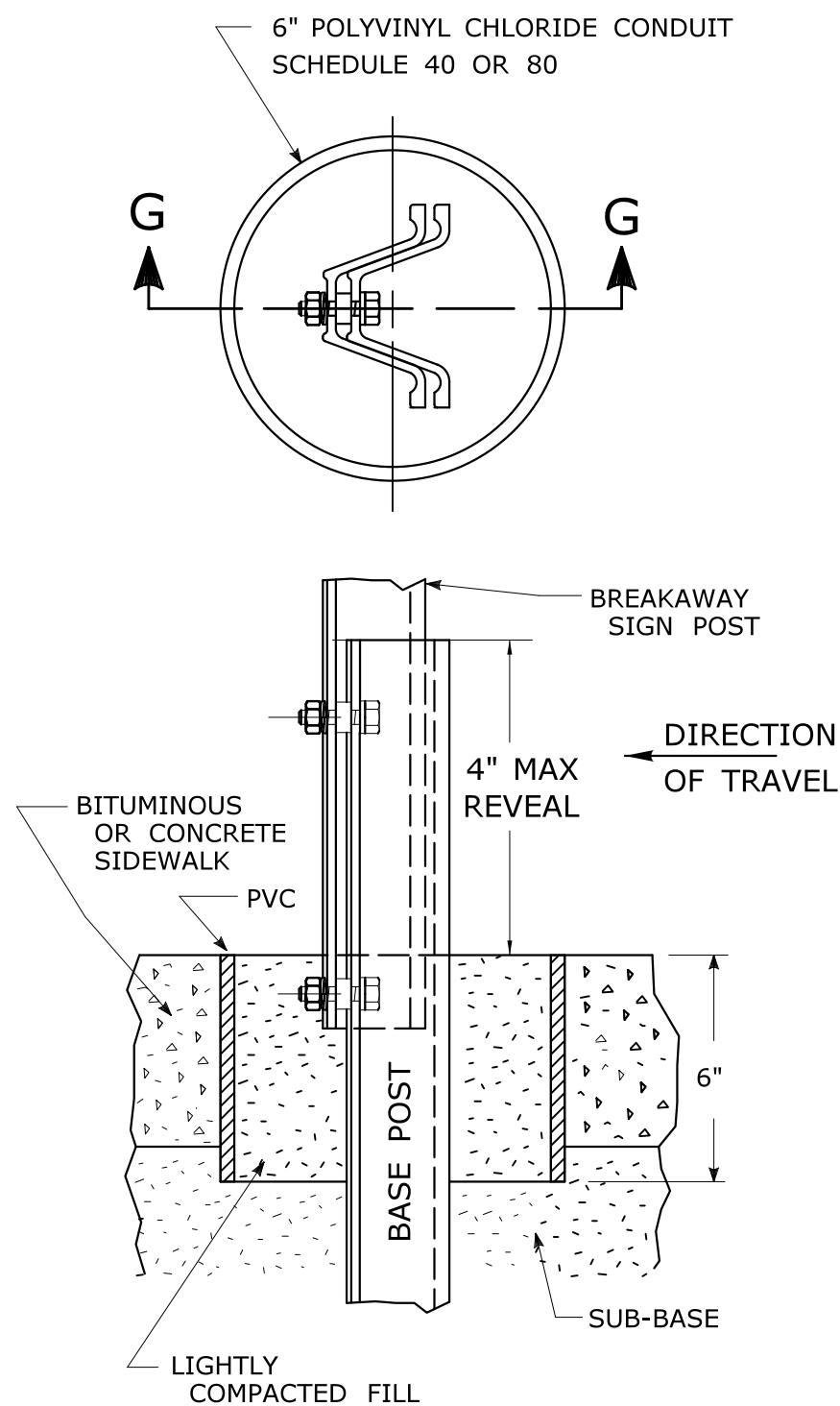
TYPICAL SIGN POST INSTALLATION IN LEDGE

LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".

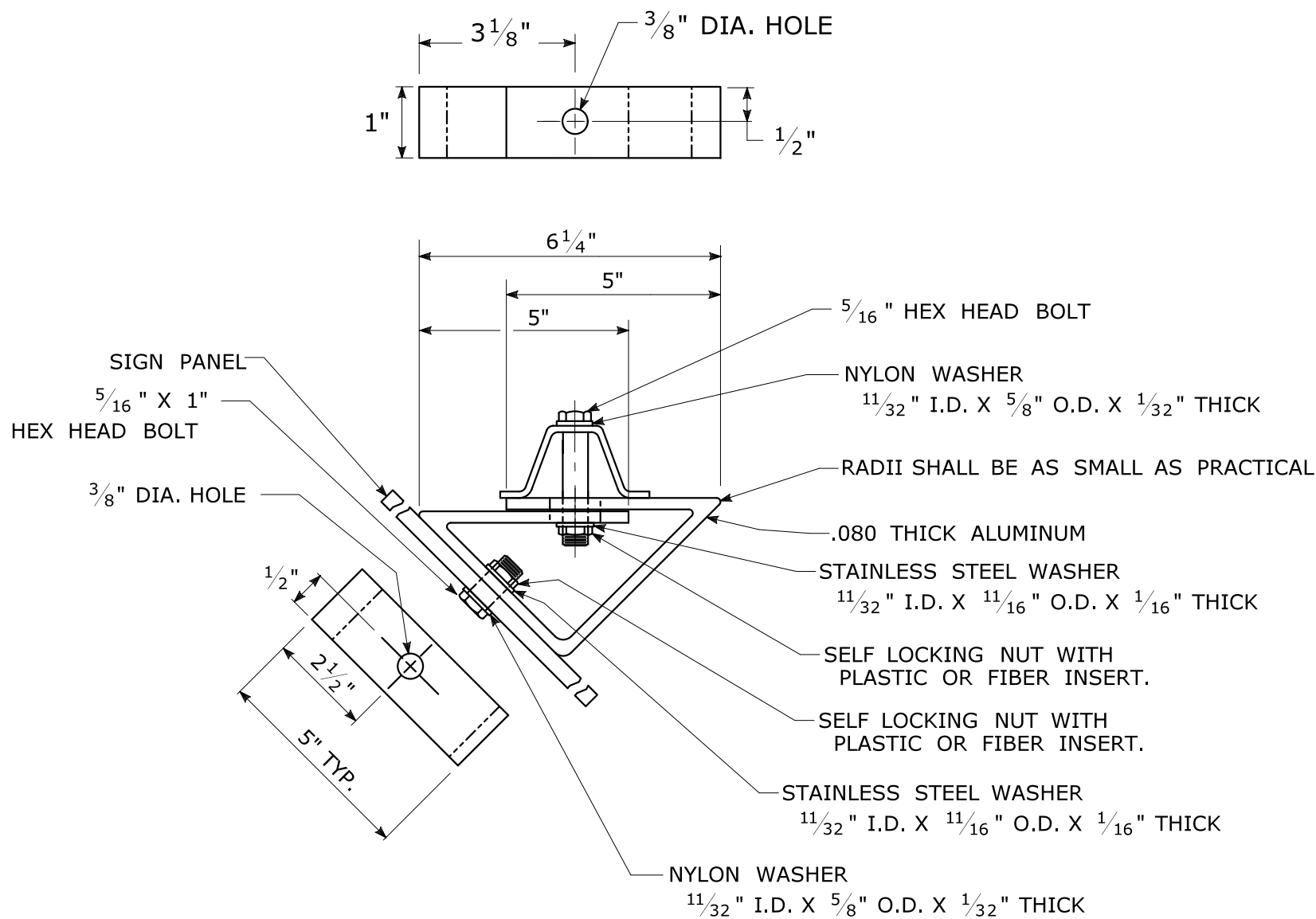
HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE
FOR PAVED AREAS



45° MOUNTING BRACKET
FOR INSTALLATION OF PARKING SIGNS

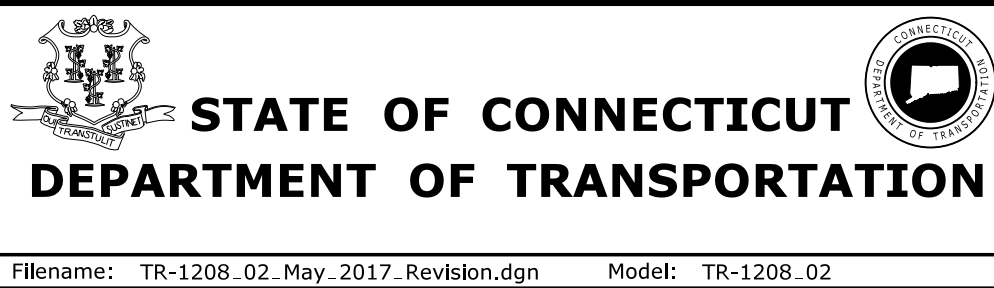


REV.	DATE	REVISION DESCRIPTION
2	6-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 6/6/2017

NOT TO SCALE

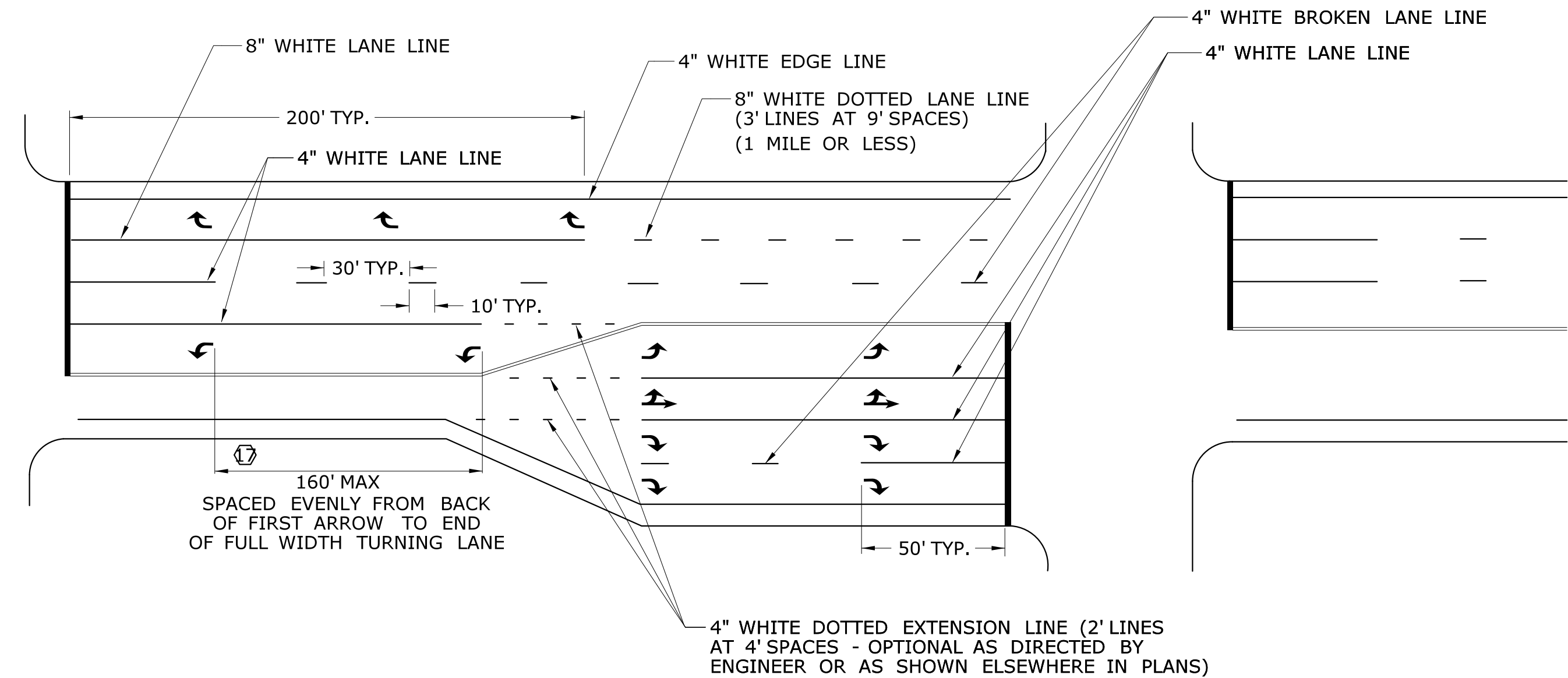


SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

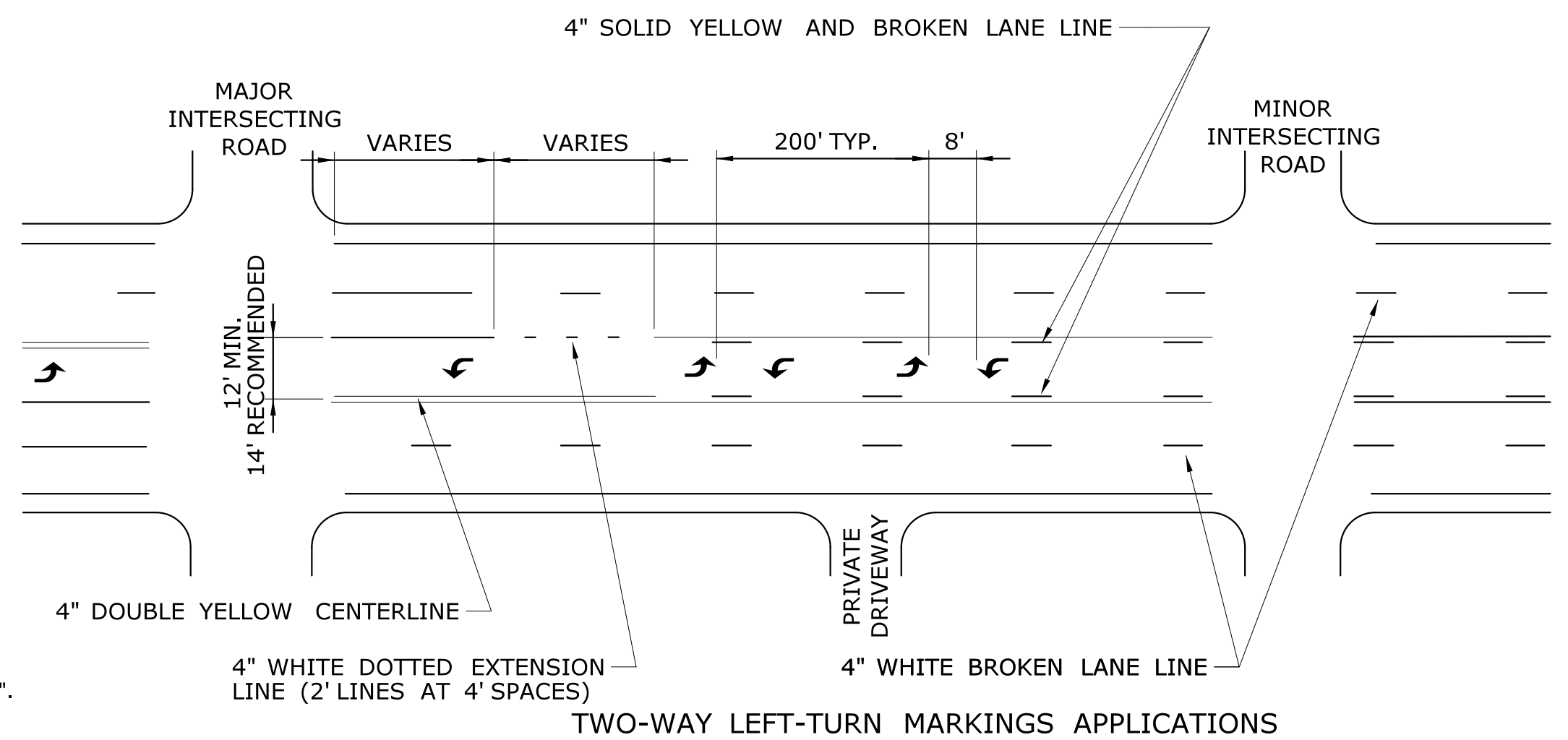
CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:	GUIDE SHEET NO.:
METAL SIGN POSTS AND SIGN MOUNTING DETAILS	TR-1208_02



PAVEMENT MARKINGS FOR CENTERLINE AND SHOULDER LINE



PAVEMENT MARKINGS FOR STOP BARS, YIELD LINES, AND CROSSWALKS

4" WHITE DOTTED EXTENSION
LINE (2' LINES AT 4' SPACES)

-
- EDGE OF ROADWAY
- 4" WHITE EDGE LINE
- DISTANCE BETWEEN LANE REDUCTION SIGN AND START OF TAPER (D)
- D/2
- D/4
- SIGN PLACEMENT BASED ON MUTCD (TABLE 2C-4)
- LANE REDUCTION ARROWS SHALL BE INSTALLED FOR POSTED SPEEDS OF 45 MPH AND GREATER (OPTIONAL FOR POSTED SPEEDS LESS THAN 45 MPH)
- 4" WHITE BROKEN LANE LINE

4" DOUBLE YELLOW DOTTED EXTENSION
LINE (2' LINES AT 4' SPACE - OPTIONAL)




- ## PAVEMENT MARKINGS FOR TURNING LANES

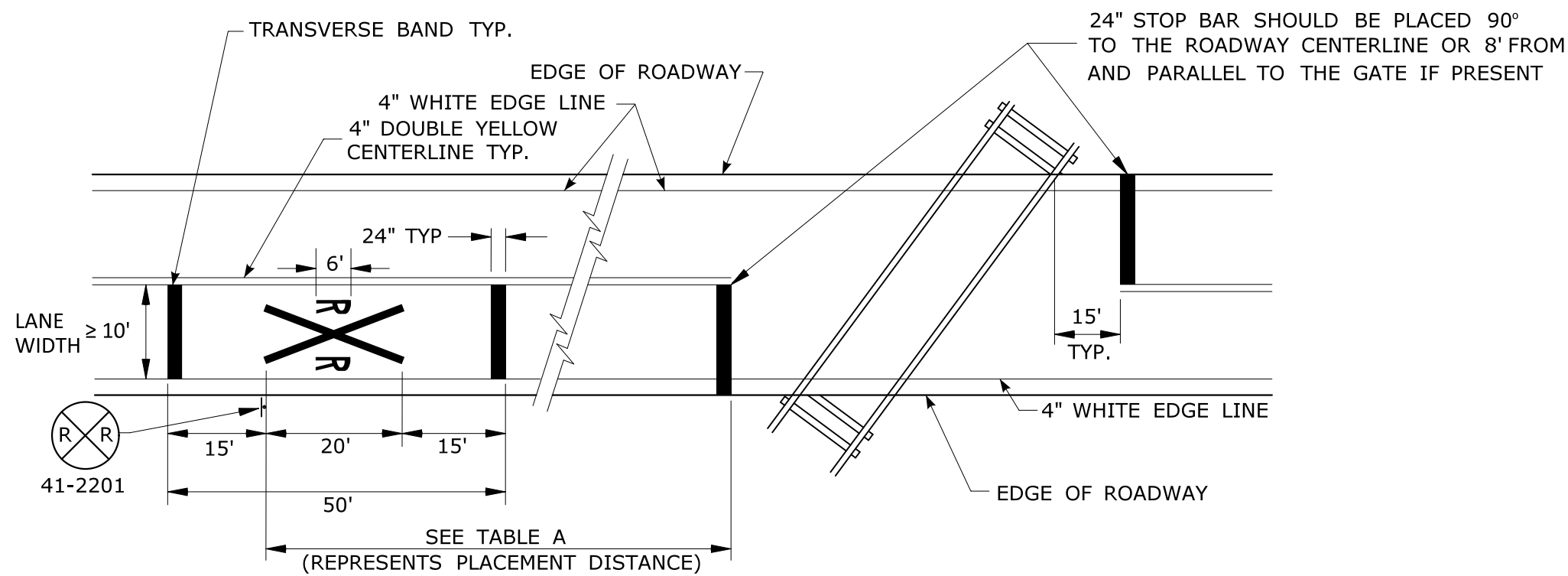
TURNING EXTENSIONS

PAVEMENT MARKINGS FOR TURNING LANES

17. INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		NOT TO SCALE	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION		SUBMITTED BY: _____ NAME/DATE/TIME: _____ APPROVED BY: _____ NAME/DATE/TIME: _____		CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: PAVEMENT MARKINGS FOR NON FREEWAYS	STANDARD SHEET NO.: TR-1210_08
						Filename: TR-1210_08.DGN Model: TR-1210_05						
1 8-2018 REVISED YIELD LINE SIGNAGE AND NOTES. REV. DATE REVISION DESCRIPTION			Plotted Date: 8/10/2018									

TYPICAL RAILROAD GRADE CROSSING DETAIL (LANE WIDTH ≥ 10')



TYPICAL RAILROAD GRADE CROSSING DETAIL (LANE WIDTH < 10')

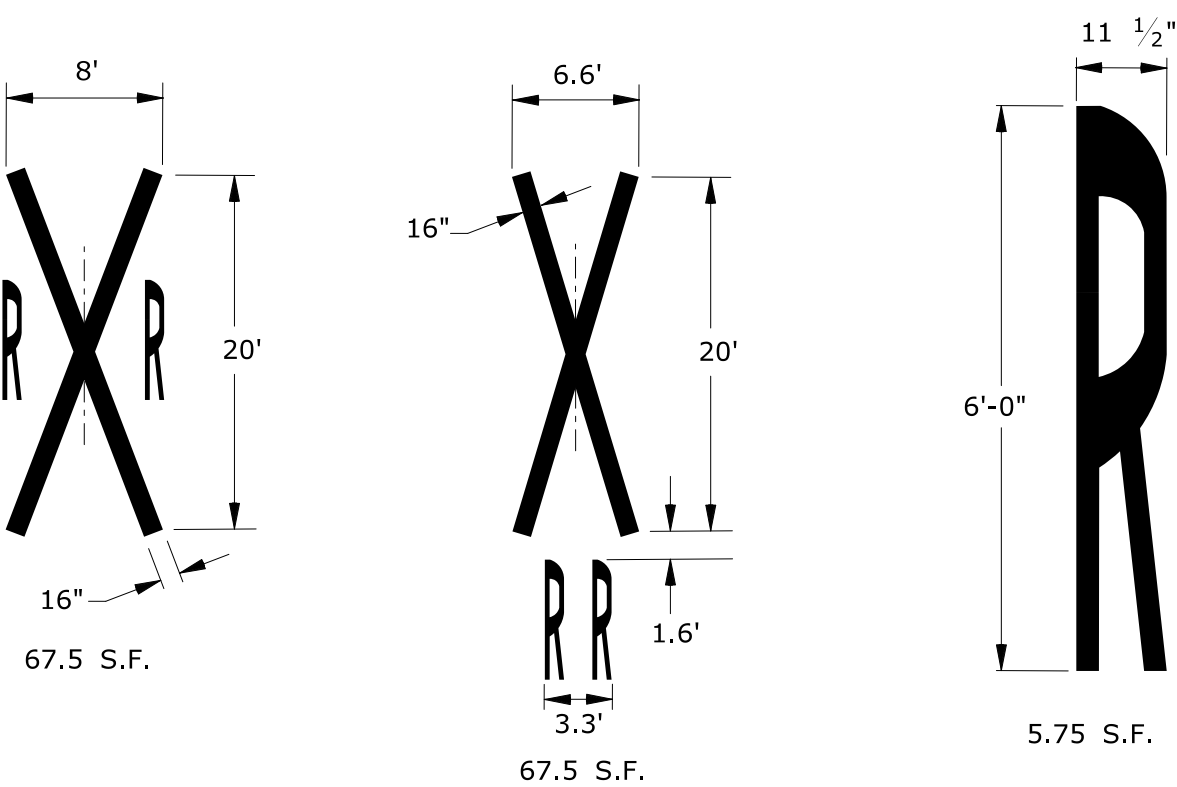
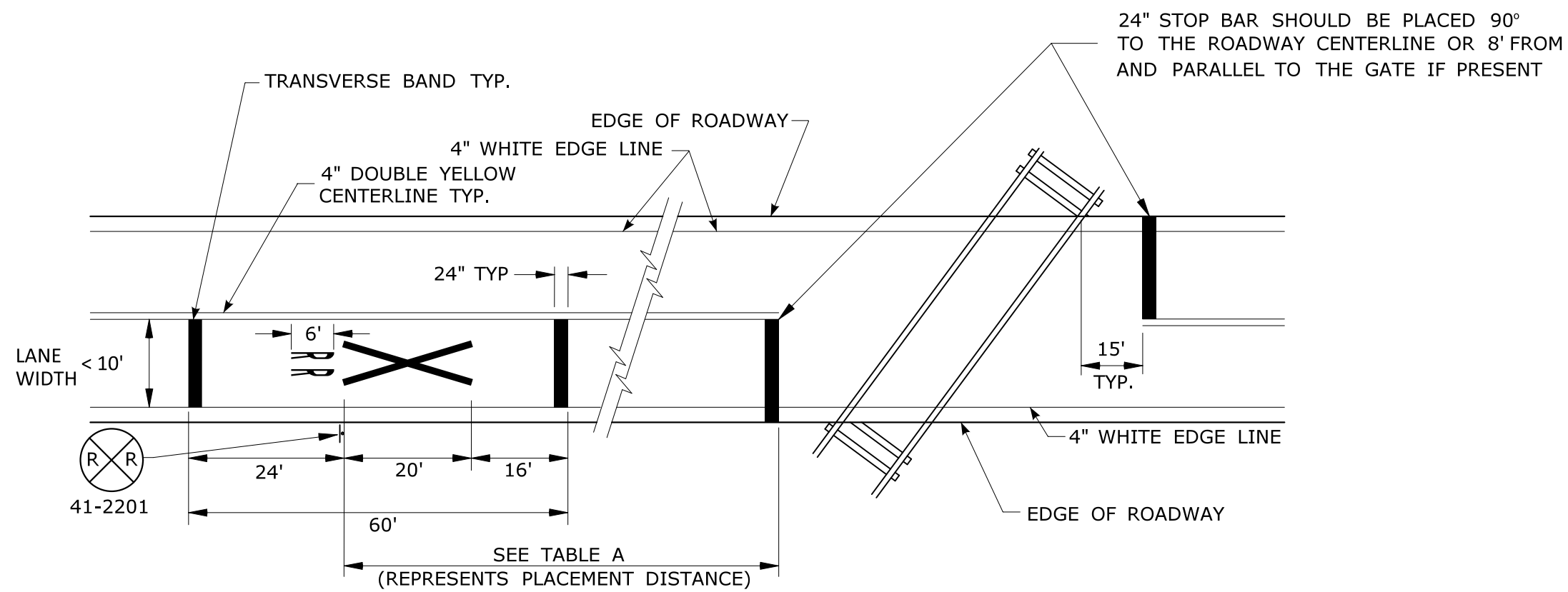


TABLE A	
POSTED OR 85 PERCENTILE SPEED M.P.H.	MINIMUM DISTANCE FT.
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

NOTES:

GENERAL:

1. AREA OF PAVEMENT MARKING SYMBOLS AS INDICATED IS APPROXIMATE.
2. REFER TO STANDARD SHEET TR-1210_04 FOR PAVEMENT MARKING LINE DETAILS.

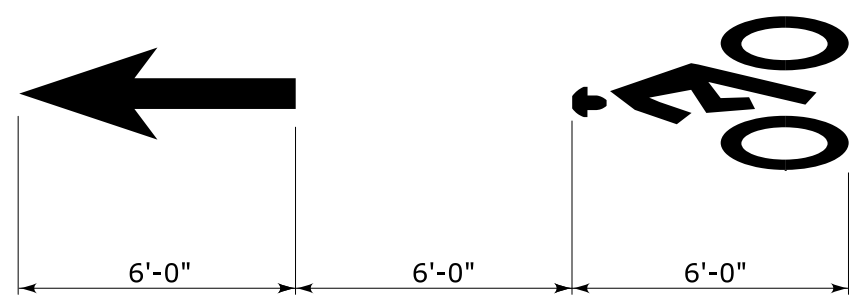
RAILROAD GRADE CROSSINGS:

3. RAILROAD MARKINGS SHALL BE WHITE.
4. ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS THE APPROACH LANES AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

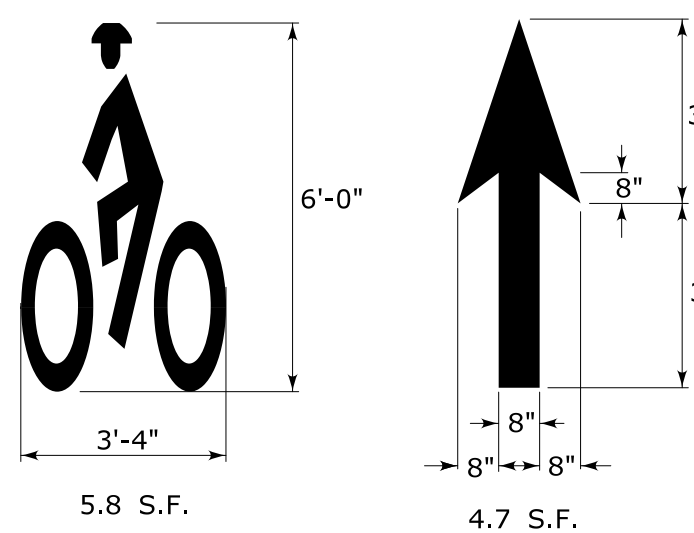
PARKING STALLS:

- 5) AUTOMOBILE ACCESSIBLE PARKING SPACES SHALL BE 15' WIDE INCLUDING 5' OF CROSSHATCH.
- 6) VAN ACCESSIBLE PARKING SPACES SHALL BE 16' WIDE INCLUDING 8' OF CROSSHATCH.
- 7) ACCESS AISLES FOR ANGLED VAN PARKING SPACES SHALL BE LOCATED ON THE PASSENGER SIDE OF THE PARKING SPACE.
8. CROSS HATCHED ACCESS AISLES SHALL NOT BE SHARED BETWEEN PARKING SPACES.

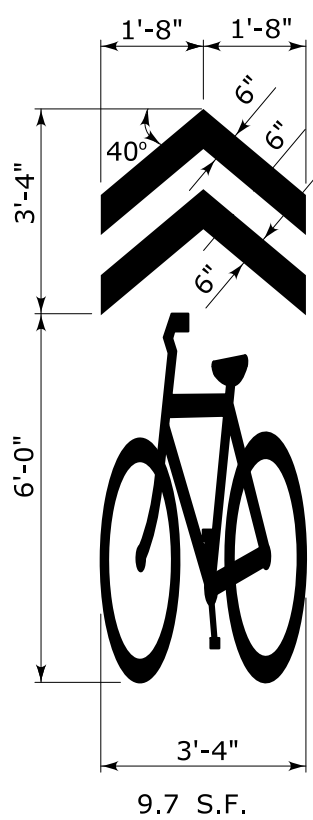
TYPICAL LONGITUDINAL SPACING



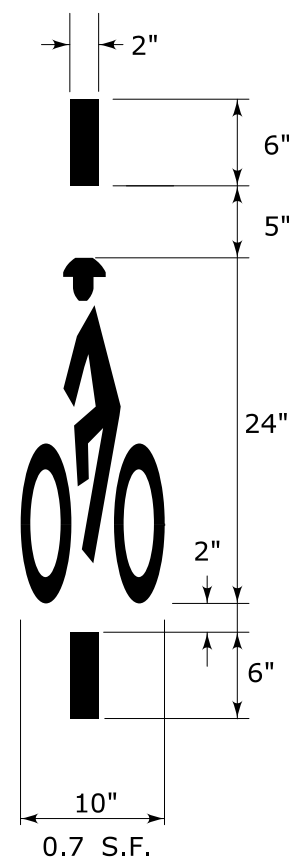
BICYCLE LANE SYMBOL MARKINGS



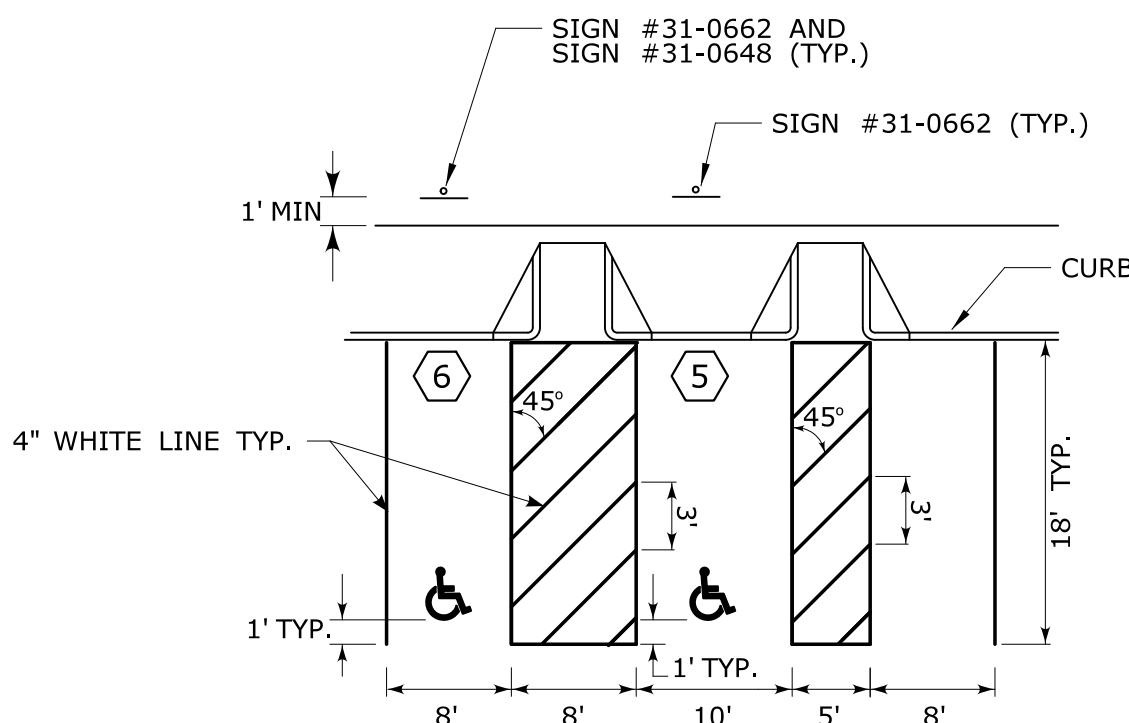
SHARED LANE SYMBOL MARKING



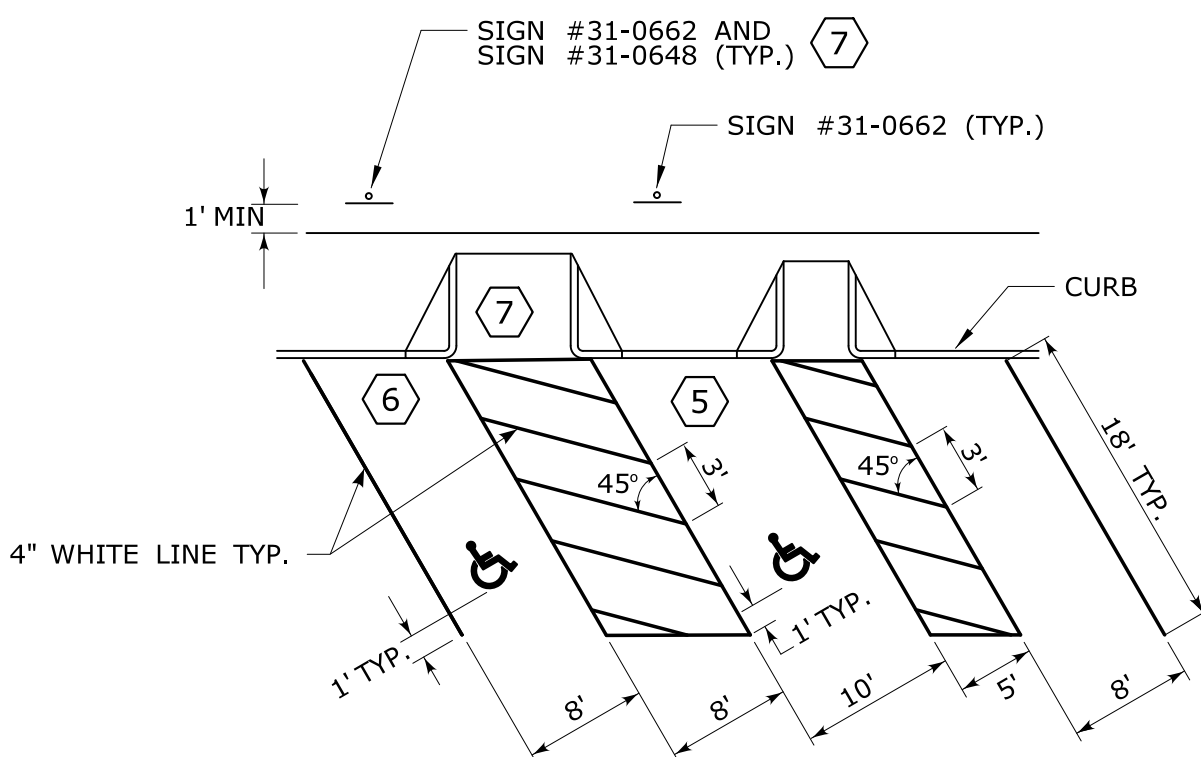
BICYCLE DETECTOR SYMBOL MARKING



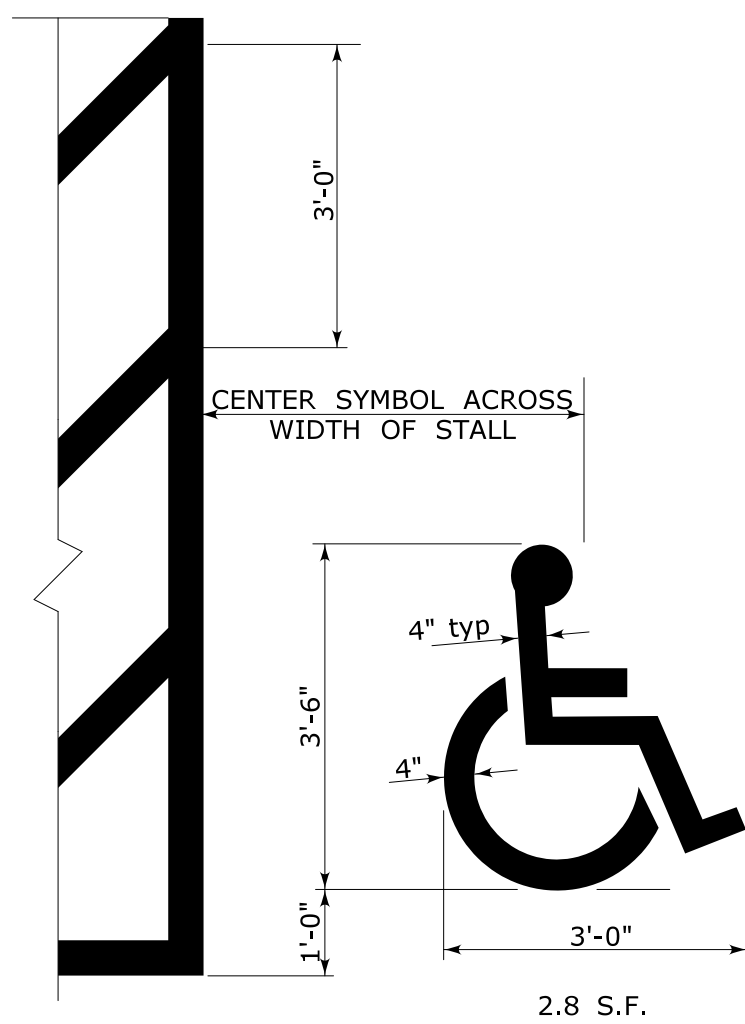
TYPICAL PERPENDICULAR PARKING STALLS DETAIL


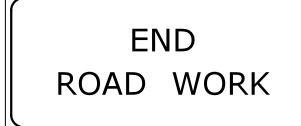




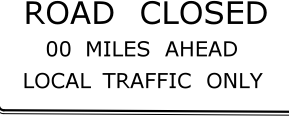



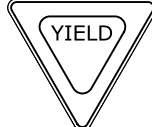


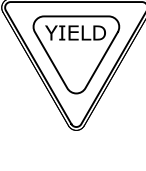


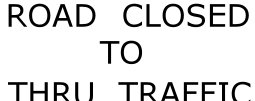

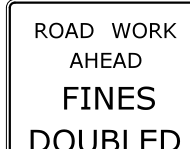
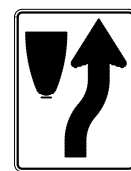


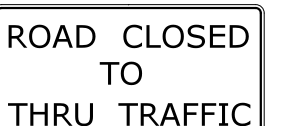


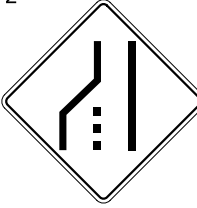
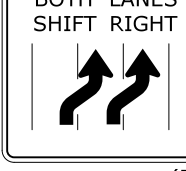
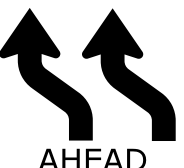

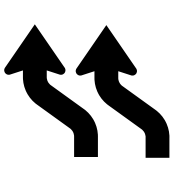



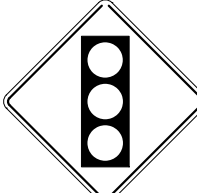
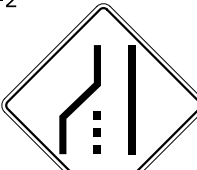
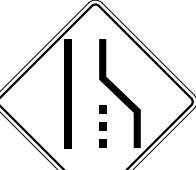
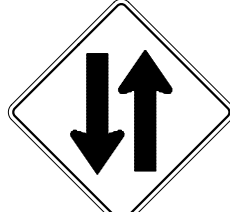
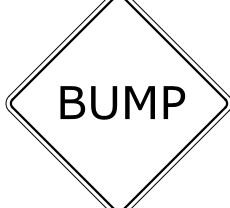





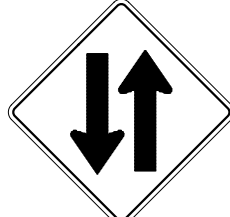
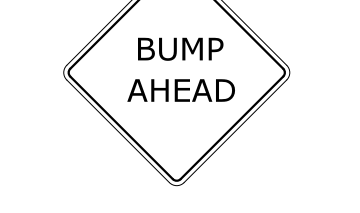


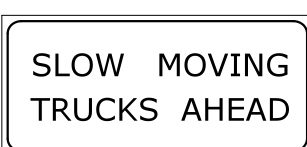





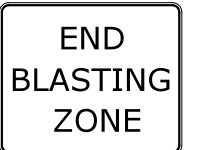
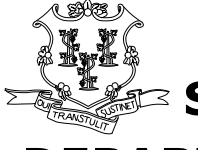


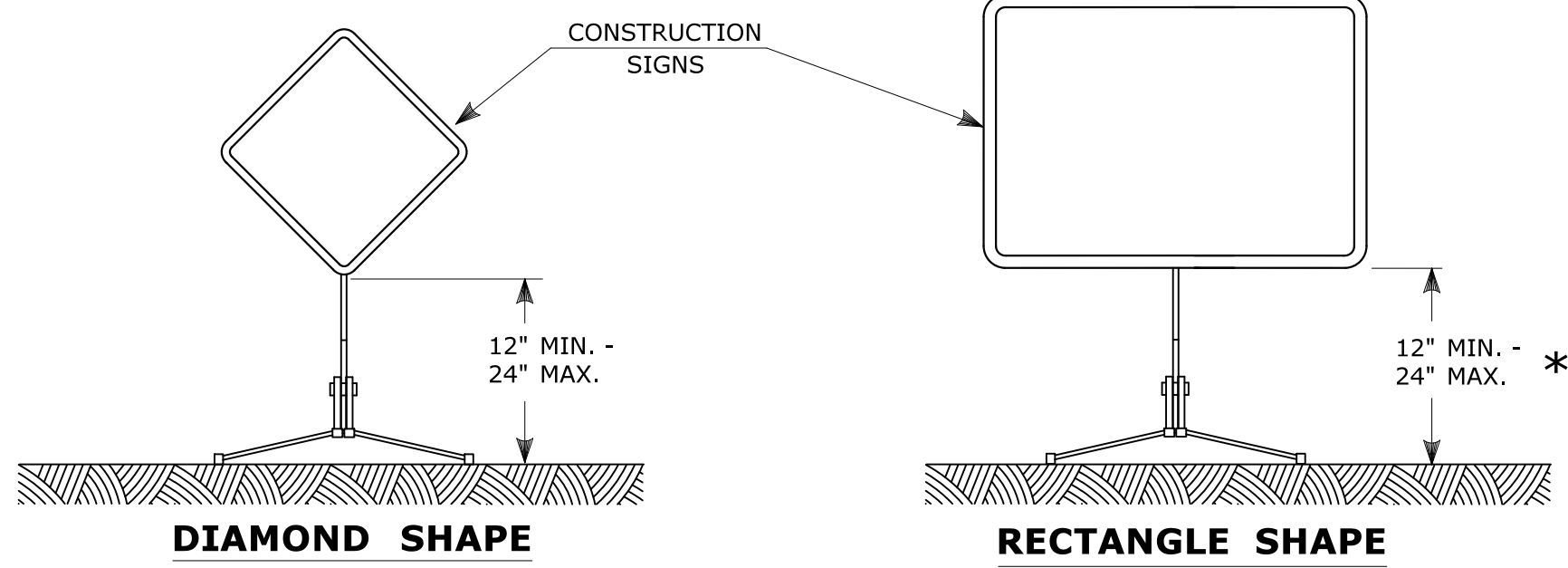
TYPICAL ANGLE PARKING STALLS DETAIL



ACCESSIBLE PARKING SPACE SYMBOL



E5 - SERIES				G20 - SERIES				M4 - SERIES				R1 - SERIES				R9 & R11 - SERIES				W1 - SERIES				W3 - SERIES																																																																																																																																																																																																																																																																															
<div>E5-1</div> <div></div> <div>COPY & BORDER - WHITE BACKGROUND - GREEN</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>16.0</td><td>48</td><td>51-6147</td><td>2</td></tr></table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	51-6147	2	<div>G20-2a</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>8.0</td><td>48X24</td><td>80-9612</td><td>2</td></tr></table></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9711</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9712</td><td>2</td></tr></table></div>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	8.0	48X24	80-9612	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9711	1	16.0	48	80-9712	2	<div>M4-8</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>2.0</td><td>24X12</td><td>80-9707</td><td>1</td></tr></table></div> <div>M4-9b</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>5.0</td><td>30X24</td><td>80-9703</td><td>1</td></tr></table></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>3.0</td><td>24X18</td><td>80-9708</td><td>1</td></tr><tr><td>6.25</td><td>30X30</td><td>80-9706</td><td>1</td></tr></table></div> <div>M4-10</div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>6.0</td><td>48X18</td><td>80-9701R</td><td>2</td></tr><tr><td>6.0</td><td>48X18</td><td>80-9702L</td><td>2</td></tr></table></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>5.0</td><td>30X24</td><td>80-9710</td><td>1</td></tr></table></div>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9707	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.0	30X24	80-9703	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.0	24X18	80-9708	1	6.25	30X30	80-9706	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	6.0	48X18	80-9701R	2	6.0	48X18	80-9702L	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.0	30X24	80-9710	1	<div>R1-1</div> <div></div> <div>COPY & BORDER - WHITE BACKGROUND - RED</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>5.19</td><td>30</td><td>31-0552</td><td>1</td></tr><tr><td>13.30</td><td>48</td><td>31-0557</td><td>2</td></tr></table> <div>R1-2</div> <div></div> <div>COPY & BORDER - RED BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>3.90</td><td>36</td><td>31-0523</td><td>1</td></tr><tr><td>10.83</td><td>60</td><td>31-0528</td><td>2</td></tr></table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.19	30	31-0552	1	13.30	48	31-0557	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.90	36	31-0523	1	10.83	60	31-0528	2	<div>R9-9</div> <div></div> <div>COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>3.75</td><td>30X18</td><td>80-9076</td><td>1</td></tr></table> <div>R11-3a</div> <div></div> <div>VARIABLE MILEAGE COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>12.5</td><td>60X30</td><td>80-9077</td><td>2</td></tr></table> <div>R9-11</div> <div></div> <div>VARIABLE ARROW COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>3.0</td><td>24X18</td><td>80-9074</td><td>1</td></tr></table> <div>R11-3b</div> <div></div> <div>VARIABLE MILEAGE COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>12.5</td><td>60X30</td><td>80-9078</td><td>2</td></tr></table> <div>R9-11a</div> <div></div> <div>VARIABLE ARROW COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>2.0</td><td>24X12</td><td>80-9075</td><td>1</td></tr></table> <div>R11-3b</div> <div></div> <div>COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>12.5</td><td>60X30</td><td>80-9081</td><td>2</td></tr></table> <div>R11-2</div> <div></div> <div>COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>10.0</td><td>48X30</td><td>80-9080</td><td>2</td></tr></table> <div>R11-2</div> <div></div> <div>COPY & BORDER - BLACK BACKGROUND - WHITE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>22.5</td><td>60X54</td><td>31-1907</td><td>2</td></tr></table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.75	30X18	80-9076	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60X30	80-9077	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.0	24X18	80-9074	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60X30	80-9078	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9075	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60X30	80-9081	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.0	48X30	80-9080	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	22.5	60X54	31-1907	2	<div>W1-4</div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9432L</td><td>1</td></tr><tr><td>9.0</td><td>36</td><td>80-9431R</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9452L</td><td>2</td></tr><tr><td>16.0</td><td>48</td><td>80-9451R</td><td>2</td></tr></table></div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>16.0</td><td>48</td><td>80-9435R</td><td>2</td></tr></table></div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>25.0</td><td>60</td><td>80-9483L</td><td>2</td></tr><tr><td>25.0</td><td>60</td><td>80-9485R</td><td>2</td></tr></table></div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>16.0</td><td>48</td><td>80-9436R</td><td>2</td></tr></table></div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>25.0</td><td>60</td><td>80-9443L</td><td>2</td></tr><tr><td>25.0</td><td>60</td><td>80-9445R</td><td>2</td></tr></table></div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>25.0</td><td>60</td><td>80-9446R</td><td>2</td></tr></table></div>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9432L	1	9.0	36	80-9431R	1	16.0	48	80-9452L	2	16.0	48	80-9451R	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9435R	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9483L	2	25.0	60	80-9485R	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9436R	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9443L	2	25.0	60	80-9445R	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9446R	2	<div>W3-1</div> <div></div> <div>OCTAGON - RED W/ WHITE BORDER ARROW & BORDER - BLACK BACKGROUND - FLUORESCENT ORANGE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9050</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9051</td><td>2</td></tr></table> <div>W3-2</div> <div></div> <div>TRIANGLE - RED W/ WHITE BORDER ARROW & BORDER - BLACK BACKGROUND - FLUORESCENT ORANGE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9054</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9055</td><td>2</td></tr></table> <div>W3-3</div> <div></div> <div>TOP CIRCLE - RED MIDDLE CIRCLE - YELLOW BOTTOM CIRCLE - GREEN COPY & BORDER - BLACK BACKGROUND - FLUORESCENT ORANGE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9052</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9053</td><td>2</td></tr></table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9050	1	16.0	48	80-9051	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9054	1	16.0	48	80-9055	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9052	1	16.0	48	80-9053	2
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FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16-S	10.0	48X30	80-1619	2	<div>W4-2</div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>16.0</td><td>48</td><td>80-9918L</td><td>2</td></tr><tr><td>16.0</td><td>48</td><td>80-9917R</td><td>2</td></tr></table></div> <div>W6-3</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>16.0</td><td>48</td><td>80-9945</td><td>2</td></tr></table></div>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9918L	2	16.0	48	80-9917R	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9945	2	<div>W8-1</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9901</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9902</td><td>2</td></tr></table></div> <div>W9-2</div> <div></div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9801</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9802</td><td>2</td></tr><tr><td>16.0</td><td>48</td><td>80-9910L</td><td>2</td></tr><tr><td>16.0</td><td>48</td><td>80-9911R</td><td>2</td></tr></table></div>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9901	1	16.0	48	80-9902	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9801	1	16.0	48	80-9802	2	16.0	48	80-9910L	2	16.0	48	80-9911R	2	<div>W13-1</div> <div></div> <div>SUBPLATE VARIABLE SPEED</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>4.0</td><td>24</td><td>80-9569</td><td>1</td></tr><tr><td>6.25</td><td>30</td><td>80-9567</td><td>1</td></tr></table> <div>W13-5</div> <div></div> <div>VARIABLE SPEED</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9506</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9508</td><td>2</td></tr><tr><td>9.0</td><td>36</td><td>80-9805</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9521</td><td>2</td></tr></table> <div>W16-15P</div> <div></div> <div>COPY & BORDER - BLACK BACKGROUND - ORANGE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>2.0</td><td>24X12</td><td>80-9049</td><td></td></tr></table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	4.0	24	80-9569	1	6.25	30	80-9567	1	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9506	1	16.0	48	80-9508	2	9.0	36	80-9805	1	16.0	48	80-9521	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9049		<div>W20-1</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>6.25</td><td>30</td><td>80-9602</td><td>1</td></tr><tr><td>9.0</td><td>36</td><td>80-9603</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9604</td><td>2</td></tr></table></div> <div>W20-1</div> <div></div> <div>VARIABLE DISTANCE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9614</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9615</td><td>2</td></tr></table> <div>W20-2</div> <div></div> <div>VARIABLE MILEAGE</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. 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D.O.T. #	POSTS	9.0	36	80-9607	1	<div>W22-1</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>9.0</td><td>36</td><td>80-9620</td><td>1</td></tr><tr><td>16.0</td><td>48</td><td>80-9625</td><td>2</td></tr></table></div> <div>W22-2</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>10.5</td><td>42X36</td><td>80-9623</td><td>2</td></tr></table></div> <div>W22-3</div> <div></div> <div><table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr><tr><td>10.5</td><td>42X36</td><td>80-9621</td><td>2</td></tr></table></div>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9620	1	16.0	48	80-9625	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.5	42X36	80-9623	2	AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.5	42X36	80-9621	2																																
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<div>6</div> <div>8-2018</div> <div>REVISED POST REQUIREMENTS AND SHEETING TYPE.</div>				<div>5</div> <div>8-2015</div> <div>UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.</div>				<div>4</div> <div>6-2012</div> <div>REVISED NOTE #1 TO REFERENCE "O.S.T.A."</div>				<div>3</div> <div>4-2012</div> <div>REVISED NEW SIGNAL SIGN(S) TO CONFORM TO 2009 MUTCD.</div>				<div>2</div> <div>2-2011</div> <div>MINOR REVISIONS.</div>				<div>1</div> <div>3-2010</div> <div>REMOVED OBSOLETE SIGNS (50-5925, 50-5935).</div>				<div>REV. DATE</div> <div>REVISION DESCRIPTION</div>				<div>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</div> <div>Plotted Date: 8/10/2018</div>				<div>NOT TO SCALE</div>				<div> <div>STATE OF CONNECTICUT</div> <div>DEPARTMENT OF TRANSPORTATION</div></div> <div>Filename: TR-1220_01.1.2018.dgn</div> <div>Model: TR-1220_01</div>				<div>SUBMITTED BY:</div> <div>NAME/DATE/TIME:</div>				<div>APPROVED BY:</div> <div>NAME/DATE/TIME:</div>				<div>CTDOT</div> <div>STANDARD SHEET</div> <div>OFFICE OF ENGINEERING</div>				<div>STANDARD SHEET TITLE:</div> <div>SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS</div>				<div>STANDARD SHEET NO.:</div> <div>TR-1220_01</div>																																																																																																																																																																																																																																															

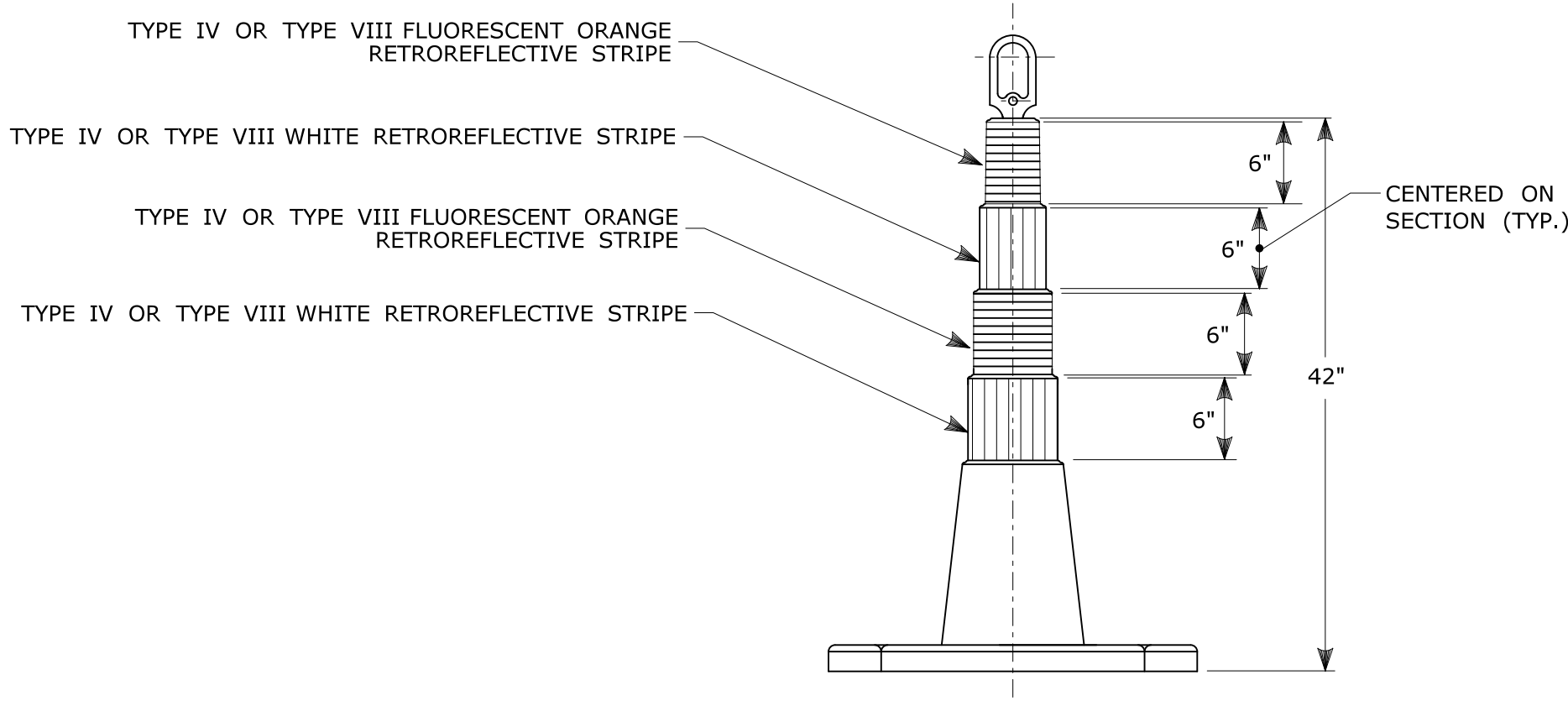


PORTABLE CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220.01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

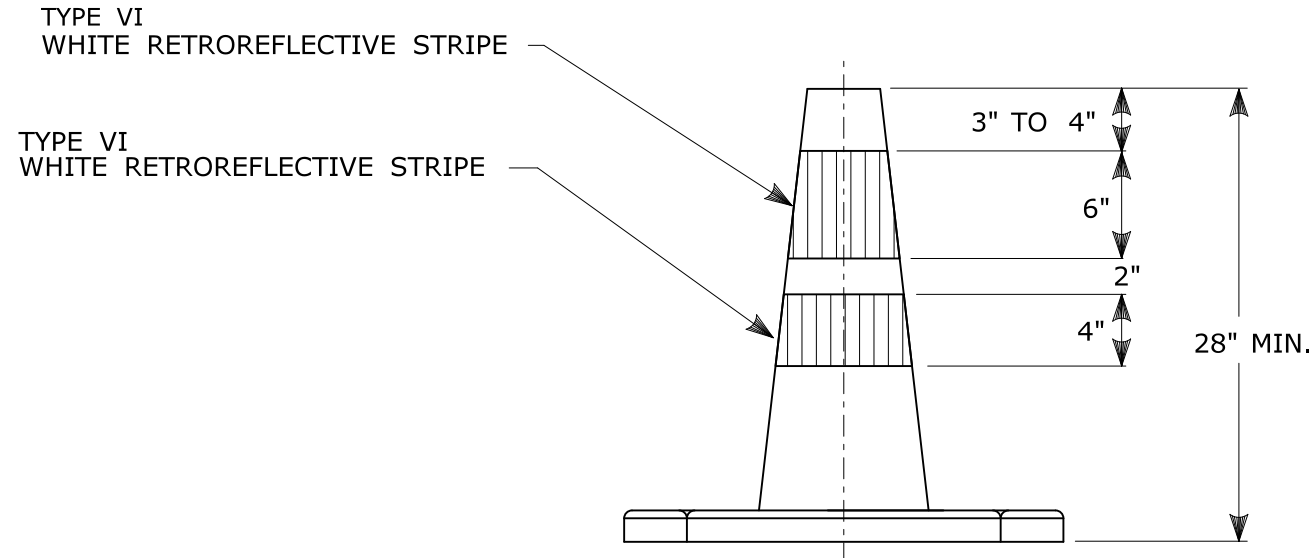
* FOR E5-1 (EXIT SIGNS) USE MIN 48".



42" TRAFFIC CONE

NOTES:

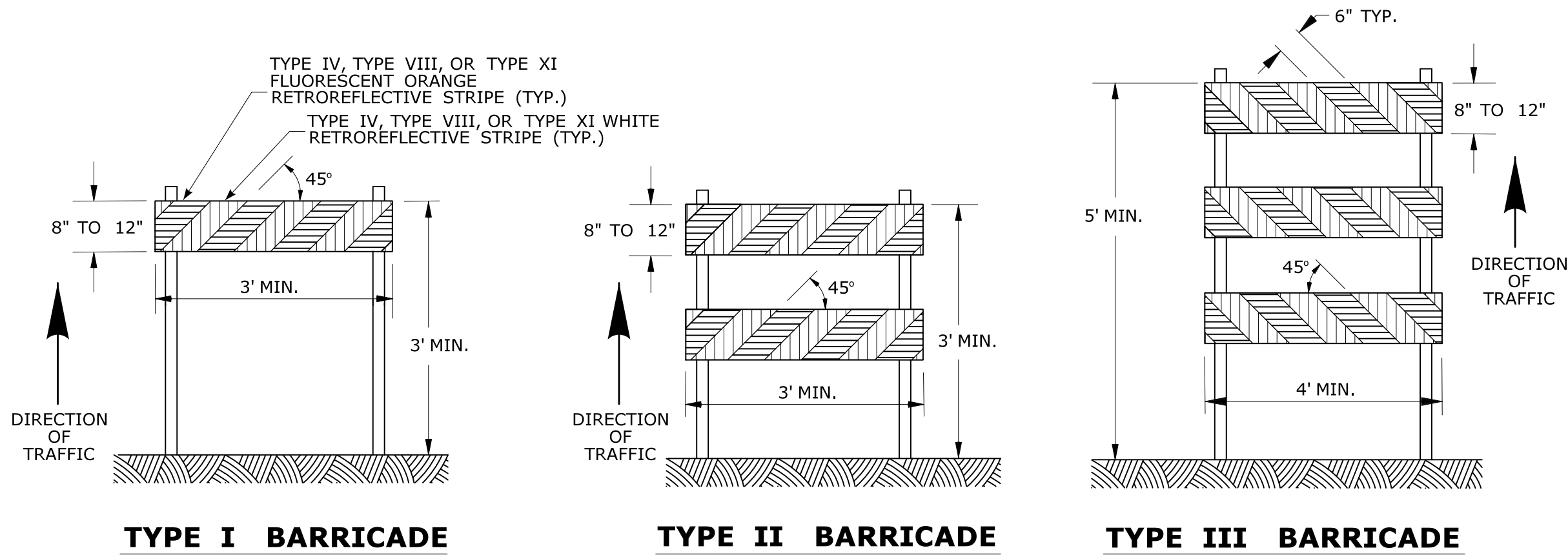
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC CONE

NOTES:

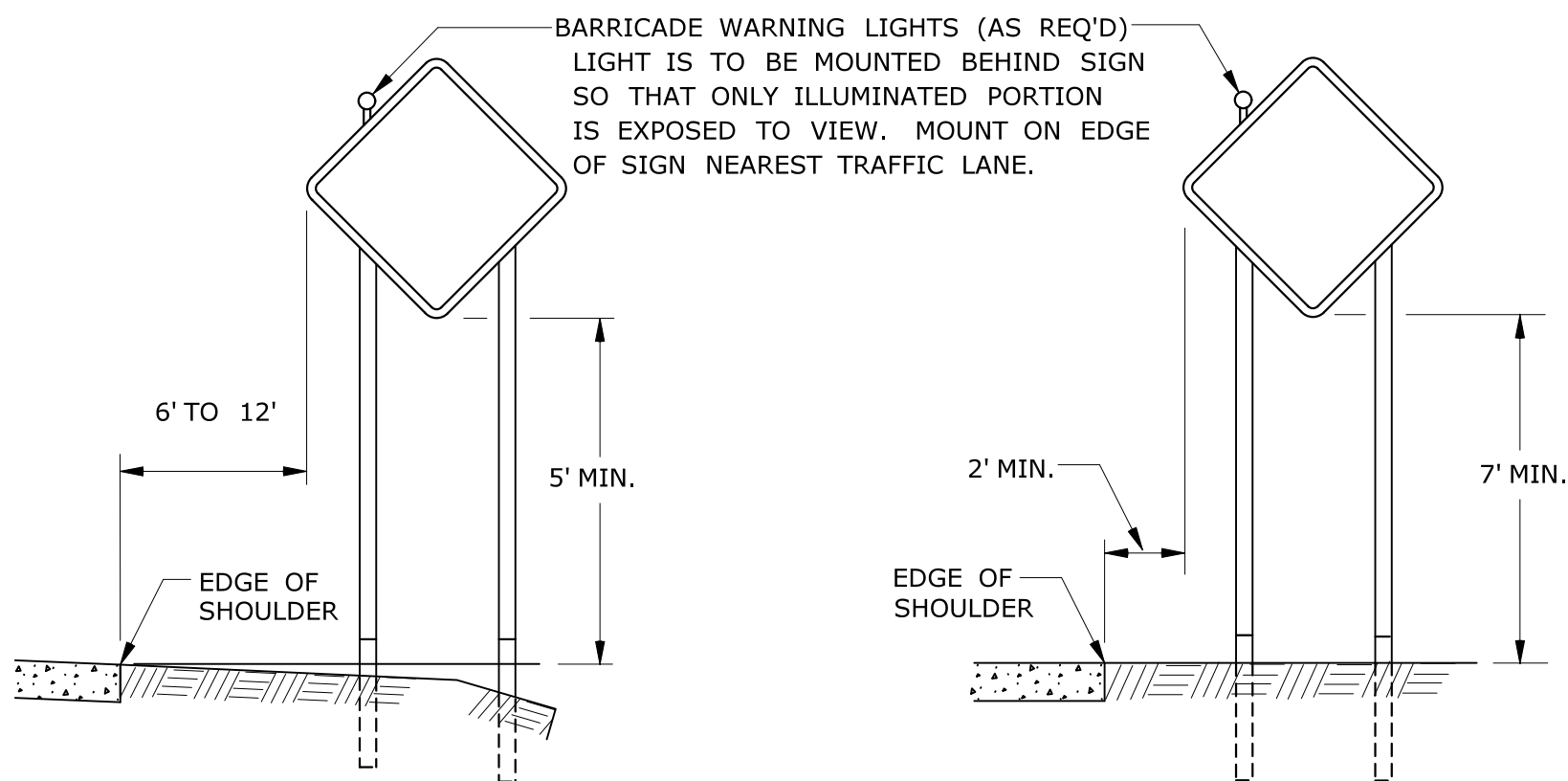
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



CONSTRUCTION BARRICADES

NOTES:

- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE FLUORESCENT ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



RURAL AREA

URBAN AREA

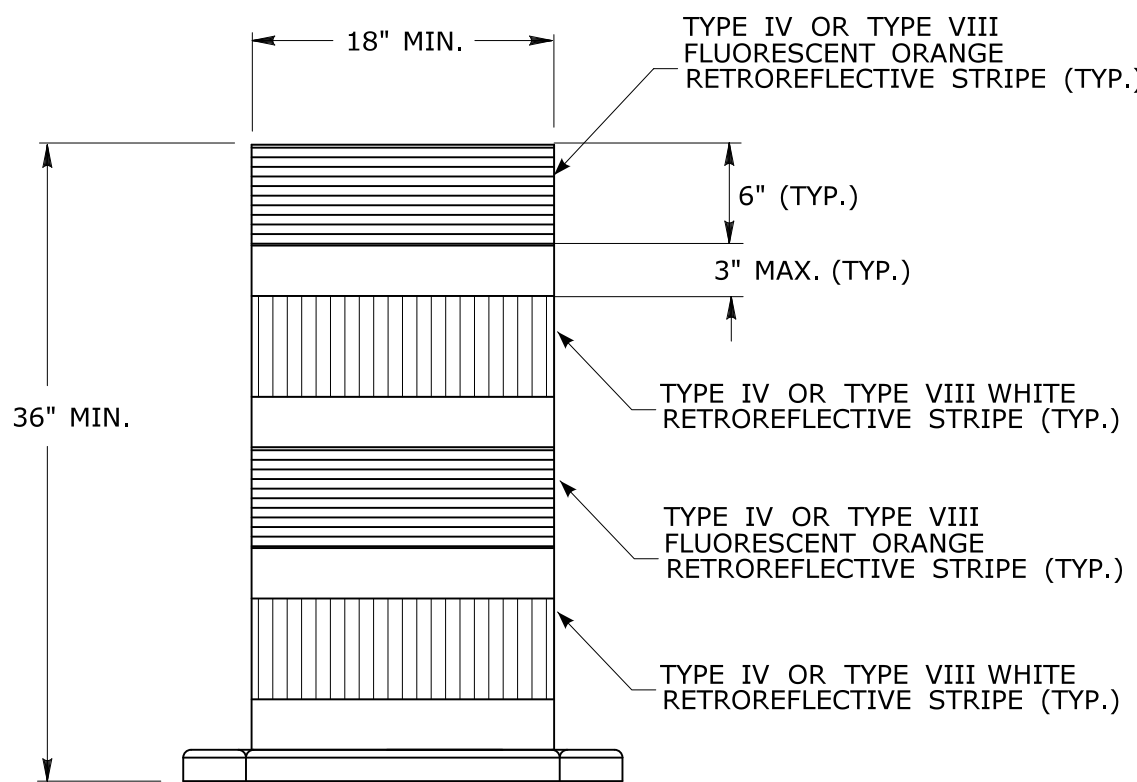
PLACEMENT OF CONSTRUCTION SIGNS TYPICAL LONG TERM INSTALLATION

NOTES:

SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.

REFER TO STANDARD SHEETS:



- TR-1208.01 - "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS."
TR-1208.02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."



TRAFFIC DRUM FRONT VIEW

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	NOT TO SCALE	<div><div>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</div></div> <div>Filename: TR-1220.02_3.2018.dgn Model: TR-1220.02</div>	SUBMITTED BY: NAME/DATE/TIME:	CTDOT STANDARD SHEET	STANDARD SHEET TITLE: CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES	STANDARD SHEET NO.: TR-1220_02
3	8-2018	UPDATED SHEETING TYPE AND COLOR.	APPROVED BY: NAME/DATE/TIME:			OFFICE OF ENGINEERING			
2	8-2015	UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.							
1	2-2011	MINOR REVISIONS.							
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 8/10/2018						